



MAYOR & COUNCIL COMMUNICATION

DATE: 9/1/15
REGULAR
ITEM # 16
RESOLUTION 2015-072

AGENDA ITEM: Northeast Metro Intermediate School District 916 Preliminary and Final Plat and PUD Development Plans

SUBMITTED BY: Kyle Klatt, Community Development Director

THROUGH: Clark Schroeder, Interim City Administrator

REVIEWED BY: Planning Commission
Ben Gozola, Consulting City Planner
Jack Griffin, City Engineer

SUGGESTED ORDER OF BUSINESS:

- Introduction of Item Consulting City Planner
- Report/Presentation..... Consulting City Planner
- Questions from Council to Staff Mayor Facilitates
- Call for Motion Mayor & City Council
- Discussion Mayor & City Council
- Action on Motion Mayor Facilitates

POLICY RECCOMENDER: The Planning Commission is recommending conditional approval of preliminary and final development plans and a preliminary and final plat for a new public school building for special needs students to be located within the Eagle Point Business Park. As a part of the Planning Commission recommendation and findings, the Commission did review the project for conformance with the Lake Elmo Design Guidelines and Standards Manual.

FISCAL IMPACT: None for public infrastructure – all utilities and improvements have either been installed as part of the Eagle Point Business Park or will be completed by the applicant at the time a building permit is issued for the building. The applicant has entered into a services agreement with the City that will require payment of \$30,000 on an annual basis to cover the City’s increase costs for providing services to the facility.

SUMMARY AND ACTION REQUESTED: The City Council is being asked to consider a request from Northeast Metro Intermediate School District 916, 2450 County Road Road F East,

White Bear Lake, MN, for approval of preliminary and final plat and preliminary and final Planned Unit Development (PUD) plans to construct a 75,000 square foot public school facility for children with special needs in grades kindergarten through eighth grade on Outlot A of Eagle Point 7th Addition. Because the site under consideration has previously been subdivided and the proposed platting into a buildable lot will not change the boundaries of the previously platted outlot, Staff is recommending that the City process its review of the preliminary and final development plans simultaneously.

The Planning Commission is recommending approval of the preliminary and final plat and PUD plans with conditions of approval. The suggested motion to adopt the Planning Commission recommendation is as follows:

“Move to adopt Resolution No. 2015-072 approving the preliminary and final plat and preliminary and final PUD plans for the Northeast Metro Intermediate School District 916 school facility”

LEGISLATIVE HISTORY/PLANNING COMMISSION REPORT: The attached staff report to the Planning Commission provides an overview of the request and a detailed description of the issues associated with the request as identified by Staff. The Planning Commission considered the PUD plans at its August 24th meeting and conducted a public hearing on the applicant’s request. The Commission did not receive any public comments concerning the proposed building. Representatives of the applicant addressed the Commission and provided additional information concerning the design of the building and the operation of the school.

The Planning Commission reviewed the project for consistency with the Lake Elmo Design Guidelines and Standards Manual, and found that the project complied with all applicable provisions for development in the City’s BP – Business Park Zoning District. The one area of concern voiced by the Commission was the proposed roof design; the Commission wanted to see additional articulation included in the final design and supported the inclusion of a condition of approval to address this matter. The Commission also requested the addition of a requirement that the developer address all review comments from the City Engineer in his memorandum dated August 21, 2015.

The Planning Commission adopted a motion to recommend approval of the preliminary and final plat and preliminary and final PUD development plans with the findings and conditions as noted in the attached Resolution 2015-072. The motion passed unanimously.

BACKGROUND INFORMATION (SWOT):

Strengths

- The proposed building is consistent with the Lake Elmo Design Guidelines and Standards Manual
- The project is consistent with the Lake Elmo Comprehensive Plan and BP – Business Park Zoning requirements
- The proposed development will occupy an existing, platted,

vacant lot within the Eagle Point Business Park

- Weaknesses**
- Roof articulation meets minimum requirements, but will be improved with the final plans.
- Opportunities**
- The proposed development will bring a active into Lake Elmo to the City of Lake Elmo (with 75 new jobs anticipate within the facility).
 - The proposed building and landscaping will be a nice addition to the Eagle Point Business Park
- Threats**
- The facility will bring bus traffic onto Eagle Point Boulevard.

RECOMMENDATION: Based upon the above background information, Staff report and Planning Commission recommendation, it is recommended that the City Council approve the preliminary and final plat and preliminary and final PUD development plans to allow Northeast Metro Intermediate School District 916 to construct a public school facility for special needs students to be located within the Eagle Point Business Park with the 16 conditions of approval as documented in the resolution of approval by taking the following action / with the following motion:

“Move to adopt Resolution No. 2015-072 approving the preliminary and final plat and preliminary and final PUD plans for the Northeast Metro Intermediate School District 916 school facility”

ATTACHMENTS:

1. Resolution No. 2015-072
2. Area Map
3. Planning Commission Report 8/24/15
4. City Engineer’s Memo
5. Staff Report
6. Engineering Memo
7. Resolution Approving the CUP authorizing the school use
8. Applicant’s Submittals
9. Fire Chief Review Comments
10. Applicant Letter to Bremer Bank
11. Services Agreement

**CITY OF LAKE ELMO
WASHINGTON COUNTY, MINNESOTA**

RESOLUTION NO. 2015-072

A RESOLUTION APPROVING A PRELIMINARY AND FINAL PLAT AND PRELIMINARY AND FINAL DEVELOPMENT PLANS FOR THE NORTHEAST METRO INTERMEDIATE SCHOOL DISTRICT 916 SCHOOL FACILITY WITHIN THE EAGLE POINT BUSINESS PARK

WHEREAS, the City of Lake Elmo is a municipal corporation organized and existing under the laws of the State of Minnesota; and

WHEREAS, Northeast Metro Intermediate School District 916, 2540 County Road F East, White Bear Lake, MN, (Applicant) has submitted an application to the City of Lake Elmo (City) for a preliminary and final plat and preliminary and final PUD plans associated with a new public school facility for special needs students to be located on a property which is legally defined as: Outlot A, Eagle Point Business Park 7th Addition, according to the recorded plat thereof, Washington County, Minnesota; and

WHEREAS, the current lot is proposed to be replatted as Lot 1, Block 1 of Eagle Point Business Park 8th Addition, a copy of which is on file in the City of Lake Elmo Planning and Zoning Department; and

WHEREAS, the City has agreed to conduct a simultaneous review of the preliminary and final plat and preliminary and final PUD plans for the Applicant's school facility because the site has previously been platted and improved as future buildable lot within the Eagle Point Business Park and is appropriate because of the limited scale of the proposal; and

WHEREAS, the Lake Elmo Planning Commission held a public hearing on August 24, 2015, to consider the preliminary and final plat for Eagle Point Business Park 8th Addition and the preliminary and final PUD development plans for the school facility; and

WHEREAS, on August 24, 2015, the Lake Elmo Planning Commission adopted a motion to recommend that the City Council approve the preliminary and final plat for Eagle Point Business Park 8th Addition and the preliminary and final PUD plan for the school facility with conditions; and

WHEREAS, the Lake Elmo Planning Commission has submitted its report and recommendation concerning the Eagle Point Business Park 8th Addition preliminary and final plat and the preliminary and final development plans as part of a memorandum to the City Council from Community Development Director Kyle Klatt for the September 1, 2015 Council Meeting; and

WHEREAS, the City Council reviewed the Eagle Point Business Park 8th Addition preliminary and final plat and the preliminary and final development plans for school facility at its meeting held on August 18th, 2015.

NOW, THEREFORE, based upon the testimony elicited and information received, the City Council makes the following:

FINDINGS

- 1) That the procedure for obtaining approval of said Final Plat is found in the Lake Elmo City Code, Section 153.08.
- 2) That all the requirements of said City Code Section 153.07 related to the Final Plat have been met by the Applicant.
- 3) That the procedure for obtaining approval of said Preliminary and Final PUD plans is found in the Lake Elmo City Code, Section 154.800.
- 4) That all the requirements of said City Code Section 154.800 related to the Preliminary and Final PUD plans have been met by the Applicant.
- 5) That the proposed Preliminary and Final PUD Plans for the Northeast Metro Intermediate School District 916 consists of an approximate 75,000 square foot public school facility for special needs students, and will provide 154 parking stalls in accordance with plans and specifications submitted to the City, and will be located on property legal described as follows: Lot 1, Block 1, Eagle Point Business Park 8th Addition.
- 6) That the proposed PUD will allow a more flexible, creative, and efficient approach to the use of the land.
- 7) That the proposed PUD is in conformance with the Comprehensive Guide Plan for the City.
- 8) The application is conforming to the standards and requirements for a PUD within the Eagle Point Business Park;
- 9) The building design largely employs guidance from the City's design standards including building placement, open space, landscaping, controlled parking lot lighting, high quality exterior materials and a cohesive architectural style;
 - a. The applicant has agreed to improve the building design through the addition of more parapet walls on all four sides of the building to bring the roofline into compliance with required design standards;

- b. The applicant has updated the lighting plan to eliminate the nonconforming illumination at the northern entry point;
- 10) Engineering design standards are being met or can be met via conditions; and
- 11) Review of the plan did not identify any environmental issues that cannot be addressed through the proposed design.

NOW, THEREFORE, BE IT RESOLVED THAT the City Council does hereby approve the Eagle Point Business Park 8th Addition Preliminary and Final Plat and the Preliminary and Final PUD Plans for the Northwest Metro Intermediate School District 916 public school facility subject to the following conditions:

- 1) The City Engineer shall approve all final easements prior to consideration of the Preliminary & Final PUD Plan/Plat by Council.
- 2) All changes and modifications to the plans requested by the City Engineer in review memo dated 8/20/15 shall be incorporated into these final plans before they are approved.
- 3) The City Engineer shall work with the applicant to finalize all stormwater management plans to bring them into compliance with all applicable state and local regulations.
- 4) All necessary permits from other State, Regional, or local agencies shall be provided to the City prior to issuance of a building permit.
- 5) Fire related conditions include:
 - a. FIRE HYDRANTS
 - i. Spacing – prior to filing of the final plat, the City Engineer must approve proposed spacing of fire hydrants after confirming proposed placements meet City requirements for commercial development.
 - ii. The hydrant on the East side of the property, currently identified as “FUTURE ADDITION,” shall be installed at the same time as all other fire hydrants.
 - b. The FDC (Fire Department Connection) location, Annunciator Panel location, lock box location, and lock box type shall all be approved by the Fire Chief prior to installation.
 - c. Fire lanes shall be identified with the Fire Chief, and all such lanes shall be marked by painted curbing and signage when constructed.

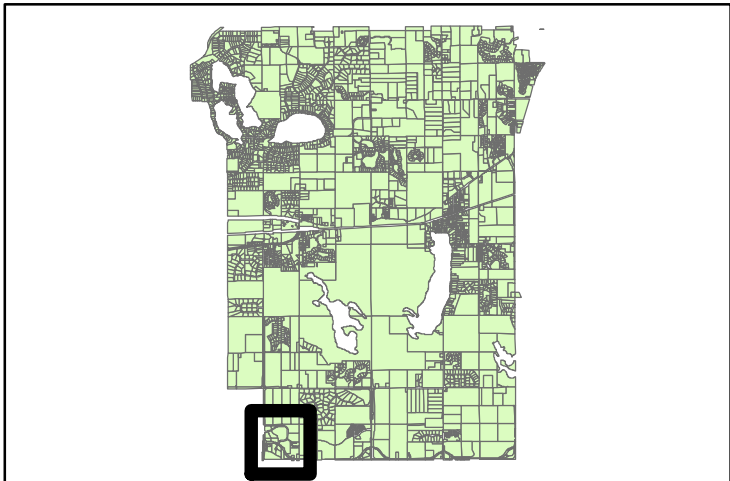
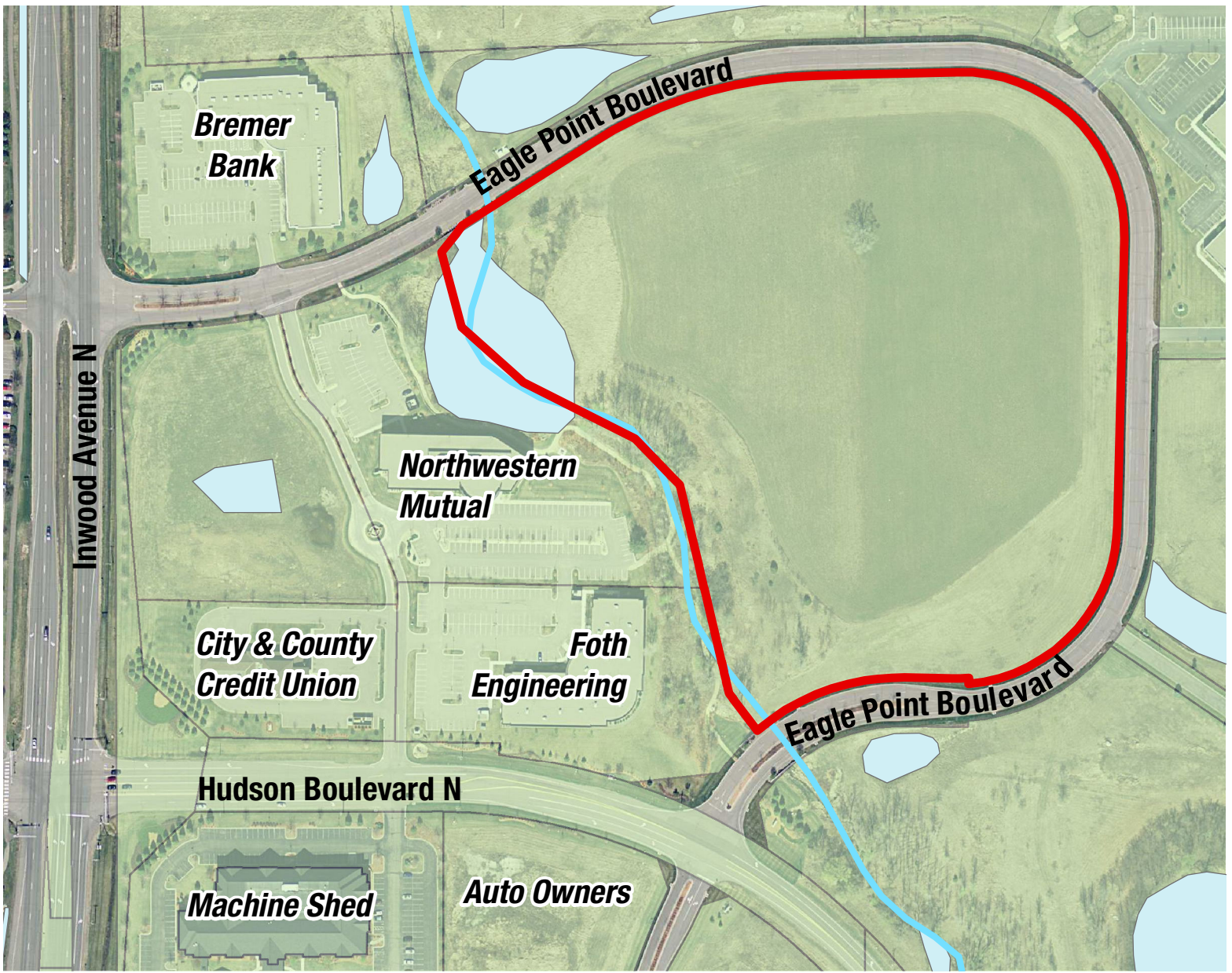
- 6) Height of all lighting standards shall not exceed 30'.
- 7) Silt fencing and/or other erosion control BMPs as directed by the City Engineer shall be shown at the construction limits with the future building permit application.
- 8) Telephone, electric, and/or gas service lines are to be placed underground in accordance with the provisions of all applicable City ordinances.
- 9) Design of both monument signs shall be brought into compliance with City standards for maximum sign size [thirty-two (32) square feet given the characteristics of the adjacent roadway].
- 10) A park dedication payment in the amount of \$50,938.86 shall be paid to the City prior to the issuance of a building permit for the proposed school building.
- 11) Access charges for municipal services (as may be applicable) shall be paid prior to the issuance of a building permit for the proposed school building.
- 12) The final landscape plan be reviewed and approved by the City's consulting landscape architect prior to a building permit being issued.
- 13) Building plans shall be updated to include additional parapet walls on all four sides of the building to address an existing lack of roof articulation.
- 14) All exterior metal shall be painted or colored with earth tones which complement the other proposed siding materials for the building.

Passed and duly adopted this 1st day of September, 2015, by the City Council of the City of Lake Elmo, Minnesota.

Mike Pearson, Mayor

ATTEST:

Julie Johnson, City Clerk



Location within the City of Lake Elmo

ISD 916



SITE IDENTIFICATION MAP



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City of Lake Elmo Planning Department
Preliminary & Final PUD Plan/Plat

To: **Planning Commission**

From: **Kyle Klatt, City Planner**

Meeting Date: **August 24th, 2015**

Applicants: **Northeast Metro Intermediate School District 916**

Location: **Outlot A, Eagle Point Business Park, 7th Addition**

Introductory Information

Request: The Northeast Metro Intermediate School District 916 is seeking approval of a preliminary and final plan/plat for Eagle Point Business Park 8th Addition within the Eagle Point Business Park.

The original platting and approval of the Eagle Point Business Park entailed the creation of multiple Outlots which were to be developed through platting as businesses came forward. This request would re-plat Outlot A of Eagle Point Business Park, 7th Addition into Lot 1, Block 1, Eagle Point Business Park, 8th Addition.

In addition to the requested platting, this PUD application would approve a site plan authorizing construction of an approximate 75,000 square foot building. The building would house a school for children with special needs in first through eighth grade. The school will have a total of twenty-one (21) classrooms, although two (2) of these rooms are anticipated to be earmarked for special activities and will not be used full time. Each classroom will hold a maximum of six (6) students, and it is anticipated that only eighty (80) students will be on-site in any given year. Should every classroom be used to its assigned maximum, a total of 126 students could be served from this site. A total of seventy-five (75) jobs are anticipated from this development (~50 teachers and 25 administrative/support positions). The typical hours of operation for the school are 7:00 a.m. to 4:00 p.m. daily.

Construction is anticipated to begin in spring of 2016 with completion of the building in the summer of 2017.

Process: The Eagle Point Business Park was approved as a Planned Unit Development in the early 2000's, and the initial installation of roads and utilities has already occurred throughout the park. Development of individual building sites such as this is to be completed through the simultaneous review of preliminary and final plans, with this review ensuring that everything is ready for final approval.

(cont.) In 2013, BWBR Architects, on behalf of ISD 916, applied for and received a conditional use permit to allow construction of a new public school on the subject site. This review takes the conditions of that approval under consideration, and provides updates where necessary.

- Site Data:**
- *Existing Zoning* – BP (Business Park)
 - *Land Use Guidance* – Business Park
 - *Parcel size* – 19.49 acres (848,981 square feet)
 - *Property Identification Numbers (PIDs)* – 33-029-21-42-0013
 - *Current Legal Description* – Outlot A, Eagle Point Business Park 7th Addition, according to the recorded plat thereof, Washington County, Minnesota

Review

IN GENERAL:

Site Character:

- The entire site is largely natural grasses with a few scattered trees. The eastern portion of the site and the middle of the site is nearly flat. The western portion of the site gently slopes downward from east to west to a DNR protected water course (intermittent stream) that runs along the west lot line.

Lot Configuration:

- This plat would create a single lot utilizing existing Outlot A, 7nd Addition.
- The lot configuration was previously reviewed and approved as a future building site by the Eagle Point Business Park 7nd Addition PUD plat review.

Area Calculations:

- **Parks and public open space:** No public parks are proposed as part of this developmet.
- **Public street rights-of-way:** none proposed
- **Wetlands:** the applicant is in the process of seeking a no loss determination (application included within this packet). As a condition of approval, staff would recommend that approval of all wetlands (or lack thereof) shall be completed prior to filing of the final plat.

Future parcel development:

- The proposed plat and site plan would fully develop this parcel.

- Adjacent parcel dev.:** Adjacent parcels to the southwest are already fully developed, so this proposal will not impact future development.
- Proposed Use:**
- “Schools, public and private” is a conditionally permitted use within the Eagle Point Business Park.
 - On December 17th, 2013, the Lake Elmo City Council passed Resolution No. 2013-107, approving a conditional use permit to allow a public school building within the Eagle Point Business Park. The CUP was granted with the following conditions:
 - *The applicant shall submit final development plans and a final plat consistent with the City’s Planned Unit Development and Subdivision requirements prior to the issuance of a building permit and prior to the commencement of any grading activity on the site.*
 - This is the process that is currently under review. **Criteria met.**
 - *The applicant shall secure any required permits form the South Washington Watershed District prior to commencing any grading or construction activity on the site.*
 - As a condition of approval, any further watershed district approvals that might be necessary shall be obtained prior to issuance of a building permit. **Criteria met.**
 - *The final development plans shall include detailed landscape plans that conform to the Lake Elmo Zoning Ordinance and Eagle Point Business Park PUD Standards.*
 - Detailed landscape plans have been provided and are reviewed herein. **Criteria met.**
 - *The applicant shall submit detailed architectural plans at the time of the final development plan review by the City. These plans shall conform to the City’s Design Guidelines and Standards Manual and the Eagle Point Business Park Design Guidelines.*
 - Details of the proposed exterior have been provided for review and are analyzed herein. **Criteria met.**
 - *The final development plans shall include a signage plan.*
 - Details of the proposed signs have been provided for review and are analyzed herein. **Criteria met.**

- (cont.)
- *The applicant shall pay a fee in lieu of park land dedication as determined by the City prior to the final plat being released for recording.*
 - Required park dedication is analyzed herein, and is typically paid for prior to issuance of a building permit. **Criteria met.**
 - *The final plat shall include all easements for drainage and utility and other purposes as required by the City Engineer.*
 - Proposed easements are shown on the preliminary and final plat/plans, and are analyzed herein. **Criteria met.**
 - *The applicant shall enter into a service agreement with the City prior to issuance of a building permit for the site.*
 - This condition of approval would carry over with any approvals for this application. **Criteria met.**
 - *The Conditional Use Permit shall be valid for a period of 24 months. The permit shall become void if an application for a final development plan is not submitted within 24 months of the date on which the conditional use permit was granted by the City Council.*
 - Application for preliminary and final plat/plan approval was submitted prior to the 24 month deadline. **Criteria met.**
 - *The applicant shall work with the City and neighboring property owners to develop an amicable resolution to the traffic and access spacing concerns that were voiced during the public hearing by a business owner within the Eagle Point Business Park.*
 - The concerns over possible problems between bus routes and employee traffic have been addressed since the conditional use permit was approved. Further details on this resolution are listed under “Resident Concerns” on page 6. **Criteria met.**

**Building
Location:**

- The proposed building will be more than 50’ from ROW and more than 10’ away from all other lot lines as required by code.
- Building placement is in accordance with encouraged development standards allowing for effective auto and pedestrian circulation, building setbacks, and open space along Eagle Point Boulevard.

- Building Size:** | ■ The proposed building foundation will exceed the required minimum of 10,000 square feet.
- Building Height:** | ■ No Business Park structure shall exceed 60' in height, and parapet walls shall not exceed more than 4 feet above the height permitted of the building.
■ This one-story structure will be compliant with the allowed height maximum.
- Building Design:** | ■ While not mandated, design standards encourage architectural treatments around the building with windows and doors that reflect the architectural style of the structure. As demonstrated by the plans, there are interesting elements included on all four sides of the structure, and the intermixing of brick and metal will provide visual interest from all directions.
- While not noted on the plans, all exterior metal appears to be painted or colored with earth tones, but such is listed herein as a condition of approval to ensure that is the case.
■ Except for four (4) chimney/vents central to the structure, all roof equipment is proposed to be enclosed within the new building thereby eliminating a main concern to exterior aesthetics.
■ Entryways for school facilities have special security considerations not typical for uses within the Business Park district, so staff is inclined to defer to the school district on best locations and positioning that best accomplish those goals. That said, overhangs or awnings are provided at the two main entry points off the front of the building adjacent to the proposed parking lot in accordance with the design standards.
■ The one area that is currently lacking in the current design is the need to incorporate parapets of varying heights. While some minor deviations are proposed, the vast majority of the building from the north, west, and south will appear to be of a single uniform height. It is recommend that the plans be updated to include additional parapet walls on all four sides of the building to address this deficiency.
- Lot Access:** | ■ The lot will have direct access to a public road (Eagle Point Boulevard).
■ There will be two entry points from Eagle Point Boulevard, one at the northern edge of the site and the other at the eastern edge. Both will connect to the building's parking facilities.
■ Both accesses to the lot will have a monument sign indicating the name and address of the school. These signs are analyzed under the heading of "Signage" on page 7.

- Easements:***
- All standard drainage and utility easements are shown on the preliminary plan and final plat documents.
 - Engineering comments regarding easements may be found on the Engineer's memo attached to this report.

- Resident Concerns:***
- Owners of the Bremer Bank adjacent to the site initially raised concerns about traffic conflicts with school buses during the Conditional Use public hearing. The applicant met with Kathy Tucci of Bremer Bank, and the Bank has since communicated with the City in writing that all concerns have been addressed.
 - A letter from ISD 916 to Bremer Bank is also included in this packet documenting closure of this concern.

INFRASTRUCTURE:

- Road System:***
- The road system in this area is fully built out, and no new roads are necessary to service this parcel.

- Water System(s):***
- The site will be served by 6" service lines which tie into the public 12" water line in Eagle Point Boulevard.
 - Engineering comments regarding the proposed water service may be found on the Engineer's memo attached to this report.

- Sanitary System(s):***
- Engineering comments regarding the proposed sanitary service may be found on the Engineer's memo attached to this report.

- Storm water /Grading:***
- Engineering comments regarding stormwater management and grading may be found on the Engineer's memo attached to this report.

- Utilities:***
- Telephone, electric, and/or gas service lines are to be placed underground in accordance with the provisions of all applicable City ordinances.

**Parking
Facilities:**

- The required parking ratio is one (1) space per staff member plus one (1) space per five (5) students of legal driving age based on design capacity
 - *The school will have a capacity of 75 staff members which would require 75 parking stalls*
 - *The school will have a capacity of 0 students of legal driving age, requiring 0 parking stalls*
- **As proposed, 154 stalls are being provided which meets the minimum requirement.**
- All parking stalls (except for a few accessible stalls and compact car stalls) are proposed to be 9 feet wide and 18 feet long. Minimum standards per Eagle Point Business Park Development Standards are 9'x18' north of Hudson Boulevard, and up to 20% of stalls may be designed for compact cars which require an 8'x16' space. All stall dimensions are in accordance with these requirements.
- The proposed parking lot conforms to minimum parking setbacks (front, 20'; sides and rear, 10'; side street, 20').
- As encouraged by the City's development standards, landscape islands are included within the parking lot, and lot lighting can be well controlled on site with one minor revision.

**Sidewalks or
Trails:**

- As there are no sidewalks or trails leading to the site, none are proposed as part of this development.

Signage:

- No new street signs will be required as no new roads are being constructed.
- The applicants are proposing two monument signs on the property; one located at each driveway access point off of Eagle Point Boulevard. According to code, two monument/ground signs are allowed for any lot with street frontage exceeding 1,000 feet. This property has 2,572 feet of street frontage along Eagle Point Boulevard, so two monument signs are allowed by permit.
- The allowed size of ground signs depends on the characteristics of the adjacent road. Since Eagle Point Boulevard is a two-lane road, with a speed limit of 30 mph, a ground sign may be up to ten (10) feet in height and thirty-two (32) square feet in area.
 - *The proposed ground signs are nine (9) feet high;*
 - *Liberally measuring the signs to **only** account for the main signage area results in the signs being roughly 38 square feet in size.*

- (cont.)**
- *Conservatively measuring each sign to include ALL of the potential sign face (including the base) results in a measurement of roughly 70 square feet.*
 - *Regardless of how the calculation should be made, the signs as proposed currently exceed allowed requirements and must be updated to be in compliance prior to sign permits being issued.*
 - Wall signs are currently limited to entrance markings, and all such letter appears to be in conformance with required standards.

- Fire Issues:**
- Fire hydrant locations appear to be adequate according to the Fire Chief, although the hydrant on the east side of the site currently labeled as “FUTURE ADDITION” must be installed immediately and should not be delayed.
 - The FDC (Fire Department Connection) location needs to be approved by the Fire Chief with the future building permit application.
 - Once the building is nearing completion, the school district shall contact the fire chief for instruction on how to order the required lock box and to get approval of its final location on-site.
 - Fire lanes must be identified, and be marked by painted curbing and signage.
 - The location of the Annunciator Panel must be approved by the Fire Chief.

- Streetlights:**
- No streetlights are required as part of this development.

- Lighting Plan:**
- The lighting plan for the site shows that illumination at the boundaries of the property will be within required standards with the exception of the light proposed for the northern entry point.
 - Per section 150.038 of City Code, the maximum illumination level at the property boundary is 1.5 foot candles if the fixture has a total cut off angle of greater than 90 degrees, and 3.0 if the fixture has a total cut off angle of less than 90 degrees. Regardless of the fixture type, the current lighting plan indicates that 3.5 foot candles will exist on the property line. As a condition of approval, the lighting plan must be updated to relocate the northern entrance light until illumination at the property line is within acceptable levels.
 - The height of all lighting standards shall not exceed 30’.

- Monuments:**
- Because the plat will not be changing any lot lines, existing monumentation for Outlot A will continue to be accurate following approval of the proposed plat.

ENVIRONMENTAL & OTHER NEIGHBORHOOD IMPACTS:

- | | |
|---|---|
| <i>Environmental Impacts:</i> | <ul style="list-style-type: none">▪ Staff has not identified any need for an in-depth environmental analysis based on the current proposal (i.e. EAW, EIS, AUAR, etc.) |
| <i>Wetlands & Public Waters:</i> | <ul style="list-style-type: none">▪ The applicant is in the process of seeking a no loss determination (application included within this packet). As a condition of approval, staff would recommend that approval of all wetlands (or lack thereof) shall be completed prior to filing of the final plat.▪ It has already been determined that a DNR Public Waters Work Permit is not needed for the proposed activity. |
| <i>Shoreland District:</i> | <ul style="list-style-type: none">▪ This site is not within a designated shoreland district. |
| <i>Erosion Control:</i> | <ul style="list-style-type: none">▪ Silt fencing has been proposed throughout the project site, specifically along the western ridge in order to prevent erosion into the DNR protected watercourse at the western edge of the property. Additional silt fences along drainage basins have also been proposed. Any additional erosion control BMPs as directed by the City Engineer should be shown at the construction limits with the future building permit application. |
| <i>Traffic:</i> | <ul style="list-style-type: none">▪ The proposed project will not significantly increase traffic volumes in a manner not anticipated by the original PUD approval. No traffic study is warranted |
| <i>Flood Plain & Steep Slopes:</i> | <ul style="list-style-type: none">▪ According to the February 3, 2010 FEMA Flood Insurance Rate Map (27163C0335E), the site is outside of the floodplain (Zone X).▪ The City engineer was asked to examine this proposal to determine if there are any steep slope concerns. Any such concerns will be noted in the Engineer's memo attached to this report. |
| <i>Landscaping:</i> | <ul style="list-style-type: none">▪ According to the Eagle Point Business Park development standards, the primary landscaping materials required for this type of development shall be shade trees with shrubs, hedges and other similar foliage used to complement trees. Berming and landscaping must be approved within one (1) year of City Council approval. |

- (cont.)**
- **Required Plantings:**
 - A minimum of 5 trees per disturbed acre and a minimum of two (2) trees per 100 linear feet of street frontage shall be provided.
 - Based on 16.7 disturbed acres and 2,572 linear feet of street frontage, code requires at a minimum 84 trees on the site and 52 trees along the road.
 - The applicants have submitted a landscape plan that shows 84 trees being planted on site with 52 trees along the road which does comply with minimum standards.
 - **Parking Landscaping Requirements:**
 - 5% of interior area of parking lots shall be devoted to landscape planting areas in islands or corner planting beds, and shade trees shall be provided on these islands or corner planting beds at 1 tree per 15 spaces.
 - Per the plans, there is 56,755 square feet of parking area which requires 2,838 square feet of internal landscaping areas. The plans indicate 5,600 square feet is being provided which meets minimum requirements.
 - Per plans, 154 parking spaces are being provided which requires 11 island or corner planting trees—all of which are being shown.
 - **Perimeter Parking Lot Landscaping:**
 - A landscaped frontage strip of a minimum of eight (8) feet shall be provided between the parking lot and a public street given that over one hundred spaces are proposed [Section 154.258(E)]. This is being met by the current plans.
 - Within the frontage strip, a screen created by a wall, fence, berm, or hedge is required at a minimum height of 3½ feet, and a maximum of 4 feet. This screening must not be less than fifty percent (50%) opaque on a year-round basis. Additional plantings may be necessary to meet this requirement. Staff is recommending that the final landscape plan be reviewed and approved by the City's consulting landscape architect prior to a building permit being issued.

Docks: ▪ Not applicable as the project does not include any lakeshore or deeded access to lakes.

Other Permits: ▪ All necessary permits shall be provided to the City prior to issuance of a building permit.

CHARGES, FEES, & RESPONSIBILITIES:

- In General:*** | ■ As always, the applicant is responsible for all fees related to the review of this application (including but not limited to planning, legal, engineering, wetland, environmental consultants, or other such experts as required by this application).
- Park Dedication:*** | ■ Approval of the Eagle Point Business Park included provisions which allocated individual park dedications to each development site on a square footage basis. The overall fee for the business park to be collected totaled \$252,437, which led to an allocation of \$0.06 per square foot for individual sites within the park. Accordingly, the required dedication would be as follows:
- 848,981 square feet * \$0.06/sq ft = \$50,938.86**
- Because the proposed playground and sports fields will be semi-private facilities meant for the school and not intended for daily public use, they will not receive credit against the required park dedication.
- Park dedication shall be paid prior to the issuance of a building permit for the proposed office building.
- Sewer Area Charge:*** | ■ As this subdivision will access municipal sewer services, there will be a sewer area charge assessed.
- Building Permit Fees:*** | ■ As this subdivision will access municipal services, hookup fees will be applicable.

After reviewing all criteria in code, staff believes the requested preliminary and final PUD plan/plat should be approved with conditions.

Conclusion

The Planning Commission is asked to examine the proposed Preliminary & Final PUD Plan/Plat materials and make a determination as to whether they meet all conditions of approval outlined by city code. Keep in mind that an approval at this point finalizes the subdivision—all desired/required changes must be addressed at this time.

**Commission
Options:**

The Planning Commission has the following options:

- A) RECOMMEND APPROVAL of the requested Preliminary & Final PUD Plan/Plat based on the applicant's submission, the contents of this report, public testimony and other evidence available to the Council.
- B) RECOMMEND DENIAL of the requested Preliminary & Final PUD Plan/Plat based on the applicant's submission, the contents of this report, public testimony and other evidence available to the Commission.
- C) TABLE the request for further study.

**Template
APPROVAL
Motion:
(Recommended)**

- “I move to recommend conditional **approval** of the requested Preliminary & Final PUD Plan/Plat based on the following findings of fact:
 - a) The application is conforming to the standards and requirements for a PUD within the Eagle Point Business Park;
 - b) The building design largely employs guidance from the City’s design standards including building placement, open space, landscaping, controlled parking lot lighting, high quality exterior materials and a cohesive architectural style;
 - a. The applicant has agreed to improve the building design through the addition of more parapet walls on all four sides of the building to bring the roofline into compliance with required design standards;
 - b. The applicant has agreed to update the lighting plan to eliminate the nonconforming illumination at the northern entry point;
 - c) Engineering design standards are being met or can be met via conditions; and
 - d) Review of the plan did not identify any environmental issues that cannot be addressed through the proposed design.

**Recommended
Conditions:**

1. The City Engineer shall approval all final easements prior to consideration of the Preliminary & Final PUD Plan/Plat by Council.
2. The City Engineer shall work with the applicant to finalize all stormwater management plans to bring them into compliance with all applicable state and local regulations.
3. In accordance with the conditional use permit condition, the applicant shall enter into a service agreement with the City prior to issuance of a building permit for the site.

- (cont.)
4. All necessary permits from other State, Regional, or local agencies shall be provided to the City prior to issuance of a building permit.
 5. Fire related conditions include:
 - a. FIRE HYDRANTS
 - i. Spacing – prior to filing of the final plat, the City Engineer must approve proposed spacing of fire hydrants after confirming proposed placements meet City requirements for commercial development.
 - ii. The hydrant on the East side of the property, currently identified as “FUTURE ADDITION,” shall be installed at the same time as all other fire hydrants.
 - b. The FDC (Fire Department Connection) location, Annunciator Panel location, lock box location, and lock box type shall all be approved by the Fire Chief prior to installation.
 - c. Fire lanes shall be identified with the Fire Chief, and all such lanes shall be marked by painted curbing and signage when constructed.
 6. Height of all lighting standards shall not exceed 30’.
 7. The lighting plan shall be updated to relocate the proposed lighting standard near the northern entry point to a location that brings anticipated foot candles at the property line to a conforming level.
 8. Silt fencing and/or other erosion control BMPs as directed by the City Engineer shall be shown at the construction limits with the future building permit application.
 9. Telephone, electric, and/or gas service lines are to be placed underground in accordance with the provisions of all applicable City ordinances.
 10. Design of both monument signs shall be brought into compliance with City standards for maximum sign size [thirty-two (32) square feet given the characteristics of the adjacent roadway].
 11. A park dedication payment in the amount of \$50,938.86 shall be paid to the City prior to the issuance of a building permit for the proposed school building.
 12. Access charges for municipal services (as may be applicable) shall be paid prior to the issuance of a building permit for the proposed school building.

- (cont.)
13. The final landscape plan be reviewed and approved by the City's consulting landscape architect prior to a building permit being issued.
 14. Building plans shall be updated to include additional parapet walls on all four sides of the building to address an existing lack of roof articulation.
 15. All exterior metal shall be painted or colored with earth tones which complement the other proposed siding materials for the building.

Template

DENIAL

Motion:

(Not Recommended)

- "I move to **recommend denial** of the requested Preliminary & Final PUD Plan/Plat based on the following findings of fact:
(Please list your findings of fact as to why the application is not in compliance with zoning or subdivision regulations)

MEMORANDUM

FOCUS ENGINEERING, inc.

Cara Geheren, P.E. 651.300.4261
Jack Griffin, P.E. 651.300.4264
Ryan Stempski, P.E. 651.300.4267
Chad Isakson, P.E. 651.300.4285

Date: August 21, 2015

To: Ben Gozola, Senior Planner, Sambatek
Cc: Kyle Klatt, Planning Director
From: Jack Griffin, P.E., City Engineer
Re: ISD 916 Intermediate School Building
Final Plat/Construction Plan Review

An engineering review has been completed for the Northeast Metro Intermediate School District Building. The Final Plat and Construction Plan submittal consisted of the following documentation prepared by BRBW dated July 22, 2015:

- Existing Conditions Survey, Preliminary and Final Plat.
- Grading, Drainage and Erosion Control Plans.
- Paving and Dimension Plan.
- Utility Plan.
- Landscape Plan.
- Civil Details and Landscape Details.
- Stormwater Management Plan including SWPPP.

STATUS/FINDINGS: An engineering review has been completed for the Eagle Point Business Park 8th Addition Final Plat and Construction Plans. Please see the following comments relating to the Final Plat application.

FINAL PLAT – EAGLE POINT BUSINESS PARK 8TH ADDITION

1. Additional easements dedicated to the City must be provided as part of the Final Plat. A 30-foot wide utility easement must be placed over all 8-inch watermain pipe, 6-inch hydrant leads and the hydrants, centered over the pipe such that 15-foot easements exist in all directions of the pipe and hydrants. Easements must be shown on the site plan, grading plan, utility plan and Plat.
2. No construction may occur until the applicant has received City Engineer approval for the Final Construction Plans; the applicant has obtained and submitted to the City all applicable permits needed for the project; and a preconstruction meeting has been held by the City’s engineering department.
3. The Final Plat shall not be recorded until final construction plan approval is granted.

STORM WATER MANAGEMENT PLAN

1. SWWD Permit. The Storm Water Management Plan was determined to be incomplete by the SWWD. The applicant must obtain a SWWD Permit prior to final plan review by the City. Plan revisions made to acquire this permit must be resubmitted for City review.
2. There are two Manage 2 type wetlands on the western edge of the parcel. These wetlands are subject to the requirements SWWD and DNR review. The applicant must submit information to show compliance with SWWD Rule 3 and the Wetland Conservation Act of 1991 as part of the WCA process.
3. The applicant must obtain an MPCA Stormwater Construction Permit prior to the start of construction. Plan revisions made to acquire this permit must be resubmitted for City review.

4. A HydroCAD model has been submitted showing a reduction in rate discharging for the 2, 10, and 100-yr 24-hr storm events, thereby meeting rate control requirements.
5. The applicant is proposing to use a combination of four (4) infiltration basins to meet volume control and water quality requirements. The plan complies with SWWD and state rules subject to verification of plan assumptions.
6. Each infiltration basin area must be investigated to verify infiltration assumptions shown in the storm water model. A minimum of 2 borings are required to a depth of at least 5 feet below the proposed infiltration basin bottom.
7. The storm water facilities are proposed to be constructed on private property and are not being constructed to meet City engineering design standards. The storm water facilities therefore must be privately owned and maintained. A maintenance agreement in a form acceptable to the City should be executed and recorded with the County for all permanent storm water facilities to be located on private property. The agreement shall provide a maintenance plan defining the maintenance responsibilities for the private owner, the type of maintenance and the maintenance intervals.

FINAL CONSTRUCTION PLANS & SPECIFICATIONS: The following comments have been provided to direct plan corrections necessary for final construction plan approval. When submitting revised plans, please provide a point by point response letter that details the changes made to the plans.

1. Final Construction Plans and Specifications must be prepared in accordance with the City Engineering Design Standards Manual using City standard plan notes, details and specifications for all public infrastructure.
2. City of Lake Elmo standard specifications. City of Lake Elmo standard specifications must be placed in a project manual to be available to the Contractor. The project manual must be submitted with the revised final construction plans.
3. The Specifications must include the following statement as the first clause of the supplementary provisions, *"The City Standard Specifications for Public Infrastructure, dated February 2015, shall apply to the work performed under this contract. Any supplemental specifications are intended to supplement the City Standard Specifications, however they do NOT supersede the City Standard Specifications, Details, Design Standards, or ordinances unless specific written approval has been provided by the City."*
4. Sheet 200CG:
 - City Standard Plan Notes for Grading and Erosion Control must be included on the Grading and Erosion Control Plans. Any plan notes that contradict the City plan notes must be removed.
 - A plan note must be added to specify maximum slopes of 4:1 within public R/W.
 - Turf establishment details must be added to the plans. Seed, sod, mulch types and applications must be specified including topsoil requirements and preparation.
 - Seeding mixes and turf establishment requirements for infiltration basins must be added to the plans.
 - Plan notes must be added to require the basin subgrade to be scarified to a minimum 12-inch depth prior to the placement of infiltration soil media.
 - Basin 4 outlet structure elevations and basin elevations must be corrected to be consistent with the proposed design.
 - Storm sewer should be minimum 15-inch diameter.
5. Sheet 200 CP:
 - The project proposes two driveway accesses located off Eagle Point Boulevard. It appears that the driveway locations are consistent with staff direction, however the plans must be updated to show the intersection of Island Drive with Eagle Point Boulevard so staff may review and verify access spacing relative to Island Drive.
 - Drainage, geometric and profile details must be added to the plans to verify drainage is not directed onto Eagle Point Boulevard at each access driveway. Valley gutters may be required to accommodate drainage along Eagle Point Boulevard.

- Curb cuts and utility cuts to the existing pavement of Eagle Point Boulevard to be completed to the limits and at the direction of the City Engineer. All cuts must be full width patches and must be sufficiently large to utilize standard compaction equipment.
 - Add plan note at each access driveway as follows: "Sawcut and remove existing B618 concrete curb and gutter at the nearest curb joint. Sawcut along existing bituminous street edge must be straight and smooth".
 - Fire Protection. Site Plans must be updated to include the placement of Fire Lanes, Yellow painted curbing and No Parking areas for fire protection purposes at the direction of the Fire Chief.
6. Sheet 200 CU:
- Connection to existing sanitary sewer stub. The project proposes to connect to the existing 8-inch sanitary sewer stub located in Eagle Point Boulevard. A 6-inch private service line is proposed to connect to the building. No public utilities are proposed.
 - Connection to existing watermain stub. The project proposes to connect to an existing 12-inch watermain at two locations within Eagle Point Boulevard to create a looped watermain network. In addition, an 8-inch watermain is also proposed interior to the site and looped around the building with 5 hydrants for fire protection. A 4-inch water service line and 8-inch fire service line are proposed to connect to the building (private service lines). The interior site watermain loop with hydrants must be publically owned and maintained. Easements must be provided as part of the revised Plat.
 - The City minimum pipe size is 8inch. The 6-inch watermain shown on the northwest side of the building must be revised to 8-inch pipe.
 - City Standard Plan Notes for Watermain must be included on the Utility Plans. Any plan notes that contradict the City plan notes must be removed.
7. Sheet 200LP:
- Plan notes must be added to require the Contractor to flag all tree locations for City review and approval prior to installation. All trees must be located a minimum of 5 feet from public utilities.
 - Engineering has not reviewed the proposed landscape plan (privately owned) or building plans.
8. Detail Sheets:
- City Standard Details Grading and Erosion Control must be used for the project and included on the detail sheets.
 - City Standard Details for Watermain must be used for the project and included on the detail sheets.

**CITY OF LAKE ELMO
WASHINGTON COUNTY
STATE OF MINNESOTA**

RESOLUTION NO. 2013-107

*A RESOLUTION APPROVING A CONDITIONAL USE PERMIT TO ALLOW A PUBLIC SCHOOL
BUILDING WITHIN THE EAGLE POINT BUSINESS PARK*

WHEREAS, the City of Lake Elmo is a municipal corporation organized and existing under the laws of the State of Minnesota; and

WHEREAS, BWBR Architects, acting on behalf of Northeast Metro 916 Intermediate School District, 2540 County Road F East, White Bear Lake, MN and United Properties, 3600 American Boulevard, Suite 750, Minneapolis, MN (“Applicant”) has submitted an application to the City of Lake Elmo (the “City”) for a Conditional Use Permit to allow the construction of a new public school serving disabled students in grades kindergarten through eighth grade within the Eagle Point Business Park; and

WHEREAS, notice has been published, mailed and posted pursuant to the Lake Elmo Zoning Ordinance, Section 154.102; and

WHEREAS, the Lake Elmo Planning Commission held a public hearing on said matter on November 25, 2013; and

WHEREAS, the Lake Elmo Planning Commission submitted its report and recommendation concerning the Conditional Use Permit request to the City Council as part of a Staff Memorandum dated December 3, 2013; and

WHEREAS, the City Council considered said matter at its December 3, 2013 and December 17, 2013 meetings.

NOW, THEREFORE, based on the testimony elicited and information received, the City Council makes the following:

FINDINGS

- 1) That the procedures for obtaining said Conditional Use Permit (CUP) are found in the Lake Elmo Zoning Ordinance, Section 154.106.
- 2) That all the submission requirements of said Section 154.106 have been met by the Applicant.
- 3) That the proposed Conditional Use Permit is to allow the construction of a new public school serving disabled students in grades kindergarten through eighth grade within the Eagle Point Business Park as more fully described in application materials submitted to the City.
- 4) That the proposed Conditional Use Permit will be located on property legally described as Outlot A of Eagle Point Business Park Seventh Addition according to the plat thereof filed of record in the office of the Register of Titles in and for Washington County, Minnesota.

- 5) That a public school is designated as a Conditional Use within the BP – Business Park Zoning District in Section 154.551 of the Zoning Ordinance; and
- 6) That the proposed use will not be detrimental to or endanger the public health, safety, comfort, convenience or general welfare of the neighborhood or the city.
- 7) That the proposed use conforms to the City of Lake Elmo Comprehensive Plan.
- 8) That the proposed use is compatible with the existing neighborhood.
- 9) That the proposed use meets all specific development standards for such use listed in Article 7 of the Zoning Ordinance.
- 10) That the proposed use will be designed, constructed, operated and maintained so as to be compatible in appearance with the existing or intended character of the general vicinity and will not change the essential character of that area.
- 11) That the proposed use will not be hazardous or create a nuisance as defined under this Chapter to existing or future neighboring uses.
- 12) That the proposed use will be served adequately by essential public facilities and services, including streets, police and fire protection, drainage structures, refuse disposal, water and sewer systems and schools or will be served adequately by such facilities and services provided by the persons or agencies responsible for the establishment of the proposed use.
- 13) That the proposed use will not create excessive additional requirements at public cost for public facilities and services and will not be detrimental to the economic welfare of the community.
- 14) That the proposed use will not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons, property or the general welfare because of excessive production of traffic, noise, smoke, fumes, glare or odors.
- 15) That vehicular approaches to the property, where present, will not create traffic congestion or interfere with traffic on surrounding public thoroughfares.
- 16) That the proposed use will not result in the destruction, loss or damage of a natural or scenic feature of major importance.

CONCLUSIONS AND DECISION

Based on the foregoing, the Applicants' application for a Conditional Use Permit is granted subject to the following conditions:

- 1) The applicant shall submit final development plans and a final plat consistent with the City's Planned Unit Development and Subdivision requirements prior to the issuance of a building permit and prior to the commencement of any grading activity on the site.
- 2) The applicant shall secure any required permits from the South Washington Watershed District prior to commencing any grading or construction activity on the site.

- 3) The final development plans shall include detailed landscape plans that conform to the Lake Elmo Zoning Ordinance and Eagle Point Business Park PUD Standards.
- 4) The applicant shall submit detailed architectural plans at the time of the final development plan review by the City. These plans shall conform to the City's Design Guidelines and Standards Manual and the Eagle Point Business Park Design Guidelines.
- 5) The final development plans shall include a signage plan.
- 6) The applicant shall pay a fee in lieu of park land dedication as determined by the City prior to the final plat being released for recording.
- 7) The final plat shall include all easements for drainage and utility and other purposes as required by the City Engineer.
- 8) The applicant shall enter into a service agreement with the City prior to issuance of a building permit for the site.
- 9) The Conditional Use Permit shall be valid for a period of 24 months. The permit shall become void if an application for a final development plan is not submitted within 24 months of the date on which the conditional use permit was granted by the City Council.
- 10) The applicant shall work with the City and neighboring property owners to develop an amicable resolution to the traffic and access spacing concerns that were voiced during the public hearing by a business owner within the Eagle Point Business Park.

Passed and duly adopted this 17th day of December by the City Council of the City of Lake Elmo, Minnesota.



Mike Pearson, Mayor

ATTEST:



Adam Bell, City Clerk



NORTHEAST METRO INTERMEDIATE SCHOOL DISTRICT 916
K-8 Special Education Building Lake Elmo, Minnesota

PLAT APPLICATION REVIEW
July 22, 2015

PROJECT DIRECTORY	
OWNER	NORTHEAST METRO INTERMEDIATE SCHOOL DISTRICT 916 2540 County Road F East White Bear Lake, MN 55110 T 651.415.5562 F 651.415.5510
ARCHITECT	BWBR 380 ST. PETER STREET SUITE 600 SAINT PAUL, MN 55102 T 651.222.3701 F 651.222.8961
CIVIL ENGINEER	LARSON ENGINEERING 3524 LABORIE ROAD WHITE BEAR LAKE, MN 55119-5126 T 651.481.9120 F 651.481.9201
MECHANICAL ENGINEER	DUNHAM 50 SOUTH SIXTH STREET SUITE 1100 MINNEAPOLIS, MN 55402-1540 T 612.465.7550 F 612.465.7551
ELECTRICAL ENGINEER	DUNHAM 50 SOUTH SIXTH STREET SUITE 1100 MINNEAPOLIS, MN 55402 T 612.465.7550 F 612.465.7551
STRUCTURAL ENGINEER	ERICKSEN ROED & ASSOCIATES 2550 UNIVERSITY AVENUE WEST SUITE 201-S ST. PAUL, MN 55114-1904 T 651.251.7570 F 651.521.7578
CONSTRUCTION MANAGER	KRAUS-ANDERSON 8625 Remondva Street NE Circle Pines, MN 55014 763-786-7711 fax: 763-786-2650

SHEET INDEX - FRONT END.

Plat Review	Sheet Number	Sheet Name
■	100.1	TITLE SHEET - PLAT APPLICATION REVIEW

SHEET INDEX - SITEWORK.

Plat Review	Sheet Number	Sheet Name
■	200.CG	GRADING AND EROSION CONTROL PLAN
■	200.CP	PAVING AND DIMENSION PLAN
■	200.CJ	UTILITY PLAN
■	200.LP	LANDSCAPE PLAN
■	201.LP	TREE PRESERVATION & REMOVAL
■	210	DETAILS
■	211	DETAILS
■	220	LANDSCAPE DETAILS

SHEET INDEX - ARCHITECTURAL.

Plat Review	Sheet Number	Sheet Name
■	401.FL	FIRST LEVEL - FLOOR PLAN
■	510	EXTERIOR ELEVATIONS
■	511	EXTERIOR ELEVATIONS
■	512	ISOMETRIC VIEWS

GENERAL NOTES:

- THE ARCHITECTURAL DRAWINGS SHOW PRINCIPAL AREAS AND LIMITS OF CONSTRUCTION WHERE WORK MUST BE ACCOMPLISHED UNDER THIS CONTRACT. INCIDENTAL WORK MAY BE NECESSARY IN AREAS NOT SHOWN ON ARCHITECTURAL DRAWINGS DUE TO CHANGES AFFECTING ELECTRICAL, MECHANICAL, AND PLUMBING ALONG WITH OTHER SYSTEMS. THIS INCIDENTAL WORK SHALL BE PART OF THIS CONTRACT AND ALL TRADES SHALL INSPECT THESE AREAS, ASCERTAIN WORK REQUIRED AND DO THE WORK IN ACCORDANCE OF CONTRACT REQUIREMENTS AT NO ADDITIONAL COST.
- CONTRACTORS SHALL VISIT THE SITE DURING BIDDING TO BECOME FAMILIAR WITH THE EXISTING CONDITIONS. THE GENERAL CONTRACTOR SHALL LOCATE, INSPECT AND FIELD VERIFY ALL EXISTING CONDITIONS, DIMENSIONS AND ELEVATIONS PRIOR TO DEMOLITION AND CONSTRUCTION. NOTIFY ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES.
- DO NOT SCALE DRAWINGS. NOTIFY ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES.
- WHEREVER OPENINGS ARE CUT THROUGH FIRE RATED PARTITIONS, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR FOR WHOM THE HOLE IS CUT TO PATCH AND REPAIR ANY OPENING TO MAINTAIN THE INTEGRITY OF THE FIRE RATING.
- GENERAL CONTRACTOR TO CHECK MECHANICAL DRAWINGS FOR EXISTING PIPES AND DUCTS FURRED IN WALLS. VERIFY SIZE AND LOCATION WITH MECHANICAL CONTRACTOR PRIOR TO PROCEEDING WITH REMODELING.
- FIREPROOFING SHALL BE UNPERFORMED ANY SUBCONTRACTOR PENETRATING THE FIREPROOFING SHALL BE REQUIRED TO REPLACE FIREPROOFING TO THE ORIGINAL CONDITION AND FIRE RATING, AT THE SUBCONTRACTOR'S EXPENSE.
- EQUIPMENT UNIT DIMENSIONS ARE FOR PRODUCT DESCRIPTION ONLY. VERIFY SIZE WITH MANUFACTURER.
- ALL DIMENSIONS PERTAINING TO MECHANICAL, ELECTRICAL, SERVICES OR EQUIPMENT SHALL BE VERIFIED WITH THE RESPECTIVE TRADE.
- ALL CONTRACTORS THAT PENETRATE AND/OR DISTURB ANY AREAS AT EXISTING CONDITIONS SHALL PATCH AREA TO MATCH EXISTING ADJACENT AREA OR SURFACE AND PREPARE FOR SCHEDULED FINISH APPLICATION. COORDINATE WORK WITH GENERAL CONTRACTOR PRIOR TO PROCEEDING.
- VERIFY HEIGHTS AND LOCATIONS OF ACCESS PANELS (AP) AND COORDINATE TYPES WITH TRADES WHICH REQUIRE THEM.
- PROVIDE LINTELS AND FRAMING FOR GRILLES, LOUVERS, AND ROOF VENTS AS REQUIRED BY MECHANICAL CONTRACTOR. VERIFY SIZE AND LOCATION.
- STRUCTURAL, MECHANICAL, AND ELECTRICAL ABBREVIATIONS AND SYMBOLS MAY DIFFER FROM ARCHITECTURAL. SEE RESPECTIVE SECTIONS AND/OR DRAWINGS FOR DEFINITIONS.
- AT MECHANICAL AND ELECTRICAL EQUIPMENT ROOMS, PROVIDE 3/4" X 4" PLYWOOD BACKING BEHIND ALL SURFACE MOUNTED FIXTURES AND EQUIPMENT, UNLESS NOTED OTHERWISE.
- HOUSEKEEPING PADS SHALL BE PROVIDED BY TRADES WHICH REQUIRE THEM. SEE MECHANICAL AND ELECTRICAL DRAWINGS FOR SIZES AND LOCATIONS.

■	NOT ISSUED
■	ISSUED
R	ISSUED FOR REFERENCE ONLY
N	ISSUED AS NOTED

SHEET NUMBERING AND ABBREVIATION EXPLANATION

BUILDING NUMBER	SHEET GROUPING	FLOOR LEVEL	ZONE NUMBER	PLAN TYPE	SHEET GROUPING	PLAN
1	402	1	DE		300	D DEMO PLANS
1	402	1	DE		400	DE DEMOLITION PLAN DI DIMENSION PLAN EX EXTERIOR PLAN FL FLOOR PLAN FN FINISH PLAN FP FLOOR PATTERN PLAN QU EQUIPMENT PLAN RC REFLECTED CEILING PLAN SY (SYSTEMS) FURNITURE PLAN
1	701	1	DF		700	DF DEMO FIRE PROTECTION DM DEMO MED GAS DP DEMO PLUMBING M MECH FIRE PROTECTION P MECH PLUMBING
1	701	1	DF		800	DH DEMO HVAC DP DEMO PIPING H MECH HVAC P MECH PIPING
200	AS				900	DL DEMO ELEC LIGHTING DP DEMO ELEC POWER DS DEMO ELEC SYSTEMS L ELEC LIGHTING P ELEC POWER S ELEC SYSTEMS
100	CO					TITLE SHEET
100	PH					CODE PLAN
100	PH					PHASING PLAN
100	RF					REFERENCE PLAN
200	AS					ARCHITECTURAL SITE PLAN
200	CD					CIVIL DEMOLITION PLAN
200	CG					CIVIL GRADING PLAN
200	CJ					CIVIL UTILITIES PLAN
200	CP					CIVIL PAVING PLAN
200	LP					LANDSCAPE PLAN

This document may be an electronic file or may be printed from an electronic file provided to the user. It is the sole responsibility of the user to ensure that the content and quality is consistent with the content and quality of the paper documents on file at BWBR.

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am duly licensed to practice in the State of Minnesota.

Name: _____
Date: 07/22/2015 Reg. No: _____

Issued For: _____
Item: PLAT APPLICATION REVIEW Date: 07/22/2015

NOT FOR CONSTRUCTION

This Sheet may be a Reduced Copy
The bar above is 1" long on a Full Size Sheet.
Drawing Scales apply to Full Size Sheets.

Conv. No: 32014276.00 Drawn: MNS

Sheet Title: **TITLE SHEET - PLAT APPLICATION REVIEW**

Sheet No: _____

100.1

GRADING NOTES

1. Tree protection consisting of snow fence or safety fence installed at the drip line shall be in place prior to beginning any grading or demolition work at the site.
2. All elevations with an asterisk (*) shall be field verified. If elevations vary significantly, notify the Engineer for further instructions.
3. Grades shown in paved areas represent finish elevation.
4. See Landscaping Plan for turf and plantings restoration.
5. All construction shall be performed in accordance with state and local standard specifications for construction.

LEGEND

- 405 — EXISTING CONTOURS
- 850 — PROPOSED CONTOURS MAJOR INTERVAL
- 948 — PROPOSED CONTOURS MINOR INTERVAL
- — GRADE BREAK LINE
- — GRADE SLOPE
- ○ ○ ○ SILT FENCE
- ▭ RIP-RAP / ROCK CONST. ENTRANCE
- ▭ INLET PROTECTION
- ▭ CONCRETE WASHOUT STATION
- SPOT ABBREVIATIONS
- TC - TOP OF CURB
- GL - GUTTER LINE
- B - BITUMINOUS
- C - CONCRETE
- EO - EMERGENCY OVERFLOW
- TW - TOP OF WALL
- BW - BOTTOM OF WALL (FIG)
- (*) - EXISTING TO BE VERIFIED

EROSION CONTROL NOTES

1. Owner and Contractor shall obtain MPCA-NPDES permit. Contractor shall be responsible for all fees pertaining to this permit. The SWPPP shall be kept onsite at all times.
2. Install temporary erosion control measures (inlet protection, silt fence, and rock construction entrances) prior to beginning any excavation or demolition work at the site.
3. Erosion control measures shown on the erosion control plan are the absolute minimum. The contractor shall install temporary silt fences, sediment traps or basins, additional siltation fencing, and/or disk the soil parallel to the contours as deemed necessary to further control erosion. All changes shall be recorded in the SWPPP.
4. All construction site entrances shall be surfaced with crushed rock across the entire width of the entrance and from the entrance to a point 50' into the construction zone.
5. The toe of the silt fence shall be trrenched in a minimum of 6" The trench backfill shall be compacted with a vibratory plate compactor.
6. All grading operations shall be conducted in a manner to minimize the potential for site erosion. Sediment control practices must be established on all down gradient perimeters before any up gradient land disturbing activities begin.
7. All exposed soil areas must be stabilized as soon as possible to limit soil erosion but in no case later than 14 days after the construction activity in that portion of the site has temporarily or permanently ceased. Temporary stockpiles without significant silt, clay or organic components (e.g., clean aggregate stockpiles, demolition concrete stockpiles, sand stockpiles) and the constructed base components of roads, parking lots and similar surfaces are exempt from this requirement.
8. The normal wetted perimeter of any temporary or permanent drainage ditch or swale that drains water from any portion of the construction site, or diverts water around the site, must be stabilized within 200 linear feet from the property edge, or from the point of discharge into any surface water. Stabilization of the last 200 linear feet must be completed within 24 hours after connecting to a surface water. Stabilization of the remaining portions of any temporary or permanent ditches or swales must be complete within 14 days after connecting to a surface water and construction in that portion of the ditch has temporarily or permanently ceased.
9. Pipe outlets must be provided with energy dissipation within 24 hours of connection to surface water.
10. All pipes shall be installed with a filter material or soil separation fabric and comply with the Minnesota Department of Transportation Standard Specifications.
11. All storm sewers discharging into wetlands or water bodies shall outlet at or below the normal water level of the respective wetland or water body at an elevation where the downstream slope is 1 percent or flatter. The normal water level shall be the invert elevation of the outlet of the wetland or water body.
12. All storm sewer catch basins not needed for site drainage during construction shall be covered to prevent runoff from entering the storm sewer system. Catch basins necessary for site drainage during construction shall be provided with inlet protection.
13. In areas where concentrated flows occur (such as swales and areas in front of storm catch basins and inlets) the erosion control facilities shall be backed by stabilization structure to protect those facilities from the concentrated flows.
14. Inspect the construction site once every seven days during active construction and within 24 hours after a rainfall event greater than 0.5 inches in 24 hours. All inspections shall be recorded in the SWPPP.
15. All BMPs must be repaired, replaced, or supplemented when they become nonfunctional or the sediment reaches 1/3 of the BMP capacity. These repairs must be made within 24 hours of discovery, or as soon as field conditions allow access. All repairs shall be recorded in the SWPPP.
16. If sediment escapes the construction site, off-site accumulations of sediment must be removed in a manner and at a frequency sufficient to minimize off-site impacts.
17. All soils tracked onto pavement shall be removed daily.
18. All infiltration areas must be inspected to ensure that no sediment from ongoing construction activity is reaching the infiltration area and these areas are protected from compaction due to construction equipment driving across the infiltration area.
19. Temporary soil stockpiles must have silt fence or other effective sediment controls, and cannot be placed in surface waters, including stormwater conveyances such as cuts and gutter systems, or conduits and ditches unless there is a bypass in place for the stormwater.
20. Collected sediment, asphalt and concrete millings, floating debris, paper, plastic, fabric, construction and demolition debris and other wastes must be disposed of properly and must comply with MPCA disposal requirements.
21. Oil, gasoline, paint and any hazardous substances must be properly stored, including secondary containment, to prevent spills, leaks or other discharge. Restricted access to storage areas must be provided to prevent vandalism. Storage and disposal of hazardous waste must be in compliance with MPCA regulations.
22. External washing of trucks and other construction vehicles must be limited to a defined area of the site. Runoff must be contained and waste properly disposed of. No engine degreasing is allowed onsite.
23. All liquid and solid wastes generated by concrete washout operations must be contained in a leak-proof containment facility or impermeable liner. A connected clay liner that does not allow washout liquids to enter ground water is considered an impermeable liner. The liquid and solid wastes must not contact the ground, and there must not be runoff from the concrete washout operations or areas. Liquid and solid wastes must be disposed of properly and in compliance with MPCA regulations. A sign must be installed adjacent to each washout facility to inform concrete equipment operators to utilize the proper facilities.
24. Upon completion of the project and stabilization of all graded areas, all temporary erosion control facilities (silt fences, hay bales, etc.) shall be removed from the site.
25. All permanent sedimentation basins must be restored to their design condition immediately following stabilization of the site.
26. Contractor shall submit Notice of Termination for MPCA-NPDES permit within 30 days after Final Stabilization.

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Eric C. Meyer, P.E.
Date: 07/23/2015 Reg. No. 44592

Issued For:
Date: 06/01/2015
07/23/2015

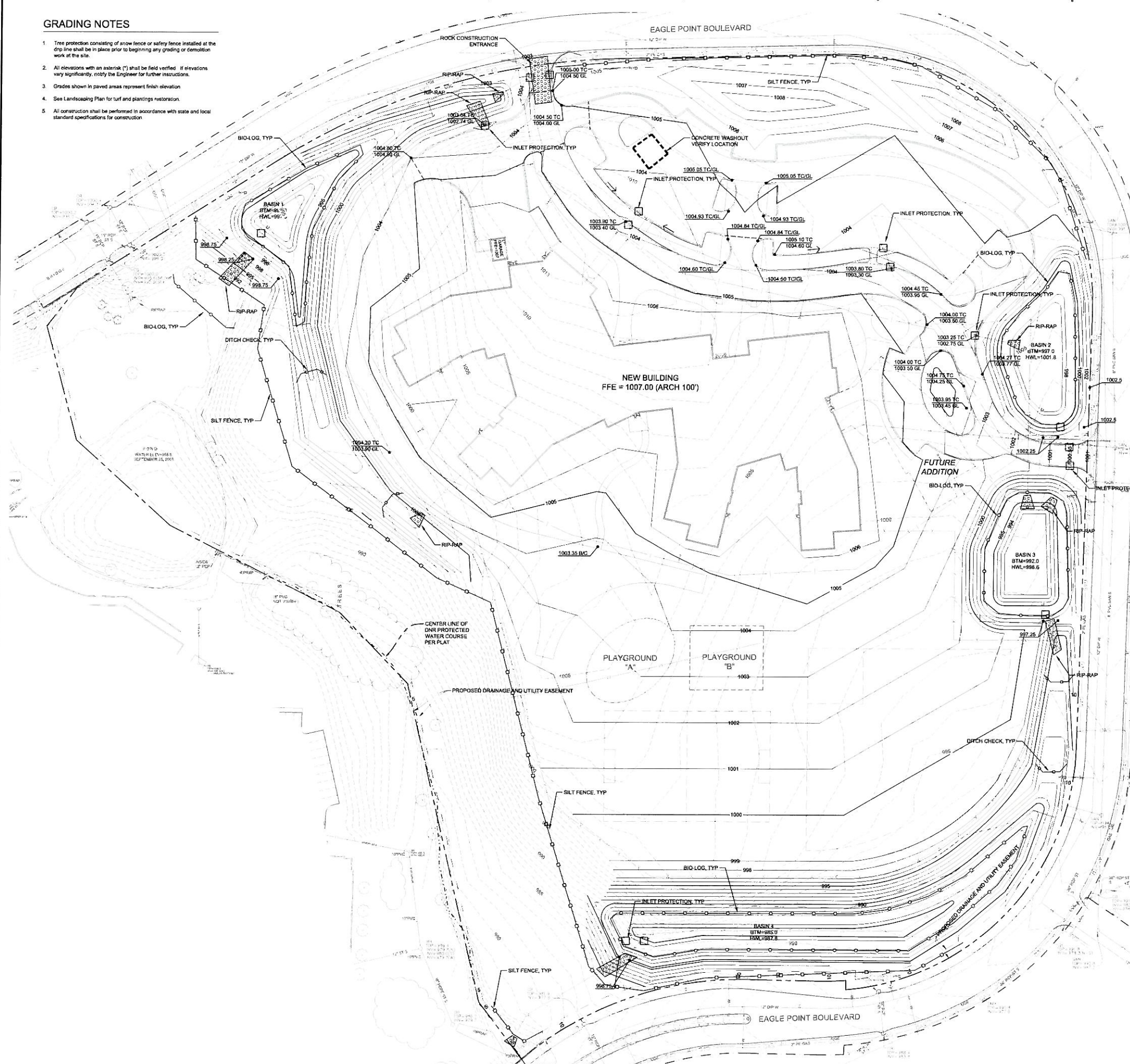
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Drawn:
32156033 KBK

Sheet Title:
**GRADING AND
EROSION CONTROL
PLAN**

Sheet No:

200.CG



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Date: 07/22/2015 Reg. No. 44592

Item	Date
SD Pricing Package	06/01/2015
PLAT APPLICATION REVIEW	07/22/2015

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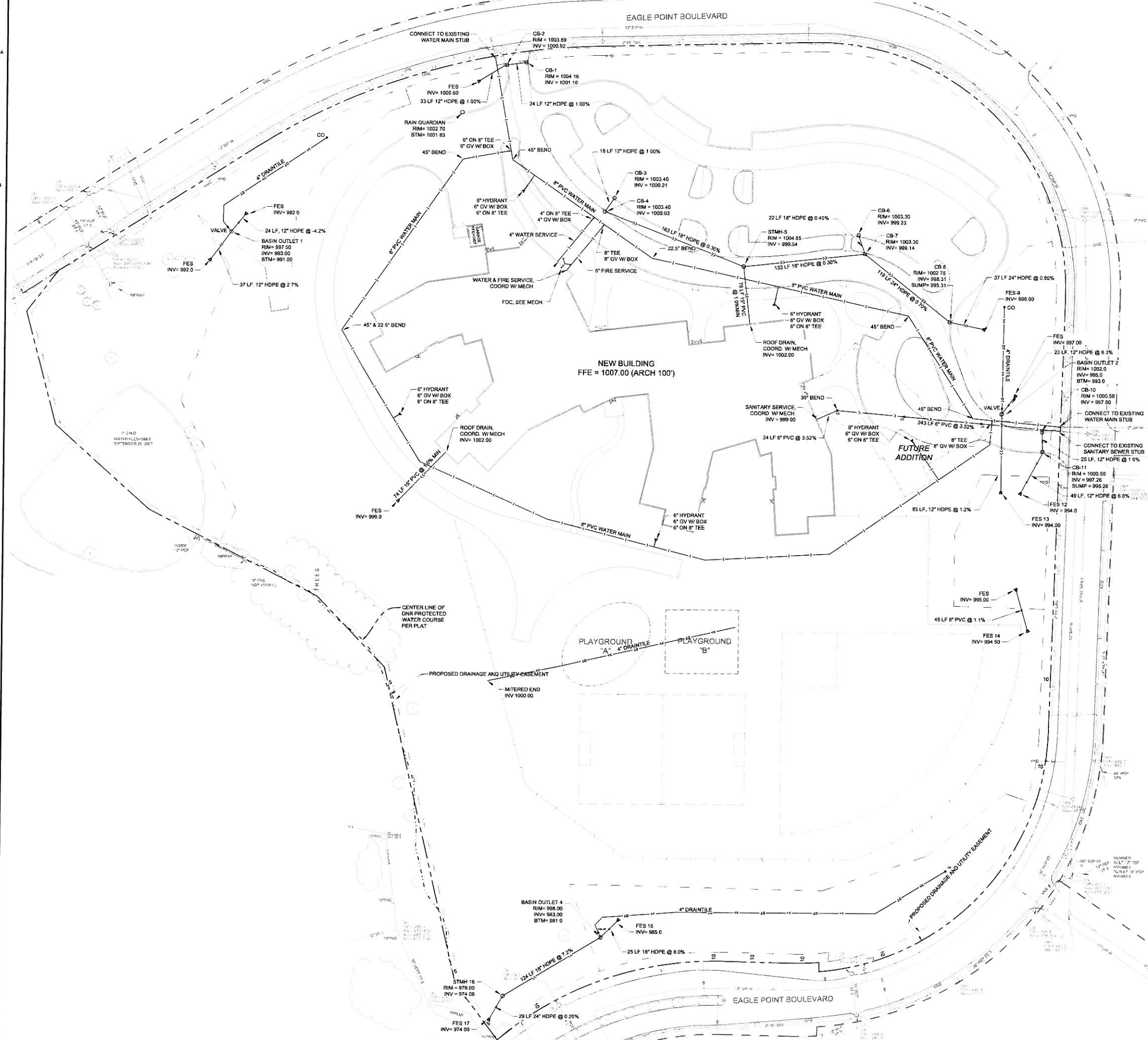
Comp. No. 12156033
Drawn: KMK
Sheet Title: UTILITY PLAN

Sheet No. 200.CU

LEGEND

- STORM MANHOLE
- CATCH BASIN
- △ CURB INLET
- FLARED END
- SANITARY MANHOLE
- HYDRANT
- ⊕ GATE VALVE & BOX
- ⊗ WATER SHUTOFF
- LIGHT POLE
- TV — CABLE UNDERGROUND LINE
- UE — ELECTRIC OVERHEAD LINE
- UE — ELECTRIC UNDERGROUND LINE
- FO — FIBER OPTIC UNDERGROUND LINE
- G — NATURAL GAS UNDERGROUND LINE
- S — SANITARY SEWER PIPE
- SW — STORM SEWER PIPE
- T — TELEPHONE UNDERGROUND LINE
- WM — WATERMAIN PIPE
- D — DRAIN TILE PIPE

- UTILITY NOTES**
- It is the responsibility of the contractor to perform or coordinate all necessary utility connections and relocations from existing utility locations to the proposed building, as well as to all onsite amenities. These connections include but are not limited to water, sanitary sewer, cable TV, telephone, gas, electric, site lighting, etc.
 - All service connections shall be performed in accordance with state and local standard specifications for construction. Utility connections (sanitary sewer, watermain, and storm sewer) may require a permit from the City.
 - The contractor shall verify the elevations at proposed connections to existing utilities prior to any demolition or excavation.
 - The contractor shall notify all appropriate engineering departments and utility companies 72 hours prior to construction. All necessary precautions shall be made to avoid damage to existing utilities.
 - Storm sewer requires testing in accordance with Minnesota plumbing code 4715.2820 where located within 10 feet of waterlines or the building.
 - HDPE storm sewer piping and fittings must meet materials and installation standards per Minnesota plumbing code 4715.0420, 6C(4) and 4715.0540 C including ASTM D3212 joint pressure test and ASTM C2321 installation practices. The City requires RCP pipe within the public right-of-way.
 - All RCP pipe shown on the plans shall be MNDOT class 5.
 - Maintain a minimum of 7' of cover over all water lines and sanitary sewer lines. Install water lines 18" above sanitary sewers, where the sanitary sewer crosses over the water line, install sewer piping of materials equal to watermain standards for 8 feet on both sides and maintain 18" of separation.
 - Where 7' of cover is not provided over sanitary sewer and water lines, install 2" rigid polystyrene insulation (MNDOT 3760) with a thermal resistance of at least 5 and a compressive strength of at least 25 psi. Insulation shall be 6" wide, centered over pipe with 6" sand cushion between pipe and insulation. Where depth is less than 5', use 4" of insulation.
 - All watermain piping shall be class 52 ductile iron pipe unless noted otherwise.
 - See Project Specifications for bedding requirements.
 - Pressure test and disinfect all new watermains in accordance with state and local requirements.
 - Sanitary sewer piping shall be PVC, SDR-35 for depths less than 12', PVC SDR-26 for depths between 12' and 26', and class 52 D.I.P. for depths of 26' or more.



LANDSCAPE REQUIREMENTS / PROVIDED OVERVIEW		
ITEM/REQUIREMENT:	REQUIRED:	PROVIDED:
1. ONE (1) TREE SHALL BE PROVIDED PER 50' OF STREET FRONTAGE	2,572 L.F. AT SITE PERIMETER / 50: 52 TREES (13 CONIF. MIN.) REQUIRED	52 TREES (14 CONIF.) PROVIDED
2. FIVE (5) TREES MUST BE PROVIDED FOR EVERY ONE (1) ACRE OF DISTURBED LAND	5 TREES X 16.7 ACRES: 84 TREES REQUIRED (21 CONIF. MIN.)	84 TREES (22 CONIF.) PROVIDED
3. 5% OF PARKING AREA SHALL BE LANDSCAPED	56,755 S.F. TOTAL PARKING AREA X 5%: 2,838 S.F. REQUIRED	5,600 S.F. PROVIDED
4. ONE (1) TREE SHALL BE PROVIDED PER 15 PARKING STALLS	155 TOTAL STALLS / 15 STALLS: 11 TREES (3 CONIF. MIN.) REQUIRED	11 TREES (3 CONIF.) INCLUDED IN ITEM 2. PROVIDED
5. ONE (1) TREE SHALL BE PROVIDED PER 50 L.F. OF PARKING EDGE FACING STREET	431 L.F. PARKING EDGE / 50 L.F.: 9 TREES (3 CONIF. MIN.) REQUIRED	9 TREES (3 CONIF.) INCLUDED IN ITEM 2. PROVIDED
6. 8' CLEAR LANDSCAPE BUFFER SHALL BE PROVIDED AT PARKING PERIMETER	8' CLEAR MIN. LANDSCAPE BUFFER REQUIRED	8' CLEAR MIN. LANDSCAPE BUFFER PROVIDED
TOTAL TREES REQUIRED/PROVIDED	34 CONIF. MIN. REQUIRED 136 TOTAL TREES REQUIRED	100 DECIDUOUS TREES PROVIDED 35 CONIF. TREES PROVIDED 135 TOTAL TREES PROVIDED

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DAMON FARBER
LANDSCAPE ARCHITECTS

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I hereby certify that this plan, specification or report was prepared by me or under the direct supervision and that I am a duly licensed landscape architect in the state of Minnesota.

Damon Farber, P.L.A.
Date: 07/22/2015 Reg. No. 26970

Item	Date
SD Pricing Package	06/01/2015
PLAY APPLICATION REVIEW	07/22/2015

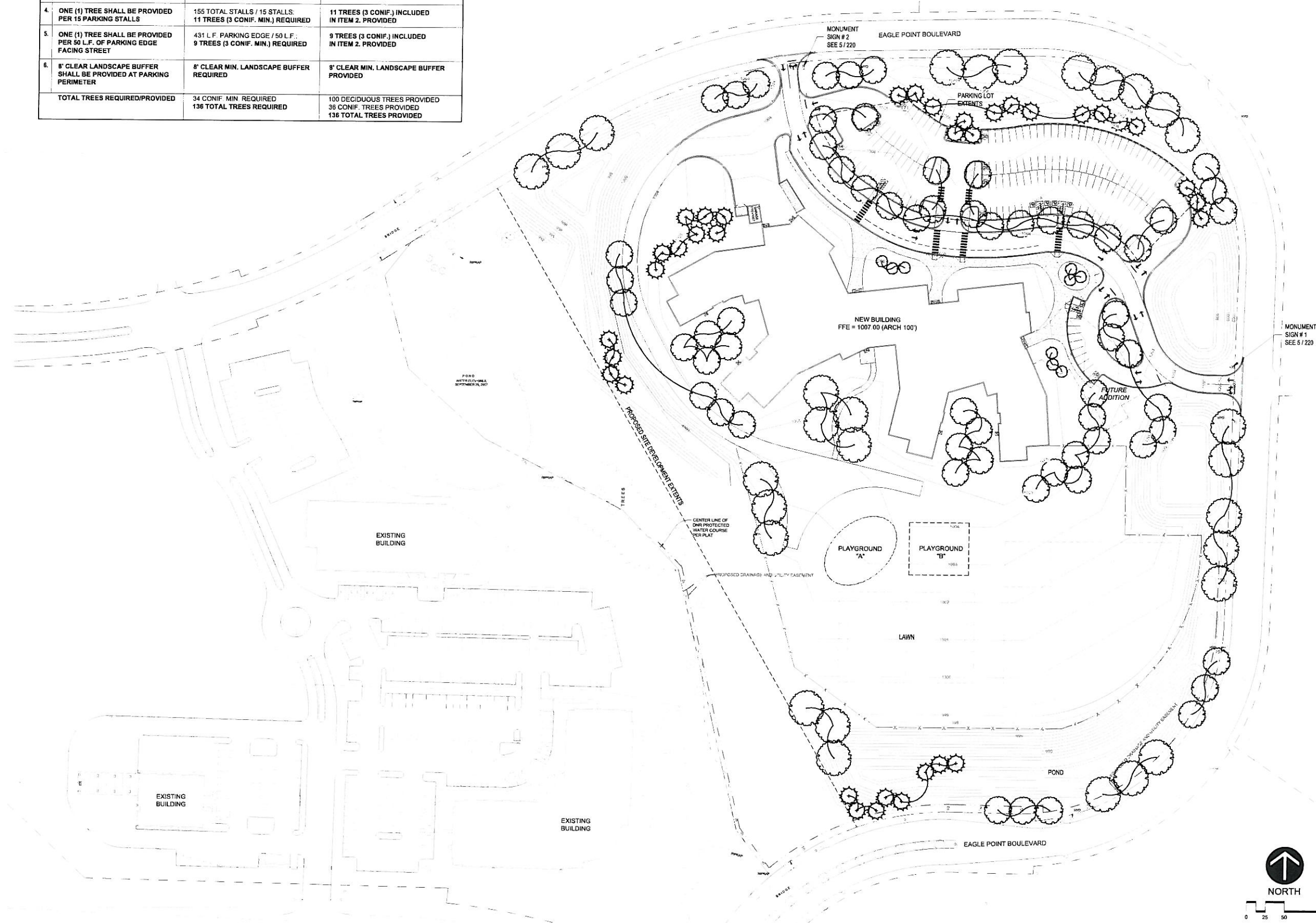
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Drawn: JS

Sheet Title: LANDSCAPE PLAN

Sheet No:

200.LP



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I hereby certify that this site plan, specification or report was prepared by a duly licensed professional and that I am a duly licensed architect or engineer in the state of Minnesota.

Issue: Sympy/kjw/2, P.1 A

Date: 07/22/2015 Reg. No. 26870

Issued For:

Item	Date
SD Pricing Package	06/01/2015
PLAT APPLICATION REVIEW	07/22/2015

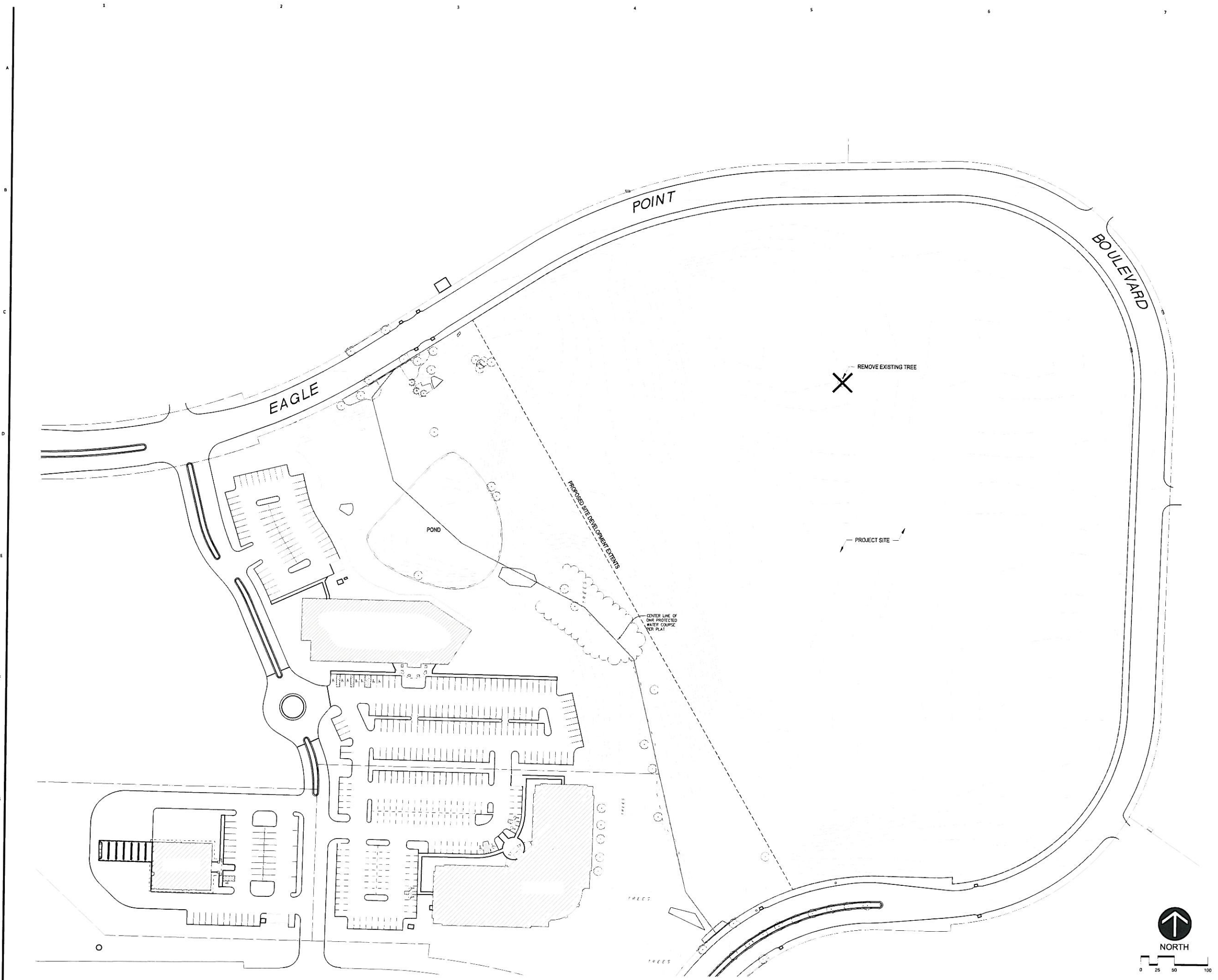
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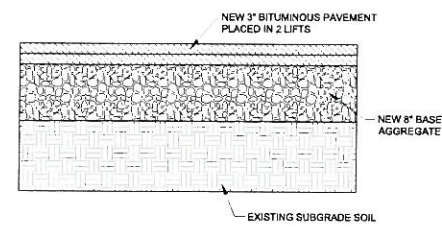
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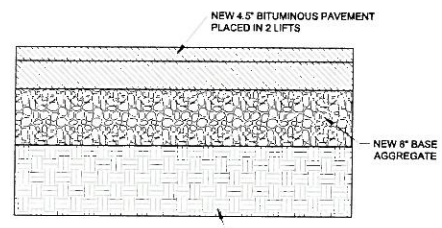
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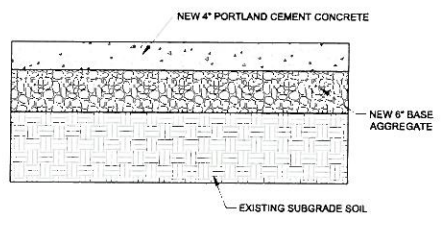




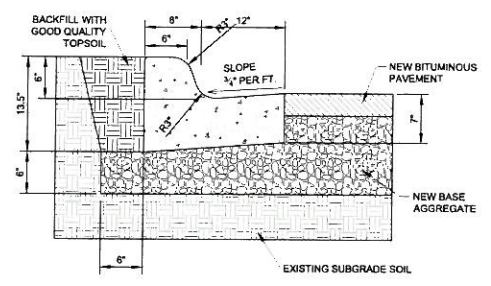
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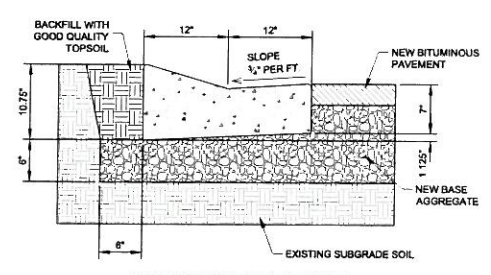
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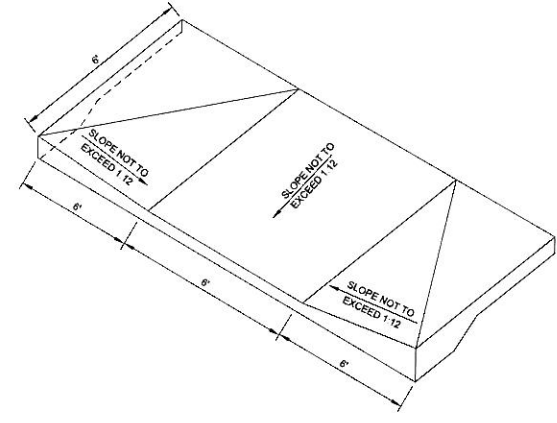
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CONCRETE CONSTRUCTION DETAIL
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4
210
B612 CONCRETE CURB & GUTTER DETAIL
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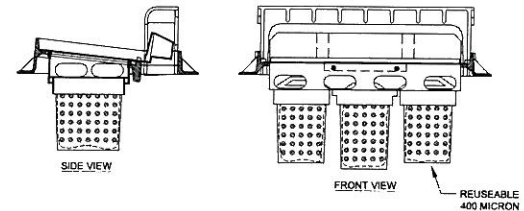


5
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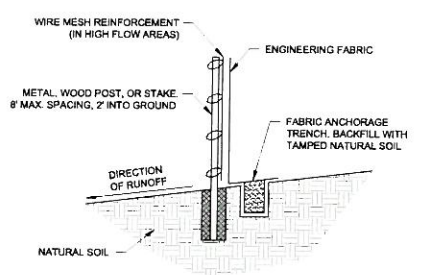


6
210
ACCESSIBLE RAMP DETAIL
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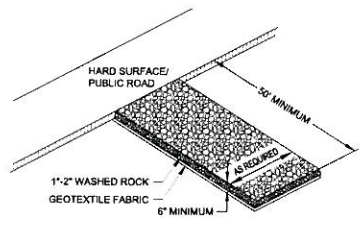
FILTER AREA	6.9 FT ²
OVERFLOW AREA	0.6 FT ²
MAXIMUM OVERFLOW RATE (@ 7\"/>	
MAXIMUM OVERFLOW RATE (@ 13\"/>	
BASKET WEIGHT (EMPTY)	1.1 LB
BASKET WEIGHT (FULL APPROX.)	70 LB



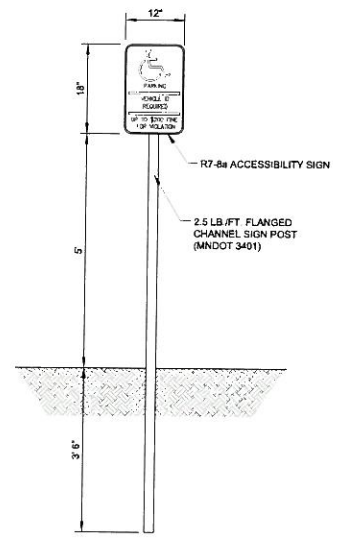
7
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INFRA SAFE INLET PROTECTION DEVICE (OR EQUAL)
NOT TO SCALE



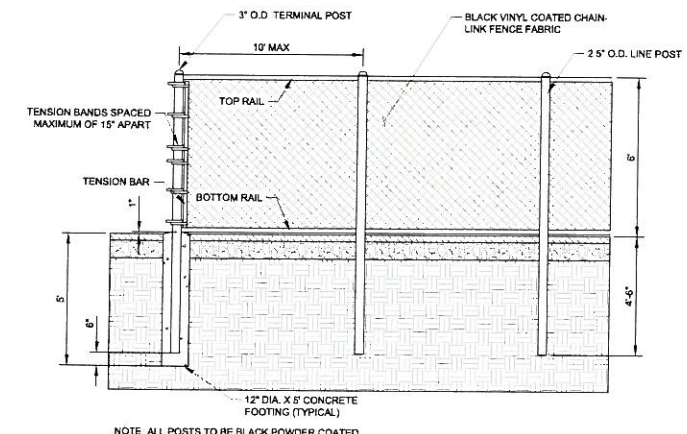
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SILT FENCE INSTALLATION DETAIL
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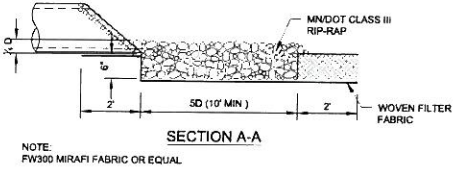
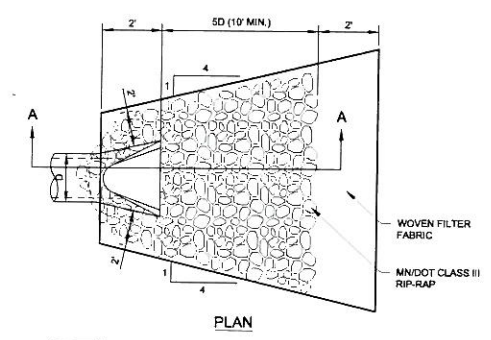
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ROCK CONSTRUCTION ENTRANCE
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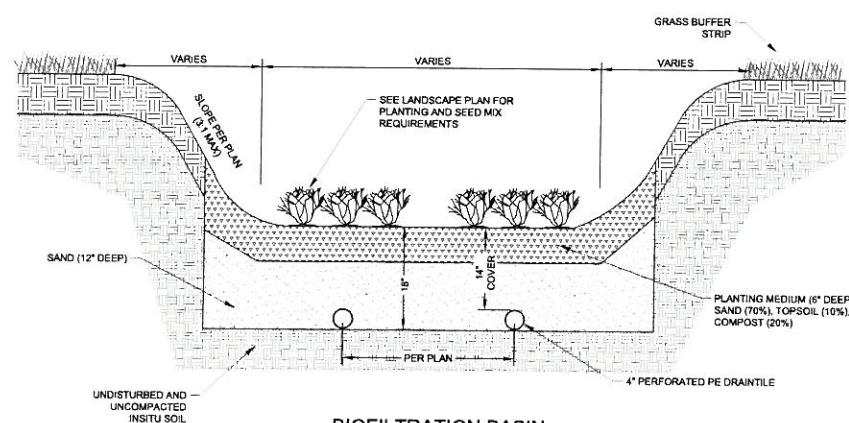
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210
ACCESSIBLE PARKING SIGN DETAIL
NOT TO SCALE



13
210
FENCE DETAIL
NOT TO SCALE



11
210
RIP-RAP AT OUTLETS
NOT TO SCALE



12
210
BIOFILTRATION BASIN SECTION DETAIL
NOT TO SCALE

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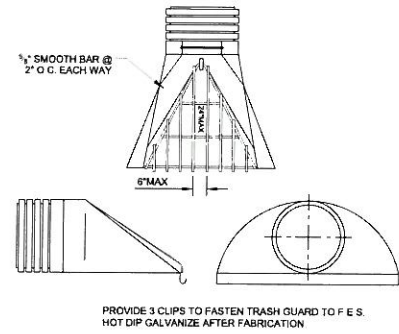
Eric G. Meyer, P.E.
Date: 07/22/2015 Reg. No. 44592

Item	Date
SD Pricing Package	06/01/2015
PLAT APPLICATION REVIEW	07/22/2015

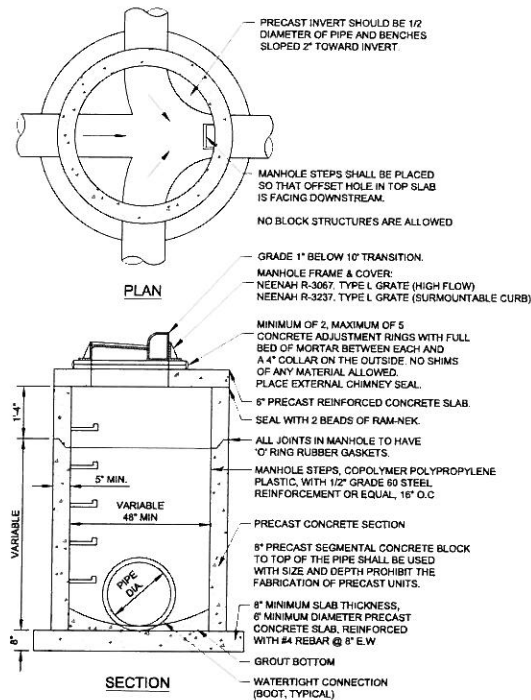
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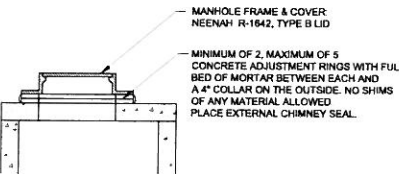
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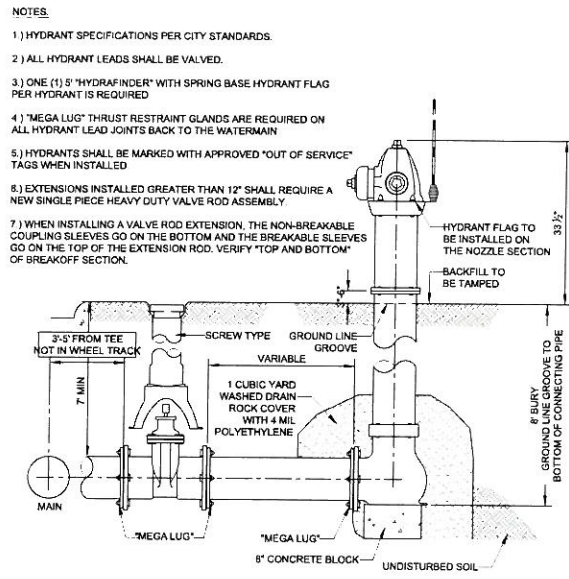
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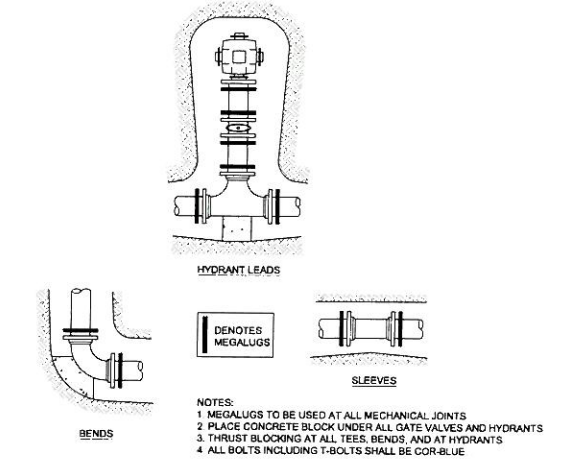
CATCH BASIN MANHOLE DETAIL
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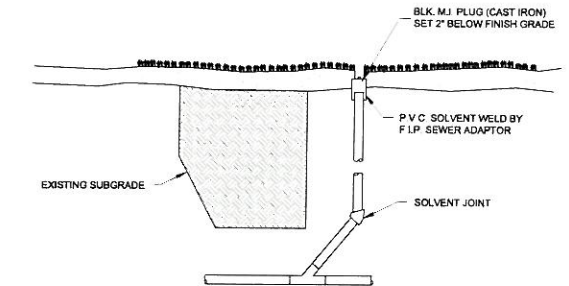
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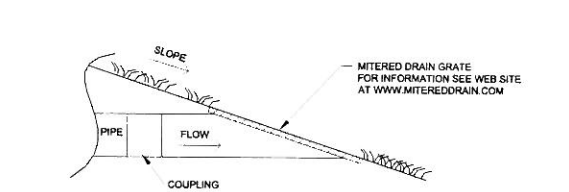
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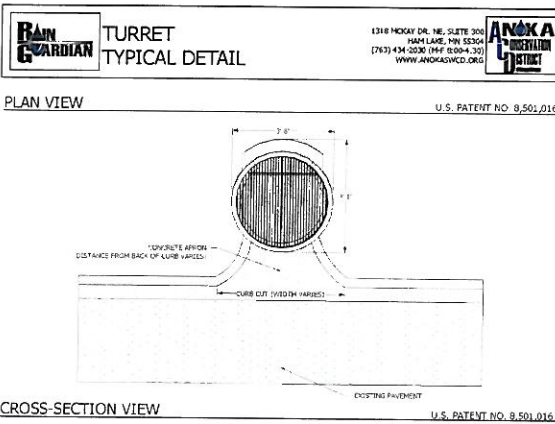
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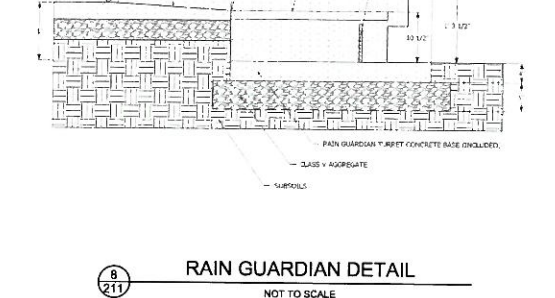
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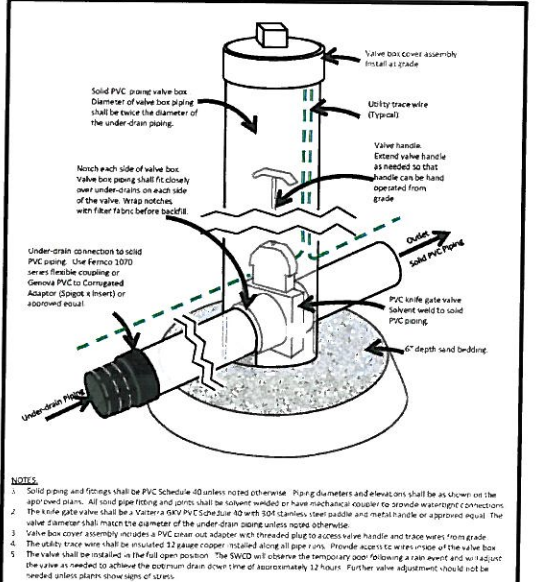
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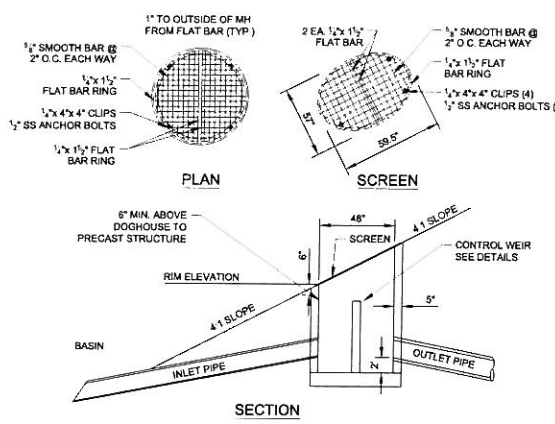
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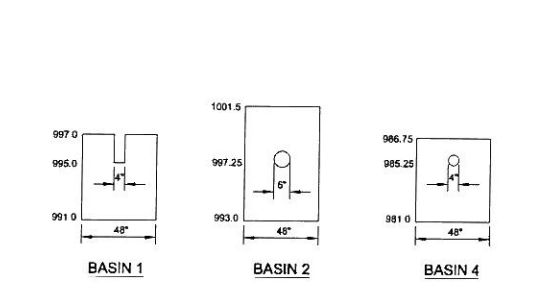
BASIN OUTLET CONTROL WEIR DETAILS
NOT TO SCALE



UNDERDRAIN VALVE & VALVE BOX DETAIL
NOT TO SCALE



BASIN OUTLET STRUCTURE DETAIL
NOT TO SCALE



BASIN OUTLET CONTROL WEIR DETAILS
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GENERAL NOTES

- CONTRACTOR SHALL INSPECT THE SITE AND BECOME FAMILIAR WITH EXISTING CONDITIONS RELATING TO THE NATURE AND SCOPE OF WORK.
- CONTRACTOR SHALL VERIFY PLAN LAYOUT AND BRING TO THE ATTENTION OF THE LANDSCAPE ARCHITECT DISCREPANCIES WHICH MAY COMPROMISE THE DESIGN OR INTENT OF THE LAYOUT.
- CONTRACTOR SHALL ASSURE COMPLIANCE WITH APPLICABLE CODES AND REGULATIONS GOVERNING THE WORK AND MATERIALS SUPPLIED.
- CONTRACTOR SHALL PROTECT EXISTING ROADS, CURBS/GUTTERS, TRAILS, TREES, LAWNS AND SITE ELEMENTS DURING CONSTRUCTION OPERATIONS. DAMAGE TO SAME SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR SHALL VERIFY ALIGNMENT AND LOCATION OF UNDERGROUND AND ABOVE GRADE UTILITIES AND PROVIDE THE NECESSARY PROTECTION FOR SAME BEFORE CONSTRUCTION BEGINS (MINIMUM 10' CLEARANCE).
- CONTRACTOR SHALL COORDINATE THE PHASES OF CONSTRUCTION AND PLANTING INSTALLATION WITH OTHER CONTRACTORS WORKING ON SITE.
- UNDERGROUND UTILITIES SHALL BE INSTALLED SO THAT TRENCHES DO NOT CUT THROUGH ROOT SYSTEMS OF EXISTING TREES TO REMAIN.
- EXISTING CONTOURS, TRAILS, VEGETATION, CURB/GUTTER AND OTHER ELEMENTS ARE BASED UPON INFORMATION SUPPLIED TO THE LANDSCAPE ARCHITECT BY OTHERS. CONTRACTOR SHALL VERIFY DISCREPANCIES PRIOR TO CONSTRUCTION AND NOTIFY LANDSCAPE ARCHITECT OF SAME.
- HORIZONTAL AND VERTICAL ALIGNMENT OF PROPOSED WALKS, TRAILS OR ROADWAYS ARE SUBJECT TO FIELD ADJUSTMENT REQUIRED TO CONFORM TO LOCALIZED TOPOGRAPHIC CONDITIONS AND TO MINIMIZE TREE REMOVAL AND GRADING. CHANGES IN ALIGNMENT AND GRADES MUST BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO IMPLEMENTATION.
- CONTRACTOR SHALL REVIEW THE SITE FOR DEFICIENCIES IN SITE CONDITIONS WHICH MIGHT NEGATIVELY AFFECT PLANT ESTABLISHMENT, SURVIVAL OR WARRANTY. UNDESIRABLE SITE CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO BEGINNING OF WORK.
- CONTRACTOR IS RESPONSIBLE FOR ONGOING MAINTENANCE OF NEWLY INSTALLED MATERIALS UNTIL TIME OF SUBSTANTIAL COMPLETION. REPAIR OF ACTS OF VANDALISM OR DAMAGE WHICH MAY OCCUR PRIOR TO SUBSTANTIAL COMPLETION SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR.
- EXISTING TREES OR SIGNIFICANT SHRUB MASSINGS FOUND ON SITE SHALL BE PROTECTED AND SAVED UNLESS NOTED TO BE REMOVED OR ARE LOCATED IN AN AREA TO BE GRADED. QUESTIONS REGARDING EXISTING PLANT MATERIAL SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO REMOVAL.
- EXISTING TREES TO REMAIN, UPON DIRECTION OF LANDSCAPE ARCHITECT, SHALL BE FERTILIZED AND PRUNED TO REMOVE DEAD WOOD, DAMAGED AND RUBBING BRANCHES.
- CONTRACTOR SHALL PREPARE AND SUBMIT A WRITTEN REQUEST FOR THE SUBSTANTIAL COMPLETION INSPECTION OF LANDSCAPE AND SITE IMPROVEMENTS PRIOR TO SUBMITTING FINAL PAY REQUEST.
- CONTRACTOR SHALL PREPARE AND SUBMIT REPRODUCIBLE AS-BUILT DRAWING(S) OF LANDSCAPE INSTALLATION, IRRIGATION AND SITE IMPROVEMENTS UPON COMPLETION OF CONSTRUCTION INSTALLATION AND PRIOR TO SUBSTANTIAL COMPLETION.
- SYMBOLS ON PLAN DRAWING TAKE PRECEDENCE OVER SCHEDULES IF DISCREPANCIES IN QUANTITIES EXIST. SPECIFICATIONS AND DETAILS TAKE PRECEDENCE OVER NOTES.
- ALL PERVIOUS AREAS NOT PLANTED WITH SEED OR TURF SHALL BE COVERED WITH 4" DEEP DOUBLE SHREDDED HARDWOOD MULCH.
- CONTRACTOR SHALL PROVIDE A DESIGN-BUILD IRRIGATION SYSTEM, UTILIZING DRIP IRRIGATION METHODS WHEN POSSIBLE.
- CONTRACTOR SHALL PROVIDE A TWO (2) YEAR GUARANTEE ON ALL LANDSCAPE MATERIALS, REPLACING ANY MATERIAL THAT DIES OR BECOMES DISEASED BEFORE THE END OF THE TWO (2) YEAR PERIOD AFTER INSTALLATION.

GRADING NOTES

- GRADING LIMITS ARE DEFINED AS THE JUNCTURE OF PROPOSED GRADE WITH EXISTING GRADE UNLESS NOTED OTHERWISE.
- GRADING LIMITS AND LIMITS OF WORK SHOWN ON PLAN ARE ONLY APPROXIMATE AND MAY BE ADJUSTED IN FIELD BY LANDSCAPE ARCHITECT. WORK OUTSIDE OF THESE LIMITS WILL BE DONE AT LANDSCAPE CONTRACTORS EXPENSE UNLESS DIRECTED BY LANDSCAPE ARCHITECT OR OWNER IN WRITING.
- FILL/CUT AS NECESSARY TO PROVIDE A 1% MINIMUM GRADE AWAY FROM BUILDINGS WITHIN LIMITS OF CONSTRUCTION.
- MAINTAIN A UNIFORM GRADE BETWEEN CONTOURS IN AREAS TO BE GRADED UNLESS NOTED OTHERWISE.
- ELEVATIONS, IF SHOWN ARE FINISHED ELEVATIONS. SPOT ELEVATIONS TAKE PRECEDENCE OVER CONTOURS.
- CONTRACTOR SHALL CONTACT PUBLIC UTILITIES FOR LOCATION OF UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. LANDSCAPE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE IF DAMAGED DURING CONSTRUCTION AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR SHALL PROVIDE PROPER EROSION CONTROL MEASURES AS REQUIRED TO INSURE THAT EROSION IS KEPT TO AN ABSOLUTE MINIMUM - SEE CIVIL SPECIFICATIONS.
- PROVIDE TEMPORARY COVERING FOR CATCH BASINS AND MAN HOLES UNTIL FINISHED GRADING IS COMPLETE - SEE CIVIL SPECIFICATIONS.

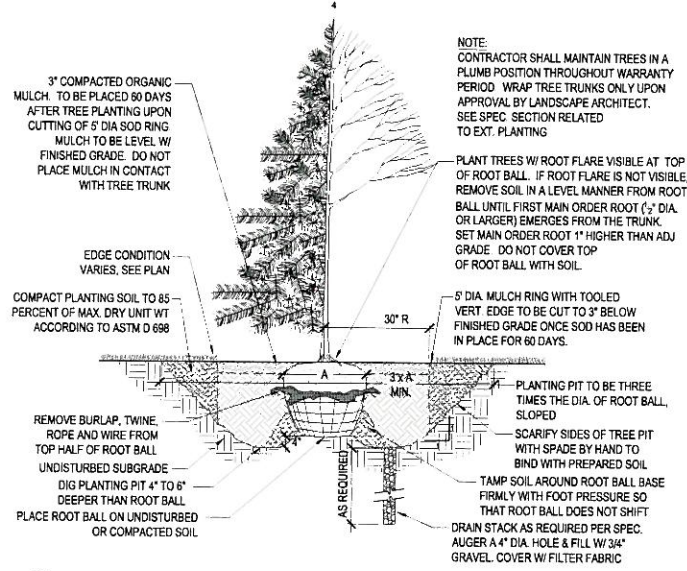
PLANTING NOTES

- NO PLANTS WILL BE INSTALLED UNTIL FINAL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
- PROPOSED PLANT MATERIAL SHALL COMPLY WITH THE CURRENT EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, ANSI Z60.1
- STREET AND BOULEVARD TREES SHALL BEGIN BRANCHING NO LOWER THAN 6' ABOVE PAVED SURFACE.
- PROPOSED PLANT MATERIAL SHALL BE LOCATED AND STAKED AS SHOWN ON PLAN. LANDSCAPE ARCHITECT MUST APPROVE STAKING OF PLANT MATERIAL PRIOR TO DIGGING.
- NO PLANT MATERIAL SUBSTITUTIONS WILL BE ACCEPTED UNLESS APPROVAL IS GRANTED BY THE LANDSCAPE ARCHITECT TO THE CONTRACTOR PRIOR TO THE SUBMISSION OF BID.
- ADJUSTMENTS IN LOCATION OF PROPOSED PLANT MATERIALS MAY BE NEEDED IN FIELD. LANDSCAPE ARCHITECT MUST BE NOTIFIED PRIOR TO ADJUSTMENT OF PLANTS.
- PLANT MATERIALS TO BE INSTALLED PER PLANTING DETAILS.
- TREE WRAPPING MATERIAL SHALL BE TWO-WALLED PLASTIC SHEETING APPLIED FROM TRUNK FLARE TO FIRST BRANCH. WRAP SMOOTH-BARKED DECIDUOUS TREES PLANTED IN THE FALL PRIOR TO DECEMBER 1 AND REMOVE WRAPPING AFTER MAY 1.

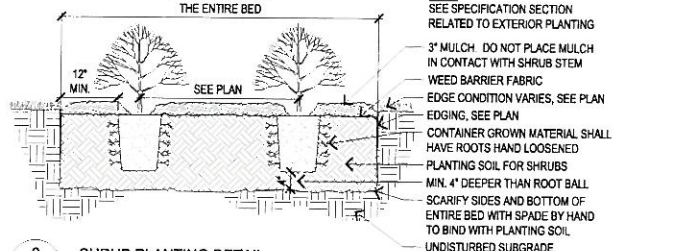
TURF NOTES

- SOD AREAS DISTURBED DUE TO GRADING UNLESS NOTED OTHERWISE.
- WHERE SOD ABUTS PAVED SURFACES, FINISHED GRADE OF SOD/SEED SHALL BE HELD 1" BELOW SURFACE ELEVATION OF TRAIL, SLAB, CURB, ETC.
- SOD SHALL BE LAID PARALLEL TO THE CONTOURS AND SHALL HAVE STAGGERED JOINTS. ON SLOPES STEEPER THAN 3:1 OR IN DRAINAGE SWALES, SOD SHALL BE STAKED SECURELY.

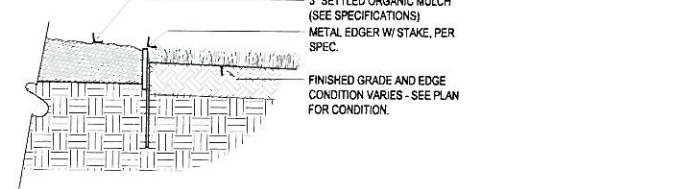
PLANTING NOTES



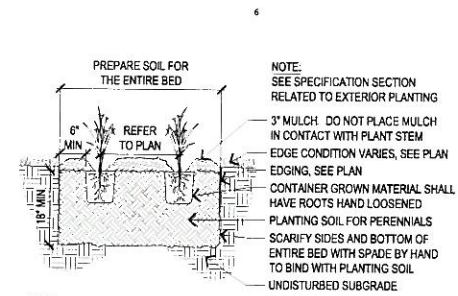
1 TREE PLANTING DETAIL
SCALE: 1/2" = 1'-0"
PLANTING DETAIL - TREE.DWG



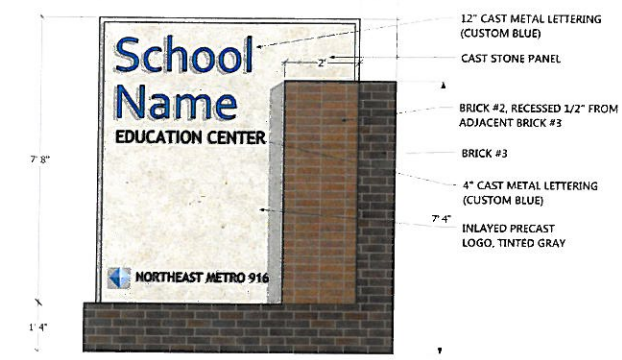
2 SHRUB PLANTING DETAIL
SCALE: 3/4" = 1'-0"
PLANTING DETAIL - SHRUB.DWG



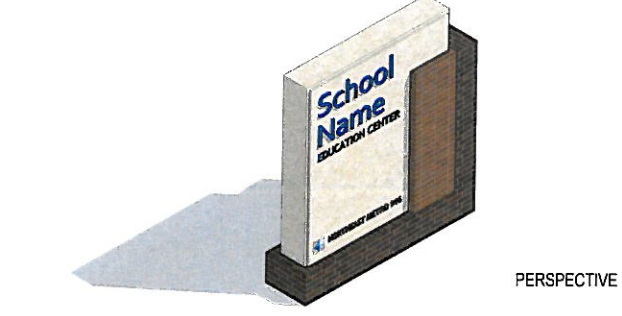
3 STEEL EDGER
SCALE: 1/2" = 1'-0"
DETAIL - STEEL EDGER.DWG



4 PERENNIAL PLANTING DETAIL
SCALE: 3/4" = 1'-0"
PLANTING DETAIL - PERENNIAL.DWG



5 MONUMENT SIGN
NOT TO SCALE



5 MONUMENT SIGN
NOT TO SCALE

QTY	BOTANICAL NAME	COMMON NAME	ROOT CONT.	SIZE	SPECIFICATIONS	USDA ZONE	STATUS - MN	WATER NEEDS	BEE FOOD	BMP BASIN
1	DECIDUOUS SHADE TREES						ADAPTABLE LANDSCAPE			
18	ACER X FREEMANII 'ARMSTRONG'	MAPLE, ARMSTRONG	B&B	2.5' CAL.	14'-16' HT., TRUNK FREE OF BRANCHES 6'-7'					
11	ACER X FREEMANII 'JEFFERSRED'	MAPLE, AUTUMN BLAZE	B&B	2.5' CAL. OR CLUMP	14'-16' HT., TRUNK FREE OF BRANCHES 6'-7'					
15	ACER X FREEMANII 'SIENNA'	MAPLE, SIENNA GLEN	B&B	2.5' CAL.	12'-14' HT., TRUNK FREE OF BRANCHES 6'-7'					
11	ACER MIYABEI 'MORTON'	MAPLE, STATE STREET	B&B	2.5' CAL.	12'-14' HT., TRUNK FREE OF BRANCHES 6'-7'					
7	ACER PLATANOIDES 'PRINCETON GOLD'	MAPLE, PRINCETON GOLD	B&B	2.5' CAL.	12'-14' HT., TRUNK FREE OF BRANCHES 6'-7'					
16	ACER RUBRUM 'AUTUMN RADIANCE'	MAPLE, AUTUMN RADIANCE	B&B	2.5' CAL.	12'-14' HT., TRUNK FREE OF BRANCHES 6'-7'		NATIVE			
13	ACER RUBRUM 'NORTHWOOD'	MAPLE, NORTHWOOD	B&B	2.5' CAL.	12'-14' HT., TRUNK FREE OF BRANCHES 6'-7'		NATIVE			
3	AMELANCHIER X GRANDIFLORA 'AUTUMN BRILLIANCE'	SERVICEBERRY, AUTUMN BRILLIANCE	B&B	12' HGT.	MULTI-STEM, 12'-14' HT., SEVEN OR MORE BRANCHES					
6	CRATAEGUS CRUS-GALLI VAR. INERMIS	HAWTHORN, THORNLESS	B&B	2.0' CAL. OR CLUMP	MATCHING FORM				YES	
18	PICEA ABIES 'CUPRESSINA'	SPRUCE, NORWAY 'CUPRESSINA'	B&B	7' HGT.	NATURAL FORM, UNSHEARED					
18	PICEA GLAUCA DENSATA	SPRUCE, BLACK HILLS	T&D	20' HGT.	ROOT PRUNED STOCK - CAN BE B&B OR SPADED					
1	CONIFEROUS SHRUBS									
1	JUNIPERUS CHINENSIS	JUNIPER, MINT JULEP®	CONT.	#5	PLANT 4' O.C.					
1	JUNIPERUS SABINA 'BROADMOOR'	JUNIPER, BROADMOOR	CONT.	#5	PLANT AS SHOWN (SALT TOLERANT G.C.)					
1	JUNIPERUS SCOPULORUM 'BLUE ARROW'	JUNIPER, BLUE ARROW	CONT.	#15						
2	JUNIPERUS SCOPULORUM 'WICHITA BLUE'	JUNIPER, WICHITA BLUE	CONT.	#7	PLANT AS INDICATED ON PLAN					
1	AMELANCHIER ALNIFOLIA 'REGENT'	SERVICEBERRY, REGENT	CONT.	#5	PLANT 5' O.C.	2		LOW		
1	ARONIA MELANOCARPA ELATA	CHOKEBERRY, BLACK	CONT.	#5	PLANT 4' O.C.					
1	ARONIA MELANOCARPA 'MORTON'	CHOKEBERRY, IRIQUOIS BEAUTY	CONT.	#5	PLANT 4' O.C.					
1	BERBERIS THUNBERGII 'CONCORDE'	BARBERRY, CONCORDE	CONT.	#5	PLANT 2' O.C.					
1	CORNUS ALBA 'AUREA'	DOGWOOD, PRAIRIE FIRE	CONT.	#5	PLANT 4' O.C.					
1	ACHILLEA MOONSHINE	YARROW, MOONSHINE	CONT.	#1	PLANT 2.0' O.C. (GROWS 24" X 18"W)					
1	ALLIUM TANGUTICUM 'SUMMER BEAUTY'	ALLIUM, 'SUMMER BEAUTY'	CONT.	#1	PLANT 1.5' O.C. (GROWS 15" X 15"W)					
1	ARUNCUS DIOLICUS	GOATS BEARD	CONT.	#1	PLANT 2.5' O.C. (GROWS 48" X 48"W)				YES	
1	ASCLEPIAS TUBEROSA	BUTTERFLY FLOWER	CONT.	#1	PLANT 2.0' O.C. (GROWS 36" X 18"W)					dry-wet soil w/ temp standing water

PLANTING SCHEDULE

NORTHEAST METRO 916 INTERMEDIATE SCHOOL DISTRICT
LAKE ELMO

B | W | B | R

380 St. Peter Street, Ste. 600
Saint Paul, MN 55102
651.222.3701
bwr.com

DF/
DAMON FARBER
LANDSCAPE ARCHITECTS

401 2nd Avenue North, Suite 410
Minneapolis, MN 55401
p 612.332.7522

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Issue: 07/22/2015
Date: 07/22/2015
Reg. No. 26970

SD Pricing Package 06/01/2015
PLAT APPLICATION REVIEW 07/22/2015

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12156033 JS

LANDSCAPE DETAILS

220

Consultants

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I hereby certify that the plan, specification or report was prepared by me or under my direct supervision and that I am a Licensed Architect under the laws of the State of Minnesota.

Name _____
Date: 07/22/2015 Reg No _____
Served For _____
Item _____ Date _____
PLAT APPLICATION REVIEW 07/22/2015

NOT FOR CONSTRUCTION

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Comm No _____ Drawn _____
3.2014276.00 MNS
Sheet Title
FIRST LEVEL - FLOOR PLAN

Sheet No _____

401.FL

1 FLOOR PLAN - FIRST LEVEL
401.FL 1/8" = 1'-0"



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Saint Paul, MN 55102
651.222.3701
bwbr.com

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Name
Date: 07/22/2015 Reg. No.

Saved For
Item Date
SO PRICING PACKAGE 06/01/2015
PLAT APPLICATION REVIEW 07/22/2015

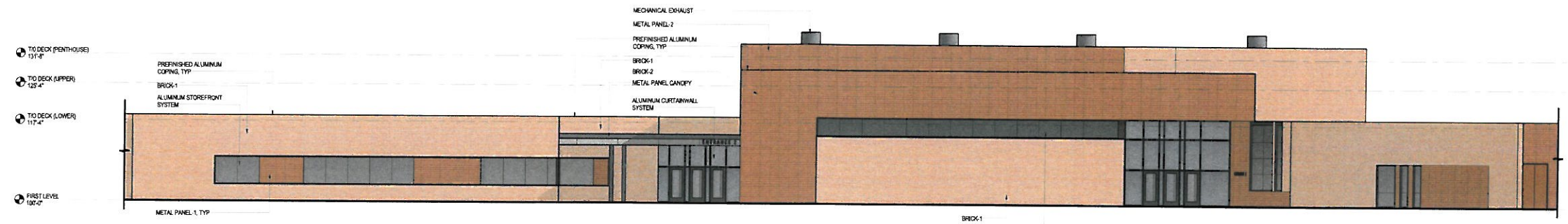
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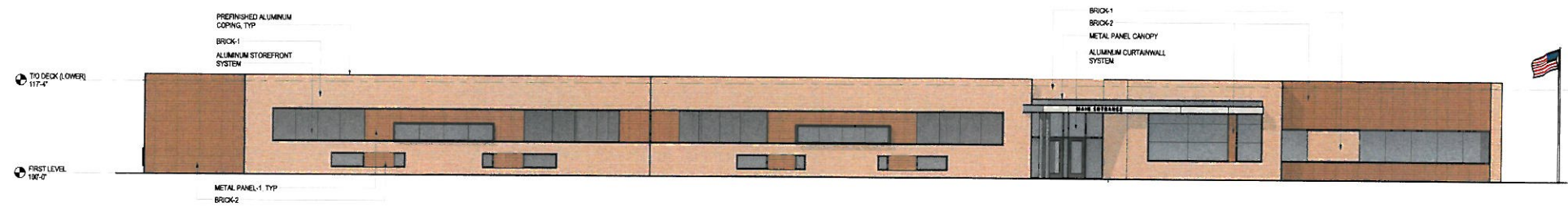
Comm. No. 3.2014276.00
Drawn SB
Sheet Title
EXTERIOR ELEVATIONS

Sheet No.

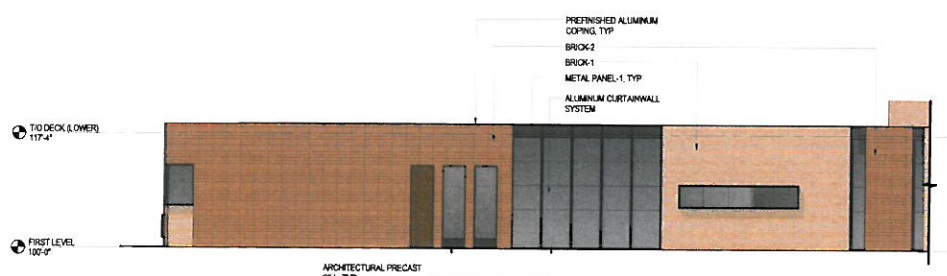
510



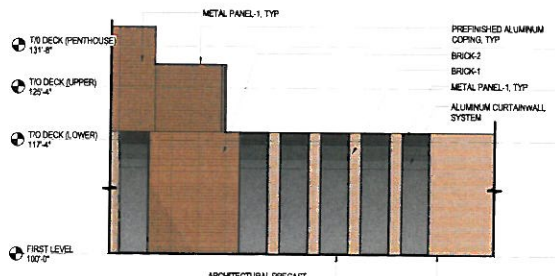
1B ELEVATION - NORTH (PARTIAL) - DESIGN DEVELOPMENT
3/32" = 1'-0"



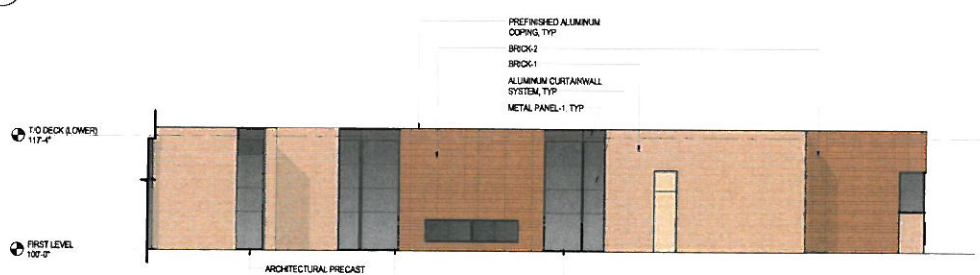
1C ELEVATION - EAST - DESIGN DEVELOPMENT
3/32" = 1'-0"



1E ELEVATION - COURTYARD (WEST, TYP) - DESIGN DEVELOPMENT
3/32" = 1'-0"



4E ELEVATION - COURTYARD (SOUTH, TYP) - DESIGN DEVELOPMENT
3/32" = 1'-0"



1F ELEVATION - COURTYARD (EAST, TYP) - DESIGN DEVELOPMENT
3/32" = 1'-0"



4F ELEVATION - SOUTH (CLASSROOM POD) - DESIGN DEVELOPMENT
3/32" = 1'-0"

NORTHEAST
METRO 916
INTERMEDIATE
SCHOOL
DISTRICT

LAKE ELMO

B | W | B | R

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I hereby certify that the plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the State of Minnesota.

Name	
Date	07/22/2015
Reg. No.	
Issued For	
Item	Date
SD PRICING PACKAGE	06/01/2015
PLAT APPLICATION REVIEW	07/22/2015

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CONSTRUCTION**

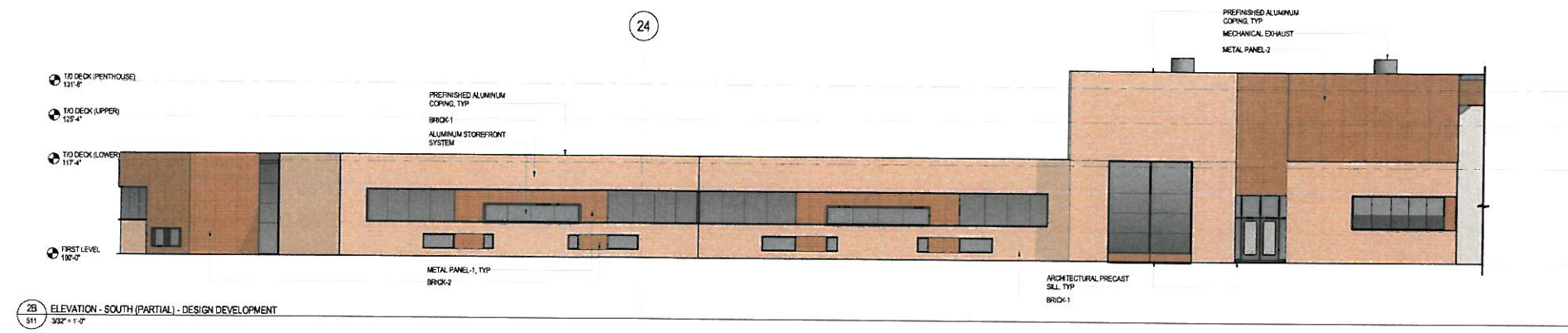
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Comm. No.	Drawn
3.2014276.00	SR

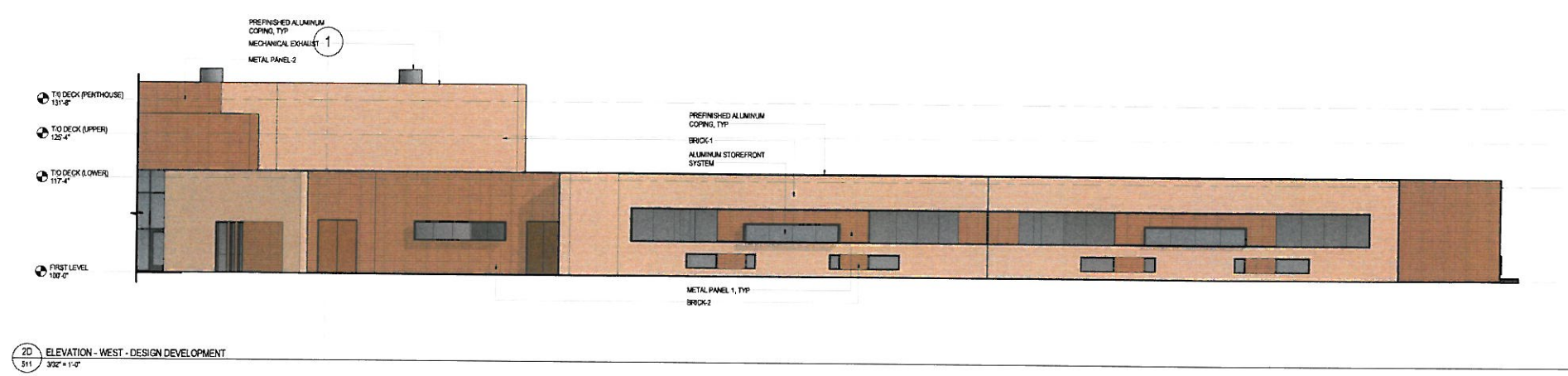
Sheet Title:
EXTERIOR ELEVATIONS

Sheet No.

511



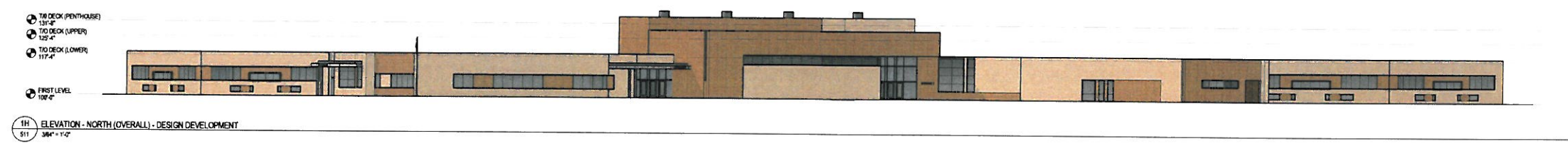
28 ELEVATION - SOUTH (PARTIAL) - DESIGN DEVELOPMENT
S11 302' x 1'-0"



20 ELEVATION - WEST - DESIGN DEVELOPMENT
S11 392' x 1'-0"



19 ELEVATION - SOUTH (OVERALL) - DESIGN DEVELOPMENT
S11 386' x 1'-0"



18 ELEVATION - NORTH (OVERALL) - DESIGN DEVELOPMENT
S11 386' x 1'-0"

7/21/2015 10:42:27 AM

3.2014276.00

NORTHEAST
METRO 916
INTERMEDIATE
SCHOOL
DISTRICT

LAKE ELMO



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651.222.3701
bwbr.com

Consultants

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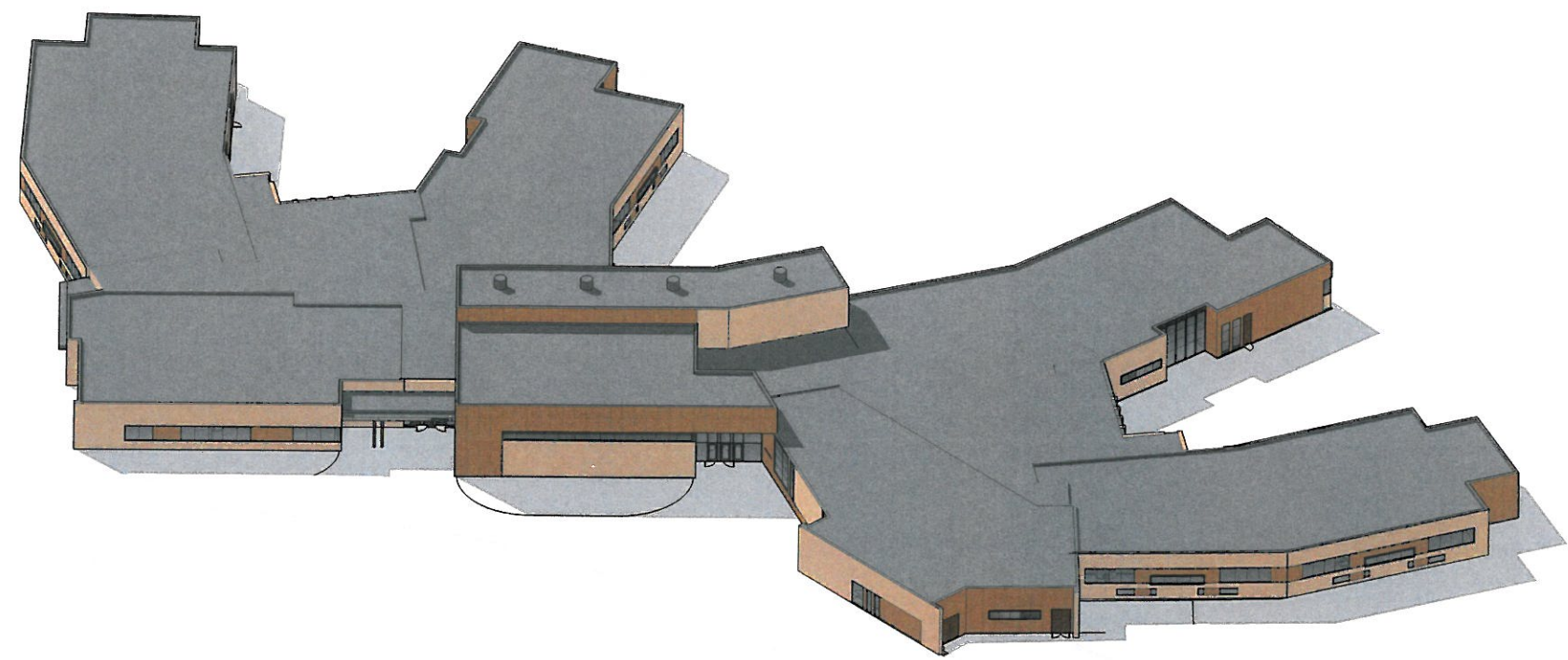
I hereby certify that the seal, location or report was prepared by me or under my direct supervision and that I am a duly Licensed Architect under the laws of the State of Minnesota.

Name	
Date	07/22/2015
Reg. No.	
Issued For	
Item	Date
SD PRICING PACKAGE	06/01/2015
PLAT APPLICATION REVIEW	07/22/2015

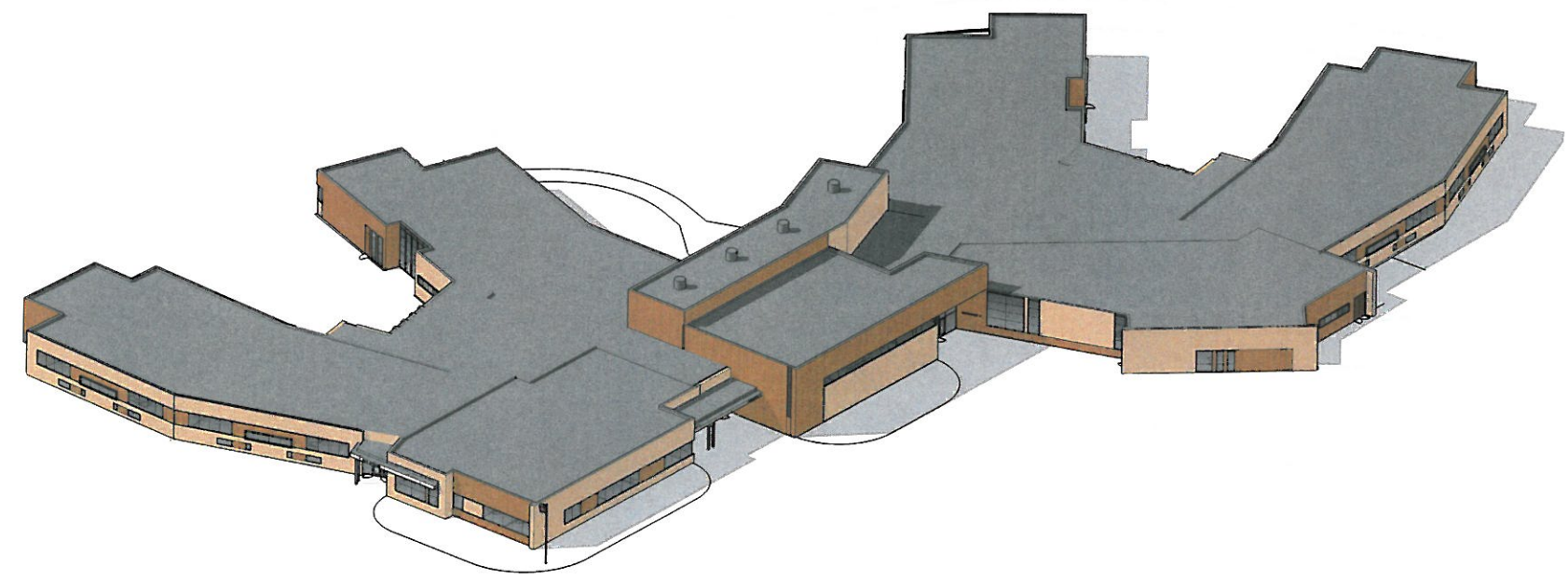
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Comm. No.	Drawn
3.2014276.00	SB
Sheet Title	
ISOMETRIC VIEWS	



20 ISOMETRIC VIEW - NORTHWEST -
DESIGN DEVELOPMENT
512 NOT TO SCALE



21 ISOMETRIC VIEW - NORTHEAST -
DESIGN DEVELOPMENT
512 NOT TO SCALE



Final Plat Application

Northeast Metro 916 Intermediate School District Lake Elmo

July 22, 2015

Application Statements

A. Owner of Record

Kristine Carr
NE Metro ISD 916
2540 County Road F East
White Bear Lake, MN 55110

Authorized Regent

Steve Erickson
BWBR
380 Saint Peter Street
St. Paul, MN 55102

Engineer

Eric Meyer
Larson Engineering, Inc.
3524 Labore Road
White Bear Lake, MN 55110

Surveyor

Sunde Land Surveying
9001 East Bloomington Freeway
Bloomington, MN 55420

B. Current Zoning

Approved Conditional Use (Public and Private Schools) in Eagle Point Business Park PUD

Parcel Size: 14.49 acres or 848,981 s.f.

Property ID No. : TBD

Legal Description: Lot 1, Block 1, Eagle Point Business Park 8th Addition

C. Final Plat Name: Eagle Point Business Park 8th Addition

Lot and Block Number: Lot 1 Block 1

Lot Size: 19.47 Acres

Lot Width: Irregular

Lot Depth – Irregular

Dedicated Public Open Space – None

Estimated Value of Dedicated Land and Improvements – None

Area of Wetlands and Buffers – No delineations performed

Area of Dedicated Right of Way – Zero

Legal Description of Lots, Easements Created and Vacated – See plat

Final Plat Application

Northeast Metro 916 Intermediate School District Lake Elmo

Page 2 of 2

Preliminary and Final Plat are Combined

Project Density – N/A

Construction of all infrastructure improvements will commence in spring of 2016 and be complete approximately 14 months later.

Bremer Bank expressed concern for traffic conflicts with school buses. Kathy Tucci from Bremer Bank acknowledged in a letter, dated 6/18/15, that she is satisfied that her concerns have been addressed.

There have been no concerns expressed regarding conflicts with nearby land use.

This development will not put excessive burden on roads, traffic, public utilities, etc., as it is a school occupied Monday – Friday, 7:00 – 4:00 only.

There is no lakeshore associated with this development.

According to the PUD Amendment, the property owner will pay a fee in lieu of Park Land Dedication.

EAGLE POINT BUSINESS PARK 8TH ADDITION

KNOW ALL BY THESE PRESENTS, That UNITED LAND, LLC, a Minnesota limited liability company, fee owner and Northeastern Metropolitan Intermediate School District No. 916 State of Minnesota, a Minnesota special intermediate school district, contract purchaser, of the following described property, situated in the County of Washington, State of Minnesota to wit:

Outlot A, EAGLE POINT BUSINESS PARK 7TH ADDITION, according to the plat thereof filed of record in the office of the Registrar of Titles in and for Washington County, Minnesota

As evidenced by Certificate of Title No. _____

Have caused the same to be surveyed and platted as EAGLE POINT BUSINESS PARK 8TH ADDITION, and do hereby dedicate to the public for public use the drainage and utility easements as created by this plat

In witness whereof said UNITED LAND, LLC, a Minnesota limited liability company, has caused these presents to be signed by its proper officers this ____ day of _____ 20____

UNITED LAND, LLC

BY: _____ its _____ and _____ its _____

STATE OF _____
COUNTY OF _____

This instrument was acknowledged before me this ____ day of _____ 20____ by _____ its _____ and _____ its _____ of said UNITED LAND, LLC, a Minnesota limited liability company, on behalf of said company.

Notary Public, _____ County, _____

My Commission Expires _____

NORTHEASTERN METROPOLITAN INTERMEDIATE SCHOOL DISTRICT NO. 916 STATE OF MINNESOTA

BY: _____ its _____ and _____ its _____

STATE OF _____
COUNTY OF _____

This instrument was acknowledged before me this ____ day of _____ 20____

by _____ its _____ and _____ its _____

its _____ of said NORTHEASTERN METROPOLITAN INTERMEDIATE SCHOOL DISTRICT NO. 916 STATE OF MINNESOTA a Minnesota special intermediate school district, on behalf of said district.

Notary Public, _____ County, _____

My Commission Expires _____

I Mark S. Hanson do hereby certify that this plat was prepared by me or under my direct supervision, that I am a duly Licensed Land Surveyor in the State of Minnesota, that this plat is a correct representation of the boundary survey, that all mathematical data and labels are correctly designated on this plat, that all monuments depicted on this plat have been or will be correctly set within one year; that all water boundaries and wet lands, as defined in MS 505.01, Subd. 3, as of the date of this certification are shown and labeled on this plat, and that all public ways are shown and labeled on this plat.

Dated this ____ day of _____ 20____

Mark S. Hanson, Licensed Land Surveyor
Minnesota License No. 15480

STATE OF MINNESOTA
COUNTY OF _____

The foregoing Surveyor's Certificate was acknowledged before me this ____ day of _____ 20____ by Mark S. Hanson, Minnesota License No. 15480

Notary Public, _____ County, Minnesota

My Commission Expires _____

Lake Elmo Planning Commission

Approved by the Planning Commission of the City of Lake Elmo, Minnesota, this ____ day of _____ 20____

Signed _____

Chair, Planning Commission

Signed _____

Secretary, Planning Commission

City Council

This plat was approved by the City Council of Lake Elmo, Minnesota, this ____ day of _____ 20____, and hereby certifies compliance with all requirements as set forth in Minnesota Statutes, Section 505.03, Subd. 2.

SIGNED _____

Mayor

SIGNED _____

Clerk

County Surveyor

Pursuant to Chapter 820, Laws of Minnesota, 1971, and in accordance with Minnesota Statutes, Section 505.021, Subd. 11, this plat has been reviewed and approved this ____ day of _____ 20____

BY: _____
Washington County Surveyor

BY: _____

County Auditor/Treasurer

Pursuant to Minnesota Statutes, Section 505.021, Subd. 9 and Section 272.12, taxes payable in the year 20____, on real estate hereinbefore described, have been paid, and there are no delinquent taxes, and transfer has been entered on this ____ day of _____ 20____

BY: _____
Washington County Auditor/Treasurer

BY: _____
Deputy

County Registrar of Titles

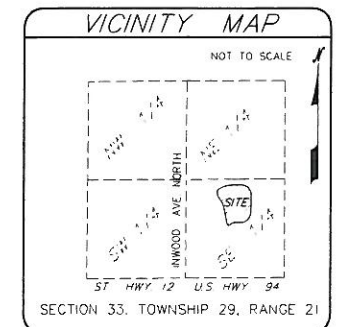
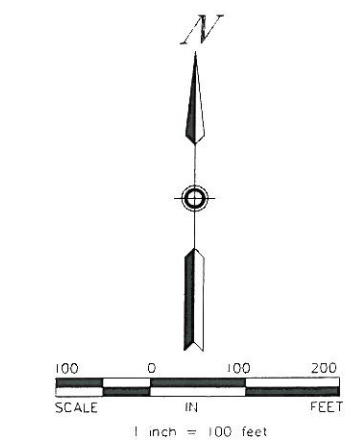
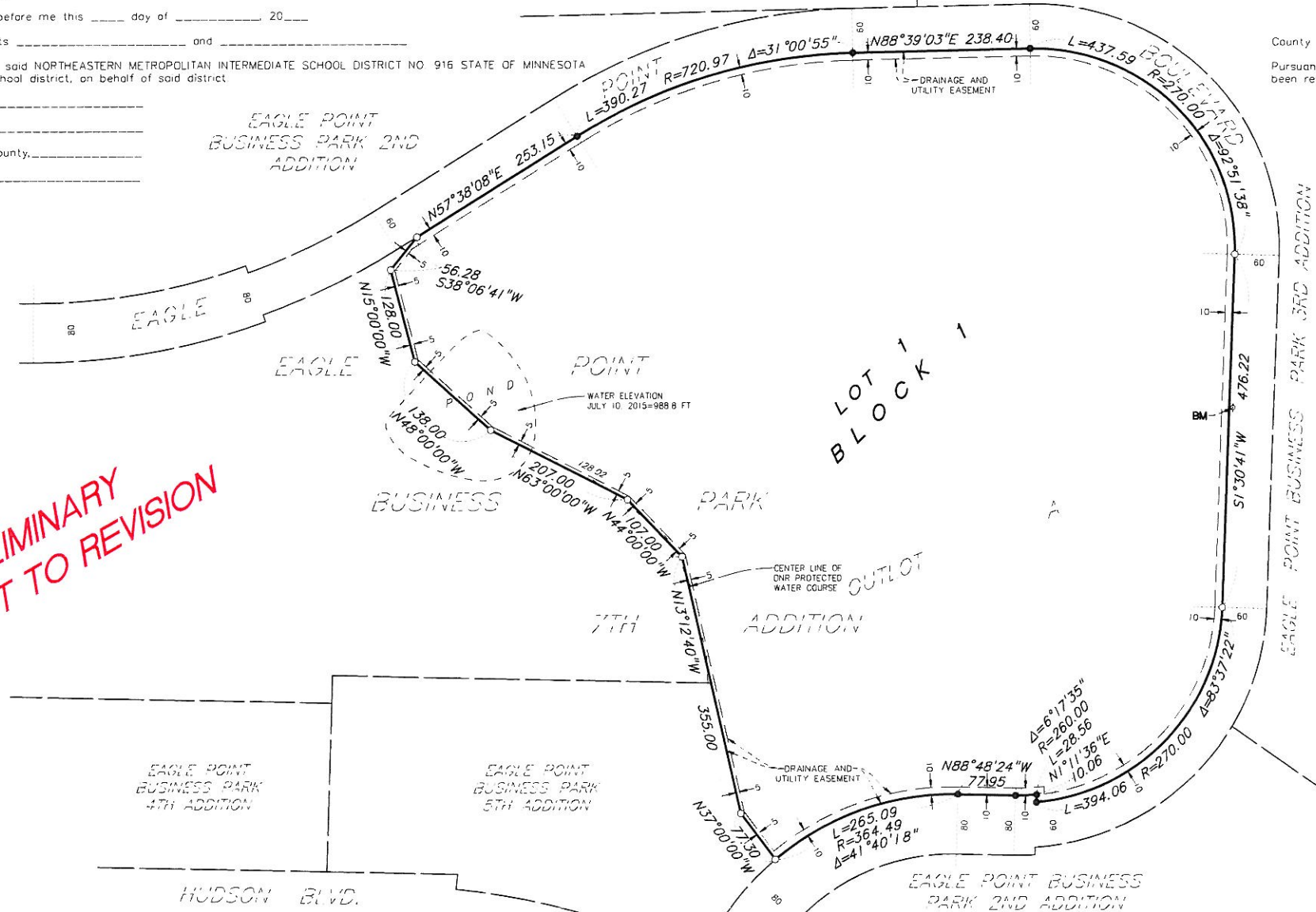
Document Number _____

I hereby certify that this instrument was filed in the Office of the Registrar of Titles for record on this ____ day of _____ 20____ at _____ o'clock ____ M., and was duly recorded in Washington County Records

BY: _____
Washington County Registrar of Titles

BY: _____
Deputy

**PRELIMINARY
SUBJECT TO REVISION**



○ Denotes set, 1/2 inch by 14 inch iron pipe with plastic cap inscribed RLS 15480, unless otherwise noted.

● Denotes found, 1/2 inch by 14 inch iron pipe with plastic cap inscribed RLS 16456, unless otherwise noted.

BENCH MARK:

Top of top nut of fire hydrant third north of Hudson Boulevard West side of Eagle Point Boulevard. Elevation = 1002.72 feet (N.G.V.D. 1929)

Orientation of this bearing system is based on the Washington County Coordinate system (NAD83)

SUNDE
LAND SURVEYING

DESCRIPTION OF PROPERTY SURVEYED

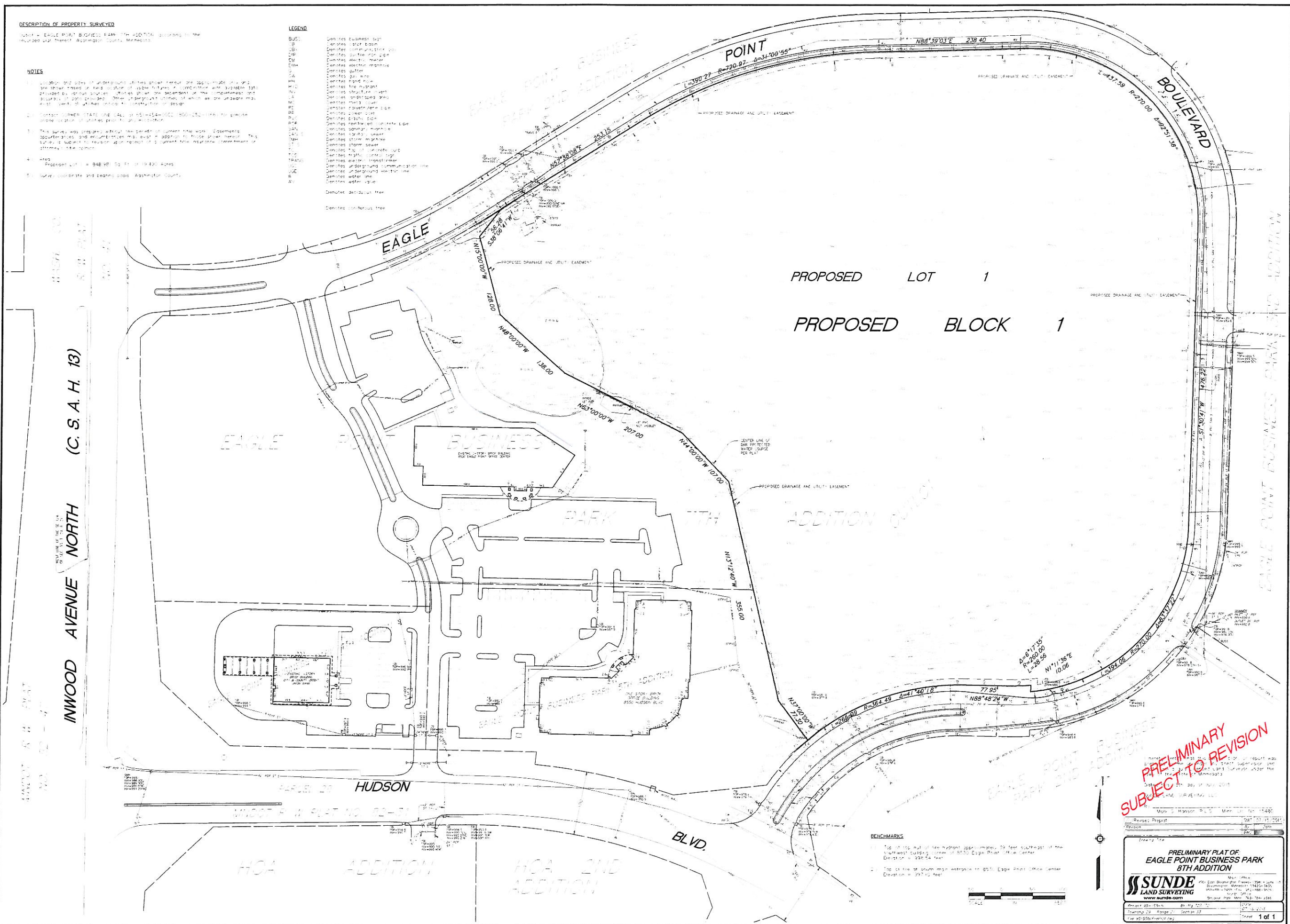
POINT - EAGLE POINT BUSINESS PARK THE HOA TRAIL ADDITION TO THE INCLUDED SURVEYED, WASHINGTON COUNTY, MINNESOTA.

NOTES

1. Location and area of underground utilities shown herein are approximate and are shown based on field notes of visible features in conjunction with available data provided by various sources. Utilities shall be dependent on the completeness and accuracy of data provided. When underground utilities of which we are unaware exist, they shall be at their own risk to the contractor or owner.
2. Contact MINNESOTA STATE LINE (SAL) at 651-434-9500 (651-252-1148) for precise state location of utilities prior to any excavation.
3. This survey was prepared without the benefit of current title work. Easements, appurtenant rights and encumbrances may exist in addition to those shown herein. This survey is subject to revision upon receipt of a current title insurance commitment or statement of title opinion.
4. Area Proposed Lot 1 = 848.98 Sq. Ft. or 0.020 Acres.
5. Survey coordinate and bearing data, Washington County.

LEGEND

- BSJ: Denotes Business sign
- CB: Denotes curb basin
- CI: Denotes construction pit
- CM: Denotes curb manhole
- CM: Denotes electric meter
- CM: Denotes electric manhole
- CM: Denotes gutter
- CM: Denotes gas line
- CM: Denotes hand truck
- CM: Denotes fire hydrant
- CM: Denotes structure vent
- CM: Denotes underground water
- CM: Denotes manhole cover
- CM: Denotes driveway apron edge
- CM: Denotes street curb
- CM: Denotes utility side
- CM: Denotes reinforced concrete edge
- CM: Denotes apron manhole
- CM: Denotes storm sewer
- CM: Denotes storm manhole
- CM: Denotes storm sewer
- CM: Denotes TX, TX manhole curb
- CM: Denotes traffic control sign
- CM: Denotes water tap/stop valve
- CM: Denotes underground communication line
- CM: Denotes underground electric line
- CM: Denotes water tap
- CM: Denotes water tap
- CM: Denotes deciduous tree
- CM: Denotes coniferous tree



**PRELIMINARY
SUBJECT TO REVISION**

BENCHMARKS

1. Top of the end of the hydrant approximately 39 feet southwest of the Northwest corner of 8530 Eagle Point Office Center. Elevation = 138.54 feet.
2. Top of the south manhole to 8530 Eagle Point Office Center. Elevation = 137.10 feet.

PRELIMINARY PLAT OF
EAGLE POINT BUSINESS PARK
8TH ADDITION

SUNDE
LAND SURVEYING

www.sunde.com

2025-05-14 8:43 AM
Drawing 23 Range 21 Section 23
1 of 1

Station #1
3510 Laverne Ave. No.
Lake Elmo, MN 55042
651-770-5006



Station #2
4259 Jamaca Ave. No.
Lake Elmo, MN. 55042
651-779-8882

LAKE ELMO FIRE DEPARTMENT

August 10, 2015

After review of the FINAL PLAT – NORTHEAST METRO ISD 916, the following items need to be addressed:

- FIRE HYDRANTS
 - Spacing/Location – Locations appear to be adequate. We MUST ensure spacing meets our commercial requirement. The hydrant on the East side, identified as “FUTURE ADDITION” needs to be installed.
- FDC (Fire Department Connection) – Location needs to be approved by Fire Chief.
- Lock Box – Contact Fire Chief for ordering and location approval.
- Fire Lanes will need to be identified. Will be marked by painted curbing and signage.
- Location of Annunciator Panel to be approved by Fire Chief.

Thank you.

Reviewed by

Greg Malmquist, Fire Chief



June 18, 2015

Kathy Tucci
Vice President Facilities
Bremer Financial Services, Inc.
8555 Eagle Point Blvd
Lake Elmo MN 55042

Kathy:

Thank you for meeting with BWBR Architects and me earlier this spring and then again this morning to review the site plan for our proposed K-8 school in Lake Elmo.

As you are aware, the conditional use permit for our school has as a condition that we meet and develop an amicable resolution to the traffic and access spacing concerns that were voiced during the public hearing by you as a business owner within the Eagle Point Business Park. This letter is to serve as documentation to the City Administration and Council that we have met with and addressed the concerns to everyone's satisfaction.

Our staff and students look forward to becoming your neighbor in Eagle Point Park. Please don't hesitate to contact me at any point in the future if you have questions or concerns regarding our project.

Sincerely,

Kristine Carr, MBA, CPA (inactive)
Director of Administrative Services

cc Nick Johnson, City Planner

District Office

2540 County Road F East | White Bear Lake, Minnesota 55110

p | 651.415.5500 • f | 651.415.5510

www.nemetro.k12.mn.us

SERVICES AGREEMENT

Services Agreement, entered into this 15th day of April, 2014, by and between the City of Lake Elmo, a municipal corporation of the State of Minnesota (“City”) and Northeast Metro 916 Intermediate School District (“School”).

WHEREAS, School is the fee owner of a parcel legally described as Outlot A Eagle Point Business Park 7th Addition, Lake Elmo, Minnesota, (the “Property”), and intends to construct and operate a public school on the Property serving special needs students pursuant to its organizational charter (the “Activity”);

WHEREAS, School anticipates, based on its operation of similar schools, that the Activity will require public safety services provided by City (“Services”) at a level which exceeds those generally provided to other businesses or institutions in the City;

WHEREAS, City and School have agreed on a fixed rate of reimbursement to be paid to City by School for Services rendered to the Property, and such agreement is evidenced by this Services Agreement;

NOW, THEREFORE, in consideration of the mutual agreements contained herein, School and City hereby agree as follows:

1. **Services.** City agrees to provide the Services to the Property during the time that the Activity is conducted on the Property, without any charge, assessment or fee charged to School or assessment against the Property, other than the Payment described in Paragraph 2 below.

2. **Payment.** School agrees to make an annual payment (“Payment”) to City in consideration of City providing the Services. Each payment shall be payable on December 31, commencing December 31, 2017, and shall be attributable to Services rendered during the school year commencing September 1st (prior to the Payment date) and concluding August 31st (subsequent to the Payment date). The amount of the Payment due December 31, 2017 shall be \$31,500.00; the Payment amount in each subsequent year shall equal 1.02% of the previous year’s payment (a two percent annual increase).

3. **Term.** The term of this Agreement (“Term”) shall commence as of the date hereof, and shall terminate ten (10) years later; provided; however, that School’s obligation to make the Payments shall cease at such time prior to the end of the Term if it ceases to conduct the Activity on the Property. Six months prior to the end of the Term, City and School shall meet to consider a possible extension of the Term and any appropriate modifications to the Payment amount or other terms of this Agreement.

4. **Miscellaneous.** Any modifications to this Agreement must be in writing and signed by City and School. Both parties acknowledge that the extension and performance of this Agreement have been fully authorized and approved by their respective institutions. Any notices hereunder shall be sent in writing, by certified or registered U.S. Mail, return receipt requested, or by a national overnight mail service, to addresses listed below:

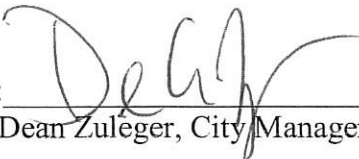
Ms. Kristine Carr
Director of Administrative Services
Northeast Metro 916
2540 County Road F East
White Bear Lake, MN 55110

Director of Finance
City of Lake Elmo
3800 Laverne Ave. N. Lake Elmo, MN 55042

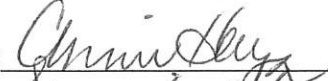
IN WITNESS WHEREOF, the parties hereto have executed this Services Agreement as of the date first above written.

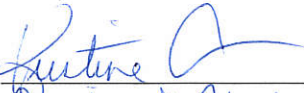
CITY OF LAKE ELMO:

By: 
Mike Pearson, Mayor

By: 
Dean Zuléger, City Manager

NORTHEAST METRO 916
INTERMEDIATE SCHOOL DISTRICT

By: 
Its: Superintendent

By: 
Its: Director of Admin Services

020588/312001/1585010_1