



## STAFF REPORT

DATE: June 20, 2017

**REGULAR**

ITEM #: 17

**MOTION**

**TO:** City Council  
**FROM:** Emily Becker, City Planner  
**AGENDA ITEM:** Lakewood Crossing 2<sup>nd</sup> Addition Preliminary and Final Plat and Planned Unit Development (PUD) Plans  
**REVIEWED BY:** Stephen Wensman, Planning Director

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### BACKGROUND:

CM Properties 94, LP has submitted application to the City for a Preliminary and Final Plat and Planned Unit Development (PUD) Plans for Outlot A of Lakewood Crossing 1<sup>st</sup> Addition. The proposal will subdivide the existing 3.82 acre parcel in to three separate parcels. These parcels will include a full service restaurant with outdoor patio; quick service restaurants with drive-throughs; and other retail activities.

*Applicant and Property Owner:* CM Properties 94, LP c/o MFL Properties Corp., 3460 Washington Dr., Ste 100 Eagan, MN 55122

*Location:* Southwest of Kwik Trip Gas Station (9955 Hudson Blvd N), PID# 3402921440015

*Existing Land Use and Zoning:* Vacant land, Commercial (C)

*Comprehensive Plan:* Commercial

*History:* The property has been under the ownership of CM Properties 94, LP for over 45 years, and it is the intent that this company will continue to own the property for years to come.

Lakewood Crossing Preliminary Plat, Final Plat and Conditional Use Permit for a gasoline station (Kwik Trip) Approval: 7/22/2014

Lakewood Crossing 2<sup>nd</sup> Addition PUD Concept Plan Approval: 3/21/2017

*Deadline for Action:* Application Complete: 5/30/2017

60 Day Deadline: 7/28/2017

Extension Letter Mailed: N/A

120 Day Deadline: N/A

*Applicable Regulations:* Article XVI – Planned Unit Developments

Article XII – Commercial Districts

Chapter 153: Subdivision Regulations

### ISSUE BEFORE COUNCIL:

The Council is respectfully being requested to review the proposed Preliminary and Final Plat and PUD Plans and either approve or deny the request.

### PROPOSAL DETAILS/ANALYSIS:

**PUD and Plat Process.** The applicant has submitted application for both Preliminary and Final Plat and PUD Plans concurrently. The applicant received PUD Concept Plan approval by Council on March 21, 2017. Approval of the PUD Concept Plan alone did not afford the developer/applicant any rights but did provide feedback to the applicant regarding the proposed development.

**Identified PUD Objectives.** The PUD process is appropriate for the proposed development to allow flexibility in the location, design, and mix of commercial uses on a single large site. The City should consider whether one or more of the objectives listed in Section 154.751 are met when reviewing requests for approval of planned unit developments. It is of Staff opinion that the following objectives are met with this request:

A. Innovation in land development techniques that may be more suitable for a given parcel than conventional approaches.

*Note: The parcel is an irregularly-shaped parcel and so meeting all of the lot dimension requirements of the Commercial zoning district would be a hardship.*

F. Coordination of architectural styles and building forms to achieve greater compatibility within the development and surrounding land uses.

*Note: The development will include additional retail and service businesses which will supplement the gas station.*

**PUD Flexibility.** The following outlines PUD flexibility the applicant seeks. In summary, the applicant is requesting flexibility from the following zoning standards:

- Medical facilities, drive-throughs, and outdoor dining as a permitted, rather than conditional, use.
- Minimum lot width
- Impervious surface allowance
- Parking lot setback
- Certain Lake Elmo Design Guidelines and Standards Manual
- Certain landscape requirements
- Waive off-street loading requirements
- Required 20% protected open space within a PUD
- Signage

**Permitted and Conditional Uses.** The proposed development will include a full service restaurant with outdoor patio; quick service restaurants with drive-throughs; chiropractic care, and other retail activities. General retail sales and restaurants are permitted uses within the Commercial zoning district, while medical facilities (chiropractic care), drive-through facilities, and outdoor dining are conditional uses.

- *Conditional Use to Permitted Use.* Because the applicant is proposing a Planned Unit Development, the applicant is requesting that uses that would normally be conditional become permitted uses within this development. This is to prevent new tenants from having to go the Conditional Use Permit process. It should be noted that if plans are significantly amended (i.e. an additional drive through is requested after the PUD is approved), the PUD would also need to be amended. If the Commission wishes to recommend that these uses be allowed as permitted, rather than conditional, uses within this PUD, the Commission should thoroughly review the proposal, requesting more information if needed, in order to properly ensure that standards are being met and that the proposed uses will not be detrimental to the development or surrounding area

**Medical Facilities.** The Code mandates that medical facilities have access to an arterial or collector street of sufficient capacity to accommodate generated traffic and that two access points are granted. Currently, the site plan only indicates one access. Therefore, this standard is not met. However, it should be noted that the proposed medical facility would be a chiropractic office, which is a less intense use than other medical facilities such as urgent care or an emergency room is, and the two access points would likely not be needed.

**Drive-Throughs.** The applicant had previously proposed four drive-throughs in the Concept PUD Plan. The applicant hired Spack Consulting to perform a Drive-Through Analysis to review information of these different drive-through types to determine whether sufficient stacking was provided. Due to feedback from this report and subsequent discussions regarding potential circulation conflicts, the previously-proposed high volume drive-through located on the east end of the Lot 2 building was eliminated, reducing the number of proposed drive-throughs to three.

- *Recommendations from Report.* Summarily, the drive-through report recommends the following to improve drive-through operations. It is a recommended condition of approval that the Applicant adhere to all recommendations made in this report:
  - Locate drive-through windows in buildings to maximum vehicle stacking.
  - Offset median islands and parking lot driving lanes from the drive-throughs between Lots 1 and 2 to avoid driver confusion.
    - *Note: The Applicant has offset the landscape medians to address this comment.*
  - Provide one-way eastbound circulation around the west, south, and east sides of Lots 1 and 2 to avoid unnecessary conflicts with drive-through vehicles at the windows.
    - *Note: The updated Site Plan does not indicate one-way traffic along the south of Lots 1 and 2 nor the east side of Lot 1. It should be noted that these aisles are wide enough by City standards for a 2-way aisle driveway in a parking lot. Lines clearly delineating the two aisles should be provided on the site plan if the City is to allow two-way traffic along these aisles.*
  - Provide appropriate signage and pavement markings for all drive-through lanes to inform drivers of expected operations. A convex mirror may be necessary in select areas to improve sight distance and see other approaching vehicles.
- *Explanation of Low-Volume and High-Volume Drive-Throughs.* Spack included reports that looked at drive-through lane usage of five different land uses in Minnesota: banks, car washes, coffee shops, fast food restaurants, dry cleaners and pharmacies.
  - Proposed Low-Volume Drive Throughs provide enough storage space for approximately four vehicles before they begin to spill out into the drive-through and block drive aisles. Based on the maximum number of cars observed in queue at any one time in Spack’s report, land uses with low-volume drive-throughs include: car wash, pharmacy, dry cleaner, and sit-down takeaway restaurant.
  - Proposed High-Volume Drive Throughs provide enough storage space for approximately twelve stacked vehicles. Based on the maximum number of cars observed in queue at any one time in Spack’s report, land uses with high-volume drive-throughs include: fast food restaurant, coffee shop, donut/bakery shop, and bank.
  - It is a recommended condition of approval that uses utilizing these drive-throughs be limited to those which the drive-through can support.
- *Adherence to Standards.* The Applicant has still not provided enough detail for Staff to analyze if adherence to all standards for restaurants with drive-throughs have been met. It is a recommended condition of approval that the Applicant provide this information.

Standard	Required	Proposed
<b>Sec. 154.304: Standards for Food Services</b>		
<i>Restaurant with Drive-Through</i>	1. Drive-through elements shall not be located between the front façade of the principal building and the street. No service shall be rendered, deliveries made or sales conducted within the required front yard, although tables may be provided for customer use.  2. Site design shall accommodate a	1. The drive-through elements are not outlined. One of the elements appears to be in front yard of Lot 3. 2. This is hard to determine without knowing exact locations of speakers and service windows, however the applicant has provided a report prepared by a traffic consultant which analyzes the site design and has made necessary changes. 3. Canopy detail and other structure detail

	<p>logical and safe vehicle and pedestrian circulation pattern. Adequate queuing lane space shall be provided, without interfering with on-site parking/circulation.</p> <p>3. Drive-through canopies and other structures, where present, shall be constructed from the same materials as the primary building, and with a similar level of architectural quality and detailing.</p> <p>4. Sound from any speakers used on the premises shall not be audible above a level of normal conversation at the boundary of any surrounding residential district or on any residential property.</p> <p>5. Each food or beverage drive-through business shall place refuse receptacles at all exits.</p>	<p>not provided.</p> <p>4. Unable to determine.</p> <p>5. Information not provided.</p>
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**Outdoor Dining.** The standard set forth in Section 154.554 of the Zoning Code for outdoor dining is that tables cannot block a public sidewalk or other walkway needed for pedestrian circulation. Minimum of 5 ft. of sidewalk must remain open. There is only one small sidewalk in front of the building located on Lot 3 which provides access from the parking lot to the building. Because the exact placement of the tables will likely be up to the occupant of the building, which may change from time to time, this standard has been added as a recommended condition of approval.

**Lot Dimensions and Bulk Requirements.** Generally, the proposed development meets lot dimension and bulk requirement standards. Flexibility is being requested on:

- *Lot width minimum.* The parcel that is being developed is a uniquely-shaped parcel, and so the manner in which the parcel is being subdivided is unique.
- *Impervious surface for Lot 2.* The overall impervious surface of the three parcels averages 75%, which meets the Commercial zoning district’s maximum impervious surface requirement. Lot 2 individually, however, exceeds this maximum requirement at 80%.
- *Parking setback on Lot 1.* The parking lot will cover all three lots, so there is a 0 ft. setback between the three newly-created parcels. Also, the parking lot is connected to a through lane on the east side of the property with an 8.7 ft. setback. Setbacks from the south and west of the parcel are met.

**Driveway Standards.** Flexibility is being requested for the following on driveway standards:

- *Distance from driveway to side lot line (5 feet required).* Lot 3 will share a driveway access with Kwik Trip, to the east of the property, and so will not meet this standard with a 0 ft. setback.

**Commercial District Design Standards.** The following details significant design standards set forth by the City of Lake Elmo Design Guidelines and Standards that have not been met in the proposal. The Commission should consider whether or not flexibility should be allowed for the following standards:

- *Orientation of buildings.* The unique shape of the parcel that is being developed resulted in a unique shape of Lot 3. As a result, the building is oriented according to the shape of the parcel and to accommodate better traffic circulation and proximity to the parking lot.

- *Landscaped open or gathering spaces.* Being that this a small commercial development located in close proximity to the highway that will likely serve quick visits, Staff does not feel it necessary to provide this open space. An outdoor dining area is being proposed, and the restaurants will likely provide adequate seating for guests.
- *Sidewalks.* No sidewalk is provided along Hudson Blvd. However, there are no other sidewalks along Hudson Blvd. to which it could connect. There is an on-road bike lane on Hudson Blvd that will accommodate bikers.
- *Streetscape Lighting.* No lighting is provided along Hudson Blvd. Lighting is provided within the interior of the parking lot.
- *Fencing of Outdoor Dining Areas.* The applicant has not indicated on the site plan where the outdoor dining will be located.
- *Site furnishings.* The manual details that furnishings such as decorative fencing, trash receptacles, planters, bicycle racks, and benches are recommended – design elements from Branding & Theming Study encouraged. The Applicant has indicated that the site will include decorative fencing, lighting, trash enclosures, and benches.
- *Parking.* There is minimal exterior parking lot landscaping and screening provided. Also, the parking lot is located in the front of 2 buildings and exceeds 60% of street frontage.

**Landscape Requirements.** The applicant has amended the landscape plans to comply with some of the deviations from City standards that were outlined during the Concept PUD Plan review. However, there are still standards to which the proposed plans do not adhere. The proposed Landscape Plan does not meet the following standards of the Zoning Code. The Commission should consider whether flexibility should be provided via the PUD process or if these standards should be met.

- There are only five trees proposed along Hudson Blvd, and six are required.
- Thornless Hawthorn (deciduous ornamental) are required to be 2” caliper, not 1.5” as proposed.
- There is no screening consisting of masonry wall, fence, berm, or hedge provided along Hudson Blvd that is 3.5-4’ in height and less than 50% opaque as required, but the drive-through lane is screened with a spire hedge.

**Tree Preservation Requirements.** There are no trees currently on the site, and so a tree preservation plan is not required.

**Off-Street Parking.** The applicant meets general parking space size and aisle width standards. The applicant has provided a narrative that explains that adequate parking has been provided.

- *Specific Minimum Off-Street Parking Requirements.* The applicant is unsure exactly what the final tenant mix will be, but has shown that assuming 19,000 square feet of the buildings are retail and 8,700 square feet are restaurants, that a total of 164 parking spaces would be required. The applicant is providing 165 parking stalls, or 5.92 stalls per 1,000 square feet, so the proposed parking is more than adequate.
- *Shared Parking.* The applicant has indicated in a narrative that a Reciprocal Easement and Operating Agreement has been drafted and will be recorded to account for shared parking and access. This is a recommended condition of approval.

**Off-Street Loading Areas.** Section 154.211 of the Zoning Code requires that off-street loading areas be provided in all districts for any nonresidential use which involves the receipt or distribution of materials or merchandise by trucks or similar vehicles and has a gross floor area of 5,000 square feet or more. The proposed site plan does not provide an off-street loading area. The applicant has provided a narrative that indicates that deliveries to Lake Elmo Shoppes will occur behind the buildings and will utilize the service door in the back of each tenant space and that no dock doors or drive-in doors are needed for the types of tenants that occupy Lake Elmo Shoppes. Staff is supportive of waiving the off-street loading requirement.

**Sign Regulations.**

- *Comprehensive Sign Plan.* A Comprehensive Sign Plan is required of any applicant for all planned developments and commercial or industrial multi-tenant developments where different occupancies will compete for permitted square footage on a single lot. The Applicant has submitted a Comprehensive Sign Plan, attached to this report, detailing proposed wall signage and two ground signs. The Applicant states in the narrative that an increased amount of signage is being requested in order to be seen by the vast number of commuters and the local traffic patterns. Their signs will provide consistency in size and construction materials, while allowing each tenant the flexibility and square footage to showcase building storefronts, brand names, and individual site locations as best as possible.
- *Wall signs.* The current standard for wall signs is 1 square footage per 1 lineal foot of storefront. The Applicant has submitted an exhibit which shows what signs of this size would look like on the proposed building, stating that this ratio looks quite small on the façade. The applicant has submitted the request for 2.5 square feet of signage for every one lineal foot of building on the front and rear façades and 1.5 square feet of signage for every one lineal foot of building on the east and west elevations.
- *Ground Signs.* One ground sign per street frontage is allowed per lot, and ground signs on lots that front streets with the number of traffic lanes and speed limit of these lots are allowed to be a maximum height of 12 feet and 80 square feet. The applicant has proposed two ground signs: one 12 feet, 4 inches in height and 80 square feet, and one 30 feet in height and 273 square feet. The proposed signs meet the setback requirement of 15 feet of a crosswalk, or within 15 feet of the intersection of any circulation lane, driveway, or alley. Because the property abuts the Keats Ave N to WB I-94 ramp and Hudson Blvd, the Applicant is looking for visibility from both frontages.

**Open Space.** The City’s PUD ordinance sets forth the requirement that at least 20% of the project area within a PUD be preserved as protected open space. This requirement appears to be more appropriate or applicable to residential development. However, other public and site amenities may be approved as an alternative to this requirement. The proposed PUD does not meet the open space requirement. The applicant has provided a narrative that proposes that this development enhances the site and retail shopping and dining experience with enhanced architectural materials and site furnishings through natural colors, textures, and shapes; flanking that offers a three-dimensional quality and skyline interest; mixture of materials; unique cornices; variety of awnings; ornamental light fixtures; and four-sided architecture.

**Lighting.** The applicant has stated that a photometric plan has been ordered and will be forwarded to the City once received. It is a recommended condition of approval that a lighting plan be submitted meeting Sections 150.035-150.038 of the City Code.

**Engineering Comments.** Attached is a memorandum from the City Engineer dated May 31, 2017, which details a number of comments that will need to be addressed. A condition of approval has been added that requires that the Applicant address all of the comments outlined in this memo. Outlined comments include the following:

- No construction for Lakewood Crossing 2<sup>nd</sup> Addition may begin until the applicant has received Engineer approval for Final Construction Plans; all applicable permit approvals; easements and permissions for the project; and a preconstruction meeting has been held.
- Preliminary plans must be updated and the final construction plans must include a right turn lane along eastbound Hudson Boulevard and any additional fire hydrants as required by the Fire Chief/Building Official.
- Preliminary and final construction plans and plat must be updated to include all necessary drainage and utility easements as required for the public sanitary sewer and watermain/hydrants.

**Traffic.** The applications have been sent to Minnesota Department of Transportation (MNDOT), as the development abuts the Keats Ave N to WB I-94 ramp, and Washington County, as the County has indicated a study will be done on the Keats Ave N and Hudson Blvd N intersection, to which this project is near. MNDOT had made comments that the grading should stay within the limits of the development;

the Hudson Blvd right-of-way should be shown as City of Lake Elmo rather than MNDOT; and MNDOT drainage permit will be required.

**Access Management.** Hudson Blvd is planned as a major collector road. The Comprehensive Plan's access management guidelines limit full commercial driveway access to 660 ft spacing. The PUD Concept Plan had shown a second access on the site approximately 240 ft away from the access to Kwik Trip. It was a recommended condition of approval of the PUD Concept Plan that the Applicant work to include PID# 34.292.1440004 (Ebertz Property – the small property to the west) as part of the Preliminary Plat and PUD Plans or work with the owner of this property to provide shared access. The Applicant details in the narrative that because CM Properties 94, LP does not own or control this property, access was not able to be shifted. The Applicant eliminated the second access on the Preliminary and Final Plat and PUD Plans application as a result, and Staff recommends the developer continue to work with the Ebertz's to gain access to a secondary access to Hudson Blvd on the Ebertz property in the future. A future access driveway connection to the adjacent westerly property has been shown on this site plan. There are currently no plans for a raised center median along Hudson Boulevard, and so right-in/right-out access locations cannot be allowed along Hudson Boulevard.

A right turn lane should be required on Hudson Boulevard. There is an existing westbound turn lane on Hudson Boulevard at the existing access location to this development. However, an eastbound right turn should be required as part of this project to handle increased traffic volumes. Hudson Boulevard is expected to receive significant growth in traffic volume as the I-94 corridor develops. It is the goal of the City to maintain Hudson Boulevard as a two-lane road. Therefore, left and right turn lanes will need to be implemented throughout the corridor to facilitate the turning movements for the developing areas while maintaining mobility of the through traffic.

**Fire Chief and Building Official Comments.** The Fire Chief and Building Official have reviewed the proposed site plan and have the following comments as they relate to the Utility Plan:

- The drive-through aisle and lane to the west of the drive-through are 12 and 16 feet in width. The drive-through aisle on Lot 3 also indicates a width of 12 feet. These should be at least 20 feet in width to allow for required apparatus access around the perimeter of the buildings.
- Additional fire hydrants will be needed on the northwest end of the building on Lot 2, the northeast end of the building on Lot 1, and the southwest end of the building on Lot 3.
- Additional watermain and fire hydrant easements will be required over the additional fire hydrant locations and watermains.

**Circulation.** In addition to issues raised in the drive-through section of this report, there is a bit of concern about circulation on the site. The shared access with Kwik Trip could prove to be confusing to site visitors, as the existing median is essentially where the right turn lane for the Kwik Trip portion of the access is. Additionally, drivers accessing Kwik Trip from the proposed development may or may not know to stop for oncoming traffic from those exiting back portion of Kwik Trip. Additional striping may improve this situation. A recommended condition of approval is that the Applicant provide additional striping and/or a stop sign in this area to facilitate proper traffic circulation.

**Municipal Sanitary Sewer and Water Supply.** The proposed site is located within the Stage 1 Regional Sewer area. The property is currently served with municipal sewer and water, and no phasing is required for infrastructure improvements. The Applicant has indicated in the submitted narrative that sanitary sewer and water service be extended to the property to the west, provided the owner is willing to pay the cost to extend. The owner at this time appears willing, but no agreement has been reached. The Applicant has suggested as an alternative that the property access utilities under Hudson Blvd from the North. This, however, is a business decision that should ultimately be made by Council. The City should only accept one boring under Hudson Blvd. A boring was made for Kwik Trip, so Staff recommends that no additional borings be allowed. As such, the Applicant should be made to extend sanitary sewer and water

service to the westerly parcel regardless of whether or not the owner of the aforementioned parcel agrees to help pay for it.

**Stormwater.** An infiltration basin is provided on the east side of the property. Stormwater runoff will be routed through two underground parking lot storm chamber systems that will provide infiltration and retention in addition to one above ground infiltration basin. The above ground infiltration basin will be obtained by converting an existing storm water pond to an infiltration basin. Pretreatment for the infiltration basins will be provided by oversized sump manholes equipped with scour prevention devices. Due to proximity to the Kwik Trip site, the MPCA should be consulted to verify infiltration practices will be allowed. Written landowner permission may be required for any off-site storm water discharges to adjacent properties to avoid negative impacts to downstream properties.

**Phasing.** The Applicant has requested to plat all three lots at this time, but construction will occur in three phases: Three phases: 1<sup>st</sup>: 14,700 sf building on Lot 1 2<sup>nd</sup>: 10,120 sf building on Lot 2 3<sup>rd</sup>: 3,192 sf building on Lot 3. The Applicant should submit construction plans for approval by the City which will detail phasing of utilities and grading and site improvements.

**Existing Easements.** The Applicant has indicated the need to vacate existing easements over the property. The Applicant will need to submit a separate application for this, and the public hearing may be held at the City Council meeting concurrently with Preliminary and Final Plat and PUD Plan approval.

**Comprehensive Plan.** The property is guided for and zoned Commercial. The proposed development is commercial in nature. Commercial development is guided for 4.5-7 residential equivalency units (REU) per acre.

*Residential Equivalency Units (REU).* Because the development is within the beginning stages, the Met Council has not yet made a determination for WAC/SAC Charges. However, the following outlines REU information for the proposed uses within the development:

**Restaurant**

Fixed Seating (actual number of seats)	10 seats	1
Non-Fixed Seating (the greater of the square feet of dining area @ 15 square feet/seat or number of seats shown on the plan)	10 seats	1

**Outdoor patios and sidewalk seating are counted same as inside seating.**  
(See Section 5.2.1.7.1 for discount)

Drive-in (See Section 5.2.1.7 for discount)	9 parking	1
Take-out (no seating)	3,000 square feet	1
Outpatient clinic	*17 fixture units	1
Sterilizer (4 hours x gallons per minute x 60 minutes)	274 gallons	1
X-ray film processor (4 hours x gallons per minute x 60 minutes)	274 gallons	1
<b>Retail Store</b> (deduct mechanical rooms, elevator shafts, stairwells, escalators, restrooms and unfinished storage areas) (for remainder use other criteria) (i.e. Gas Pumping)	3,000 square feet	1
Shower (if lockers are included use Locker Room criteria)	*17 fixture units	1

**Park Dedication/Parks and Trails.** The parkland dedication requirement for the proposed commercial development is presently \$4,500 per acre in lieu of dedicated land. The proposed development area is 3.82 acres in size, and so the required parkland dedication based on the present fee schedule would total \$17,190. The Parks Commission was informed of the proposed development at the March 20, 2017 meeting.



**PUD Agreement.** A PUD agreement that clearly articulates permitted and conditional uses, placement of structures, development intensity, density, setbacks, building requirements, lot requirements, signage, or other elements of the plan that deviate from the Commercial Zoning District standards will be incorporated into the Findings of the Resolution if the PUD is approved. The PUD Agreement will provide the development regulations that prevail for the site. Those items not addressed by the PUD Agreement will default to the underlying Commercial Zoning standards.

**PLANNING COMMISSION/PUBLIC HEARING:**

The Planning Commission held a public hearing and considered this request at its June 12, 2017 meeting. No public inquiries or comments were received prior to the public hearing, and no one from the public spoke at the public hearing.

**Amended Findings of Fact.** The Planning Commission recommended adding the following finding of fact to the Staff-recommended findings of fact:

12. The Applicant will be extending municipal sewer and water to the westerly property (PID# 34.029.21.44.0004), providing benefit to the City.

**Amended Conditions of Approval.** The Planning Commission also recommended the following amendments to the Staff-recommended conditions of approval (in red, underlined text):

9. The Applicant shall amend the proposed Landscape Plan to comply with City standards and obtain approval by the City. The Landscape Plan shall include additional landscaping along the Keats Ave N to WB I-94 ramp.

- *Staff Comment:* This motion was made with an affirmative vote of 3-2. The reasoning for this is to keep headlights of cars in the parking lot and drive-throughs from shining towards cars on the ramp. Because the ramp does not constitute as street frontage, landscaping standards that apply to street frontage do not apply.

21. Permitted medical facilities shall be limited to non-urgent facilities, including but not limited to clinics, eye doctors, and dental facilities.

22. The Applicant shall add site amenities such as benches, trash receptacles, decorative lighting, and signage compliant with the Lake Elmo Design Guidelines and Standards Manual to the site plan.

23. Fencing shall be provided enclosing all outdoor dining areas.

The Planning Commission also discussed concern about the traffic flow near the shared entrance to Kwik Trip, specifically that which may be caused by the median that blocks the approach to the right-hand turn lane exit. Additionally, the Planning Commission wanted to see the two-lane drive through lanes on the southern portion of the site both be one-way.

The Planning Commission recommended approval with the amended findings of fact and conditions of approval with an affirmative vote of 5-0.

**RECOMMENDED FINDINGS OF FACT AND CONDITIONS OF APPROVAL:**

Findings of fact and conditions of approval as recommended by Staff and the Planning Commission are outlined in the attached Resolution 2017-063.

**FISCAL IMPACT:**

The development of this currently vacant site will create three taxable parcels. Additionally, it will contribute SAC/WAC fees and park dedication fees of \$17,190.

**RECOMMENDATION:**

Staff and the Planning Commission recommend the Council adopt Resolution 2017-063 approving the Lakewood Crossing 2<sup>nd</sup> Addition Preliminary and Final Plat and PUD Plans with the 23 conditions of approval as recommended by Staff and the Planning Commission. Suggested motion:

***“Move to adopt Resolution 2017-063 approving Lakewood Crossing 2<sup>nd</sup> Addition Preliminary and Final Plat and PUD Plans with the 23 conditions of approval as drafted by Staff and the Planning Commission based on the findings of fact listed in the Staff Report.”***

**ATTACHMENTS:**

1. Preliminary and Final Plat and PUD Plans application.
2. Engineering Review Memo dated May 31, 2017.
3. Comprehensive Sign Plan
4. Fire Chief and Building Official Comments
5. MNDOT Review Comments
6. Resolution 2017-063 granting Preliminary and Final Preliminary Plat and PUD Plans Approval

Date Received: \_\_\_\_\_  
Received By: \_\_\_\_\_  
LU File #: \_\_\_\_\_



651-747-3900  
3800 Laverne Avenue North  
Lake Elmo, MN 55042

### PRELIMINARY PLAT APPLICATION

Applicant: C/M PROPERTIES 94 LP, C/O MFC PROPERTIES CORP.  
Address: 3460 WASHINGTON DRIVE, SUITE 100, Eagan, MN 55122  
Phone #: 651-452-3303  
Email Address: BMILLER@MFCPROPERTIES.COM

Fee Owner: SAME AS ABOVE  
Address: \_\_\_\_\_  
Phone #: \_\_\_\_\_  
Email Address: \_\_\_\_\_

Property Location (Address and Complete (long) Legal Description): \_\_\_\_\_  
OUTLOT A, LACEWOOD CROSSING, ACCORDING TO THE RECORDED  
PLAT THEREOF, WASHINGTON COUNTY, MN

General information of proposed subdivision: 3 COMMERCIAL LOT SUBDIVISION  
TO ACCOMMODATE 3 BUILDINGS TOTALING APPROXIMATELY 28,000  
SQUARE FEET.

Conducted pre-application meeting with Staff?  Yes  No

In signing this application, I hereby acknowledge that I have read and fully understand the applicable provisions of the Zoning Ordinance and current administrative procedures. I further acknowledge the fee explanation as outlined in the application procedures and hereby agree to pay all statements received from the City pertaining to additional application expense.

Signature of applicant: [Signature] Date: 4-28-17

Signature of Fee Owner: \_\_\_\_\_ Date: \_\_\_\_\_

APR 28 2017

Date Received: \_\_\_\_\_  
Received By: \_\_\_\_\_  
Permit #: \_\_\_\_\_



651-747-3900  
3800 Laverne Avenue North  
Lake Elmo, MN 55042

## LAND USE APPLICATION

- Comprehensive Plan  Zoning District Amend  Zoning Text Amend  Variance\*(see below)  Zoning Appeal
- Conditional Use Permit (C.U.P.)  Flood Plain C.U.P.  Interim Use Permit (I.U.P.)  Excavating/Grading
- Lot Line Adjustment  Minor Subdivision  Residential Subdivision Sketch/Concept Plan
- PUD Concept Plan  PUD Preliminary Plan  PUD Final Plan  Wireless Communications

Applicant: CM PROPERTIES 94, LP c/o MFC PROPERTIES CORP.  
Address: 3460 WASHINGTON DRIVE, SUITE 100, EAGAN, MN 55122  
Phone #: 651-452-3303  
Email Address: BMILLER@MFCPROPERTIES.COM

Fee Owner: SAME AS ABOVE  
Address: \_\_\_\_\_  
Phone #: \_\_\_\_\_  
Email Address: \_\_\_\_\_

Property Location (Address): \_\_\_\_\_  
(Complete (long) Legal Description: OUTLOT A, LAKEWOOD CROSSING, ACCORDING TO RECORDED PLAN THEREOF, WASHINGTON COUNTY, MN  
PID#: 34.029.21.44.0015

Detailed Reason for Request: PUD APPROVAL TO ALLOW A 3 BUILDING RETAIL COMPLEX WITH ZERO LOT LINES, 3 DRIVE THrus, A COMPREHENSIVE SIGN PLAN.

\*Variance Requests: As outlined in Section 301.060 C. of the Lake Elmo Municipal Code, the applicant must demonstrate practical difficulties before a variance can be granted. The practical difficulties related to this application are as follows:

In signing this application, I hereby acknowledge that I have read and fully understand the applicable provisions of the Zoning ordinance and current administrative procedures. I further acknowledge the fee explanation as outlined in the application procedures and hereby agree to pay all statements received from the City pertaining to additional application expense.

Signature of applicant:  Date: 4-28-17

Signature of fee owner: \_\_\_\_\_ Date: \_\_\_\_\_

Date Received: \_\_\_\_\_  
Received By: \_\_\_\_\_  
LU File #: \_\_\_\_\_



651-747-3900  
3800 Laverne Avenue North  
Lake Elmo, MN 55042

## FINAL PLAT APPLICATION

Applicant: CM PROPERTIES 94, LP  
Address: 3460 WASHINGTON DRIVE, SUITE 100, Eagan, MN 55122  
Phone #: 612-452-3303  
Email Address: B.MILLER@CMC PROPERTIES.COM

Fee Owner: SAME AS ABOVE  
Address: \_\_\_\_\_  
Phone #: \_\_\_\_\_  
Email Address: \_\_\_\_\_

Property Location (Address): \_\_\_\_\_  
Complete (long) Legal Description: OUTLOT A, LINDENWOOD CROSSING, ACCORDING TO  
TITLE RECORDED PLAT THEREOF, WASHINGTON COUNTY, MN  
PID#: 34.029.21.44.0015

General information of proposed subdivision: 3 LOT COMMERCIAL SUBDIVISION  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

In signing this application, I hereby acknowledge that I have read and fully understand the applicable provisions of the Zoning Ordinance and current administrative procedures. I further acknowledge the fee explanation as outlined in the application procedures and hereby agree to pay all statements received from the City pertaining to additional application expense.

Signature of applicant: [Signature] Date: 5-5-17

Fee Owner Signature \_\_\_\_\_ Date: \_\_\_\_\_

Date Received: \_\_\_\_\_  
Received By: \_\_\_\_\_  
Permit #: \_\_\_\_\_



651-747-3900  
3800 Laverne Avenue North  
Lake Elmo, MN 55042

### LAND USE APPLICATION

- Comprehensive Plan  Zoning District Amend  Zoning Text Amend  Variance\*(see below)  Zoning Appeal
- Conditional Use Permit (C.U.P.)  Flood Plain C.U.P.  Interim Use Permit (I.U.P.)  Excavating/Grading
- Lot Line Adjustment  Minor Subdivision  Residential Subdivision Sketch/Concept Plan
- PUD Concept Plan  PUD Preliminary Plan  PUD Final Plan  Wireless Communications

Applicant: CM PROPERTIES 94, LP  
Address: 3460 WASHINGTON DRIVE, SUITE 100, EAGAN, MN 55122  
Phone # 651-452-3303  
Email Address: BRIAN@CMCPROPERTIES.COM

Fee Owner: STATE AS ABOVE  
Address: \_\_\_\_\_  
Phone # \_\_\_\_\_  
Email Address: \_\_\_\_\_

Property Location (Address): As  
(Complete (long) Legal Description: OUTLOT A, LINDENWOOD CROSSING, ACCORDING TO THE RECORDED PUD THEREOF, WASHINGTON COUNTY, MN  
PID#: 34.029.21.44.0015

Detailed Reason for Request: FINAL PUD APPROVAL TO ALLOW A 3 BUILDING RETAIL COMPLEX WITH ZERO LOT LINES, 3 DRIVE THROUS, AND A COMPREHENSIVE SIGN PLAN

\*Variance Requests: As outlined in Section 301.060 C. of the Lake Elmo Municipal Code, the applicant must demonstrate practical difficulties before a variance can be granted. The practical difficulties related to this application are as follows:

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

In signing this application, I hereby acknowledge that I have read and fully understand the applicable provisions of the Zoning ordinance and current administrative procedures. I further acknowledge the fee explanation as outlined in the application procedures and hereby agree to pay all statements received from the City pertaining to additional application expense.

Signature of applicant: [Signature] Date: 5-15-17

Signature of fee owner: \_\_\_\_\_ Date: \_\_\_\_\_

## Written Statements:

The following are answers to Questions 2a thru 2m on the Preliminary Plat Application form:

- a. Record Owner  
CM Properties 94, L.P.  
3460 Washington Drive, Suite 100  
Eagan, MN 55122  
Attn: Bruce Miller  
(651) 452-3303
- Engineer / Surveyor  
Carlson McCain, Inc.  
3890 Pheasant Ridge Drive NE, Suite 100  
Blaine, MN 55449  
Attn: Joe Radach, PE  
(763) 489-7912
- Architect  
Architectural Consortium, LLC  
901 No. Third Street, Suite 220  
Minneapolis, MN 55401  
Attn: Kathy Anderson  
(612) 436-4030
- b. The property has an unassigned address but is currently legally described as Outlot A, Lakewood Crossing, according to the recorded plat thereof, Washington County, MN.  
PID #34.029.21.44.0015  
Zoning – Commercial  
Parcel Size – 3.82 Acres / 166,449 Sq.Ft.
- c. Subdivision Name: Lakewood Crossing  
Number of Lots: Three (3)
- d. N/A
- e. The intent of this 3 lot, 3 building project is to create a successful retail project providing a warm and inviting place for residents in the area to shop and dine. Our goal is to have a quality, sit down, full service restaurant on the east side of the project including a large outdoor patio to accommodate outside seating for restaurant patrons. In addition to a sit down restaurant, we are targeting fast casual restaurants with drive thru, coffee with drive thru, a hair salon, dry cleaner, chiropractor, bank or credit union with drive thru and other similar services and retail businesses. Our intention is to build the project in three (3) phases with the initial plan to construct at 14,700 square foot retail building and follow up with a 10,120 square foot and 3,440 square foot building as the market dictates. Our firm has owned this property for over 45 years and we intend to continue to own it for years to come. Our intention is to build something both we and the City can be proud of and that meets what the market is looking for and stands the test of time architecturally.
- f. N/A

## Written Statements

### Page 2

- g. The property is currently served with municipal sewer and water. No phasing is required for infrastructure improvements.
- h. There are only 3 non-related, non-public property owners within 350' and they are also excited about the prospect of additional development occurring on this corner. This development will have positive impact on property values in this area by providing much needed retail and service businesses.
- i. This development should not conflict with nearby land uses. As a matter of fact, it is our intent to get tenants who enhance our neighbors property values and provide goods and services to the residential areas in and around this intersection.
- j. In the grand scheme of development occurring in Lake Elmo, this project is relatively minor in terms of city services required and will not create a burden on the City. As a matter of fact, commercial tax rates are significantly higher than residential and therefore this project will only help the budgets of the City, County and School District.
- k. N/A
- l. As this is a small commercial development, we are intending on providing a park dedication fee in lieu of dedication which the City will be able to utilize to enhance its overall parks / open space plan.
- m. Our intention is to commence construction in July or August with the first phase 14,300 square foot building to be complete by year end. The Phase II and Phase III building will be constructed as the market dictates.



**Narrative Answering Questions / Concerns Raised for Concept Approval and Outlined in Resolution 2017-026:**

1. Regarding the City Engineer comments from that March 8<sup>th</sup> Memorandum, the plans have been revised to address most of the comments. To address the second access to the west, as we do not own or control the property to the west, we are not able to shift the access. However, we are willing to provide an access easement to the property owner to the west to provide them access so they won't need to request a third access. In addition, when the City determines the traffic counts warrant it, we will install a median on Hudson Boulevard to restrict access to right in and right out only. Regarding the turn lanes, our intention is not to build the secondary (westerly) access with Phase I and therefore will only utilize the existing shared access with Kwik Trip. When we build Phase II we will construct the turn lanes as needed. Regarding the extension of water and sanitary sewer to the property to the west, we will accommodate provided that owner is willing to pay the cost to extend. We have had discussions with them and they appear willing however no agreement has been reached. If we are not able to reach an agreement, the property still can be served with utilities by jacking them under Hudson Blvd. from the North. As a reminder, this is what we had to do with the water when we developed Kwik Trip.
2. It is acknowledged and we shall obtain all other necessary permits. Attached find VBWD Permit #2017-08 Permit dated March 27, 2017.
3. As mentioned above, we will extend sanitary sewer and water to the westerly property provided we can come to an agreement with the property owner. In the alternative, they can extend the utilities from the property they own to the north.
4. It is acknowledged a storm water maintenance agreement will be required. Please provide City standard form or if you don't have one, we can prepare.
5. The Landscape Plan has been revised to provide four (4) more parking islands, landscaping on north side of driveway as well as other modifications to comply with City standards.
6. Acknowledge a Letter of Credit / Financial Guaranty is required for landscape materials.
7. Attached is a Comprehensive Sign Plan along with a narrative detailing the flexibility being requested.
8. The three (3) buildings will be a mix of retail, service and restaurant uses. Section 154.2 of the Code for General Retail is 1 space per 250 square feet or 4 spaces per 1,000 square feet. Lake Elmo Shoppes will have a total of 27,860 square feet. Based on general retail of 4 spaces per 1,000 sq.ft., this results in 111 required stalls. We are providing 165 stalls or 5.92 per 1,000 sq.ft. which is significantly over what is required by code. Some uses, such as restaurants require more parking (1 stall per 100 sq.ft. = 10 stalls per 1,000 sq.ft.) and others require less than general retail such as personal services at 3.33 per 1,000 sq.ft. Although we are not sure what the final tenant mix will be, if 19,000 sq.ft. were retail that would require 77 stalls and 8,700 sq.ft. of restaurants which would require 87 stalls for a total of 164 parking stalls. Based on our experience in owning and developing retail centers, we are confident we have more than adequate parking.

9. To address the site amenity as alternate to 20% open space, we are offering to enhance the site and retail shopping and dining experience with enhanced architectural materials and site furnishings. The Lake Elmo Shoppes architecture is upgraded and features a variety of warm, natural colors, textures and shapes. The corner towers flanking each end features standing seam metal roofs and offer a 3-dimensional quality as well as skyline interest. A mixture of stone, brick and glass combine to help provide individuality to the various tenants. Unique cornices project above the signage band for added interest. A variety of both canvas and metal awnings create pedestrian scale interest. Ornamental light fixtures repeat intermittently for nighttime attraction. The architecture is four-sided being that is visible from both the highway and internal. Signage is limited to designated areas within each tenant lease lines and design controls of individually lit channel letters will ensure quality.

The overall development sets an up-scale environment with the attention to details within the streetscape. Coordinated benches, trash receptacles and bike racks, outdoor dining patios with wrought iron fencing, pedestrian scale ornamental light fixtures and landscaping all combine to enhance the shopping experience.

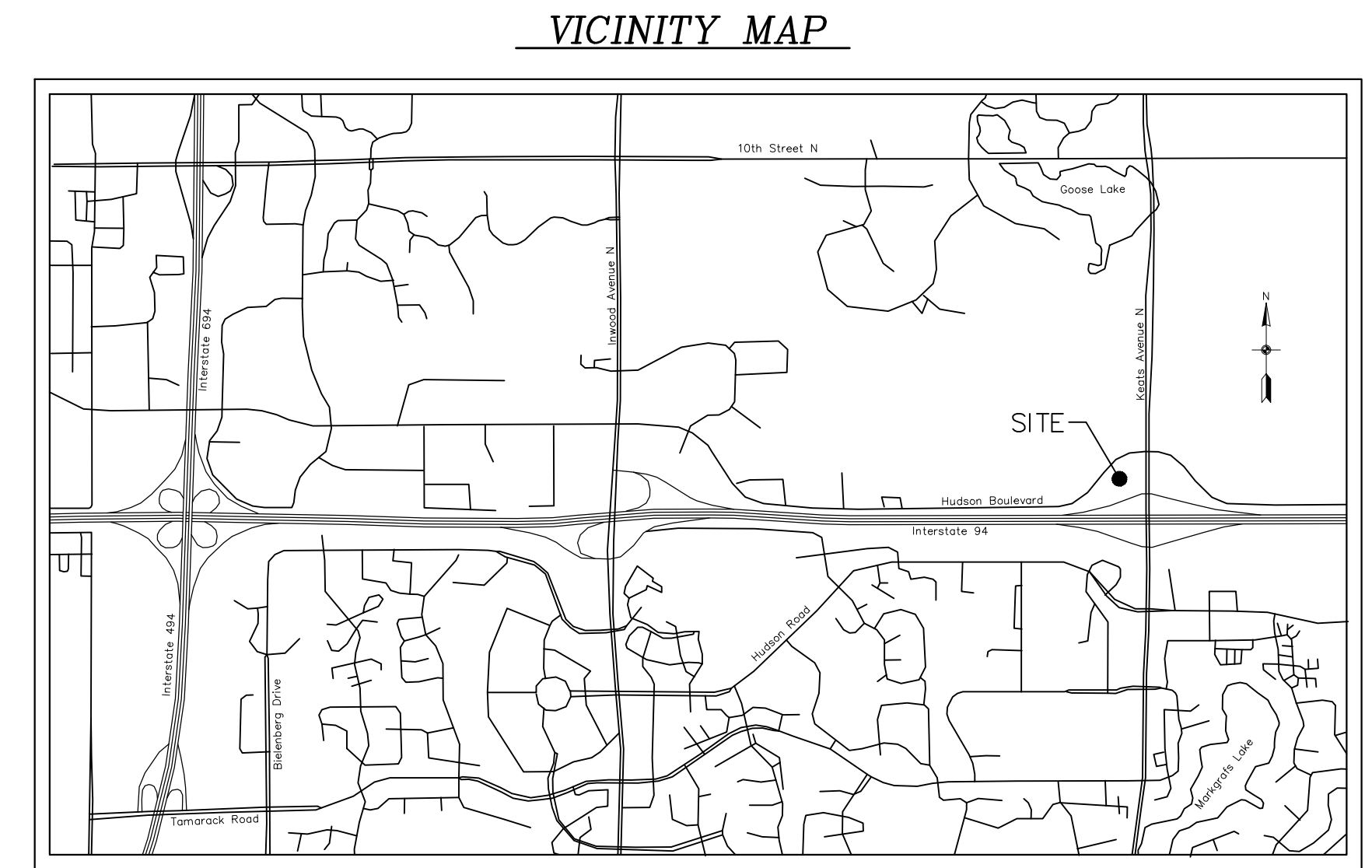
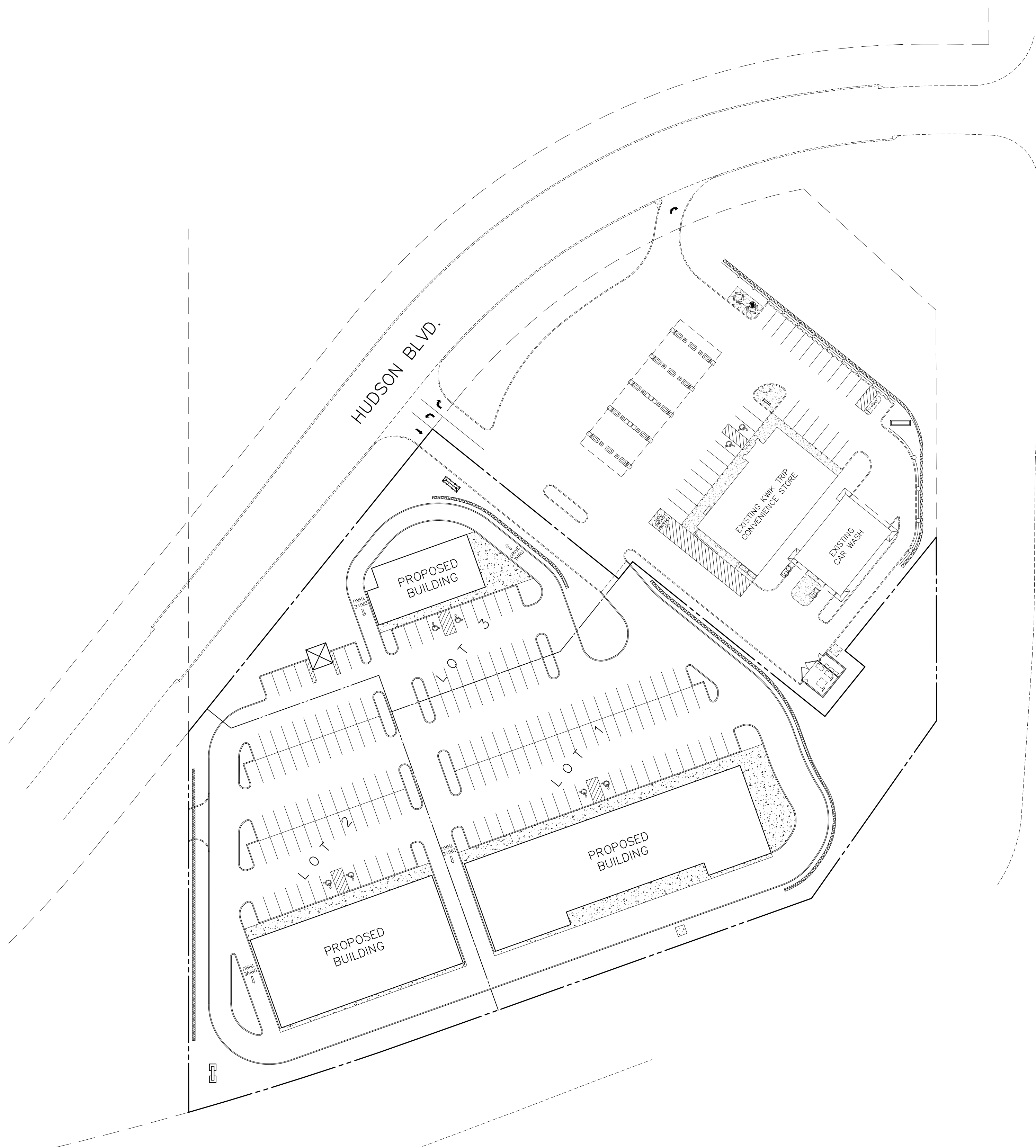
10. The plans detail the location of the drive-thru elements. Based on questions at the Planning Commission and City Council, we commissioned a drive thru analysis which will be addressed in further detail in 19 below.
11. A photometric plan has been ordered and will be forwarded to the City once received. It is acknowledged, an acceptable photometric plan is a condition of final approval.
12. It is understood the final plan showing location of fire hydrants, no parking and fire lanes need approval from the building official and fire chief.
13. The adjacent property owner to the west will not be a party of the Preliminary Plat or PUD however, we will work with them to provide shared access in the location noted on the site plan.
14. Although we are not proposing to construct any new access points with the first phase of the development, it is acknowledged turn lanes will need to be constructed with the new access.
15. It is our position, current traffic doesn't warrant widening Hudson Blvd., but depending on future development to the north and west, future road improvements may be necessary.
16. Lake Elmo Shoppes deliveries will occur behind the buildings and will utilize the service door in the back of each tenant space. No dock doors or drive-in doors are needed for the types of tenants we will be pursuing.
17. A Reciprocal Easement and Operating Agreement drafted and will be recorded to account for shared parking and access.

18. All mechanical rooftop equipment will be screened by the high parapet walls provided in the design of Lake Elmo Shoppes.
19. Attached is the drive-thru analysis prepared by Spark Consulting Engineers. As you will see the report, Spark states the two high volume and two low volume drive-thru locations have sufficient stacking and circulation. However, based on some of the comments in the report and subsequent discussions regarding potential circulation conflicts, we did decide to eliminate the “high volume” drive-thru located on the east end of the Lot 2 building. Therefore, the request for drive-thru’s has been revised to three (3).

# LAKE ELMO SHOPPES

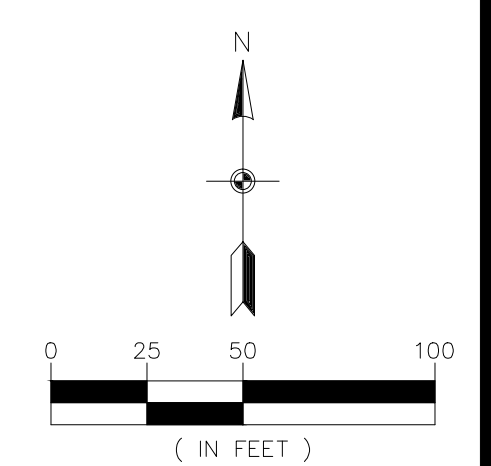
## ~ SITE IMPROVEMENT PLANS ~

### LAKE ELMO, MINNESOTA



#### SHEET INDEX

- C1. TITLE SHEET
- C2. EXISTING CONDITIONS & REMOVALS PLAN
- C3. SITE & SIGN PLAN
- C4. GRADING, DRAINAGE & EROSION CONTROL PLAN
- C5. STORMWATER POLLUTION PREVENTION PLAN
- C6. UTILITY PLAN
- C7. DETAILS
- C8. DETAILS
- C9. DETAILS
- C10. DETAILS (CMP SYSTEM)
- C11. DETAILS (CMP SYSTEM)
- L1. LANDSCAPE PLAN
- L2. LANDSCAPE PLAN



BENCHMARKS	
1.	MN/DOT GSD STATION NO. 33428 TOP OF BENCHMARK DISK ELEVATION = 974.47 (NAVD 88)
2.	MN/DOT GSD STATION NO. 33429 TOP OF BENCHMARK DISK ELEVATION = 943.86 (NAVD 88)



**Carlson McCain**  
 ENVIRONMENTAL - ENGINEERING - SURVEYING  
 3890 Pheasant Ridge Dr. NE #100, Blaine, MN  
 Phone: 763-489-7900 Fax: 763-489-7959

**TITLE SHEET**  
**LAKE ELMO SHOPPES**  
 Lake Elmo, Minnesota

**MFC PROPERTIES CORP.**  
 3460 Washington Drive, Suite 100  
 Eagan, MN 55122

REVISIONS	
1.	04/26/17 Per City comments.
2.	05/24/17 Per City comments.
3.	
4.	
5.	
6.	

DRAWN BY: JTR  
 DESIGNED BY: JTR  
 ISSUE DATE: 02/03/17

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota

Name: Joseph T. Radach, P.E.  
 Signature: *Joseph T. Radach*  
 Date: 02/03/17 License #: 45889

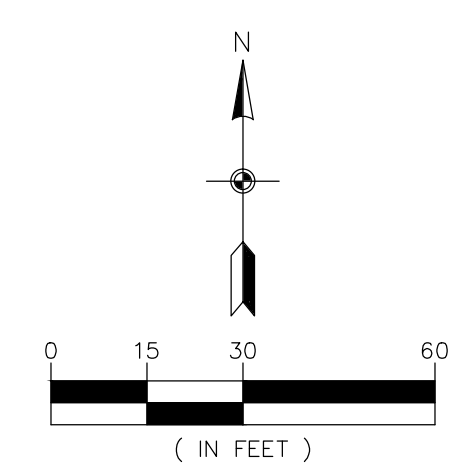


**LEGEND**

	EXISTING	REMOVAL
PROPERTY LINE	---	---
EASEMENT LINE	---	---
CURB LINE	---	---
BITUMINOUS PAVEMENT	---	---
CONCRETE WALK	---	---
SANITARY SEWER	---	---
STORM SEWER	---	---
WATER MAIN	---	---
UNDERGROUND GAS	---	---
UNDERGROUND ELECTRIC	---	---
UNDERGROUND TELEPHONE	---	---
MANHOLE	---	---
HYDRANT	---	---
LIGHT POLE	---	---
FENCE LINE	---	---
5' CONTOUR	---	---
1' CONTOUR	---	---

AS NOTED

- REMOVAL PLAN NOTES**
1. ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE LOCATION, DEPTH AND TYPES OF EXISTING UTILITIES AND TO NOTIFY THE OWNER AND ENGINEER IMMEDIATELY OF ANY DISCREPANCIES OR VARIATIONS FROM THE PLANS.
  2. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL EXISTING UTILITIES, APPURTENANCES AND STRUCTURES NOT INDICATED FOR REMOVAL. DAMAGE CAUSED BY DEMOLITION OPERATIONS SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER.
  3. CONTRACTOR TO REMOVE/RELOCATE EXISTING PRIVATE UTILITIES AS NECESSARY. THE CONTRACTOR SHALL COORDINATE THESE ACTIVITIES WITH THE UTILITY COMPANIES.
  4. THE CONTRACTOR IS RESPONSIBLE FOR ALL THE REMOVALS SHOWN ON THE PLANS AND SHALL CONFORM/ADHERE TO ALL GOVERNING STATE AND LOCAL REGULATIONS. ALL PERMITS, APPLICATIONS AND FEES ARE THE RESPONSIBILITY OF THE CONTRACTOR.
  5. ALL SAWCUTS SHALL BE FULL-DEPTH CUTS.



**BENCHMARKS**

1. MN/DOT GSID STATION NO. 33428 TOP OF BENCHMARK DISK ELEVATION = 974.47 (NAVD 88)
2. MN/DOT GSID STATION NO. 33429 TOP OF BENCHMARK DISK ELEVATION = 943.86 (NAVD 88)



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ENVIRONMENTAL - ENGINEERING - SURVEYING  
3890 Pheasant Ridge Dr. NE #100, Blaine, MN  
Phone: 763-489-7900 Fax: 763-489-7959

**EXISTING CONDITIONS & REMOVALS PLAN**

**LAKE ELMO SHOPPES**  
Lake Elmo, Minnesota

**MFC PROPERTIES CORP.**  
3460 Washington Drive, Suite 100  
Eagan, MN 55122

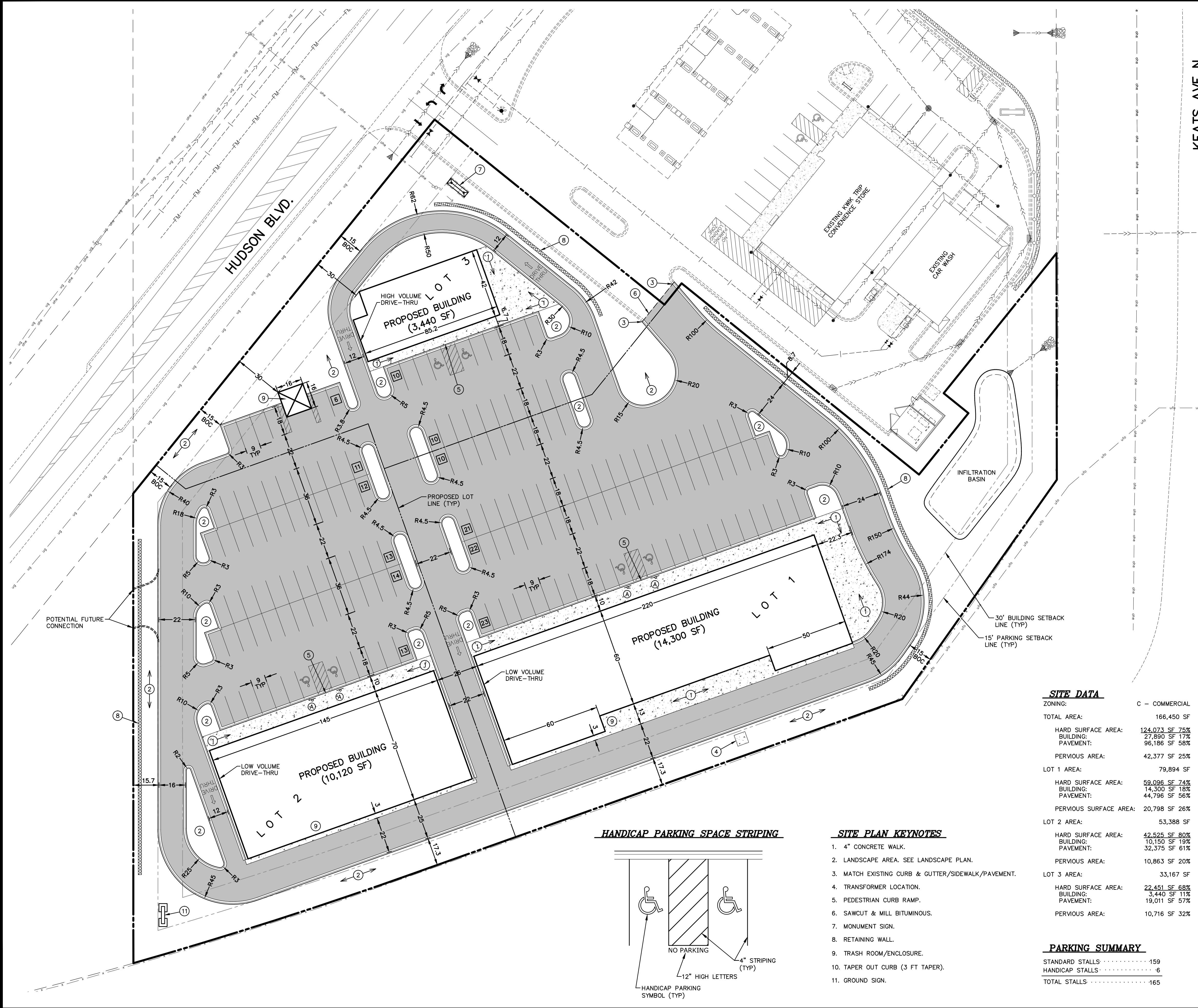
**REVISIONS**

1. 04/26/17	Per City comments.
2. 05/24/17	Per City comments.
3.	
4.	
5.	
6.	

DRAWN BY: JTR  
DESIGNED BY: JTR  
ISSUE DATE: 02/03/17

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota

Name: Joseph T. Radach, P.E.  
Signature: *J. Radach*  
Date: 02/03/17 License #: 45889



KEATS AVE N.

**LEGEND**

	EXISTING	PROPOSED
PROPERTY LINE	---	---
EASEMENT LINE	---	---
CURB LINE	---	---
BITUMINOUS PAVEMENT	---	---
CONCRETE WALK	---	---
SANITARY SEWER	---	---
STORM SEWER	---	---
WATER MAIN	---	---
UNDERGROUND GAS	---	---
UNDERGROUND ELECTRIC	---	---
UNDERGROUND TELEPHONE	---	---
MANHOLE	---	---
HYDRANT	---	---
LIGHT POLE	---	---
FENCE LINE	---	---

**SITE PLAN NOTES**

- ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
- ALL BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF THE BUILDING UNLESS OTHERWISE NOTED.
- ALL CURB AND GUTTER SHALL BE B612 UNLESS OTHERWISE NOTED.
- TYPICAL PARKING STALLS ARE 9' X 18'.
- ALL PEDESTRIAN RAMPS SHALL MEET CURRENT ADA STANDARDS AND BE INSTALLED WITH CAST IRON TRUNCATED DOMES.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES PRIOR TO THE START OF SITE WORK. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY OF DISCREPANCIES AND/OR VARIATIONS FROM THE PLAN.
- UNLESS OTHERWISE SHOWN ON THIS DRAWING, CONTRACTOR SHALL PROVIDE CONTROL JOINTS, CONSTRUCTION JOINTS AND EXPANSION JOINTS IN SLAB ON GRADE, SIDEWALKS AND DRIVES PER THE FOLLOWING REQUIREMENTS:
  - CONTROL JOINT MAX. SPACING: WALKS-8' O.C. ALL OTHERS-10' O.C.
  - SAW CUT CONTROL JOINTS MINIMUM 1/4 CONCRETE THICKNESS.
  - EXPANSION JOINTS MAX. SPACING: WALKS-24' O.C. \*ALL OTHERS-40' O.C.
  - \*AT ALL POINTS WHERE A CHANGE IN PAVEMENT THICKNESS OCCURS AND/OR WHERE NEW PAVEMENT WILL MATCH EXISTING PAVEMENT, AN EXPANSION JOINT SHALL BE PROVIDED.
  - DOWEL ALL EXPANSION JOINTS: 24" O.C. MAX.

**SIGNING AND STRIPING NOTES**

- ALL SIGNS SHALL BE PLACED 18" MINIMUM BEHIND CURB UNLESS OTHERWISE NOTED.
- SIGNAGE SHALL INCLUDE SIGN, POST, HARDWARE, CONCRETE FOOTING AND STEEL CASING (IF REQUIRED).
- PARKING LOT STRIPING SHALL BE 4" SOLID WHITE PAINT.
- ALL HANDICAP STRIPING, MARKINGS AND CROSS-HATCH SHALL BE 4" SOLID BLUE PAINT.
- ALL SIGNS SHALL MEET THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD) FOR RETRO REFLECTIVITY AND INSTALLATION.

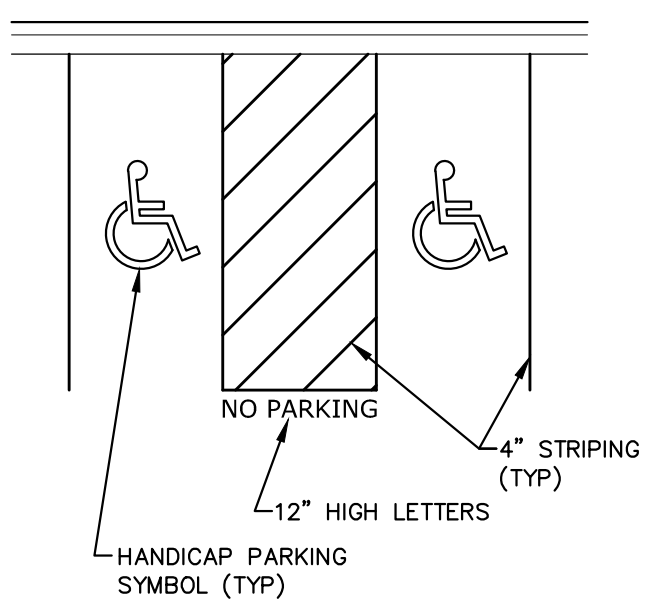
**SIGN SCHEDULE (PER MMUTCD)**

- ④ HANDICAP PARKING SIGN: R7-8m W/R7-8b (12"x 18")

**SITE DATA**

ZONING:	C - COMMERCIAL
TOTAL AREA:	166,450 SF
HARD SURFACE AREA:	124,073 SF 75%
BUILDING:	27,890 SF 17%
PAVEMENT:	96,186 SF 58%
PERVIOUS AREA:	42,377 SF 25%
LOT 1 AREA:	79,894 SF
HARD SURFACE AREA:	59,096 SF 74%
BUILDING:	14,300 SF 18%
PAVEMENT:	44,796 SF 56%
PERVIOUS SURFACE AREA:	20,798 SF 26%
LOT 2 AREA:	53,388 SF
HARD SURFACE AREA:	42,525 SF 80%
BUILDING:	10,150 SF 19%
PAVEMENT:	32,375 SF 61%
PERVIOUS AREA:	10,863 SF 20%
LOT 3 AREA:	33,167 SF
HARD SURFACE AREA:	22,451 SF 68%
BUILDING:	3,440 SF 11%
PAVEMENT:	19,011 SF 57%
PERVIOUS AREA:	10,716 SF 32%

**HANDICAP PARKING SPACE STRIPING**

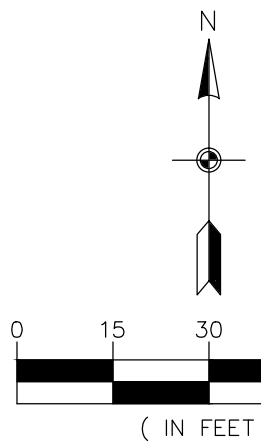


**SITE PLAN KEYNOTES**

- 4" CONCRETE WALK.
- LANDSCAPE AREA. SEE LANDSCAPE PLAN.
- MATCH EXISTING CURB & GUTTER/SIDEWALK/PAVEMENT.
- TRANSFORMER LOCATION.
- PEDESTRIAN CURB RAMP.
- SAWCUT & MILL BITUMINOUS.
- MONUMENT SIGN.
- RETAINING WALL.
- TRASH ROOM/ENCLOSURE.
- TAPER OUT CURB (3 FT TAPER).
- GROUND SIGN.

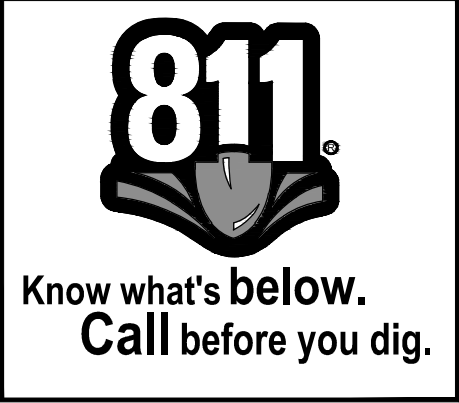
**PARKING SUMMARY**

STANDARD STALLS	159
HANDICAP STALLS	6
TOTAL STALLS	165



**BENCHMARKS**

- MN/DOT GSID STATION NO. 33428  
TOP OF BENCHMARK DISK  
ELEVATION = 974.47 (NAVD 88)
- MN/DOT GSID STATION NO. 33429  
TOP OF BENCHMARK DISK  
ELEVATION = 943.86 (NAVD 88)



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 Phone: 763-489-7900 Fax: 763-489-7959

**SITE & SIGN PLAN**

**LAKE ELMO SHOPPES**  
 Lake Elmo, Minnesota

**MFC PROPERTIES CORP.**  
 3460 Washington Drive, Suite 100  
 Eagan, MN 55122

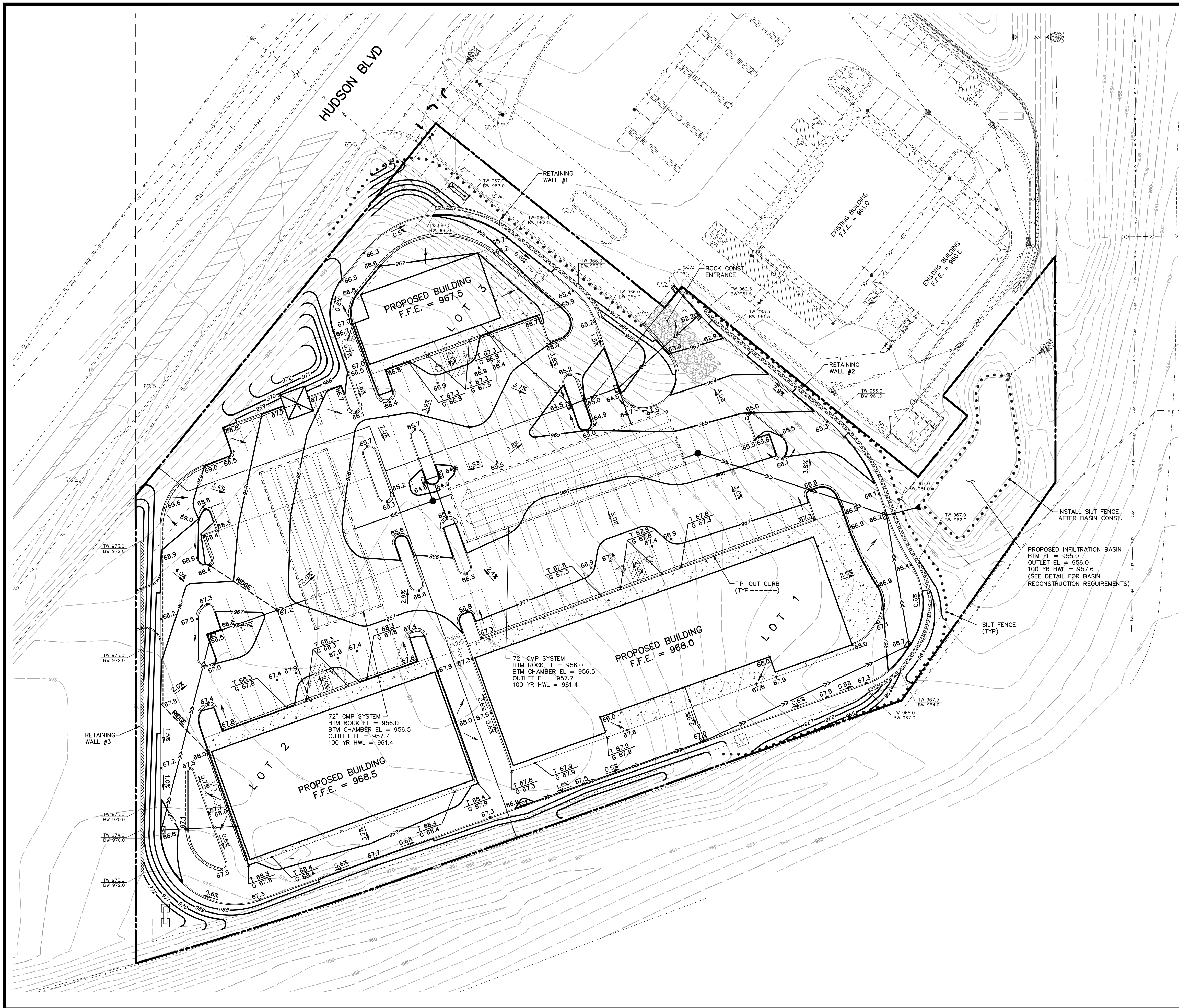
**REVISIONS**

1.	04/26/17	Per City comments.
2.	05/24/17	Per City comments.
3.		
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DRAWN BY: JTR  
 DESIGNED BY: JTR  
 ISSUE DATE: 02/03/17

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the State of Minnesota

Name: Joseph T. Radach, P.E.  
 Signature: *[Signature]*  
 Date: 02/03/17 License #: 45889

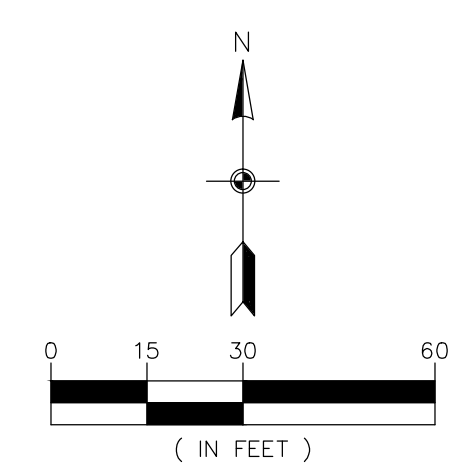


**LEGEND**

	EXISTING	PROPOSED
PROPERTY LINE	---	---
EASEMENT LINE	---	---
CURB LINE	---	---
BITUMINOUS PAVEMENT	---	---
CONCRETE WALK	---	---
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STORM SEWER	---	---
WATER MAIN	---	---
UNDERGROUND GAS	---	---
UNDERGROUND ELECTRIC	---	---
UNDERGROUND TELEPHONE	---	---
MANHOLE	○	○
HYDRANT	○	○
LIGHT POLE	○	○
FENCE LINE	---	---
5' CONTOUR	---	---
1' CONTOUR	---	---
SPOT ELEVATION (CURB ELEVATIONS ARE TO GUTTER LINE)	○	○
TOP OF CURB ELEV.	T 44.3	T 44.3
GUTTER LINE ELEV.	G 43.8	G 43.8
EMERGENCY OVERFLOW	○	○
SILT FENCE	---	---

- GOVERNING SPECIFICATIONS**
- ALL WORK SHALL CONFORM TO THE CITY OF LAKE ELMO STANDARD SPECIFICATIONS. WHERE SAID SPECIFICATIONS DOES NOT COVER THE PROPOSED WORK, THE LATEST EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR CONSTRUCTION" AND THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM) STANDARD SPECIFICATION SHALL APPLY.
  - THE LATEST EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD).

- GRADING NOTES**
- CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES AND PAVEMENTS PRIOR TO THE START OF GRADING CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY OF DISCREPANCIES OR VARIATIONS FROM THE PLAN.
  - CONTRACTOR SHALL STRIP, STOCKPILE AND RESPREAD SUFFICIENT TOPSOIL TO PROVIDE A MINIMUM OF 6" OF TOPSOIL OVER ALL DISTURBED AREAS THAT WILL BE SODDED, SEEDED OR LANDSCAPED.
  - SEE SHEET C7 FOR CITY OF LAKE ELMO STANDARD GRADING NOTES.



**BENCHMARKS**

- MN/DOT GSID STATION NO. 33428  
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TOP OF BENCHMARK DISK  
ELEVATION = 943.86 (NAVD 88)



**Carlson McCain**  
 ENVIRONMENTAL - ENGINEERING - SURVEYING  
 3890 Pheasant Ridge Dr. NE #100, Blaine, MN  
 Phone: 763-489-7900 Fax: 763-489-7959

**GRADING, DRAINAGE & EROSION CONTROL PLAN**  
**LAKE ELMO SHOPPES**  
 Lake Elmo, Minnesota

**MFC PROPERTIES CORP.**  
 3460 Washington Drive, Suite 100  
 Eagan, MN 55122

**REVISIONS**

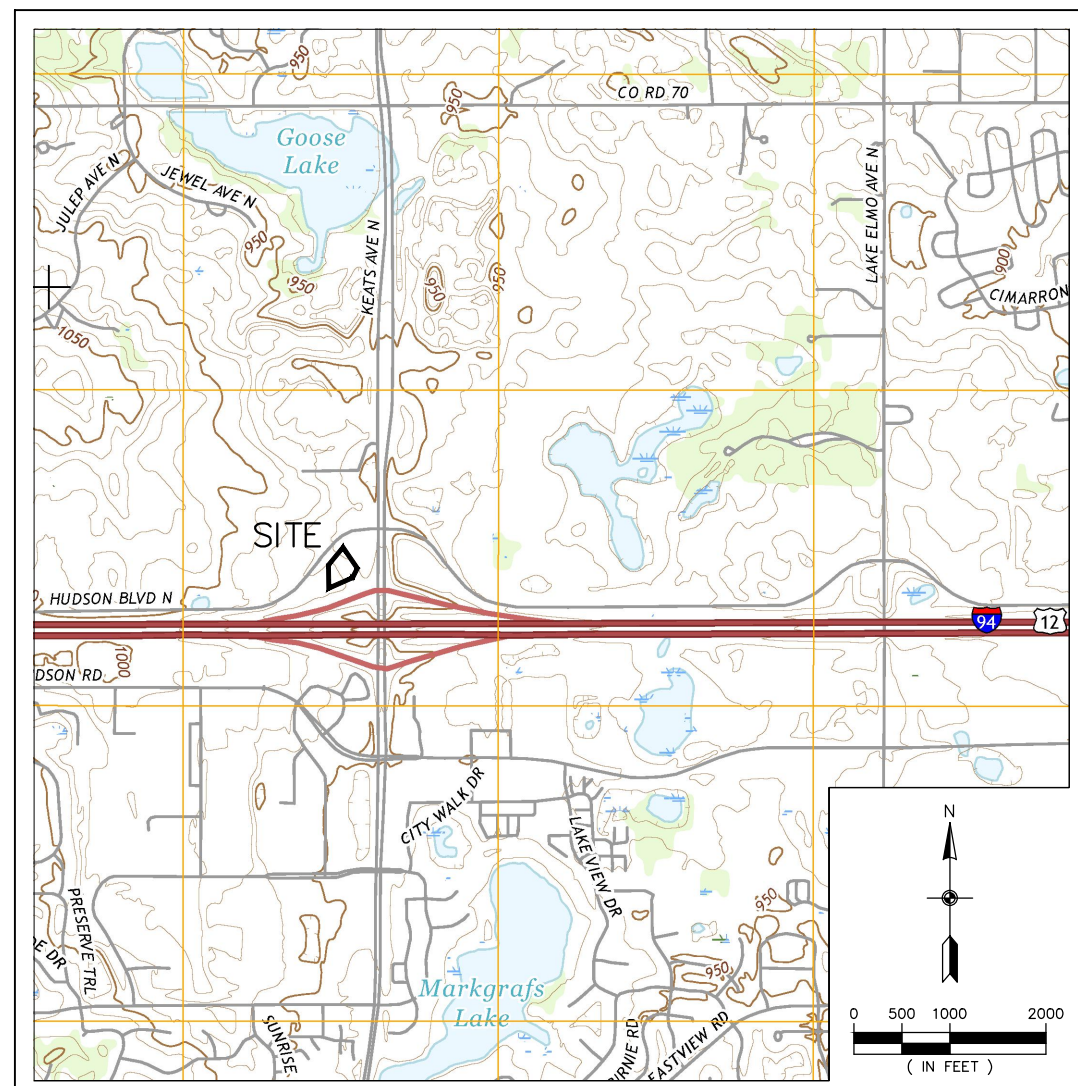
1.	04/26/17	Per City comments.
2.	05/24/17	Per City comments.
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6.		

DRAWN BY: JTR  
 DESIGNED BY: JTR  
 ISSUE DATE: 02/03/17

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota

Name: Joseph T. Radach, P.E.  
 Signature: *Joseph T. Radach*  
 Date: 02/03/17 License #: 45889

**SITE LOCATION MAP**



**INSTALL INLET PROTECTION PRIOR TO CONSTRUCTION (TYP)**

**AREA OF POTENTIAL TRACKED SEDIMENT (SWEEP AS NEEDED)**

**PROTECT BASIN FROM SEDIMENTATION W/SILT FENCE OR OTHER EFFECTIVE MEANS. PREVENT COMPACTION DURING RECONSTRUCTION.**

**MAINTAIN ROCK CONST. ENTRANCE**

**MAINTAIN PERIMETER CONTROL THROUGHOUT CONSTRUCTION (TYP)**

**INSTALL INLET PROTECTION AFTER CONSTRUCTION (TYP)**

**CONCRETE WASHOUT & MATERIALS STORAGE AREA**

**PROTECT UNDERGROUND INFILTRATION SYSTEM FROM SEDIMENTATION AND COMPACTION DURING CONSTRUCTION. DO NOT PUT ONLINE UNTIL SITE IS PAVED. SUMP AND PUMP PARKING AREA AS NEEDED DURING CONSTRUCTION UNTIL PARKING LOT IS PAVED. (TYP)**

**GENERAL INFORMATION**  
MINNESOTA'S CONSTRUCTION STORMWATER PERMIT IS AN EXTENSION OF THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM STORMWATER PROGRAM, WHICH IS PART OF THE FEDERAL CLEAN WATER ACT. REGULATED PARTIES MUST DEVELOP A STORMWATER POLLUTION PREVENTION PLAN (SWPPP). THE SWPPP PROVIDES INFORMATION ON THE EXISTING AND PROPOSED SITE CONDITIONS, CONTROL MEASURES FOR STORMWATER POLLUTION PREVENTION BEFORE, DURING AND AFTER CONSTRUCTION, INSPECTION, MAINTENANCE AND INFORMATION RELATED TO THE PERMANENT STORMWATER MANAGEMENT SYSTEM. THE SWPPP SHALL BE KEPT ON SITE AT ALL TIMES DURING ACTIVE CONSTRUCTION.

**PROJECT INFORMATION**  
PROJECT NAME: LAKE ELMO SHOPPES  
PROJECT LOCATION: LAKE ELMO, WASHINGTON COUNTY, MINNESOTA  
PROJECT OWNER: MFC PROPERTIES CORP.

**RESPONSIBLE PARTIES**  
THE OWNER MUST IDENTIFY A PERSON KNOWLEDGEABLE AND EXPERIENCED IN THE APPLICATION OF EROSION PREVENTION AND SEDIMENT CONTROL BMP'S WHO WILL OVERSEE THE IMPLEMENTATION OF THE SWPPP AND THE INSTALLATION, INSPECTION AND MAINTENANCE OF THE EROSION PREVENTION AND SEDIMENT CONTROL BMP'S.

SITE MANAGER: \_\_\_\_\_  
TRAINING DOCUMENTATION: \_\_\_\_\_

**EXISTING SITE CONDITIONS**  
THE SITE IS LOCATED IN THE NORTHWEST QUADRANT OF THE INTERSECTION OF KEATS AVENUE NORTH (COUNTY ROAD 19) AND THE WEST BOUND ON RAMP TO INTERSTATE HIGHWAY 94 IN LAKE ELMO, MINNESOTA. THE SITE IS BOUNDED ON THE SOUTH BY THE WEST BOUND ON RAMP TO INTERSTATE HIGHWAY 94, ON THE EAST BY AN EXISTING KWIK TRIP CONVENIENCE STORE, ON THE NORTH BY HUDSON BOULEVARD, AND ON THE WEST BY UNDEVELOPED AGRICULTURAL LAND. THE PROPOSED SITE BOUNDARY CONSISTS OF 3.82 ACRES. THERE ARE APPROXIMATELY 0.07 ACRES OF EXISTING IMPERVIOUS SURFACE ONSITE, WHICH IS THE SHARED DRIVE BETWEEN THE SUBJECT PROPERTY AND KWIK TRIP.

THE SITE IS CURRENTLY USED FOR AGRICULTURAL PURPOSES, TYPICALLY PLANTED WITH ROW CROPS. THE SITE HAS A SLOPING TOPOGRAPHY WITH ELEVATIONS RANGING FROM 975 IN THE SOUTHWEST, DOWN TO 955 IN THE EAST AT THE EXISTING STORMWATER BASIN LOCATED IN THE EASTERN CORNER OF THE SITE. THE MAJORITY OF THE STORMWATER FROM THE SITE DRAINS OVERLAND TO THE NORTHEAST WHERE IT IS COLLECTED IN A DITCH THAT DRAINS TO THE EXISTING STORMWATER BASIN IN THE EAST. THIS BASIN OUTLETS TO THE KEATS AVENUE DITCH PRIOR TO DISCHARGING VIA A 42" CMP CULVERT UNDER KEATS AVENUE TO A MNDOT POND LOCATED ON THE EAST SIDE OF KEATS AVENUE. A SMALL PORTION OF THE SITE DRAINS SOUTH TO THE WEST BOUND ON RAMP DITCH WHERE IT DISCHARGES VIA A 30" CMP CULVERT TO THE SOUTH. TO THE NORTHWEST OF THE SITE, ON THE NORTH SIDE OF HUDSON BOULEVARD, THERE IS APPROXIMATELY 60 ACRES OF AGRICULTURAL LAND THAT DRAINS SOUTHEAST TO A DITCH ON THE NORTH SIDE OF HUDSON BOULEVARD. THIS DITCH OUTLETS TO THE SOUTH UNDER HUDSON BOULEVARD TO THE SITE WHERE IT IS COLLECTED IN THE ONSITE DITCH AND DISCHARGES VIA THE 42" CMP CULVERT UNDER KEATS AVENUE. THERE ARE TWO LOCATIONS WHERE THE NORTH SIDE DITCH OF HUDSON BOULEVARD OUTLETS TO THE SOUTH. THE WESTERLY LOCATION OUTLETS VIA THREE 18" RCP CULVERTS. THE EASTERLY LOCATION OUTLETS VIA A SINGLE 18" RCP CULVERT.

**PROPOSED SITE CONDITIONS**  
MFC PROPERTIES CORPORATION PLANS ON DEVELOPING THE SITE INTO THREE COMMERCIAL BUILDINGS, WITH ASSOCIATED PARKING AND DRIVE AREAS. DURING CONSTRUCTION, APPROXIMATELY 3.6 ACRES WILL BE DISTURBED. THE PROPOSED CONSTRUCTION WILL CREATE APPROXIMATELY 2.81 ACRES OF NEW IMPERVIOUS SURFACE.

THE MAJORITY OF THE RUNOFF FROM THE IMPERVIOUS AREAS ONSITE WILL BE ROUTED TO TWO UNDERGROUND INFILTRATION SYSTEMS OR AN ABOVE GROUND INFILTRATION BASIN. THE TWO UNDERGROUND INFILTRATION SYSTEMS WILL BE LOCATED IN THE CENTRAL PORTION OF THE SITE UNDER THE PROPOSED PARKING LOT AND WILL CONSIST OF 72-INCH PERFORATED CMP. THE INFILTRATION BASIN WILL BE LOCATED IN THE EASTERN CORNER OF THE SITE, WHERE THE EXISTING STORMWATER BASIN IS LOCATED. HERE, THE STORMWATER BASIN WILL BE RECONSTRUCTED INTO AN INFILTRATION BASIN. PRETREATMENT FOR THE INFILTRATION SYSTEMS WILL BE PROVIDED BY OVERSIZED SUMP STRUCTURES EQUIPPED WITH PRESERVER SCOUR PREVENTION DEVICES.

**SOIL INFORMATION**  
IN MARCH 2014, INDEPENDENT TESTING TECHNOLOGIES DRILLED FOUR SOIL BORINGS TO APPROXIMATE NEAR SURFACE SOILS. THESE BORINGS INDICATED ONSITE SOILS CONSIST PRIMARILY OF SILTY SAND. THESE SOILS FALL WITHIN THE HYDROLOGIC SOIL GROUP (HSG) 'B'. ADDITIONALLY, IN FEBRUARY 2014, BRAUN INTEREC DRILLED FOUR SOIL BORINGS ON THE KWIK TRIP SITE. THESE BORINGS INDICATED SOILS CONSISTING PRIMARILY OF SANDY CLAY. THESE SOILS FALL WITHIN THE HSG 'C'. FOR THE PURPOSES OF THIS ANALYSIS, THE HSG 'C' WILL BE USED TO STAY CONSISTENT WITH THE ORIGINAL KWIK TRIP STORMWATER STUDY.

**WETLAND CONSIDERATIONS**  
THERE ARE NO KNOWN WETLANDS ONSITE.

**STORMWATER RECEIVING WATERS**  
ALL STORMWATER FROM THE SITE WILL ULTIMATELY DISCHARGE ACROSS KEATS AVENUE TO MNDOT'S STORMWATER POND.

**SPECIAL/IMPAIRED WATER CONSIDERATIONS**  
THERE ARE THREE IMPAIRED LAKES LOCATED WITHIN ONE MILE OF THE SITE. MARKGRAFS LAKE IS LOCATED APPROXIMATELY 3,200 FEET SOUTHEAST OF THE SITE. GOOSE LAKE IS LOCATED APPROXIMATELY 2,700 FEET NORTH OF THE SITE. KRAMER LAKE IS LOCATED APPROXIMATELY 2,000 FEET EAST OF THE SITE.

ADDITIONAL BMP'S REQUIRED INCLUDE: SOIL STABILIZATION WITHIN 7 DAYS AFTER CONSTRUCTION ACTIVITY HAS CEASED, TEMPORARY SEDIMENT BASIN FOR COMMON DRAINAGE AREAS OF 5 ACRES OR MORE, AND A REQUIRED WATER QUALITY VOLUME OF 1" OF RUNOFF FROM NEW IMPERVIOUS SURFACES.

**STORMWATER MANAGEMENT PLAN**  
PER VALLEY BRANCH WATERSHED DISTRICT, 1.1 INCHES OF RUNOFF OVER THE NEW IMPERVIOUS SURFACES SHALL BE RETAINED ONSITE. BASED ON 2.81 ACRES OF NEW IMPERVIOUS SURFACE, THE REQUIRED INFILTRATION VOLUME IS 0.258 ACRE FEET.

THE THREE INFILTRATION SYSTEMS PROPOSED FOR THE SITE WILL PROVIDE 0.275 ACRE FEET OF INFILTRATION VOLUME. THE SYSTEMS ALSO PROVIDE FOR RATE CONTROL FOR THE SITE. SEE STORMWATER MANAGEMENT PLAN FOR DETAILS.

**PRIOR TO START OF CONSTRUCTION**  
THE FOLLOWING STORMWATER POLLUTION PREVENTION MEASURES SHALL BE IMPLEMENTED PRIOR TO CONSTRUCTION. REFER TO GRADING AND EROSION CONTROL PLANS FOR LOCATIONS.

- SILT FENCE**  
SILT FENCE SHALL BE INSTALLED AT THE LIMIT OF GRADING ON ANY FILL SLOPE. ADDITIONAL SILT FENCE MAY BE REQUIRED IN CUT SLOPE AREAS. SILT FENCE SHALL ALSO BE INSTALLED AROUND ANY INFILTRATION/FILTRATION PRACTICE.
- ROCK CONSTRUCTION ENTRANCE**  
ROCK CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE FIELD ENTRANCES TO THE SITE.
- CATCH BASINS**  
ALL CATCH BASINS SHALL BE PROTECTED WITH INLET PROTECTION DEVICES APPROVED BY THE LOCAL GOVERNING UNIT. THESE SHALL INCLUDE, BUT ARE NOT LIMITED TO, WMCO PROTECTION DEVICES, INFRASAFE PROTECTION DEVICES, FILTER FABRIC, BIO ROLLS AND STRAW BALES.

**DURING CONSTRUCTION**  
THE FOLLOWING STORMWATER POLLUTION PREVENTION MEASURES SHALL BE IMPLEMENTED DURING CONSTRUCTION. REFER TO GRADING AND EROSION CONTROL PLANS FOR LOCATIONS.

- PHASED GRADING**  
TO THE EXTENT POSSIBLE, GRADING SHALL BE PHASED TO MINIMIZE THE AMOUNT OF DISTURBED AREAS DURING SITE CONSTRUCTION.
- TRACKED SEDIMENT**  
ANY SEDIMENT TRACKED FROM THE SITE ONTO THE STREET SHALL BE REMOVED IMMEDIATELY UPON DETECTION. THE ROCK CONSTRUCTION ENTRANCE SHALL BE INSPECTED AND REPAIRED IF INUNDATED WITH SEDIMENT.
- STOCKPILES**  
STOCKPILES SHALL BE PLACED IN AN AREA THAT WILL MINIMIZE THE NEED FOR RELOCATION. IF A STOCKPILE WILL REMAIN IN PLACE FOR AN EXTENDED PERIOD OF TIME, STABILIZATION MEASURES SHALL BE IMPLEMENTED, INCLUDING BUT NOT LIMITED TO, SEEDING AND SILT FENCING. TEMPORARY STOCKPILES MUST HAVE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS AND CANNOT BE PLACED IN SURFACE WATERS, INCLUDING STORMWATER CONVEYANCES SUCH AS CURB AND GUTTER SYSTEMS, CONDUITS OR DITCHES.
- TOPSOIL**  
UPON GRADING COMPLETION, A MINIMUM OF 4 INCHES OF TOPSOIL SHALL BE PLACED OVER ALL DISTURBED AREAS, EXCLUDING PROPOSED STREETS AND PARKING AREAS.
- RESTORATION**  
ALL DISTURBED AREAS NOT ACTIVELY WORKED SHALL BE RESTORED WITH SEED AND MULCH, EROSION CONTROL BLANKET AND/OR SOD WITHIN 7 DAYS.
- SLOPES**  
IN ORDER TO MAINTAIN SHEET FLOW AND MINIMIZE RILLS AND/OR GULLIES, THERE SHALL BE NO UNBROKEN SLOPE LENGTH OF GREATER THAN 75 FEET FOR SLOPES WITH A GRADE OF 3:1 OR STEEPER.
- DRAINAGE DITCHES**  
THE NORMAL WETTED PERIMETER OF ANY TEMPORARY OR PERMANENT DRAINAGE DITCH THAT DRAINS WATER FROM THE SITE, OR DIVERTS WATER AROUND THE SITE, MUST BE STABILIZED WITHIN 200 LINEAL FEET FROM THE PROPERTY EDGE, OR FROM THE POINT OF DISCHARGE TO ANY SURFACE WATER. STABILIZATION MUST BE COMPLETED WITHIN 24 HOURS OF CONNECTING TO A SURFACE WATER.
- PIPE OUTLETS**  
PIPE OUTLETS MUST BE PROVIDED WITH TEMPORARY OR PERMANENT ENERGY DISSIPATION WITHIN 24 HOURS OF CONNECTION TO A SURFACE WATER.
- CATCH BASINS**  
ALL CATCH BASINS SHALL BE PROTECTED WITH INLET PROTECTION DEVICES APPROVED BY THE LOCAL GOVERNING UNIT. THESE SHALL INCLUDE, BUT ARE NOT LIMITED TO, WMCO PROTECTION DEVICES, INFRASAFE PROTECTION DEVICES, FILTER FABRIC, BIO ROLLS AND STRAW BALES.
- DUST**  
CONSTRUCTION DUST SHALL BE CONTAINED TO THE EXTENT POSSIBLE. IF THE SITE BECOMES EXCESSIVELY DUSTY, APPROPRIATE MEASURES SHALL BE TAKEN TO REDUCE DUST BEING TRANSPORTED FROM THE SITE. DUST CONTROL MEASURES INCLUDE, BUT ARE NOT LIMITED TO, WATERING AND CALCIUM CHLORIDE APPLICATION.
- DEWATERING**  
DEWATERING ACTIVITIES SHALL BE CONDUCTED WITH AND APPROVED BY THE LOCAL GOVERNING UNIT. IF THERE WILL BE ANY DEWATERING OR BASIN DRAINING THAT MAY HAVE TURBID OR SEDIMENT LADEN DISCHARGE, THE WATER MUST BE DISCHARGED TO A TEMPORARY OR PERMANENT SEDIMENTATION BASIN ON THE PROJECT SITE WHENEVER POSSIBLE. APPROPRIATE BMP'S SHALL BE USED FOR EROSION AND SEDIMENT CONTROL AND ENERGY DISSIPATION.
- CONSTRUCTION MATERIALS AND DEBRIS**  
CONSTRUCTION MATERIALS SHALL BE STORED IN AN ORDERLY MANNER AND IN AN AREA THAT WILL MINIMIZE CONFLICTS WITH OTHER CONSTRUCTION ACTIVITIES. CONSTRUCTION DEBRIS SHALL BE CONTAINED IN DUMPSTERS AND REMOVED FROM THE SITE AS NECESSARY.
- CHEMICALS**  
CHEMICALS SHALL BE STORED IN A SAFE AREA IN SEALED CONTAINERS WITH THE ORIGINAL LABELING AND MATERIAL SAFETY DATA SHEETS AVAILABLE.

- SPILLS AND CONTAMINATION  
IF FUEL, OIL OR A HAZARDOUS CHEMICAL IS SPILLED OR DETECTED DURING CONSTRUCTION ACTIVITIES, ALL APPROPRIATE AGENCIES SHALL BE IMMEDIATELY NOTIFIED, INCLUDING, BUT NOT LIMITED TO, THE MINNESOTA DUTY OFFICER AT 800-422-0798.
- CONCRETE WASHOUT AREA  
ALL LIQUID AND SOLID WASTES GENERATED BY CONCRETE WASHOUT OPERATIONS MUST BE CONTAINED IN A LEAK-PROOF CONTAINMENT FACILITY OR IMPERMEABLE LINER. AN IMPERMEABLE COMPACTED CLAY LAYER IS SUFFICIENT. CONCRETE WASHOUT IN THE AGGREGATE ROAD BASE IS ALLOWED. A SIGN MUST BE INSTALLED AT EACH WASHOUT FACILITY TO DIRECT EQUIPMENT OPERATORS TO THE APPROPRIATE LOCATION.

**POST CONSTRUCTION**  
WHEN THE SITE HAS BEEN COMPLETELY CONSTRUCTED, THE SITE MUST UNDERGO FINAL STABILIZATION. FINAL STABILIZATION OCCURS WHEN ALL OF THE GRADING, INFRASTRUCTURE AND BUILDING ACTIVITIES HAVE BEEN COMPLETED. TO ACHIEVE FINAL STABILIZATION, THE FOLLOWING MEASURES SHALL BE COMPLETED.

- ALL DISTURBED AREAS WITHOUT PERMANENT IMPERVIOUS SURFACES SHALL BE STABILIZED BY A UNIFORM PERENNIAL VEGETATIVE COVER. AREAS NOT REQUIRING SOD OR EROSION CONTROL BLANKET SHALL BE SEEDED AND MULCHED.
- SEDIMENT FROM CONVEYANCES AND TEMPORARY SEDIMENTATION BASINS THAT ARE TO BE USED AS PERMANENT WATER QUALITY MANAGEMENT BASINS SHALL BE CLEANED OUT. SEDIMENTATION BASINS SHALL BE SUFFICIENTLY CLEANED OUT TO RETURN THE BASIN TO DESIGN CAPACITY. SEDIMENT MUST BE STABILIZED TO PREVENT IT FROM BEING WASHED BACK INTO THE BASIN OR CONVEYANCES DISCHARGING OFF-SITE OR TO SURFACE WATERS.
- WHEN STABILIZED VEGETATION HAS BEEN ESTABLISHED OVER 70 PERCENT OF THE PEROVIOUS SURFACE AREA, ALL SYNTHETIC TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED. THIS INCLUDES, BUT IS NOT LIMITED TO, SILT FENCE, TREE FENCE AND CATCH BASIN INLET PROTECTION DEVICES.

**NOTICE OF TERMINATION**  
THE PERMITEE(S) MUST SUBMIT A NOTICE OF TERMINATION (NOT) TO THE MPCA WITHIN 30 DAYS AFTER FINAL STABILIZATION IS COMPLETE, OR ANOTHER OWNER/OPERATOR (PERMITEE) HAS ASSUMED CONTROL OVER ALL AREAS OF THE SITE THAT HAVE NOT UNDERGONE FINAL STABILIZATION.

**INSPECTIONS & RECORD KEEPING**  
STORMWATER POLLUTION PREVENTION INSPECTIONS SHALL OCCUR ONCE EVERY SEVEN (7) DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER A RAINFALL EVENT GREATER THAN 0.5 INCHES IN 24 HOURS. INSPECTIONS MAY BE CEASED DURING FROZEN GROUND CONDITIONS. WHERE WORK HAS BEEN SUSPENDED DUE TO FROZEN GROUND CONDITIONS, THE REQUIRED INSPECTIONS AND MAINTENANCE MUST TAKE PLACE WITHIN 24 HOURS AFTER RUNOFF OCCURS AT THE SITE OR PRIOR TO RESUMING CONSTRUCTION, WHICHEVER COMES FIRST. DURING THE COURSE OF CONSTRUCTION, IT MAY BE DETERMINED THAT ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES MAY BE NEEDED OR CERTAIN MEASURES ARE NOT PRACTICAL TO INSTALL. IN THESE CASES, AN AMENDMENT TO THE SWPPP SHALL BE MADE, AND SUPPORTING REASONS SHALL BE DOCUMENTED IN THE SWPPP.

- RECORD NAME OF INSPECTOR AND DATE AND TIME OF INSPECTION.
  - RECORD RAINFALL AMOUNT SINCE MOST RECENT INSPECTION.
  - INSPECT GRAVEL CONSTRUCTION ENTRANCES FOR SEDIMENTATION. INSPECT ADJACENT STREETS FOR SEDIMENT TRACKING.
  - INSPECT SITE FOR EXCESSIVE EROSION AND SEDIMENT ACCUMULATION.
    - INSPECT SILT FENCE AND OTHER TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES FOR EROSION, SEDIMENTATION AND MALFUNCTIONING.
    - INSPECT FLARED END SECTIONS FOR EROSION AND SEDIMENTATION.
    - INSPECT PONDS, INFILTRATION BASINS, TEMPORARY SEDIMENTATION BASINS AND ALL OTHER BMP'S FOR EROSION AND SEDIMENTATION.
    - INSPECT SURFACE WATERS, INCLUDING DRAINAGE DITCHES AND CONVEYANCE SYSTEMS FOR EVIDENCE OF SEDIMENT BEING DEPOSITED BY EROSION.
  - INSPECT SITE AND ADJACENT PROPERTIES FOR CONSTRUCTION DEBRIS, TRASH AND SPILLS.
  - INSPECT STABILIZED AREAS FOR EROSION.
  - RECORD RECOMMENDED REPAIRS, MAINTENANCE AND/OR REPLACEMENTS REQUIRED TO ENSURE EROSION AND SEDIMENTATION CONTROL MEASURES ARE SUFFICIENT.
  - RECORD RECOMMENDED AMENDMENTS TO THE SWPPP.
  - RECORD REPAIRS, MAINTENANCE AND/OR REPLACEMENTS THAT WERE COMPLETED SINCE THE LAST INSPECTION.
- FOR AREAS THAT HAVE UNDERGONE FINAL STABILIZATION, INSPECTIONS CAN BE REDUCED TO ONCE PER MONTH

**MAINTENANCE**  
THE OWNER/CONTRACTOR IS RESPONSIBLE FOR THE OPERATION, INSPECTION AND MAINTENANCE OF ALL STORMWATER POLLUTION PREVENTION MEASURES FOR THE DURATION OF THE PROJECT. THE FOLLOWING GUIDELINES SHALL BE USED TO DETERMINE NECESSARY REPAIRS, MAINTENANCE AND/OR REPLACEMENT OF THE EROSION AND SEDIMENTATION CONTROL MEASURES.

- ROCK CONSTRUCTION ENTRANCES SHALL BE REPAIRED OR REPLACED IF THE ROCK BECOMES INUNDATED WITH SEDIMENT AND/OR EXCESSIVE SEDIMENT IS BEING TRACKED FROM THE SITE. SEDIMENT TRACKED ONTO ADJACENT STREETS SHALL BE REMOVED. MEASURES SHALL BE TAKEN IMMEDIATELY UPON DISCOVERY.
- SILT FENCE SHALL BE REPAIRED OR REPLACED WHEN SEDIMENT REACHES 1/3 THE HEIGHT OF THE SILT FENCE, THE SILT FENCE IS DAMAGED AND/OR THE SILT FENCE BECOMES NONFUNCTIONAL. MEASURES SHALL BE TAKEN WITHIN 24 HOURS OF DISCOVERY.
- CATCH BASIN INLET PROTECTION DEVICES SHALL BE CLEANED WHEN SEDIMENT REACHES 1/2 THE HEIGHT OF THE SEDIMENT TRAP AND/OR REPAIRED OR REPLACED IF THE DEVICE BECOMES NONFUNCTIONAL. MEASURES SHALL BE TAKEN WITHIN 72 HOURS OF DISCOVERY.
- FLARED END SECTIONS SHALL BE CLEANED IF DEBRIS IS RESTRICTING FLOW OR IF SEDIMENT HAS ACCUMULATED AT THE OUTLET. IF A FLARED END SECTION BECOMES NONFUNCTIONAL OR DAMAGED, IT SHALL BE REPAIRED OR REPLACED. MEASURES SHALL BE TAKEN WITHIN 72 HOURS OF DISCOVERY.
- IF SEDIMENT IS OBSERVED OFF-SITE OR NEAR SURFACE WATERS, THE SOURCE OF SEDIMENT SHALL BE DETECTED AND ADDITIONAL MEASURES SHALL BE IMPLEMENTED. THE PERMITEE(S) SHALL COORDINATE SEDIMENT RETRIEVAL FROM SURFACE WATERS WITH ALL APPROPRIATE AGENCIES. MEASURES SHALL BE TAKEN WITHIN 7 DAYS OF DISCOVERY.
- PONDS, INFILTRATION BASINS, TEMPORARY SEDIMENTATION BASINS AND ALL OTHER BMP'S SHALL BE CLEANED IF DEBRIS IS PRESENT AND/OR EXCESSIVE SEDIMENTATION HAS OCCURRED. TEMPORARY AND PERMANENT SEDIMENTATION BASINS MUST BE DRAINED AND THE SEDIMENT REMOVED WHEN SEDIMENT HAS FILLED THE BASIN TO 1/2 THE STORAGE VOLUME. NO SEDIMENT SHALL BE ALLOWED TO ACCUMULATE IN INFILTRATION BASINS. MEASURES SHALL BE TAKEN WITHIN 72 HOURS OF DISCOVERY.

**QUANTITIES**  
THE FOLLOWING TABLE PROVIDES ESTIMATED QUANTITIES FOR STORMWATER POLLUTION PREVENTION THROUGHOUT THE PROJECT.

ITEM	UNIT	ESTIMATED QUANTITY
ROCK ENTRANCE	EA.	1
SILT FENCE	L.F.	1,000
INLET PROTECTION	EA.	16
EROSION CONTROL BLANKET	S.Y.	350
TURF ESTABLISHMENT	AC.	0.5

**BENCHMARKS**

- MN/DOT GSD STATION NO. 33428  
TOP OF BENCHMARK DISK  
ELEVATION = 974.47 (NAVD 88)
- MN/DOT GSD STATION NO. 33429  
TOP OF BENCHMARK DISK  
ELEVATION = 943.86 (NAVD 88)

**STORMWATER POLLUTION PREVENTION PLAN**

**LAKE ELMO SHOPPES**  
Lake Elmo, Minnesota

**MFC PROPERTIES CORP.**  
3460 Washington Drive, Suite 100  
Eagan, MN 55122

**REVISIONS**

1.	04/26/17	Per City comments.
2.	05/24/17	Per City comments.
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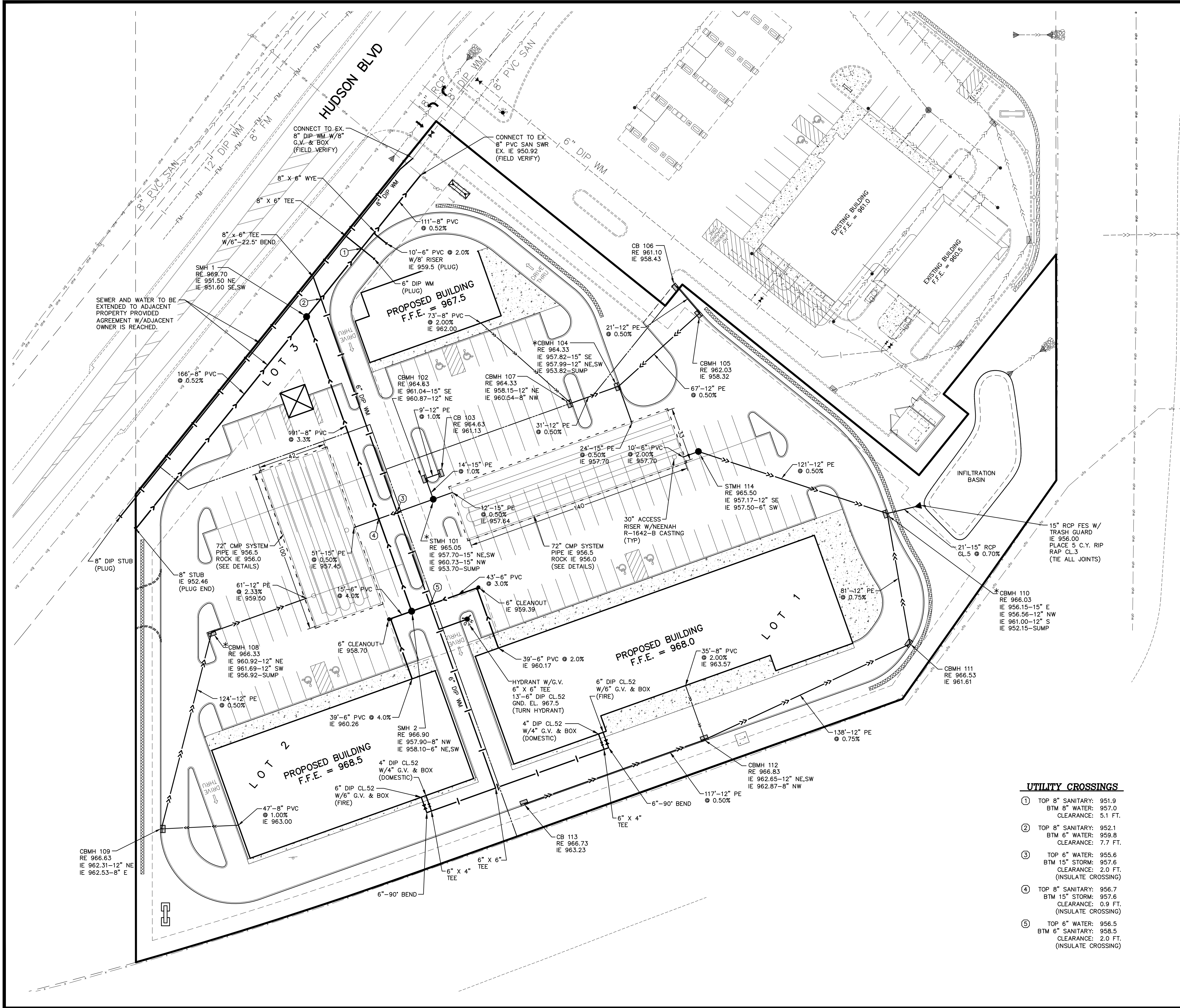
DRAWN BY: JTR  
DESIGNED BY: JTR  
ISSUE DATE: 02/03/17

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the State of Minnesota

Name: Joseph T. Radach, P.E.  
Signature: *Joseph T. Radach*  
Date: 02/03/17 License #: 45889

**Carlson McCain**  
ENVIRONMENTAL ENGINEERING - SURVEYING  
3890 Pheasant Ridge Dr. NE #100, Blaine, MN  
Phone: 763-489-7900 Fax: 763-489-7959





**LEGEND**

	EXISTING	PROPOSED
PROPERTY LINE	---	---
EASEMENT LINE	---	---
CURB LINE	---	---
BITUMINOUS PAVEMENT	---	---
CONCRETE WALK	---	---
SANITARY SEWER	---	---
STORM SEWER	---	---
WATER MAIN	---	---
UNDERGROUND GAS	---	---
UNDERGROUND ELECTRIC	---	---
UNDERGROUND TELEPHONE	---	---
MANHOLE	---	---
HYDRANT	---	---
LIGHT POLE	---	---
FENCE LINE	---	---

**GOVERNING SPECIFICATIONS**

- ALL WORK SHALL CONFORM TO THE CITY OF LAKE ELMO STANDARD SPECIFICATIONS. WHERE SAID SPECIFICATION DOES NOT COVER THE PROPOSED WORK, THE LATEST EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR CONSTRUCTION", THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM) STANDARD SPECIFICATION AND THE MINNESOTA PLUMBING CODE SHALL APPLY.
- THE LATEST EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD).

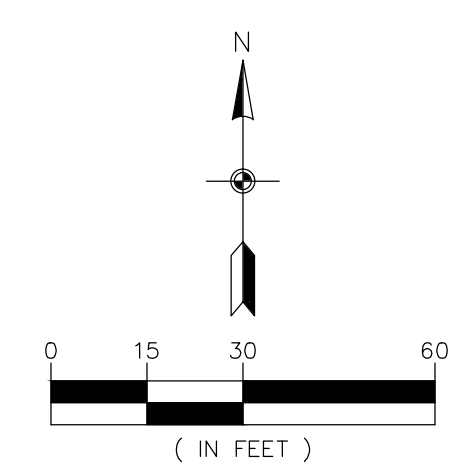
**UTILITY PLAN NOTES**

- SEE SHEET C7 FOR UTILITY NOTES.
- VERIFY LOCATIONS OF SEWER AND WATER SERVICES TO THE PROPOSED BUILDINGS WITH MEP PLANS.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES ON SITE AND TO NOTIFY THE OWNER AND ENGINEER IMMEDIATELY OF ANY DISCREPANCIES FROM THE PLAN.

**STORM SEWER SCHEDULE**

STRUCTURE	NEENAH CASTING or EQUAL
STMH-114	48" DIA. R-1642-B
CB-113	48" DIA. R-3067-VB
CBMH-112	27" DIA. R-3067-VB
CBMH-111	48" DIA. R-3067-V
*CBMH-110	48" DIA. R-3067-V
CBMH-109	48" DIA. R-3067-VB
*CBMH-108	48" DIA. R-3067-VB
CB-107	24" X 36" R-3067-VB
CB-106	24" X 36" R-3067-V
CBMH-105	48" DIA. R-3067-V
*CBMH-104	48" DIA. R-3067-VB
CB-103	27" DIA. R-3067-VB
CBMH-102	48" DIA. R-3067-VB
*STMH-101	48" DIA. R-1642-B

\* INSTALL PRESERVER ENERGY DISSIPATOR ON INLET PIPE AS MANUFACTURED BY MOMENTUM ENVIRONMENTAL, LLC. INSTALL PER MANUFACTURER'S RECOMMENDATIONS.



**BENCHMARKS**

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TOP OF BENCHMARK DISK  
ELEVATION = 974.47 (NAVD 88)
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TOP OF BENCHMARK DISK  
ELEVATION = 943.86 (NAVD 88)

**UTILITY CROSSINGS**

- TOP 8" SANITARY: 951.9  
BTM 8" WATER: 957.0  
CLEARANCE: 5.1 FT.
- TOP 8" SANITARY: 952.1  
BTM 6" WATER: 959.8  
CLEARANCE: 7.7 FT.
- TOP 6" WATER: 955.6  
BTM 15" STORM: 957.6  
CLEARANCE: 2.0 FT. (INSULATE CROSSING)
- TOP 8" SANITARY: 956.7  
BTM 15" STORM: 957.6  
CLEARANCE: 0.9 FT. (INSULATE CROSSING)
- TOP 6" WATER: 956.5  
BTM 6" SANITARY: 958.5  
CLEARANCE: 2.0 FT. (INSULATE CROSSING)

**Carlson McCain**  
ENVIRONMENTAL ENGINEERING - SURVEYING  
3890 Pheasant Ridge Dr. NE #100, Blaine, MN  
Phone: 763-489-7900 Fax: 763-489-7959

**UTILITY PLAN**

**LAKE ELMO SHOPPES**  
Lake Elmo, Minnesota

**MFC PROPERTIES CORP.**  
3460 Washington Drive, Suite 100  
Eagan, MN 55122

**REVISIONS**

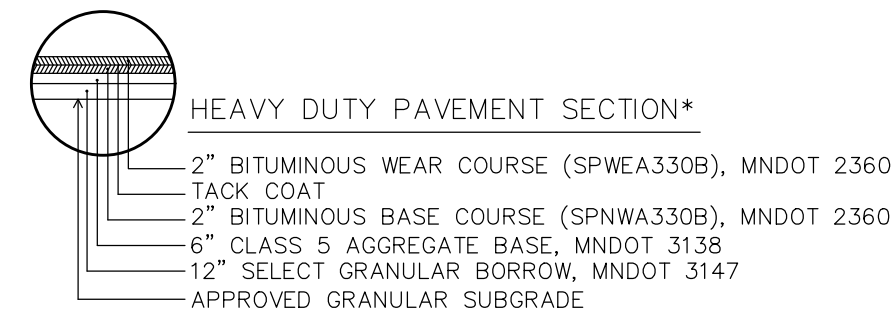
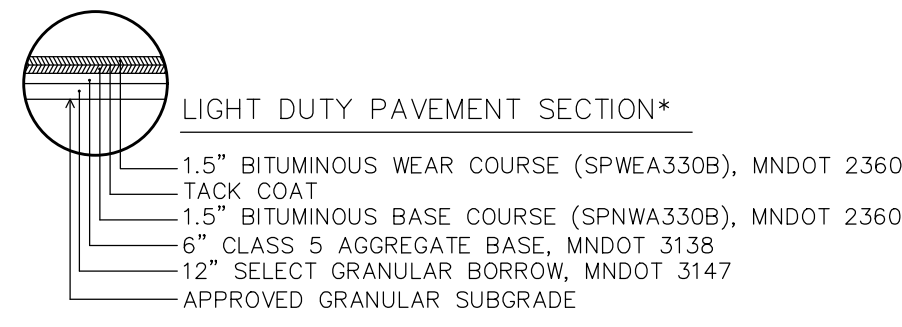
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2.	05/24/17	Per City comments.
3.		
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DRAWN BY: JTR  
DESIGNED BY: JTR  
ISSUE DATE: 02/03/17

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Signature: *[Signature]*  
Date: 02/03/17 License #: 45889

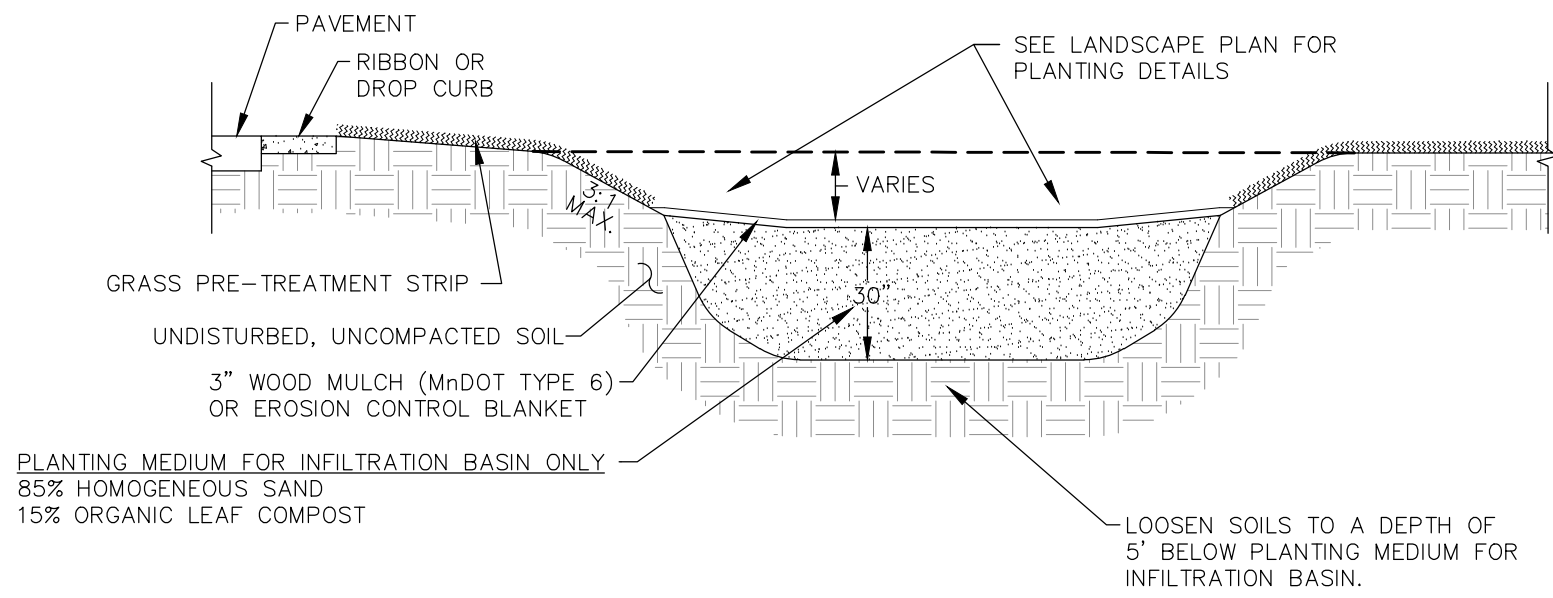
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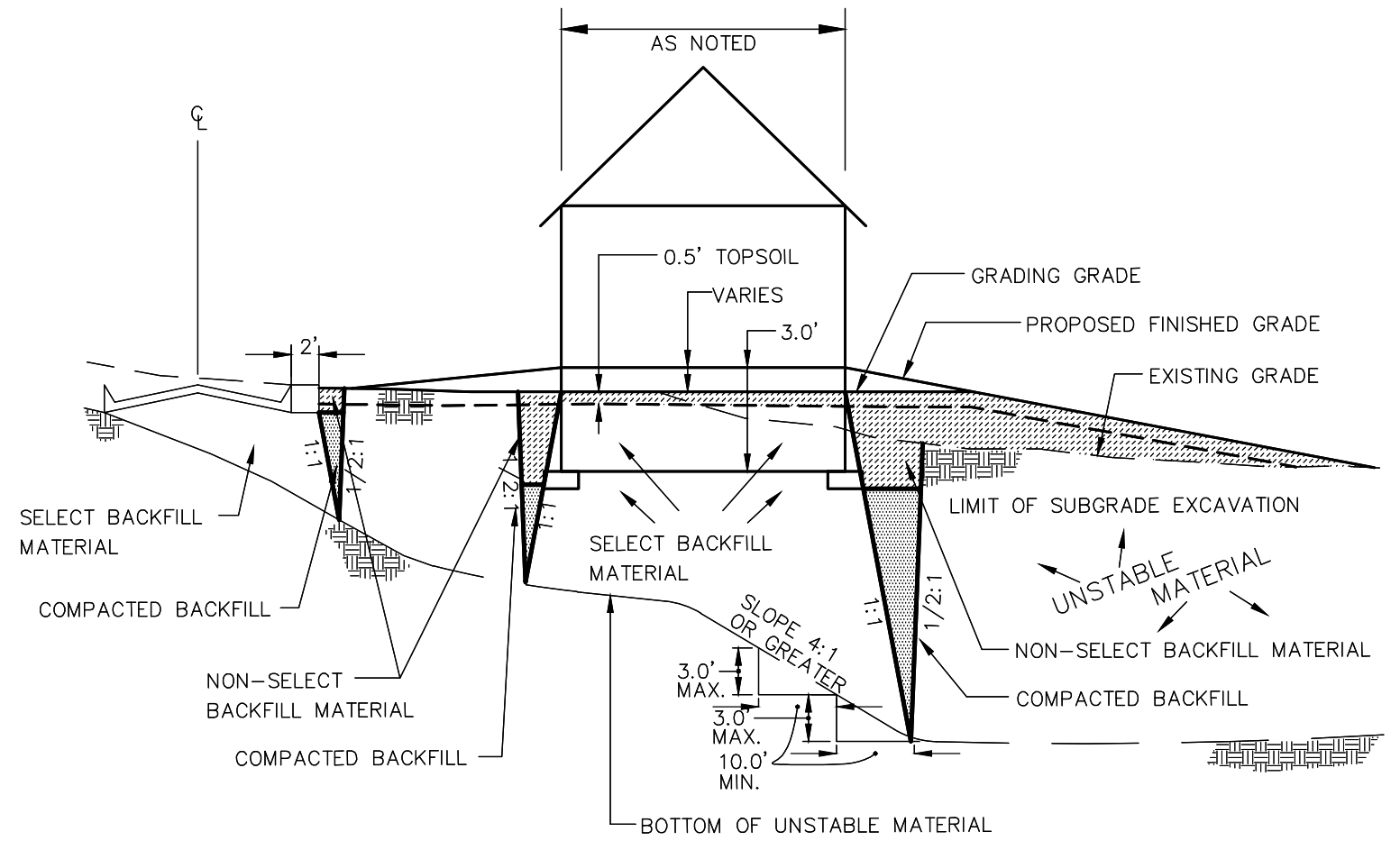
\*FINAL PAVEMENT DESIGN TO BE CONFIRMED BY GEOTECHNICAL ENGINEER OF RECORD.

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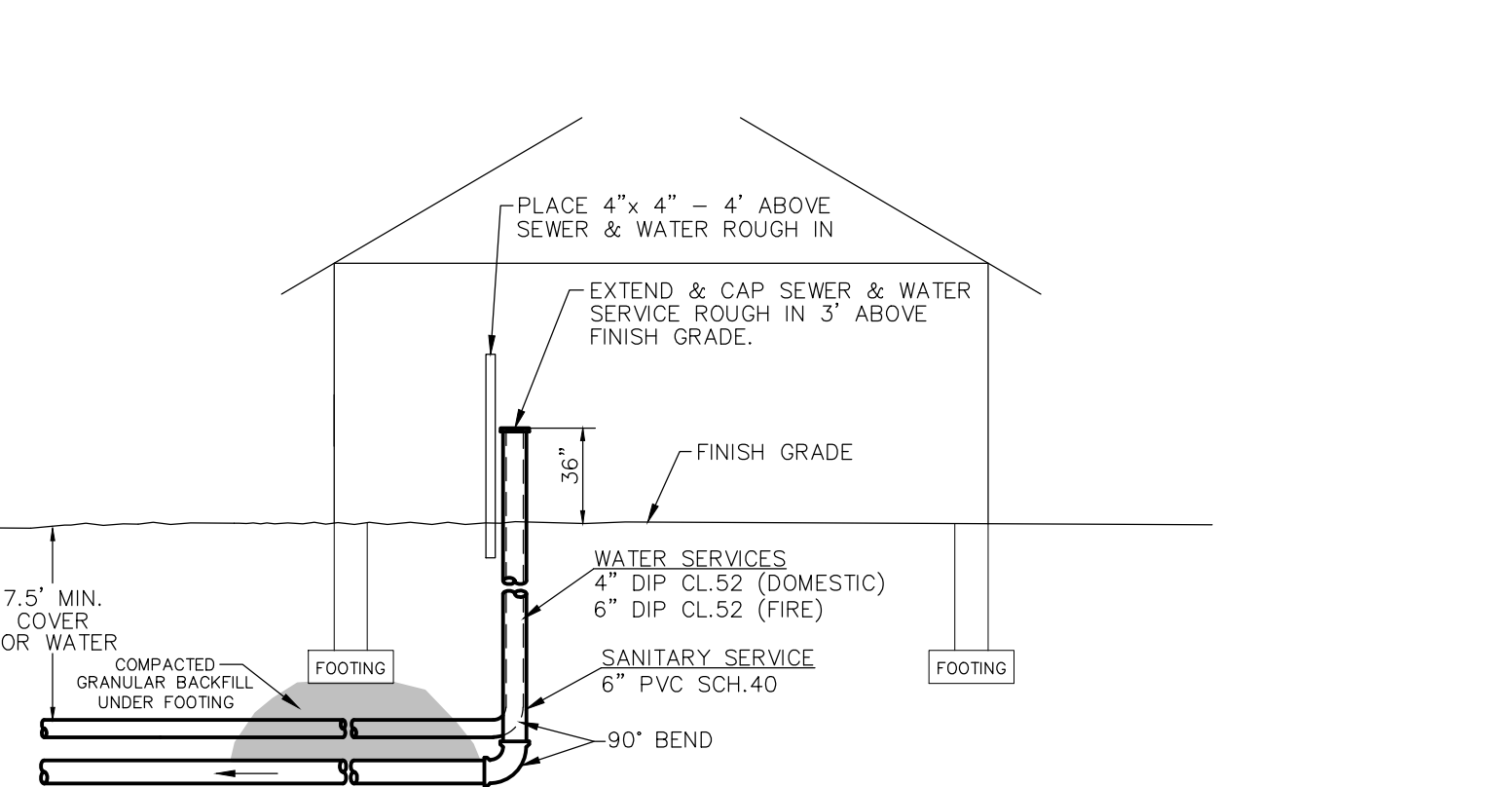
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C7 | NO SCALE



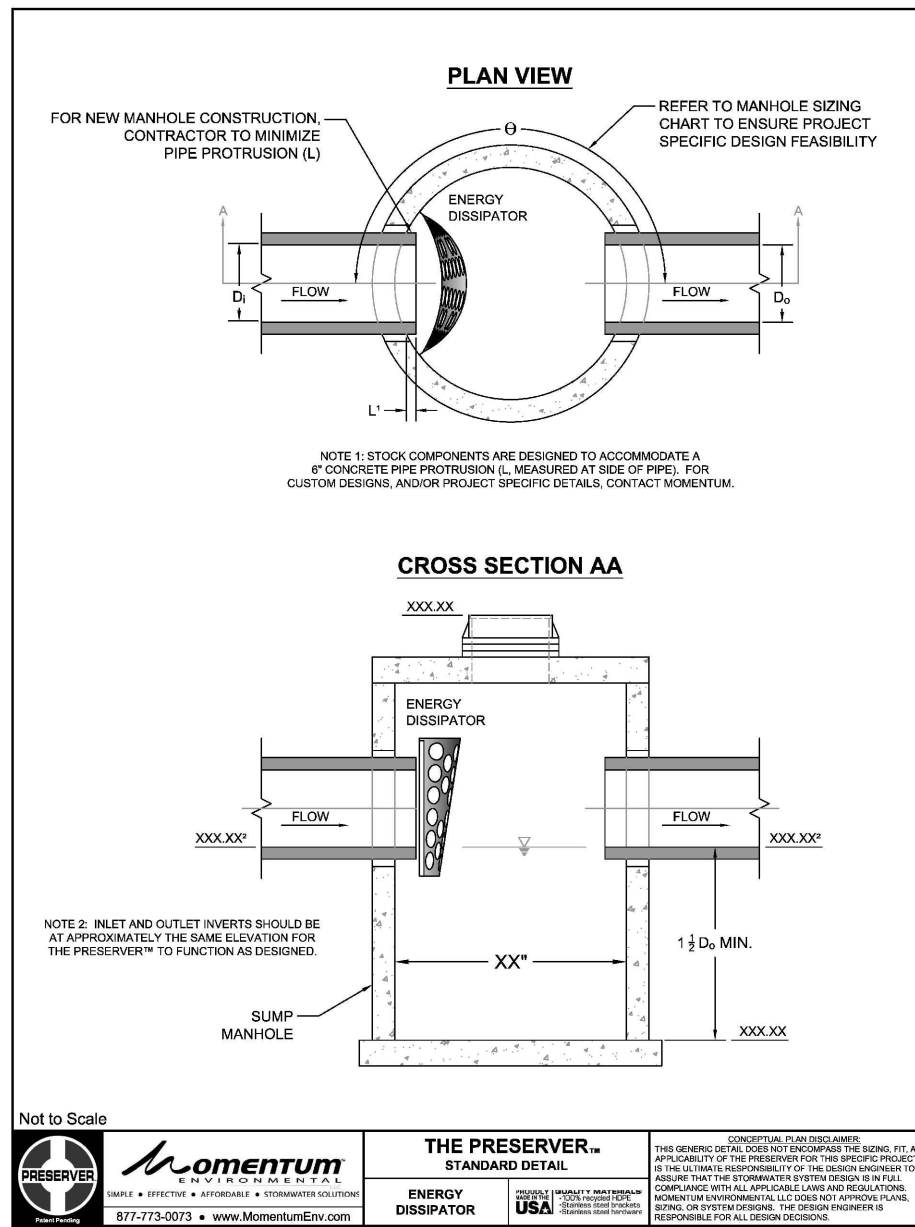
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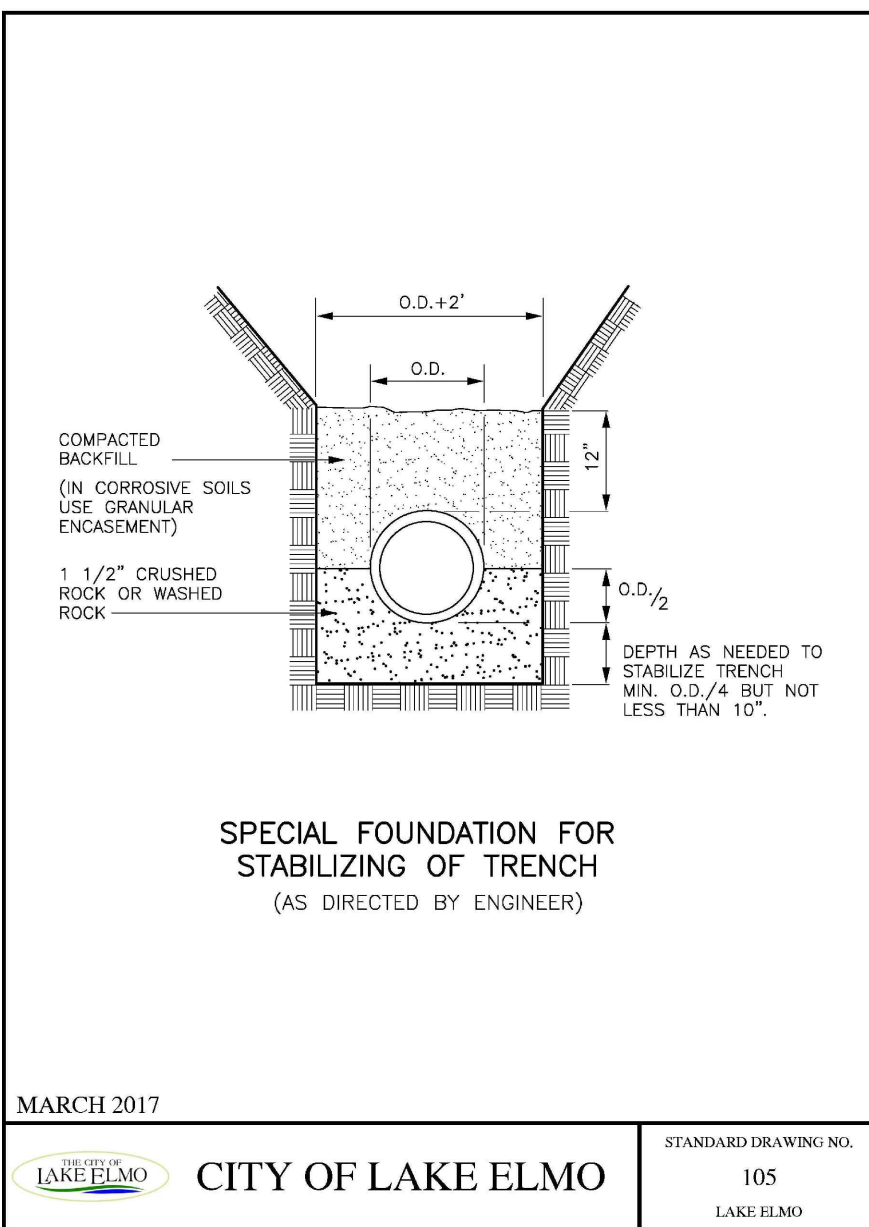
3 | SUBGRADE CORRECTION  
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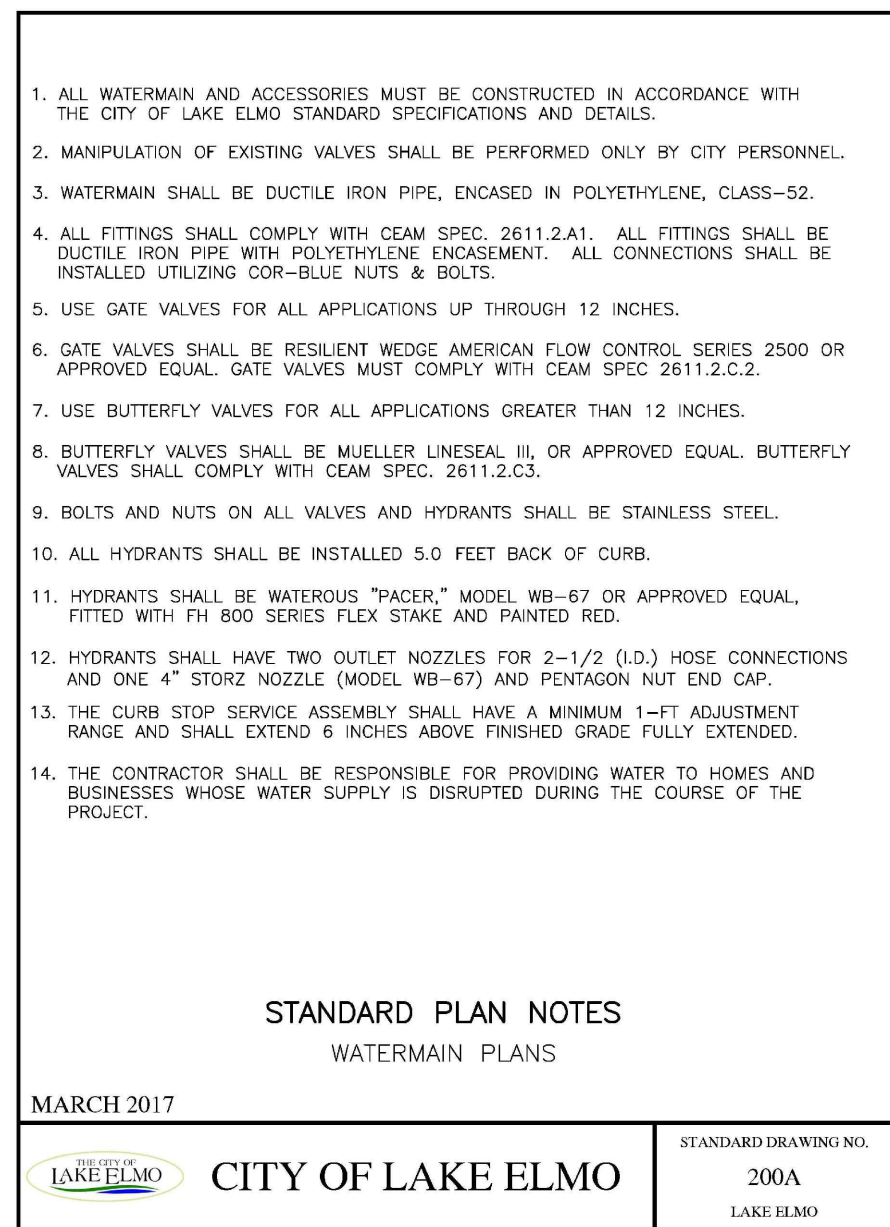
4 | UTILITY SERVICE CONNECTIONS  
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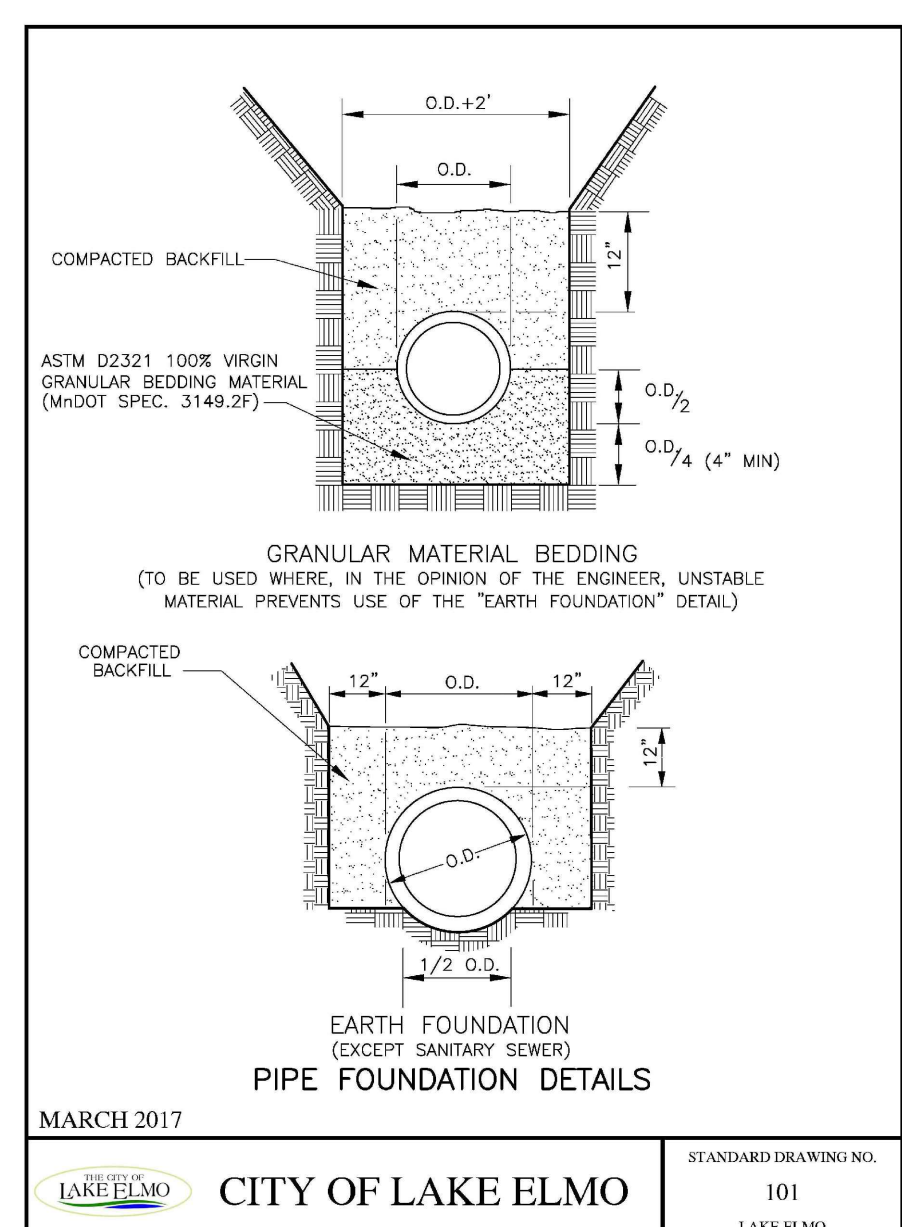
THE PRESERVER... STANDARD DETAIL... ENERGY DISSIPATOR... USA



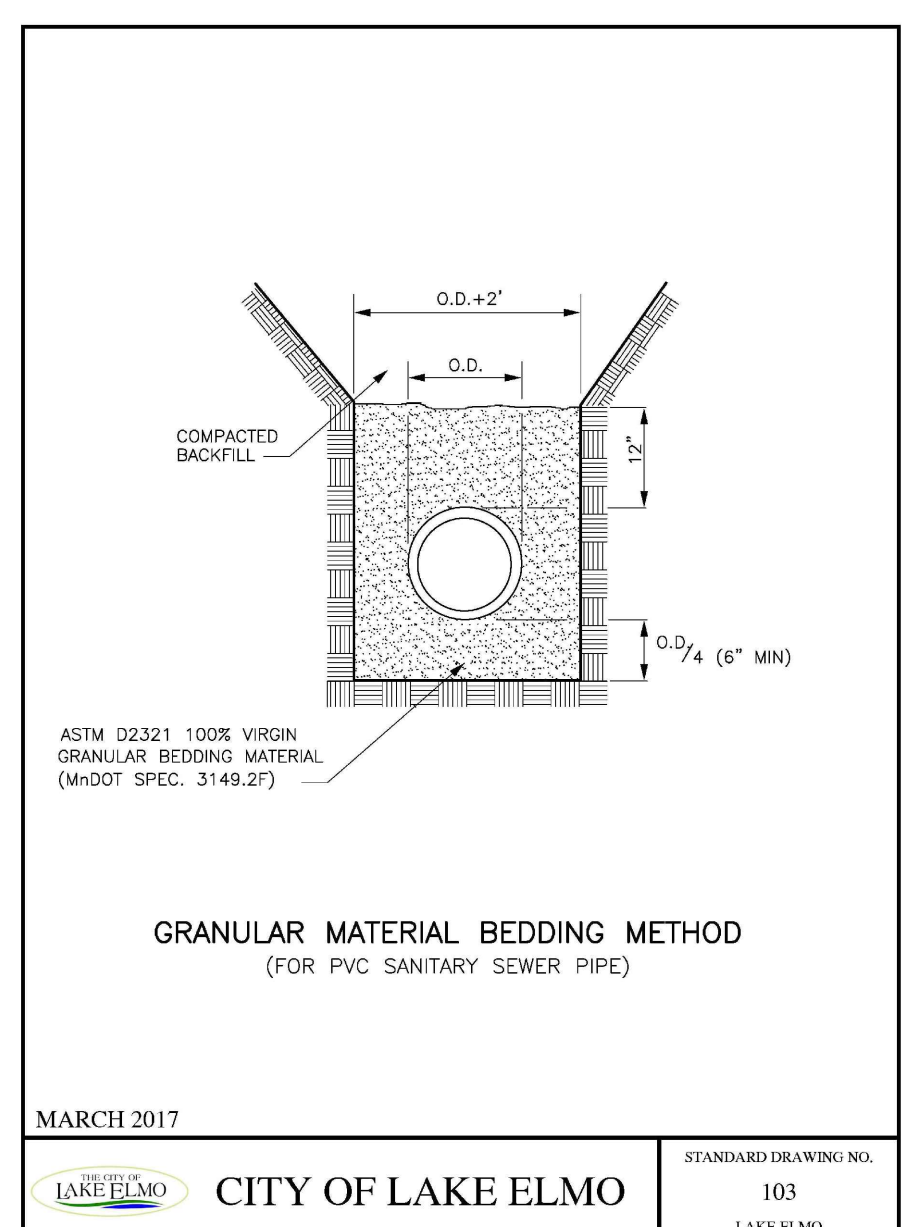
MARCH 2017 CITY OF LAKE ELMO STANDARD DRAWING NO. 105 LAKE ELMO



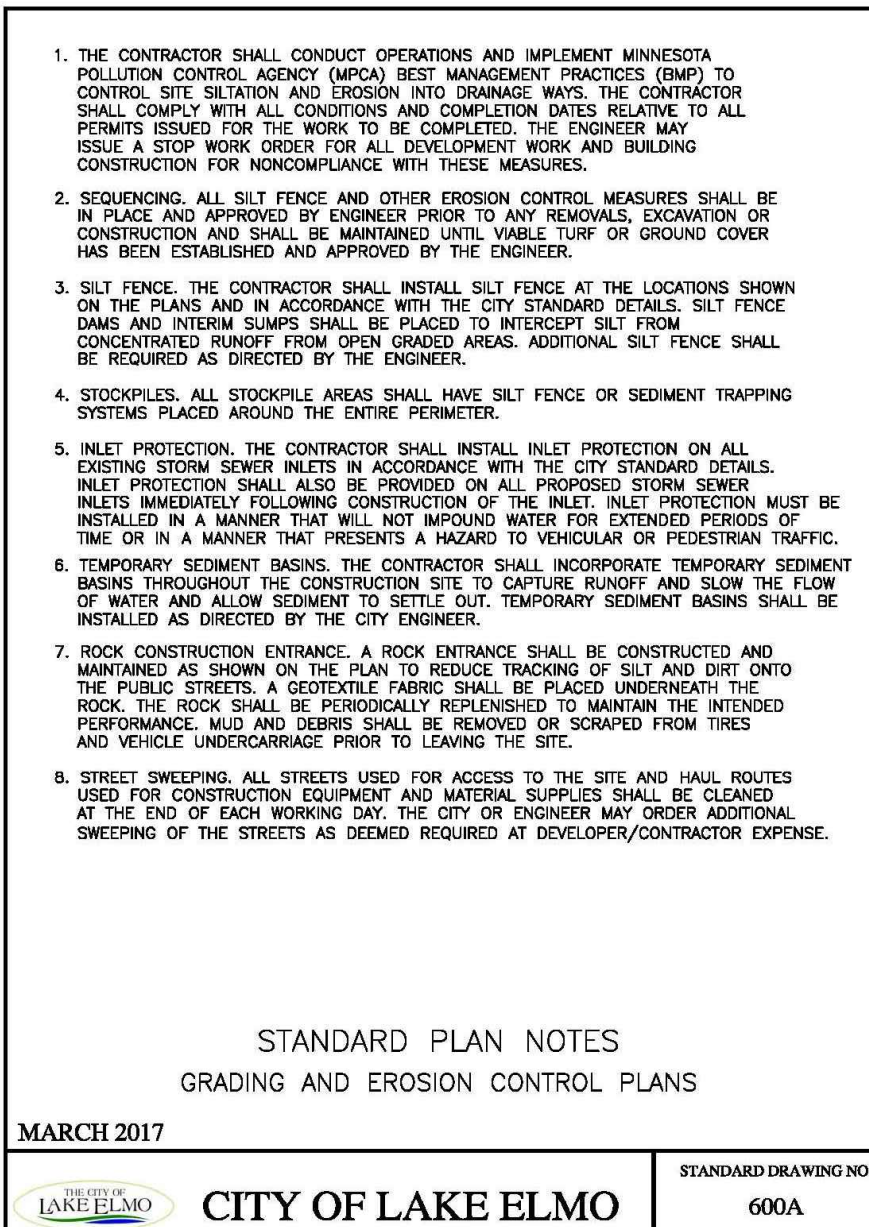
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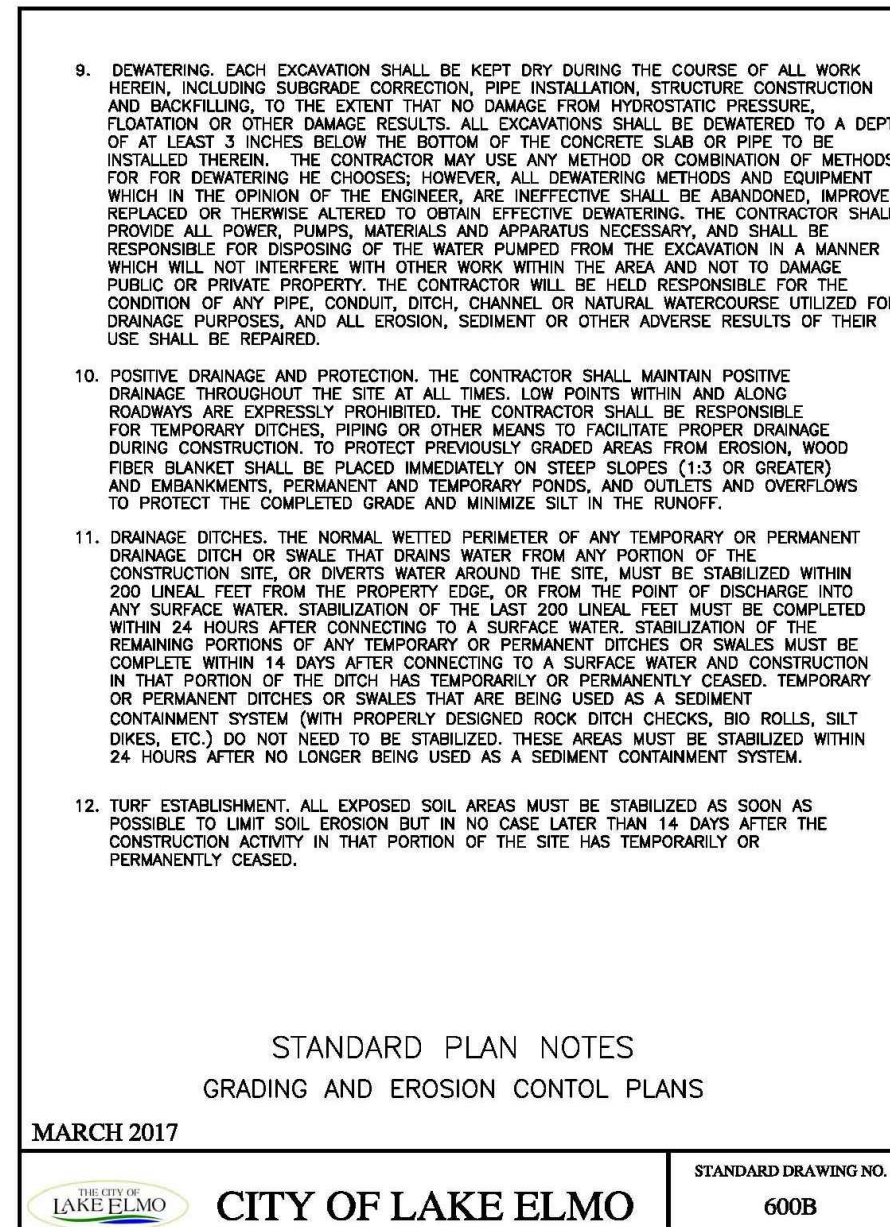
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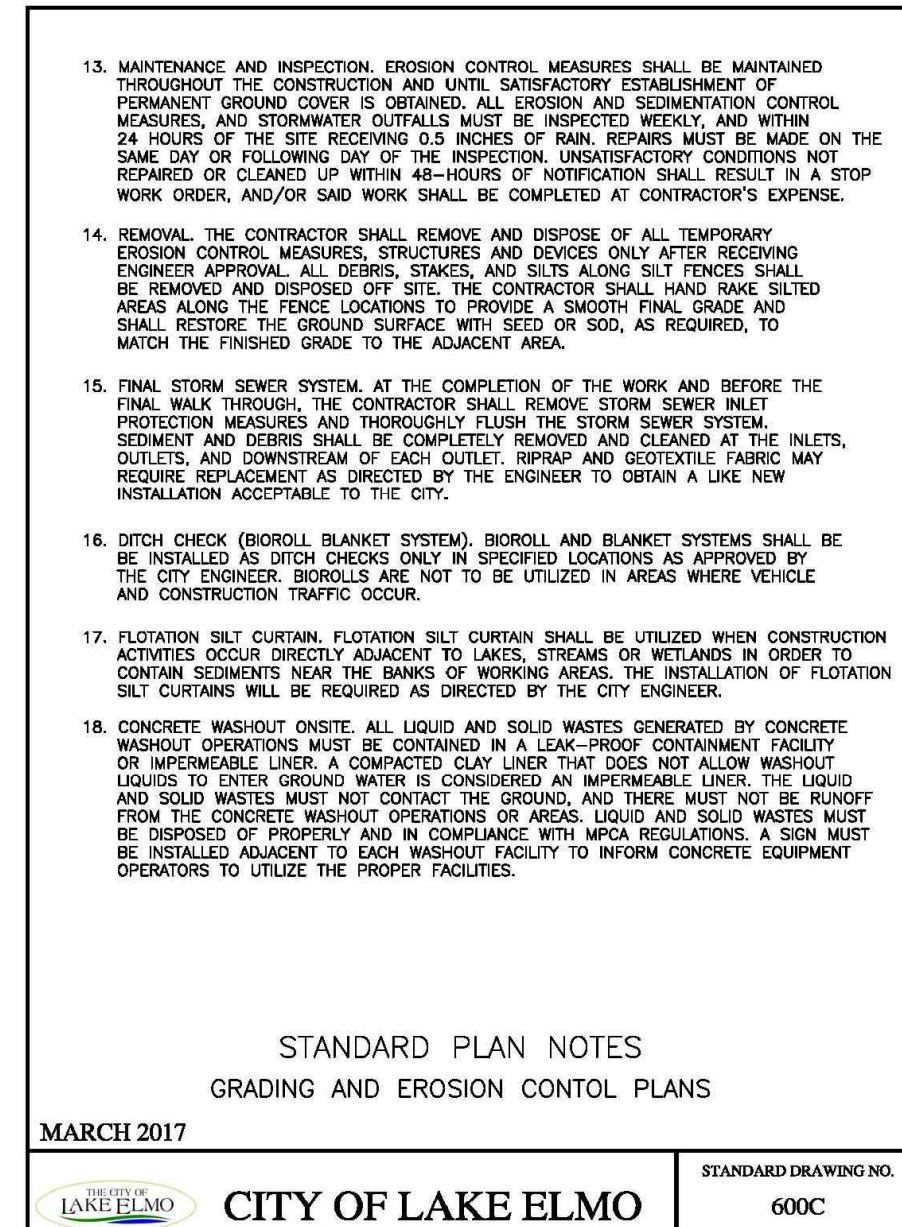
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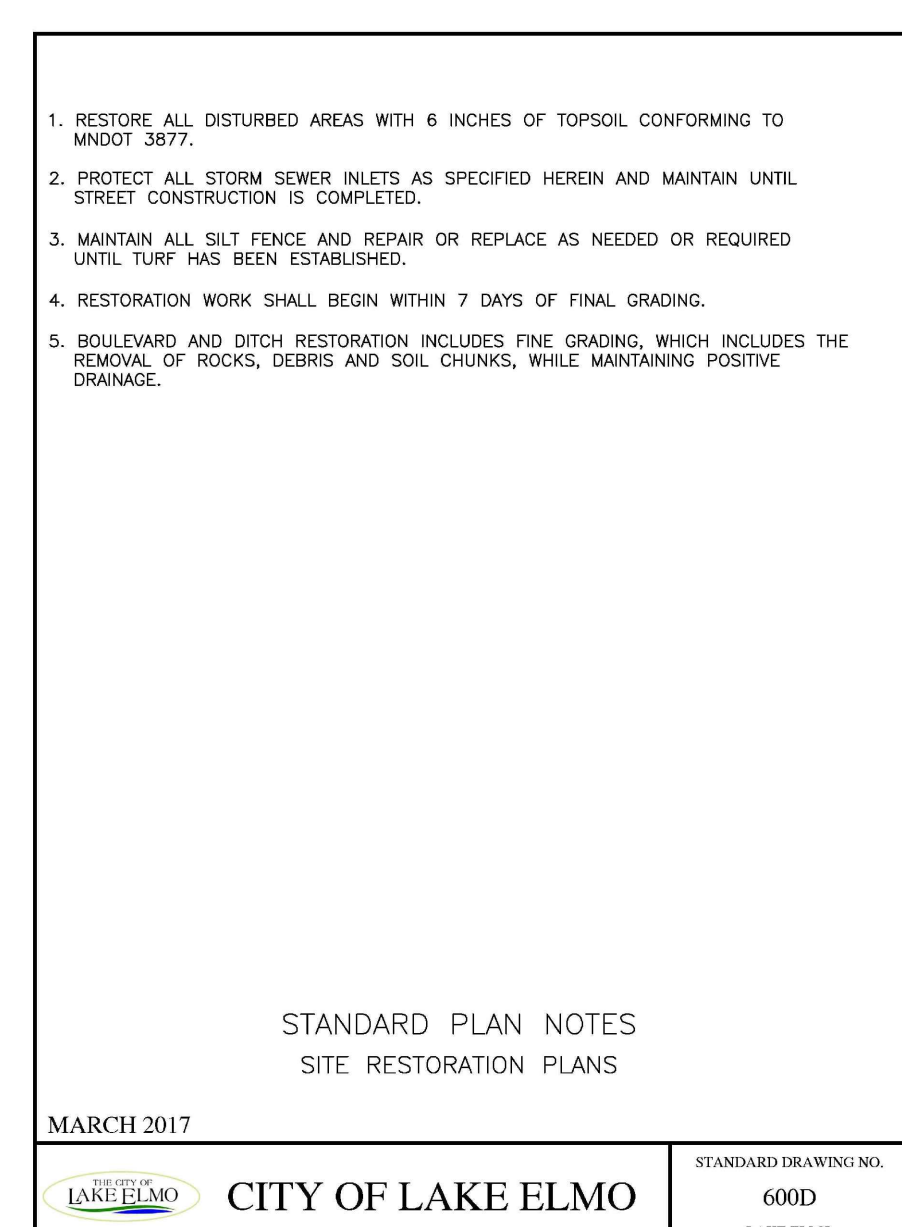
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MARCH 2017 CITY OF LAKE ELMO STANDARD DRAWING NO. 600B LAKE ELMO



MARCH 2017 CITY OF LAKE ELMO STANDARD DRAWING NO. 600C LAKE ELMO



MARCH 2017 CITY OF LAKE ELMO STANDARD DRAWING NO. 600D LAKE ELMO

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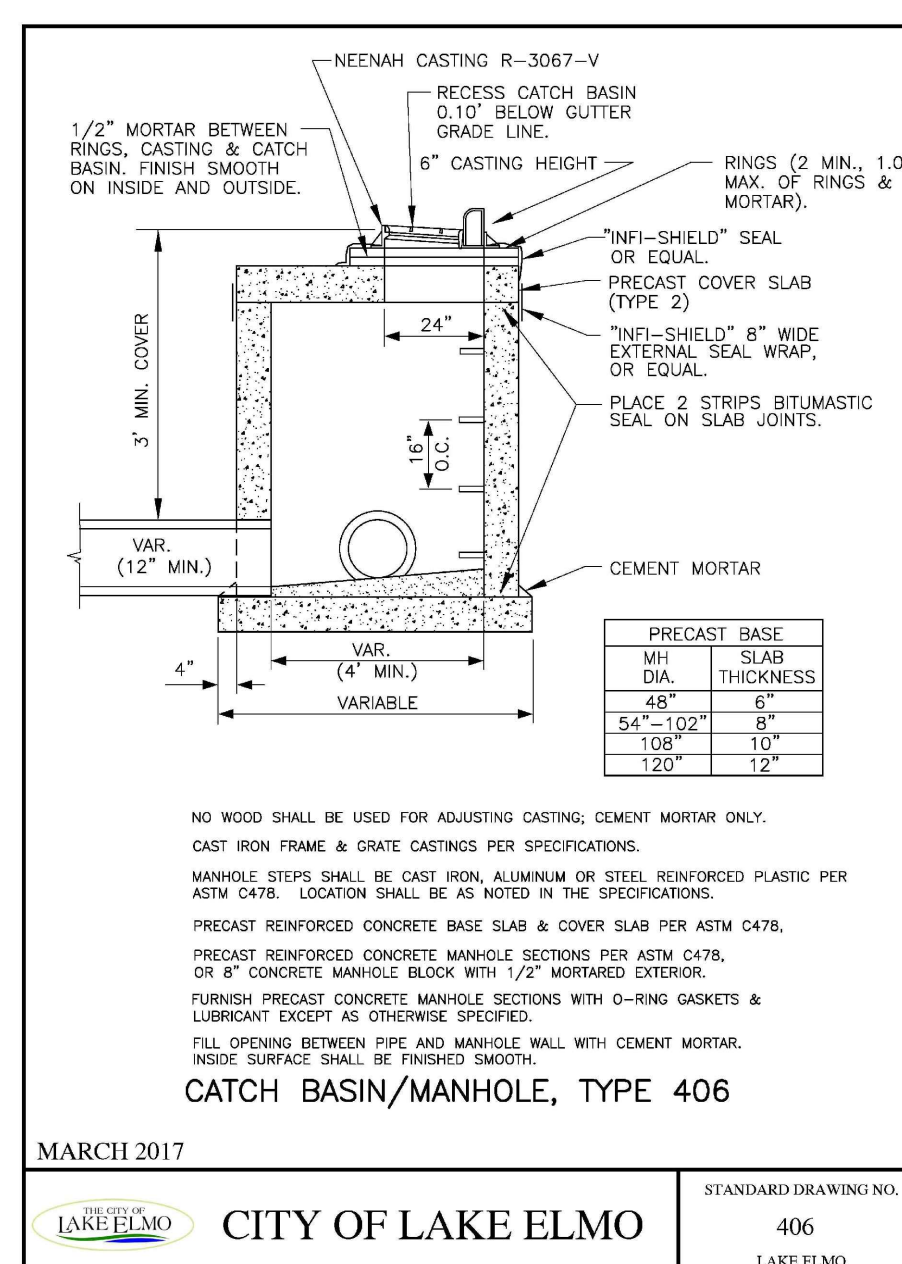
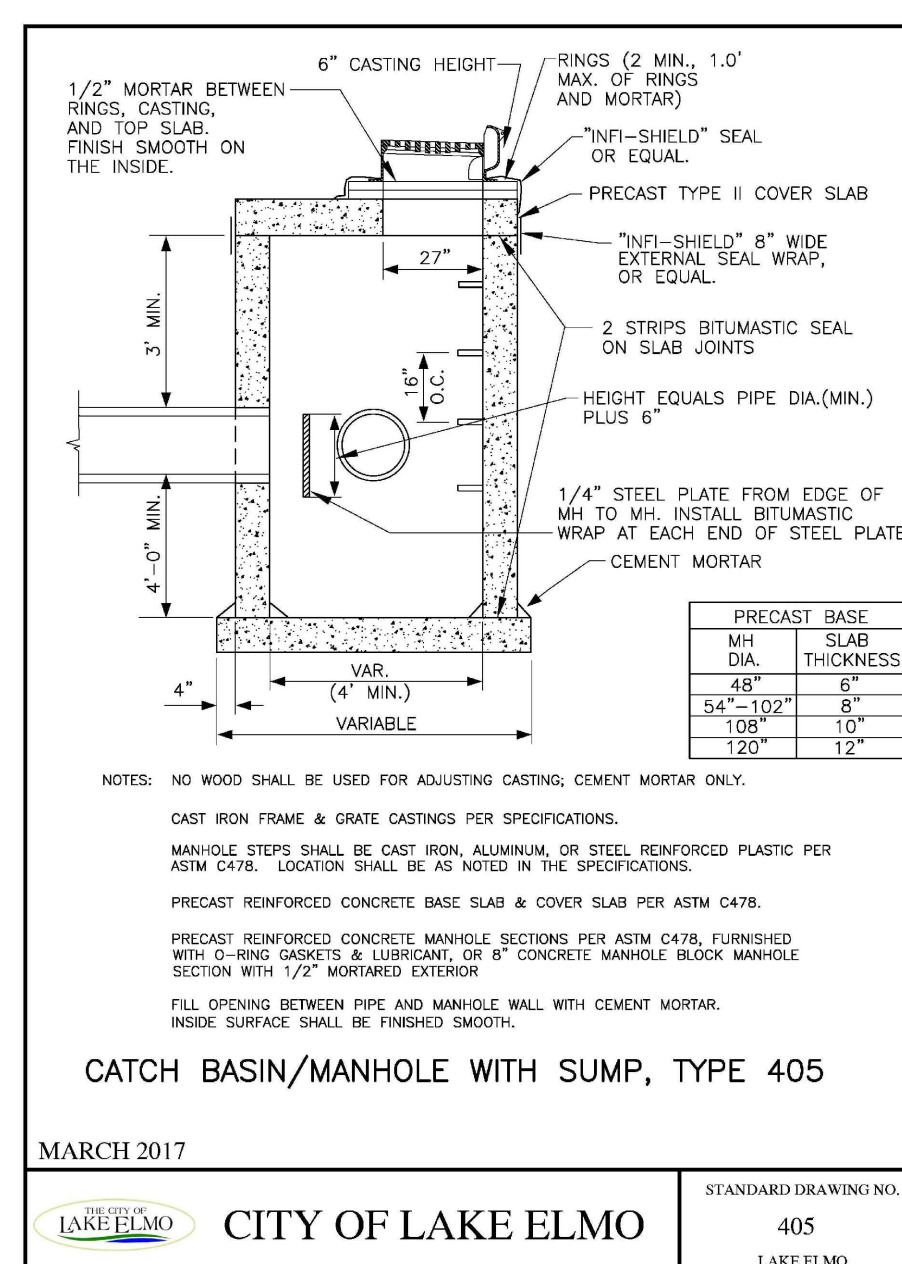
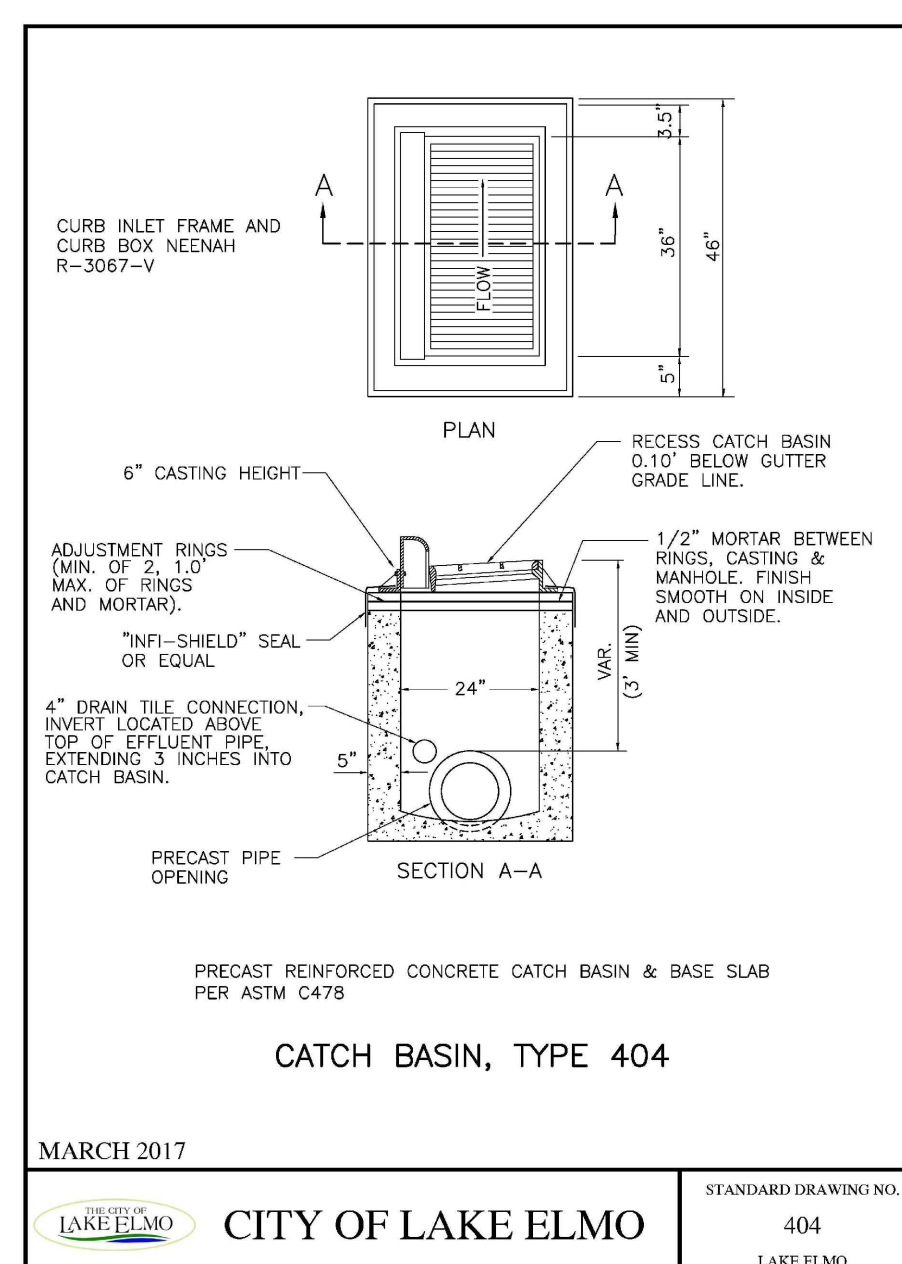
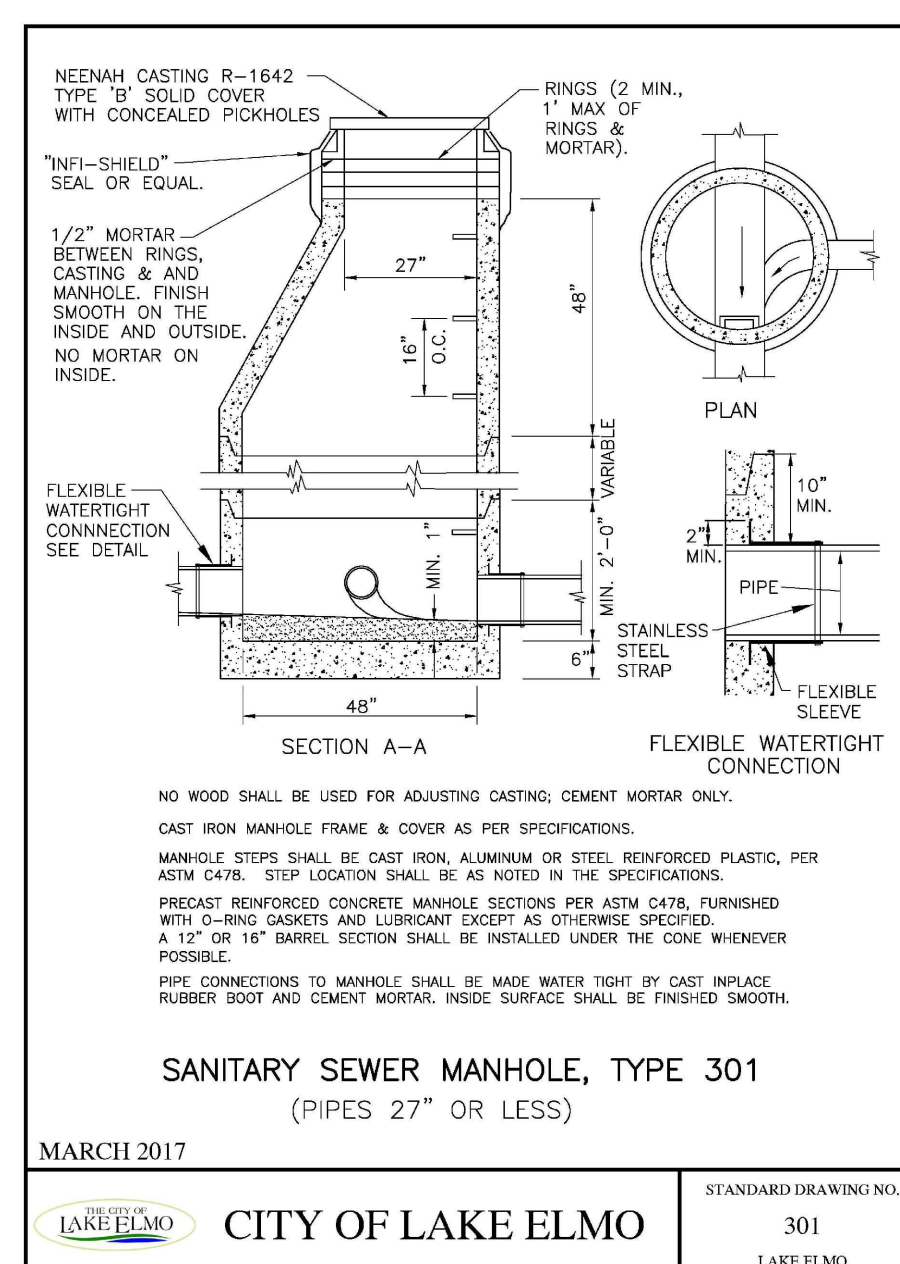
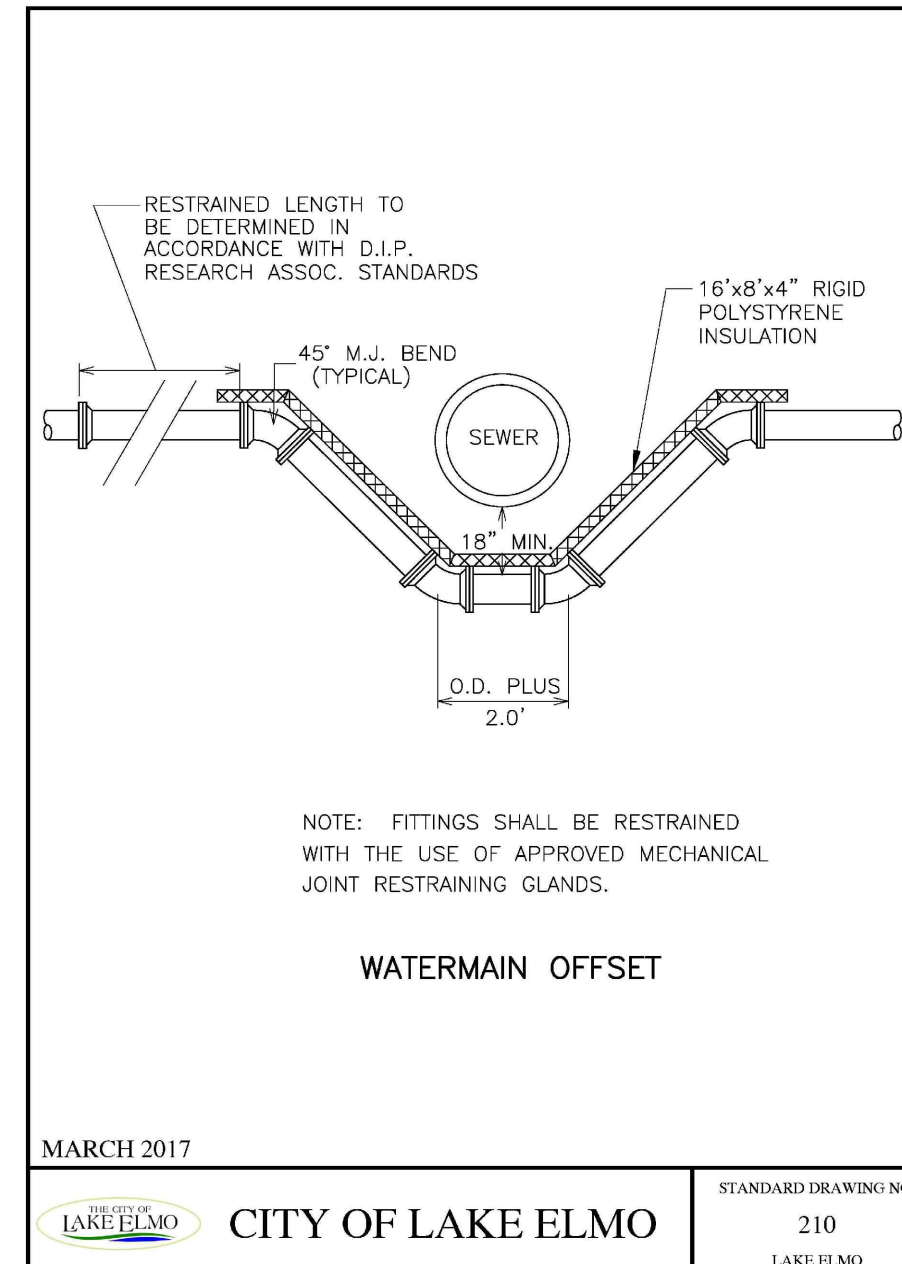
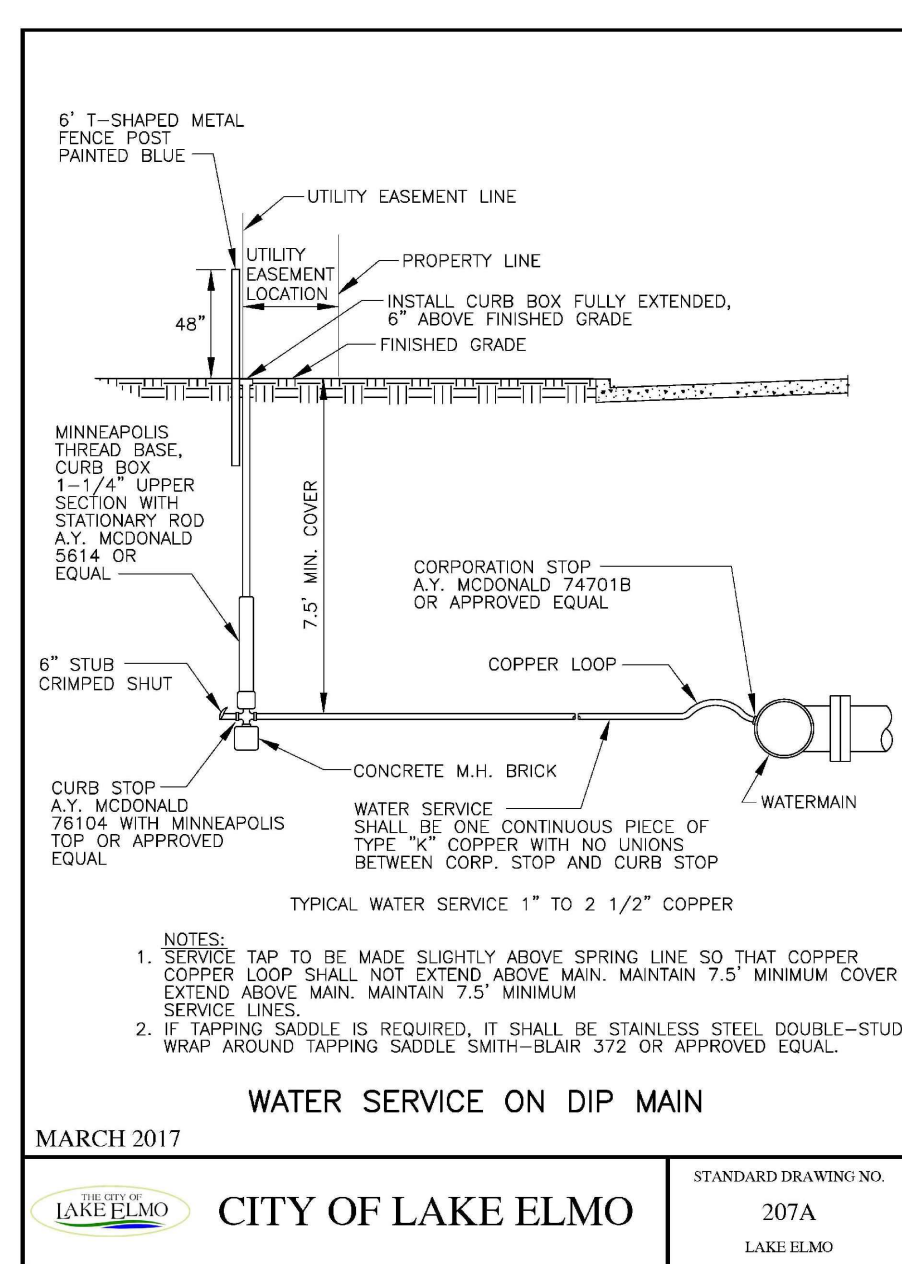
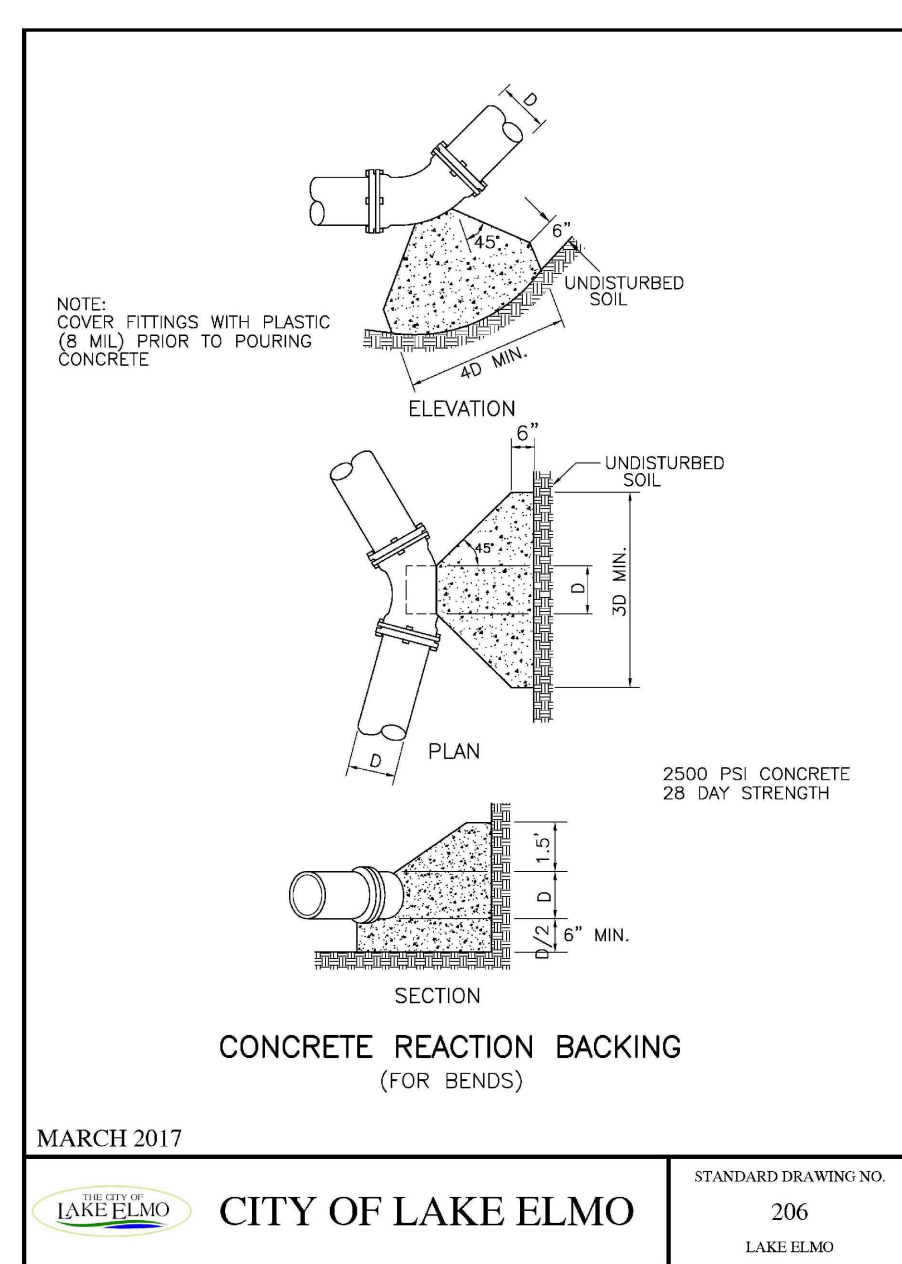
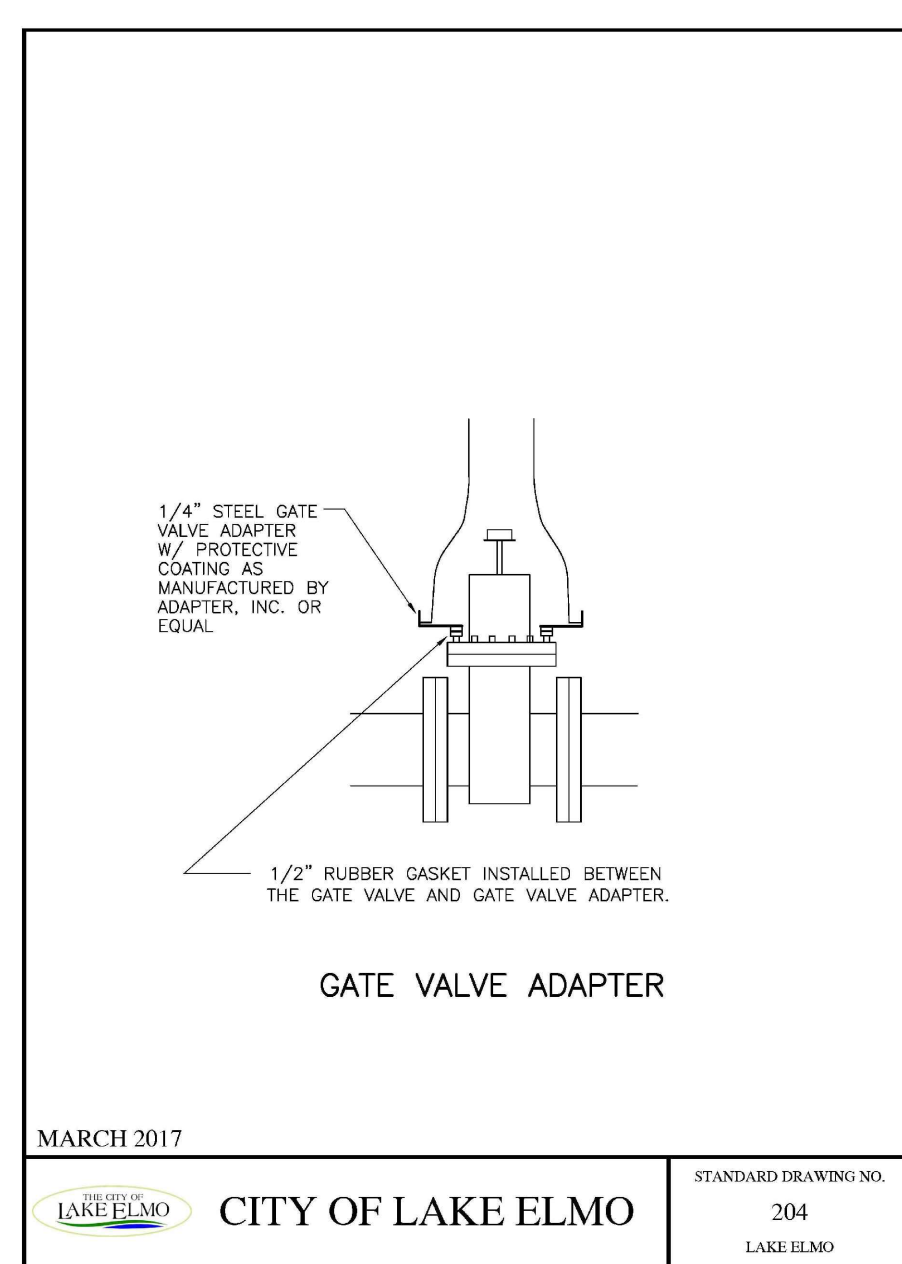
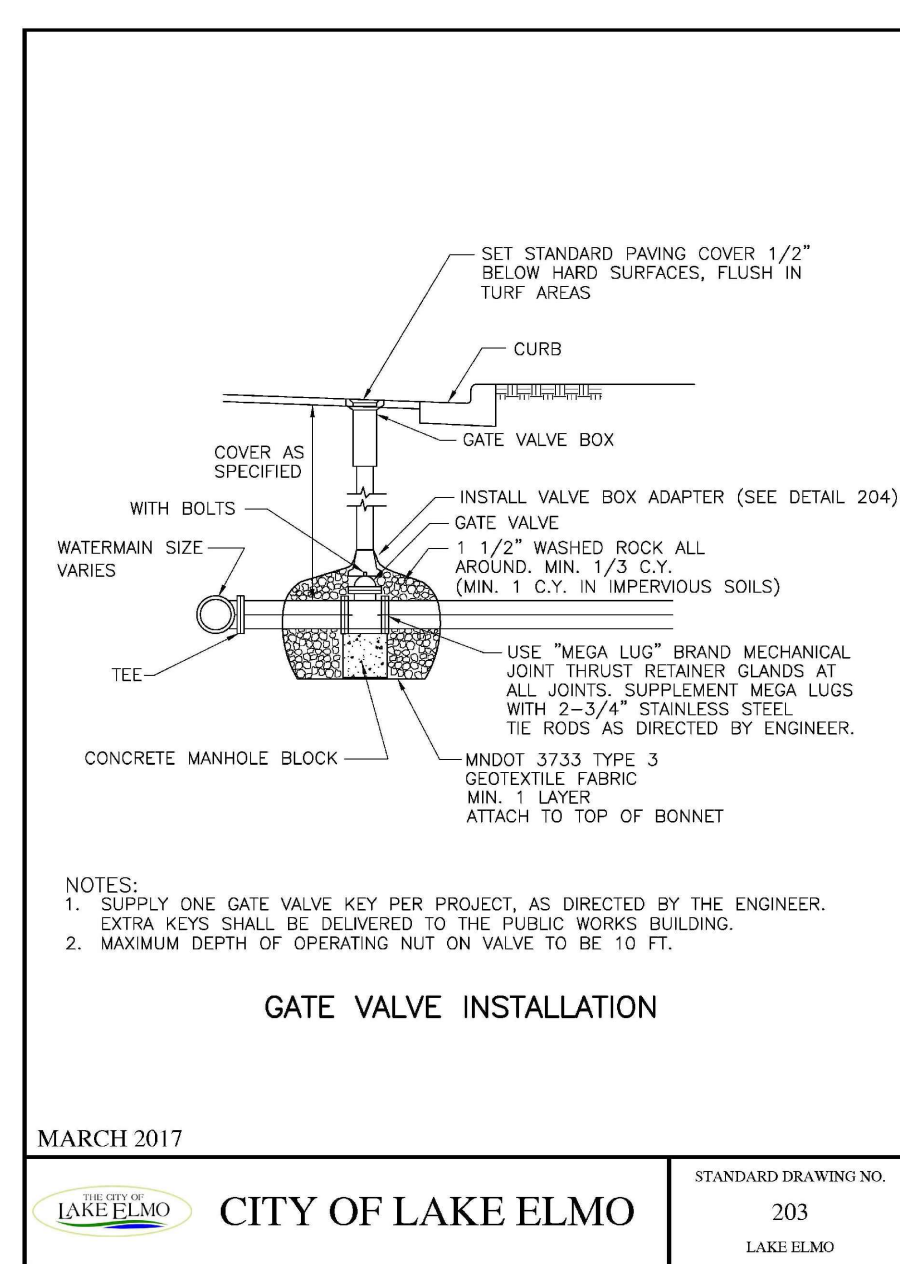
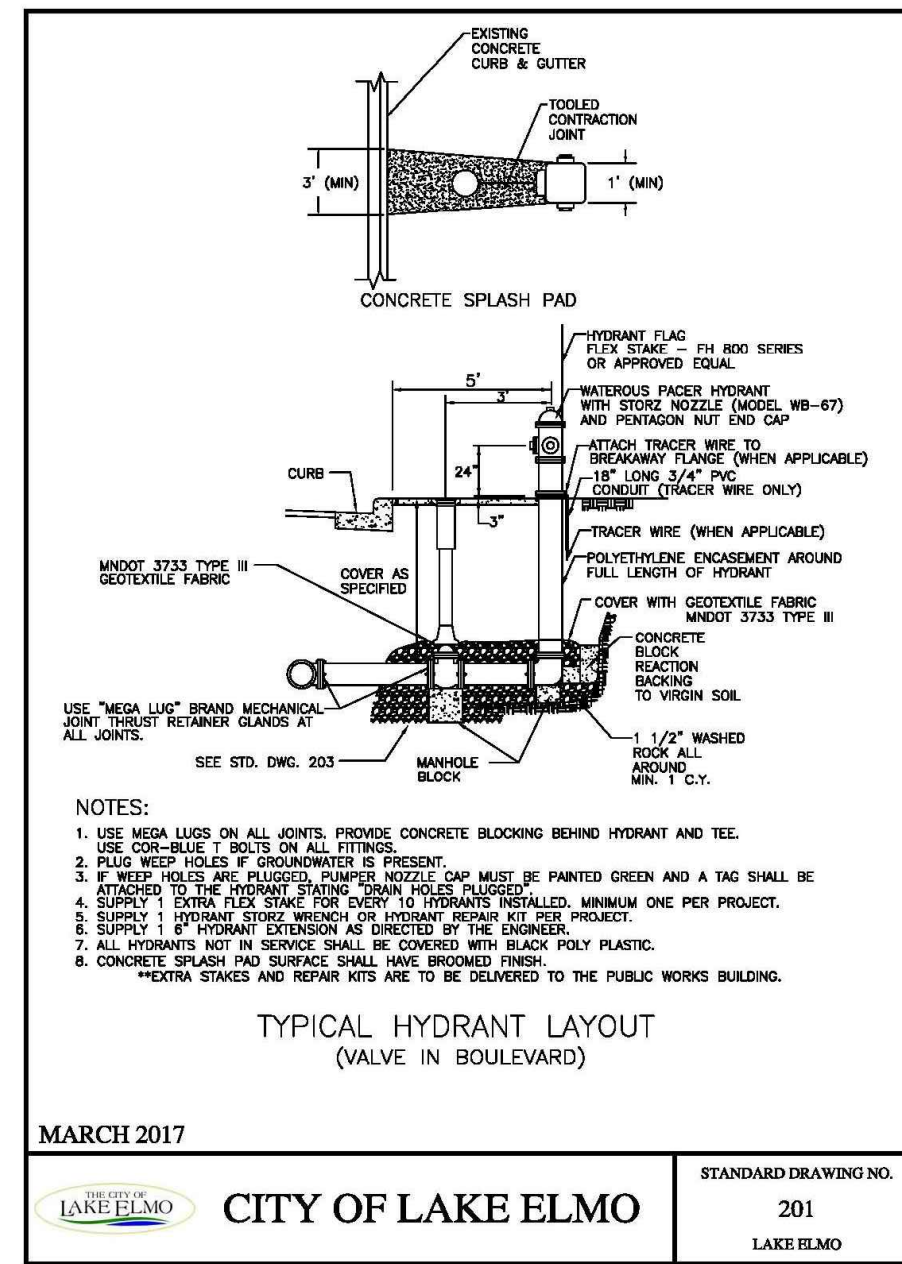
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C7 of 11



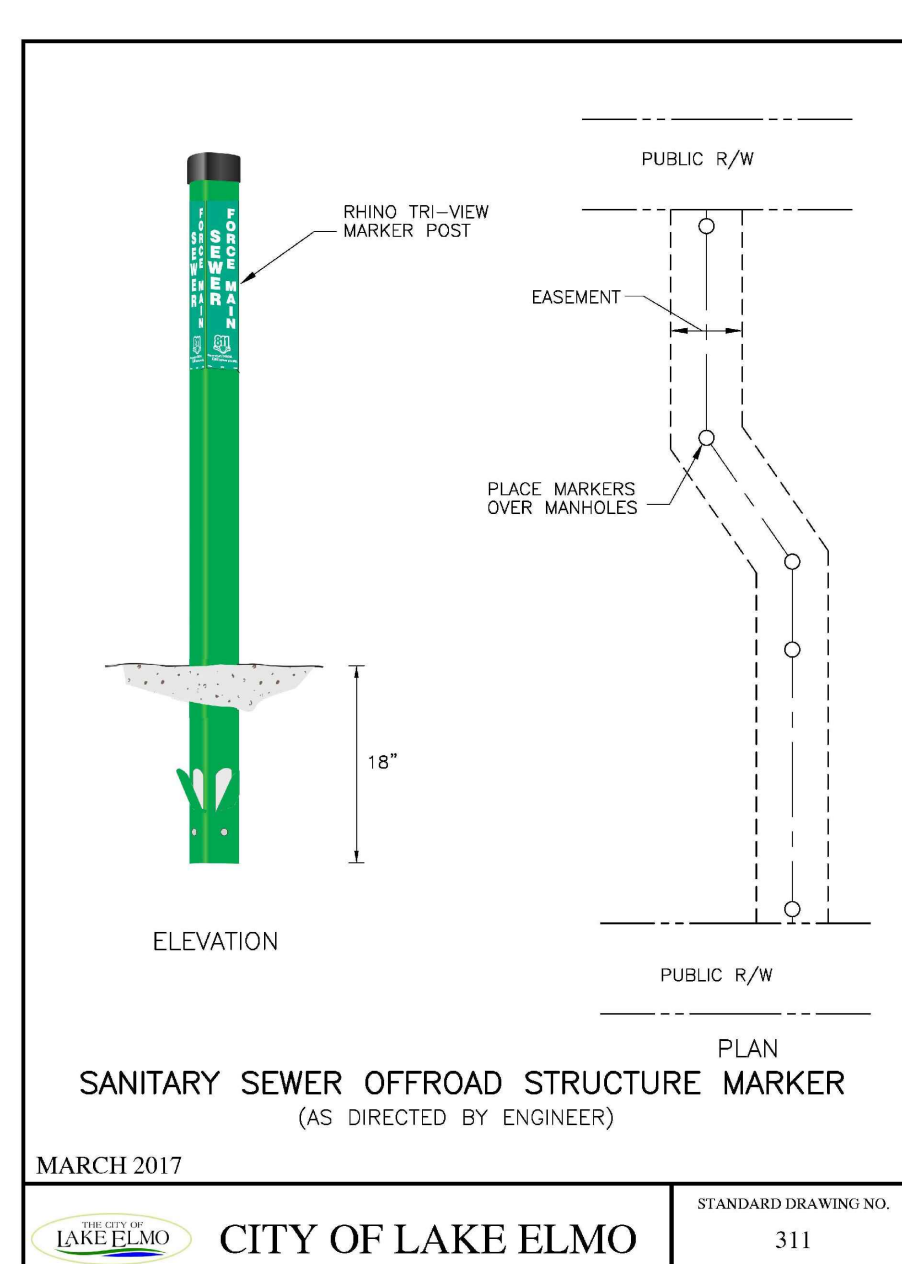
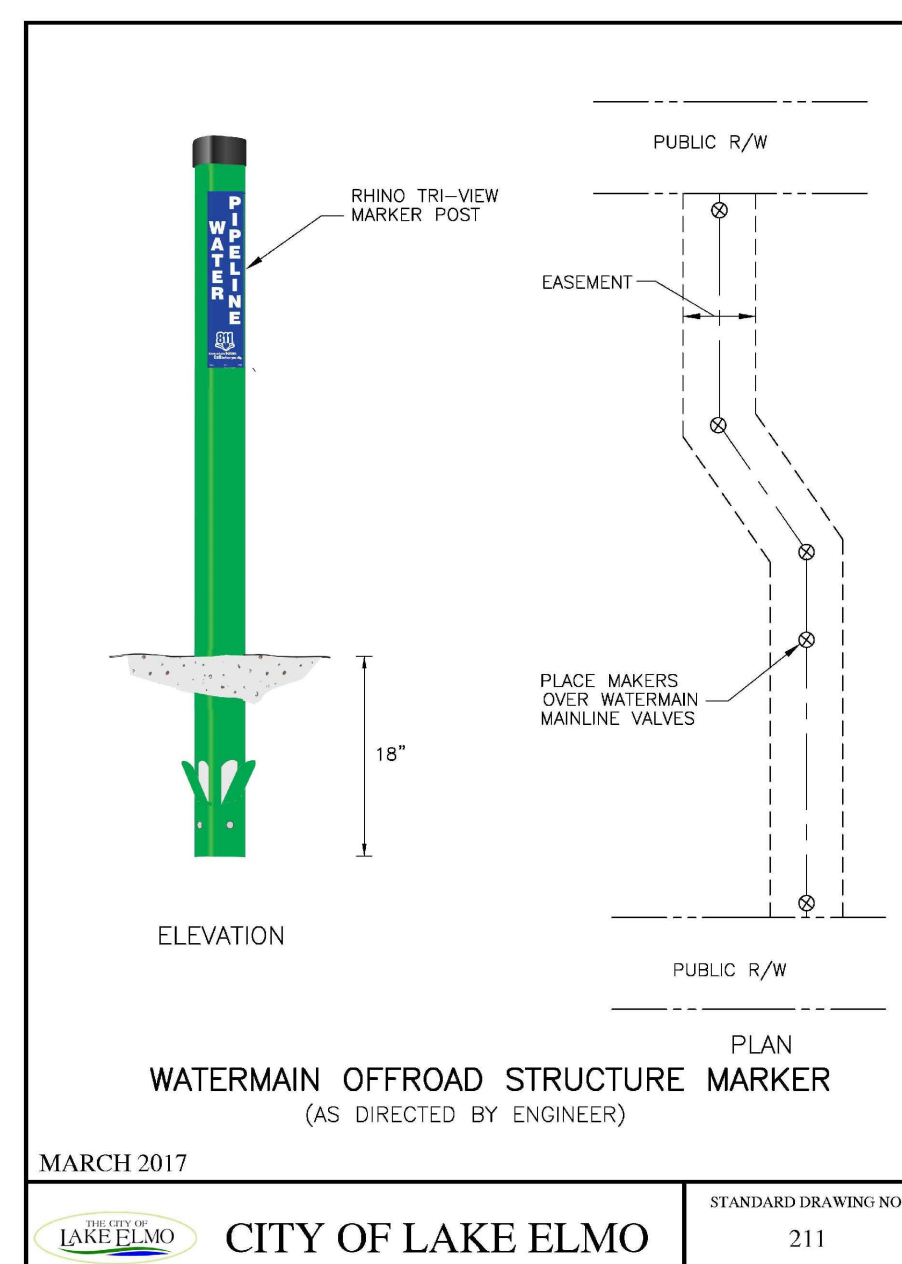
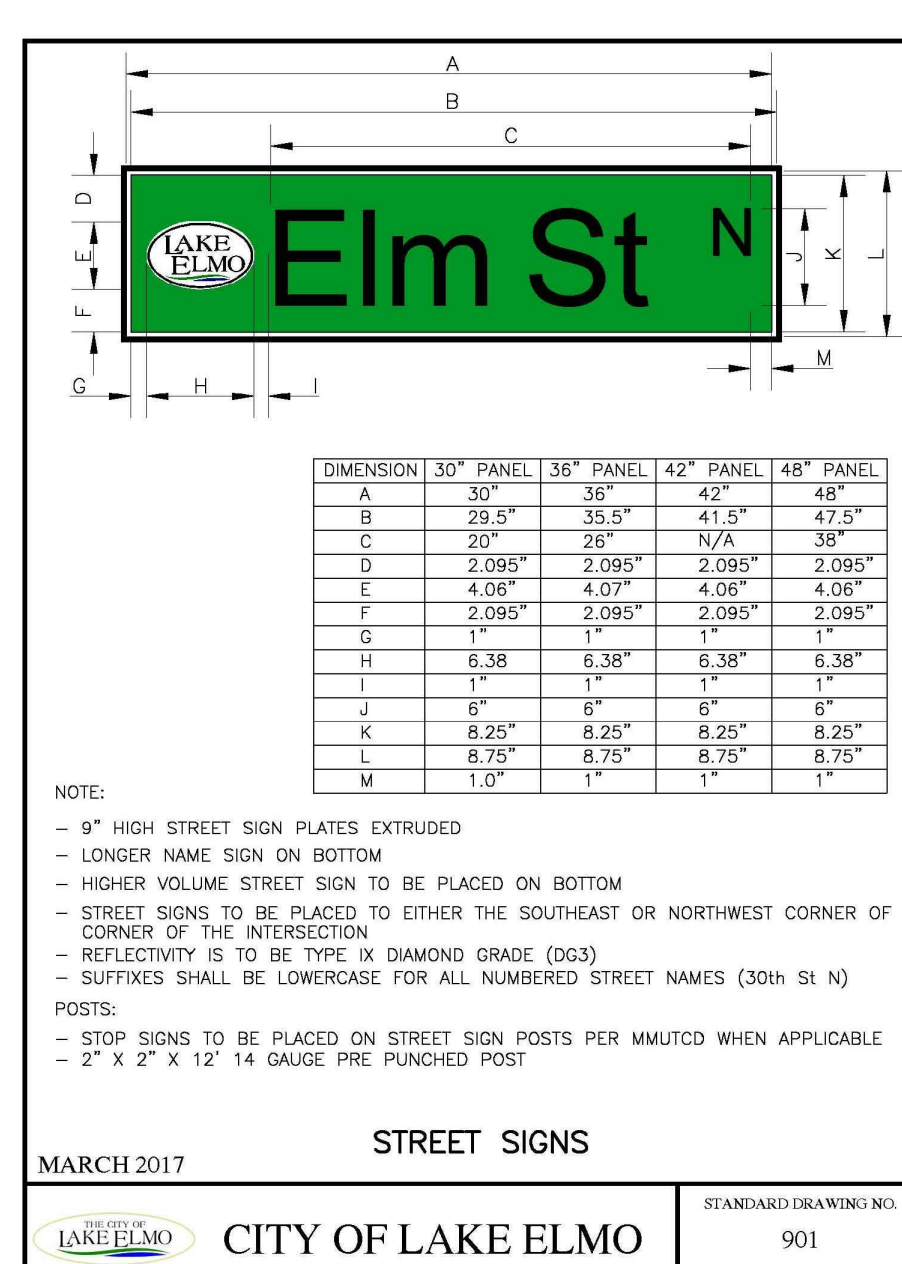
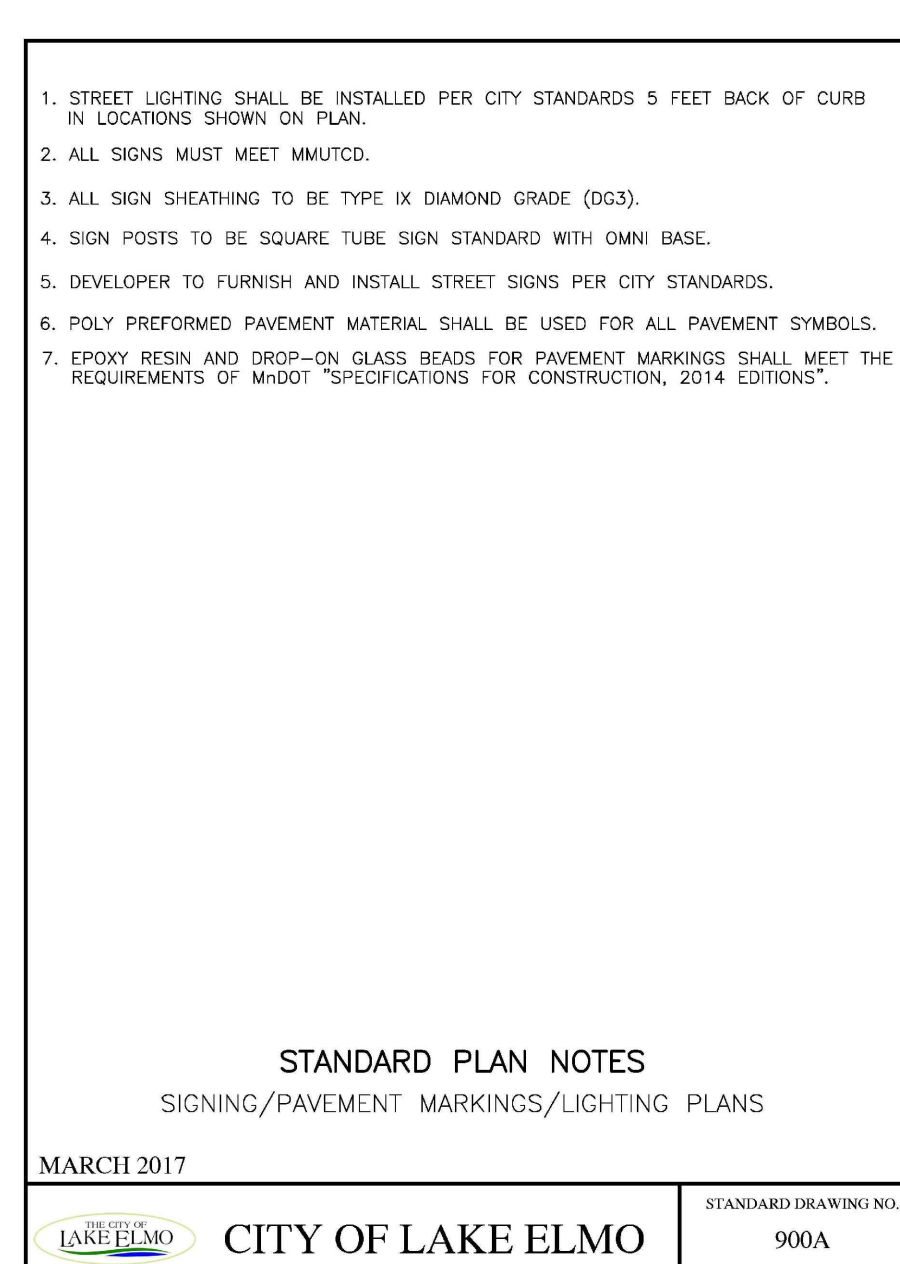
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RIPRAP AT RCP OUTLETS**

CLASS II RIPRAP AT RCP OUTLETS	CLASS II RIPRAP AT RCP OUTLETS			CLASS II RIPRAP AT RCP OUTLETS			CLASS II RIPRAP AT RCP OUTLETS				
	PIPE DIA.	DEPTH	TEXTILE UNDER	PIPE DIA.	DEPTH	TEXTILE UNDER	PIPE DIA.	DEPTH	TEXTILE UNDER		
12	8	18.0	0.5	12	12	18.0	0.5	12	12	18.0	0.5
18	10	24.0	0.5	18	18	24.0	0.5	18	18	24.0	0.5
24	12	30.0	0.5	24	24	30.0	0.5	24	24	30.0	0.5
30	14	36.0	0.5	30	30	36.0	0.5	30	30	36.0	0.5
36	16	42.0	0.5	36	36	42.0	0.5	36	36	42.0	0.5
42	18	48.0	0.5	42	42	48.0	0.5	42	42	48.0	0.5
48	20	54.0	0.5	48	48	54.0	0.5	48	48	54.0	0.5

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 411  
LAKE ELMO



**Carlson McCain**  
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C8 of 11

**MnDOT B612**  
CONCRETE CURB AND GUTTER

**MnDOT B618**  
CONCRETE CURB AND GUTTER

**SURMOUNTABLE**  
CONCRETE CURB AND GUTTER

CONCRETE CURB & GUTTER

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 501  
LAKE ELMO

PLAN

SECTION A-A

COMMERCIAL CONCRETE DRIVEWAY

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 502  
LAKE ELMO

MDOT STANDARD PLAN 5-297.250 SHEET 1 OF 5

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 507A  
LAKE ELMO

MDOT STANDARD PLAN 5-297.250 SHEET 2 OF 5

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 507B  
LAKE ELMO

MDOT STANDARD PLAN 5-297.250 SHEET 3 OF 5

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 507C  
LAKE ELMO

MDOT STANDARD PLAN 6-297.250 SHEET 4 OF 5

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 507D  
LAKE ELMO

MDOT STANDARD PLAN 6-297.250 SHEET 5 OF 5

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 507E  
LAKE ELMO

RECTANGULAR PLATES

RADIAL PLATES

SECTION A-A TRUNCATED DOME

DETECTABLE WARNING SURFACE - TRUNCATED DOMES

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 508  
LAKE ELMO

CONCRETE SIDEWALK

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 510  
LAKE ELMO

BITUMINOUS WEAR COURSE ADJACENT DETAIL

MANHOLE CASTING ADJUSTMENT

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 511  
LAKE ELMO

MILLED LAP JOINT

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 512  
LAKE ELMO

SILT FENCE

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 601  
LAKE ELMO

DITCH CHECK (FIBER ROLL)

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 603  
LAKE ELMO

SEDIMENT CONTROL AROUND STORM SEWER INLET

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 604  
LAKE ELMO

ROCK CONSTRUCTION ENTRANCE

MARCH 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. 605  
LAKE ELMO

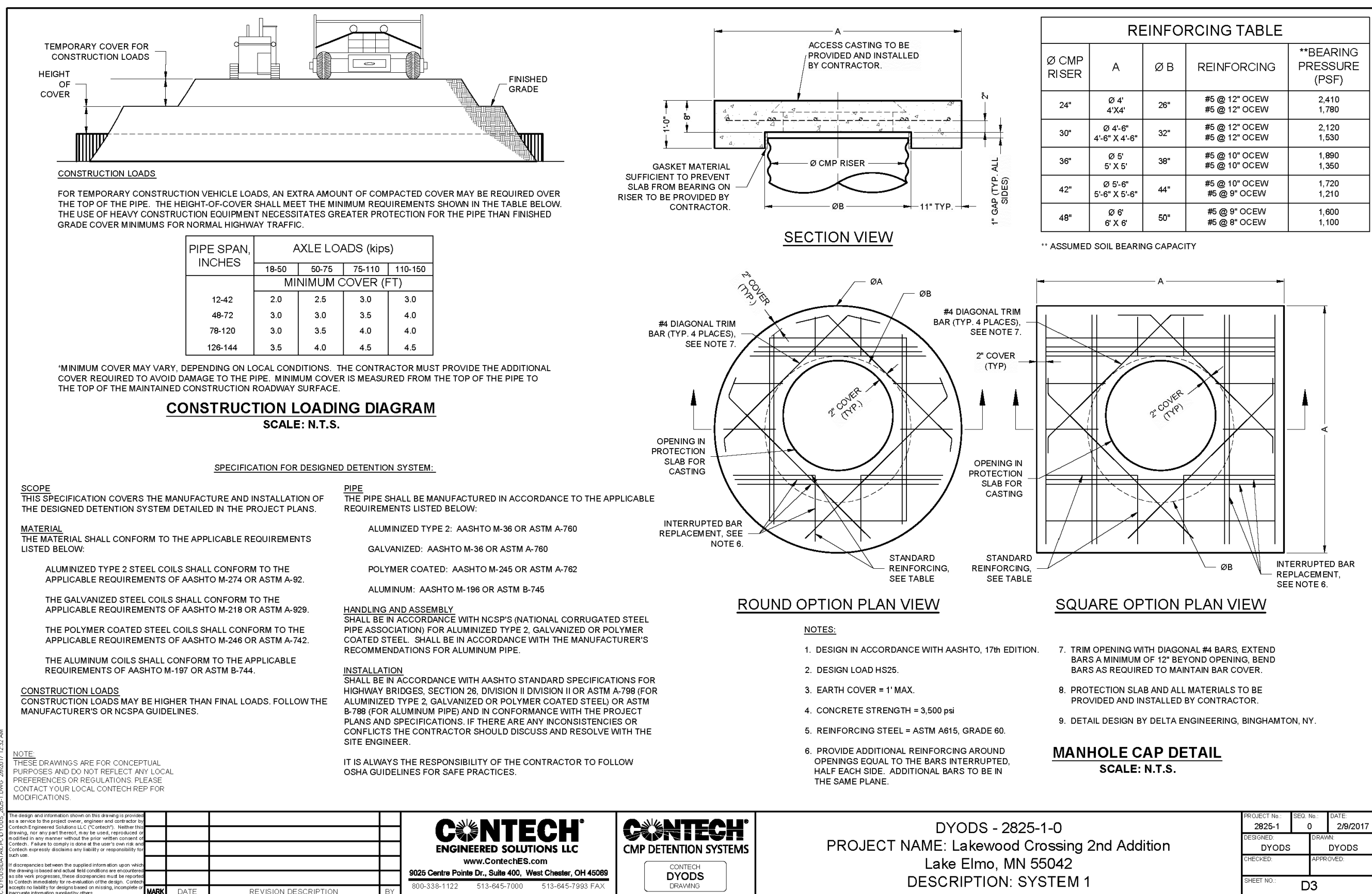
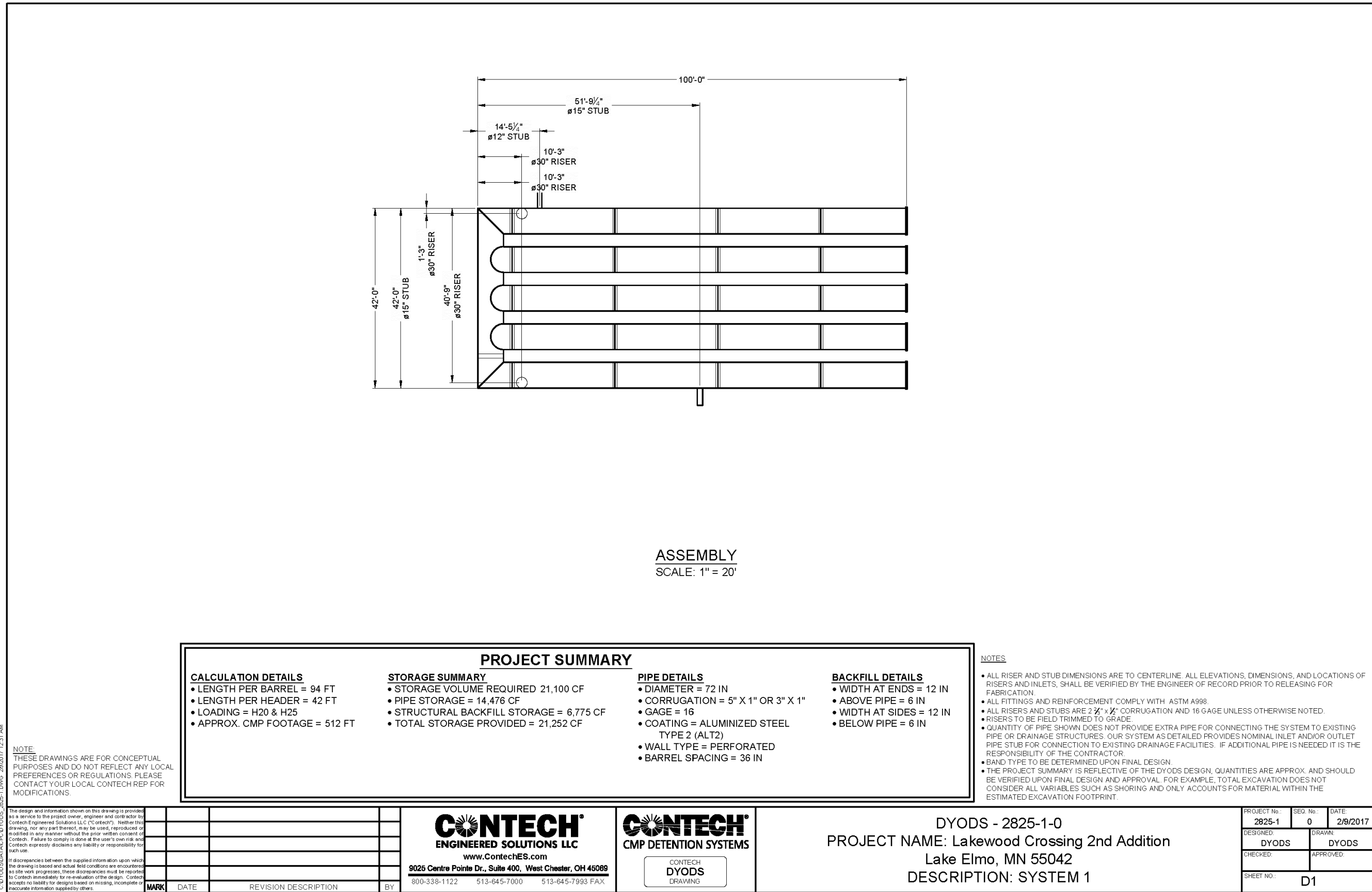
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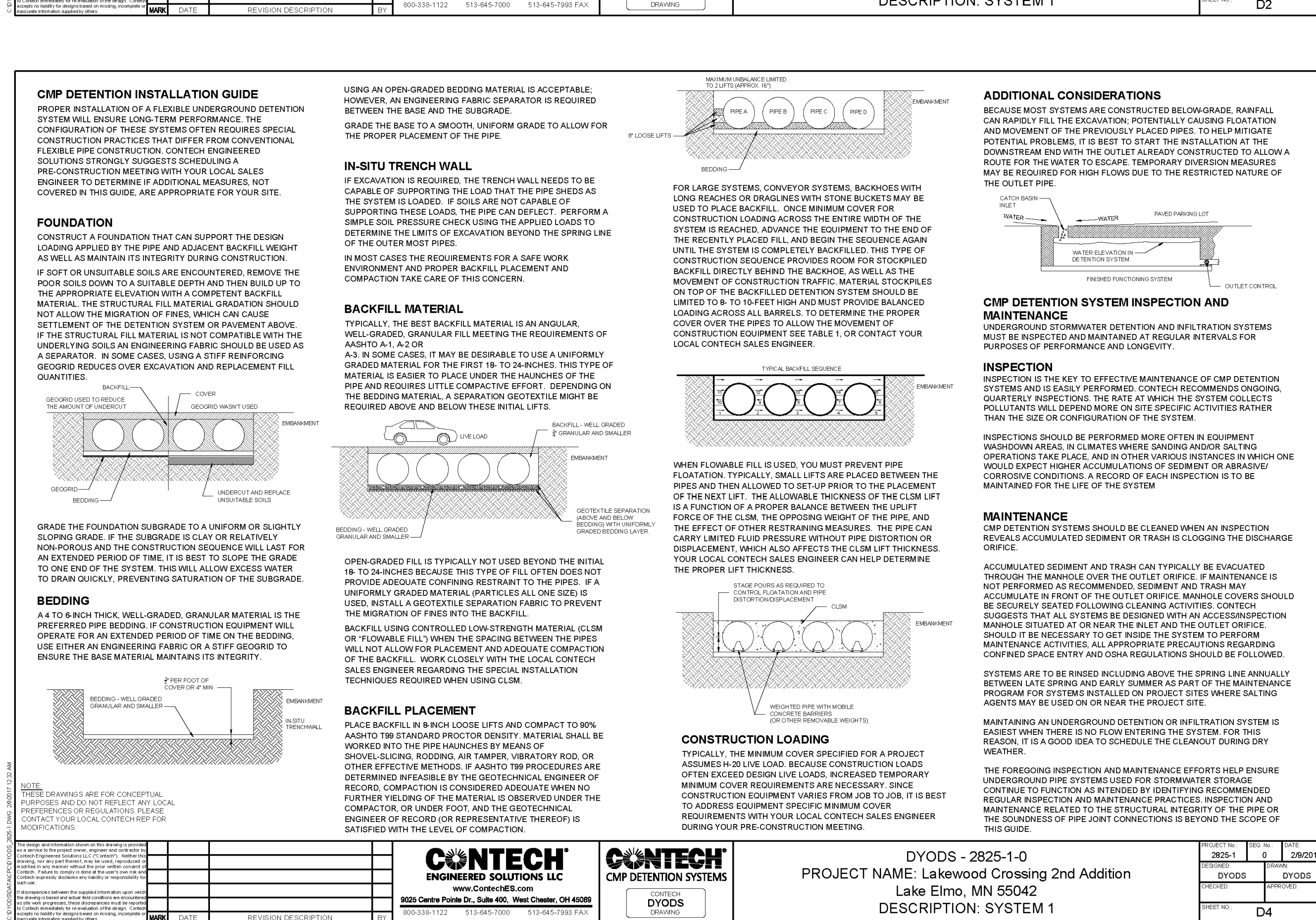
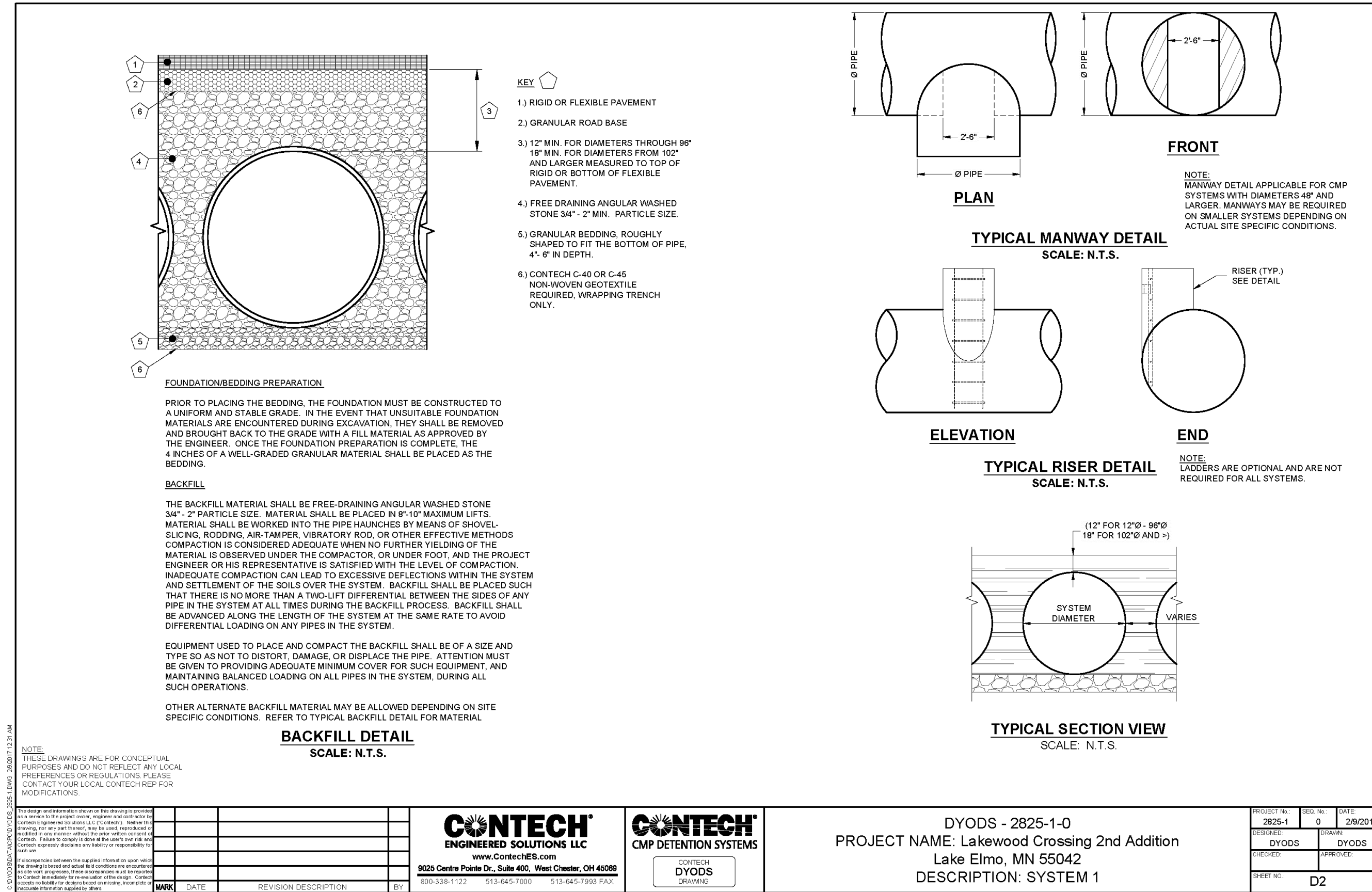
**CONTECH ENGINEERED SOLUTIONS LLC**  
3025 Centre Pointe Dr., Suite 400, West Chester, OH 45390  
909-338-1122 513-645-7000 513-645-7993 FAX

**CONTECH CMP DETENTION SYSTEMS**  
DYODS DRAWING

DYODS - 2825-1-0  
PROJECT NAME: Lakewood Crossing 2nd Addition  
Lake Elmo, MN 55042  
DESCRIPTION: SYSTEM 1

PROJECT NO.	ISS. NO.	DATE
2825-1	0	2/8/2017

DESIGNED BY: DYODS  
CHECKED BY: DYODS  
DATE: 2/8/2017  
SHEET NO: D1



NO.	DATE	REVISION DESCRIPTION	BY

**CONTECH ENGINEERED SOLUTIONS LLC**  
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DESIGNED BY: DYODS  
CHECKED BY: DYODS  
DATE: 2/8/2017  
SHEET NO: D2

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**REVISIONS**

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**KEY**

- 1) RIGID OR FLEXIBLE PAVEMENT
- 2) GRANULAR ROAD BASE
- 3) 12" MIN. FOR DIAMETERS THROUGH 96" 18" MIN. FOR DIAMETERS FROM 102" AND LARGER MEASURED TO TOP OF RIGID OR BOTTOM OF FLEXIBLE PAVEMENT.
- 4) FREE DRAINING ANGULAR WASHED STONE 3/4" - 2" MIN. PARTICLE SIZE
- 5) GRANULAR BEDDING, ROUGHLY SHAPED TO FIT THE BOTTOM OF PIPE. 4" MIN. DEPTH.
- 6) CONTECH C-40 OR C-45 NON-WOVEN GEOTEXTILE REQUIRED, WRAPPING TRENCH ONLY.

**FOUNDATION/BEDDING PREPARATION**  
 PRIOR TO PLACING THE BEDDING, THE FOUNDATION MUST BE CONSTRUCTED TO A UNIFORM AND STABLE GRADE. IN THE EVENT THAT UNSUITABLE FOUNDATION MATERIALS ARE ENCOUNTERED DURING EXCAVATION, THEY SHALL BE REMOVED AND BROUGHT BACK TO THE GRADE WITH A FILL MATERIAL AS APPROVED BY THE ENGINEER. ONCE THE FOUNDATION PREPARATION IS COMPLETE, THE 4 INCHES OF A WELL-GRADED GRANULAR MATERIAL SHALL BE PLACED AS THE BEDDING.

**BACKFILL**  
 THE BACKFILL MATERIAL SHALL BE FREE-DRAINING ANGULAR WASHED STONE 3/4" - 2" PARTICLE SIZE. MATERIAL SHALL BE PLACED IN 8"-10" MAXIMUM LIFTS. MATERIAL SHALL BE WORKED INTO THE PIPE HAUNCHES BY MEANS OF SHOVELS, SLICING, RODDING, AIR TAMPER, VIBRATORY ROD, OR OTHER EFFECTIVE METHODS. COMPACTED MATERIAL SHALL BE CONSIDERED ADEQUATE WHEN NO FURTHER YIELDING OF THE MATERIAL IS OBSERVED UNDER THE COMPACTOR, OR UNDER FOOT, AND THE PROJECT ENGINEER OR HIS REPRESENTATIVE IS SATISFIED WITH THE LEVEL OF COMPACTION. INADEQUATE COMPACTION CAN LEAD TO EXCESSIVE DEFLECTIONS WITHIN THE SYSTEM AND SETTLEMENT OF THE SOILS OVER THE SYSTEM. BACKFILL SHALL BE PLACED SUCH THAT THERE IS NO MORE THAN A TWO-LIFT DIFFERENTIAL BETWEEN THE SIDES OF ANY PIPE IN THE SYSTEM AT ALL TIMES DURING THE BACKFILL PROCESS. BACKFILL SHALL BE ADVANCED ALONG THE LENGTH OF THE SYSTEM AT THE SAME RATE TO AVOID DIFFERENTIAL LOADING ON ANY PIPES IN THE SYSTEM.

**EQUIPMENT USED TO PLACE AND COMPACT THE BACKFILL SHALL BE OF A SIZE AND TYPE SO AS NOT TO DISTORT, DAMAGE, OR DISPLACE THE PIPE. ATTENTION MUST BE GIVEN TO PROVIDING ADEQUATE MINIMUM COVER FOR SUCH EQUIPMENT, AND MAINTAINING BALANCED LOADING ON ALL PIPES IN THE SYSTEM, DURING ALL SUCH OPERATIONS.**

**OTHER ALTERNATE BACKFILL MATERIAL MAY BE ALLOWED DEPENDING ON SITE SPECIFIC CONDITIONS. REFER TO TYPICAL BACKFILL DETAIL FOR MATERIAL.**

**CONTECH ENGINEERED SOLUTIONS LLC**  
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**DYODS - 2826-1-0**  
 PROJECT NAME: Lakewood Crossing 2nd Addition  
 Lake Elmo, MN 55042  
 DESCRIPTION: SYSTEM 2

PROJECT NO. 2826-1-0  
 SHEET NO. D2

**ASSEMBLY**  
 SCALE: 1" = 20'

**PROJECT SUMMARY**

**STORAGE SUMMARY**

- STORAGE VOLUME REQUIRED 23,560 CF
- PIPE STORAGE = 16,088 CF
- STRUCTURAL BACKFILL STORAGE = 7,480 CF
- TOTAL STORAGE PROVIDED = 23,568 CF

**PIPE DETAILS**

- DIAMETER = 72 IN
- CORRUIGATION = 5" X 1" OR 3" X 1"
- GAGE = 18
- COATING = ALUMINIZED STEEL TYPE 2 (AL72)
- WALL TYPE = PERFORATED
- BARREL SPACING = 36 IN

**BACKFILL DETAILS**

- WIDTH AT ENDS = 12 IN
- ABOVE PIPE = 6 IN
- WIDTH AT SIDES = 12 IN
- BELOW PIPE = 6 IN

**NOTES**

- ALL RISER AND STUB DIMENSIONS ARE TO CENTERLINE. ALL ELEVATIONS, DIMENSIONS, AND LOCATIONS OF RISERS AND INLETS, SHALL BE VERIFIED BY THE ENGINEER OF RECORD PRIOR TO RELEASING FOR CONSTRUCTION.
- ALL FITTINGS AND REINFORCEMENT CONFORM WITH ASTM A498.
- ALL RISERS AND STUBS ARE 24" O.C. CORRUIGATION AND 18 GAGE UNLESS OTHERWISE NOTED.
- RISERS TO BE FIELD TRIMMED TO GRADE.
- QUANTITY OF PIPE SHOWINGS DOES NOT PROVIDE EXTRA PIPE FOR CONNECTING THE SYSTEM TO EXISTING PIPE OR DRAINAGE STRUCTURES. OUR SYSTEM AS DETAIL PROVIDES NORMAL INLET AND/OR OUTLET PIPE STUBS FOR CONNECTION TO EXISTING DRAINAGE FACILITIES. IF ADDITIONAL PIPE IS NEEDED IT IS THE RESPONSIBILITY OF THE CONTRACTOR.
- ROAD TYPE TO BE DETERMINED UPON FINAL DESIGN.
- THE PROJECT SUMMARY IS REFLECTIVE OF THE CIVIL DESIGN QUANTITIES ARE APPROX. AND SHOULD BE VERIFIED FOR FINAL DESIGN AND APPROVAL. FOR EXAMPLE, TOTAL EXCAVATION DOES NOT CONSIDER ALL VARIABLES SUCH AS SHORING AND ONLY ACCOUNTS FOR MATERIAL WITHIN THE ESTIMATED EXCAVATION FOOTPRINT.

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**CONSTRUCTION LOADING**

FOR TEMPORARY CONSTRUCTION VEHICLE LOADS, AN EXTRA AMOUNT OF COMPACTED COVER MAY BE REQUIRED OVER THE TOP OF THE PIPE. THE HEIGHT OF COVER SHALL MEET THE MINIMUM REQUIREMENTS SHOWN IN THE TABLE BELOW. THE USE OF HEAVY CONSTRUCTION EQUIPMENT NECESSITATES GREATER PROTECTION FOR THE PIPE THAN FINISHED GRADE COVER MINIMUMS FOR NORMAL HIGHWAY TRAFFIC.

PIPE SPAN INCHES	AXLE LOADS (kips)		
	18-60	50-75	75-110
12-42	2.0	2.5	3.0
48-72	3.0	3.0	3.5
78-120	3.0	3.5	4.0
126-144	3.5	4.0	4.5

**CONSTRUCTION LOADING DIAGRAM**  
 SCALE: N.T.S.

**REINFORCING TABLE**

Ø CMP RISER	A	Ø B	REINFORCING	**BEARING PRESSURE (PSF)
24"	0'-4"	20"	#5 @ 12" O.C/EW	2,410
	4'-8" X 4'-8"		#5 @ 12" O.C/EW	1,780
30"	0'-4"	30"	#5 @ 12" O.C/EW	2,120
	4'-8" X 4'-8"		#5 @ 12" O.C/EW	1,530
36"	0'-5"	36"	#5 @ 10" O.C/EW	1,990
	5'-0" X 5'-0"		#5 @ 10" O.C/EW	1,360
42"	0'-6"	44"	#5 @ 10" O.C/EW	1,720
	5'-5" X 5'-5"		#5 @ 10" O.C/EW	1,210
48"	0'-6"	50"	#5 @ 9" O.C/EW	1,600
	6'-0" X 6'-0"		#5 @ 9" O.C/EW	1,100

\*\* ASSUMED SOIL BEARING CAPACITY

**SECTION VIEW**

**ROUND OPTION PLAN VIEW**

**SQUARE OPTION PLAN VIEW**

**MANHOLE CAP DETAIL**  
 SCALE: N.T.S.

**NOTES:**

- DESIGN IN ACCORDANCE WITH AASHTO, 17th EDITION.
- DESIGN LOAD H25S.
- EARTH COVER = 1' MAX.
- CONCRETE STRENGTH = 3,500 psi
- REINFORCING STEEL = ASTM A615, GRADE 60.
- PROVIDE ADDITIONAL REINFORCING AROUND OPENINGS EQUAL TO THE BARS INTERRUPTED, HALF EACH SIDE. ADDITIONAL BARS TO BE IN THE SAME PLANE.
7. TRM OPENING WITH DIAGONAL #4 BARS. EXTEND BARS A MINIMUM OF 12" BEYOND OPENING, BEND BARS AS REQUIRED TO MAINTAIN BAR COVER.
- PROTECTION SLAB AND ALL MATERIALS TO BE PROVIDED AND INSTALLED BY CONTRACTOR.
- DETAIL DESIGN BY DELTA ENGINEERING, BINGHAMTON, NY.

**CONTECH ENGINEERED SOLUTIONS LLC**  
 9025 Centre Pointe Dr., Suite 400, West Chester, OH 43081  
 900-338-1122 513-645-7000 513-645-7999 FAX

**DYODS - 2826-1-0**  
 PROJECT NAME: Lakewood Crossing 2nd Addition  
 Lake Elmo, MN 55042  
 DESCRIPTION: SYSTEM 2

PROJECT NO. 2826-1-0  
 SHEET NO. D3

**CMP DETENTION INSTALLATION GUIDE**

PROPER INSTALLATION OF A FLEXIBLE UNDERGROUND DETENTION SYSTEM WILL ENSURE LONG-TERM PERFORMANCE. THE CONFIGURATION OF THESE SYSTEMS OFTEN REQUIRES SPECIAL CONSTRUCTION PRACTICES THAT DIFFER FROM CONVENTIONAL FLEXIBLE PIPE CONSTRUCTION. CONTECH ENGINEERED SOLUTIONS STRONGLY SUGGESTS SCHEDULING A PRE-CONSTRUCTION MEETING WITH YOUR LOCAL SALES ENGINEER TO DETERMINE IF ADDITIONAL MEASURES, NOT COVERED IN THIS GUIDE, ARE APPROPRIATE FOR YOUR SITE.

**FOUNDATION**  
 CONSTRUCT A FOUNDATION THAT CAN SUPPORT THE DESIGN LOADING APPLIED BY THE PIPE AND ADJACENT BACKFILL WEIGHT AS WELL AS MAINTAIN ITS INTEGRITY DURING CONSTRUCTION. IF SOFT OR UNSUITABLE SOILS ARE ENCOUNTERED, REMOVE THE POOR SOILS DOWN TO A SUITABLE DEPTH AND THEN BUILD UP TO THE APPROPRIATE ELEVATION WITH A COMPACT BACKFILL MATERIAL. THE STRUCTURAL FILL MATERIAL GRADE SHOULD NOT ALLOW THE MIGRATION OF FINES, WHICH CAN CAUSE SETTLEMENT OF THE DETENTION SYSTEM ON PAVEMENT ABOVE. IF THE STRUCTURAL FILL MATERIAL IS NOT COMPATIBLE WITH THE UNDERLYING SOILS AN ENGINEERING FABRIC SHOULD BE USED AS A SEPARATOR. IN SOME CASES, USING A STEEP REINFORCING GEOTEXTILE REDUCES OVER EXCAVATION AND REPLACEMENT FILL QUANTITIES.

**BACKFILL MATERIAL**  
 TYPICALLY, THE BEST BACKFILL MATERIAL IS AN ANGULAR, WELL-GRADED, GRANULAR FILL MEETING THE REQUIREMENTS OF AASHTO A1, A2 OR A3. IN SOME CASES, IT MAY BE DESIRABLE TO USE A UNIFORMLY GRADED MATERIAL FOR THE FIRST 18" TO 24" INCHES. THIS TYPE OF MATERIAL IS EASIER TO PLACE UNDER THE HAUNCHES OF THE PIPE AND REQUIRES LITTLE COMPACTIVE EFFORT. DEPENDING ON THE BEDDING MATERIAL, A SEPARATION GEOTEXTILE MUST BE REQUIRED ABOVE AND BELOW THESE INITIAL LIFTS.

**IN-SITU TRENCH WALL**  
 IF EXCAVATION IS REQUIRED, THE TRENCH WALL NEEDS TO BE CAPABLE OF SUPPORTING THE LOAD THAT THE PIPE SHARES AS THE SYSTEM IS LOADED. IF SOILS ARE NOT CAPABLE OF SUPPORTING THESE LOADS, THE PIPE CAN DEFLECT. PERFORM A SIMPLE SOIL PRESSURE CHECK USING THE APPLIED LOADS TO DETERMINE THE LIMITS OF EXCAVATION BEYOND THE SPRING LINE OF THE OUTLET MOST PIPES.

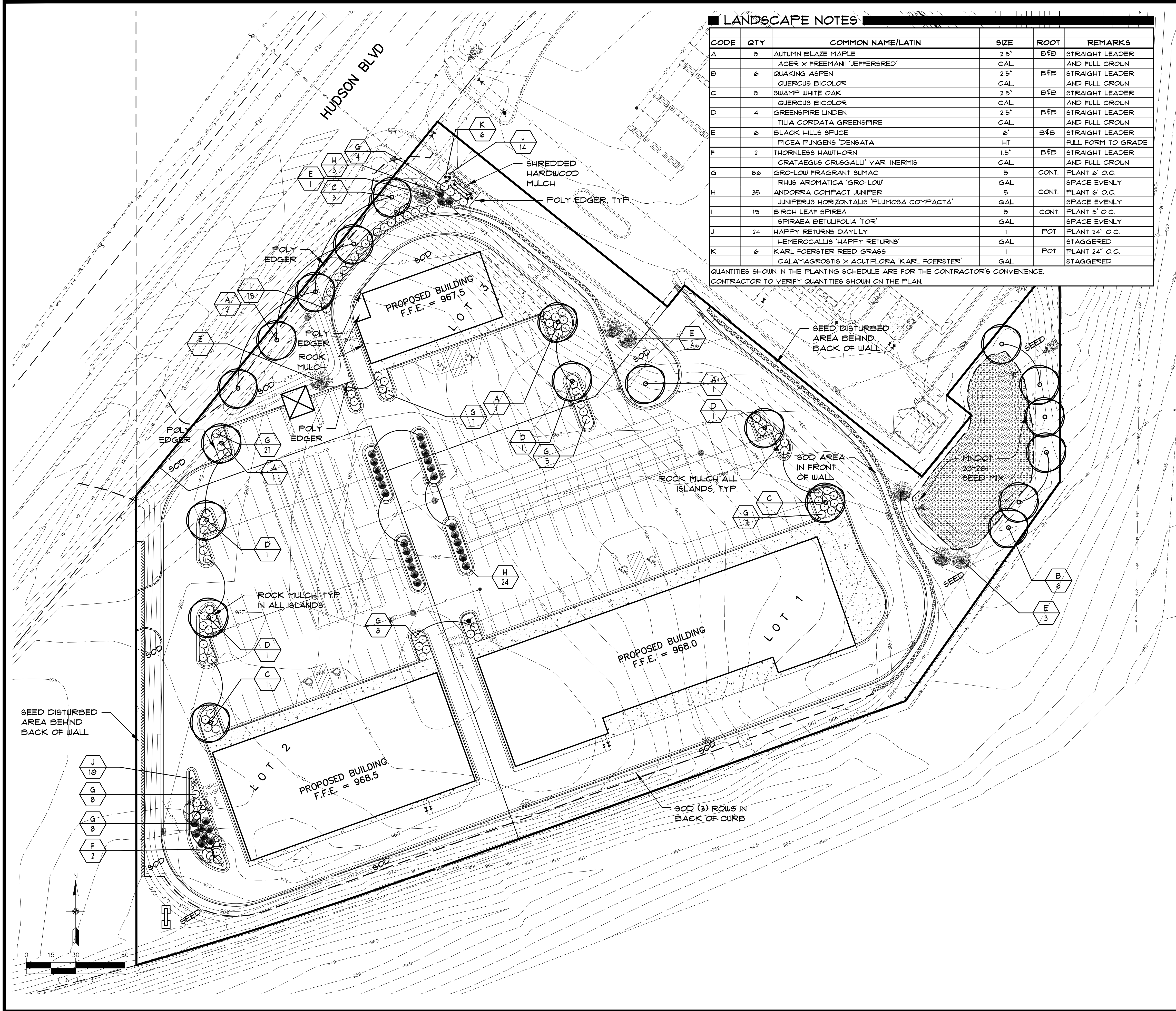
**CONSTRUCTION LOADING**  
 TYPICALLY, THE MINIMUM COVER SPECIFIED FOR A PROJECT ASSUMES H-20 LIVE LOAD. BECAUSE CONSTRUCTION LOADS OFTEN EXCEED DESIGN LIVE LOADS, INCREASED TEMPORARY MINIMUM COVER REQUIREMENTS ARE NECESSARY. SINCE CONSTRUCTION EQUIPMENT VARIES FROM JOB TO JOB, IT IS BEST TO ADDRESS EQUIPMENT SPECIFIC MINIMUM COVER REQUIREMENTS WITH YOUR LOCAL CONTECH SALES ENGINEER DURING YOUR PRE-CONSTRUCTION MEETING.

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**CONTECH ENGINEERED SOLUTIONS LLC**  
 9025 Centre Pointe Dr., Suite 400, West Chester, OH 43081  
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**DYODS - 2826-1-0**  
 PROJECT NAME: Lakewood Crossing 2nd Addition  
 Lake Elmo, MN 55042  
 DESCRIPTION: SYSTEM 2

PROJECT NO. 2826-1-0  
 SHEET NO. D4



**LANDSCAPE NOTES**

CODE	QTY	COMMON NAME/LATIN	SIZE	ROOT	REMARKS
A	5	AUTUMN BLAZE MAPLE	2.5"	BFB	STRAIGHT LEADER
		ACER X FREEMANI 'JEFFERRED'	CAL.		AND FULL CROWN
B	6	QUAKING ASPEN	2.5"	BFB	STRAIGHT LEADER
		QUERCUS BICOLOR	CAL.		AND FULL CROWN
C	5	SWAMP WHITE OAK	2.5"	BFB	STRAIGHT LEADER
		QUERCUS BICOLOR	CAL.		AND FULL CROWN
D	4	GREENSPIRE LINDEN	2.5"	BFB	STRAIGHT LEADER
		TILIA CORDATA GREENSPIRE	CAL.		AND FULL CROWN
E	6	BLACK HILLS SPUCE	6"	BFB	STRAIGHT LEADER
		FICIA FUNGUS 'DENSATA'	HT		FULL FORM TO GRADE
F	2	THORNLESS HAWTHORN	1.5"	BFB	STRAIGHT LEADER
		CRATAEGUS CRUGGALLI' VAR. INERMIS	CAL.		AND FULL CROWN
G	86	GRO-LOW FRAGRANT SUMAC	5"	CONT.	PLANT 6" O.C.
		RHUS AROMATICA 'GRO-LOW'	GAL.		SPACE EVENLY
H	35	ANDORRA COMPACT JUNIPER	5"	CONT.	PLANT 6" O.C.
		JUNIPERUS HORIZONTALIS 'PLUMOSA COMPACTA'	GAL.		SPACE EVENLY
I	19	BIRCH LEAF SPIREA	5"	CONT.	PLANT 5" O.C.
		SPIRAEA BETULIFOLIA 'TOR'	GAL.		SPACE EVENLY
J	24	HAPPY RETURNS DAYLILY	1"	POT	PLANT 24" O.C.
		HEMEROCALLIS 'HAPPY RETURNS'	GAL.		STAGGERED
K	6	KARL FOERSTER REED GRASS	1"	POT	PLANT 24" O.C.
		CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER'	GAL.		STAGGERED

QUANTITIES SHOWN IN THE PLANTING SCHEDULE ARE FOR THE CONTRACTOR'S CONVENIENCE. CONTRACTOR TO VERIFY QUANTITIES SHOWN ON THE PLAN.

**LANDSCAPE NOTES**

- IRRIGATION SHALL BE PROVIDED FOR ALL SOD AND LANDSCAPE AREAS.
- SOD AND IRRIGATE ALL DISTURBED AREAS UNLESS OTHERWISE NOTED.
- LANDSCAPE BEDS TO RECEIVE 3" OF SHREDDED HARDWOOD MULCH UNLESS OTHERWISE NOTED.
- ROCK MULCH = 1-1/2" DRESSER TRAP ROCK @ 3" DEPTH WHERE NOTED.
- HEAVY COMMERCIAL GRADE POLY EDGER SHALL BE USED AROUND THE PERIMETER OF LANDSCAPE BEDS NOTED ON PLAN.
- REFER TO DETAIL SHEET L2 FOR GENERAL NOTES, PLANTING SPECIFICATIONS, PLANTING DETAILS.
- REFER TO GRADING PLAN FOR GRADES AND EROSION CONTROL.
- REMOVE ANY BURLAP, TWINE, ROPES AND/OR WIRING FROM THE TOP AND SIDES OF ROOTBALL FOR ALL BALLED AND BURLAPPED PLANTS.
- REMOVE CONTAINERS AND CUT CIRCLING ROOTS IF PLANTS ARE CONTAINER GROWN.
- ALL PLANTINGS SHALL RECEIVE FERTILIZER AS FOLLOWS:
  - Ø-20-20 GRANULAR FERTILIZER APPLIED AT THE TIME OF PLANTING AT A RATE OF 12 OZ PER 2.5 CALIPER INCHES OF TREE AND 6 OZ PER SHRUB.
  - PLACE LANDSCAPE FILTER FABRIC UNDER ALL MULCHED AREAS. FILTER FABRIC SHALL BE 100% INTERWOVEN 5.10Z NEEDLE PUNCHED POLYPROPYLENE FABRIC MANUFACTURED BY DEWITT. USE PRO 5 "WEED BARRIER" OR APPROVED EQUAL.

**LANDSCAPE REQUIREMENTS**

- A MINIMUM OF (1) TREE PER 50 LF OF STREET FRONTAGE.
- ADDITIONALLY, (5) TREES SHALL BE PLANTED PER (1) ACRE OF DEVELOPED LAND OR DISTURBED BY DEVELOPMENT ACTIVITY

TOTAL STREET FRONTAGE = 296 LF. 6 TREES REQUIRED  
 OVERALL SITE ACREAGE = 3.82. 19 TREES REQUIRED.  
 TOTAL TREES REQUIRED = 25  
 TOTAL TREES PROVIDED = 25

**MNDOT 33-261 SEED MIX**

Common Name	Scientific Name	Rate (lb/ac)	Rate (kg/ha)	% of Mix (by weight)	Seeds/ sq ft
big bluestem	<i>Andropogon gerardii</i>	2.00	2.24	5.72%	7.35
fringed brome	<i>Bromus ciliatus</i>	2.00	2.24	5.73%	8.10
Virginia wild rye	<i>Elymus virginicus</i>	1.50	1.68	4.28%	2.31
fowl bluegrass	<i>Poa palustris</i>	1.06	1.19	3.03%	50.70
slender wheatgrass	<i>Elymus trachycaulus</i>	1.00	1.12	2.85%	2.53
switchgrass	<i>Panicum virgatum</i>	0.38	0.43	1.07%	1.93
prairie cordgrass	<i>Sporobolus pectinatus</i>	0.38	0.43	1.07%	0.91
Indian grass	<i>Sorghastrum nutans</i>	0.12	0.13	0.36%	0.55
bluejoint	<i>Calamagrostis canadensis</i>	0.06	0.07	0.18%	6.40
Grasses Subtotal		8.50	9.53	24.29%	80.78
swi-fruited sedge	<i>Carex stipata</i>	0.25	0.28	0.71%	3.10
dark green bulrush	<i>Scirpus atrovirens</i>	0.19	0.21	0.54%	31.70
woolgrass	<i>Scirpus cyperinus</i>	0.06	0.07	0.18%	39.00
Sedges & Rushes Subtotal		0.50	0.56	1.43%	73.80
golden alexanders	<i>Zizia aurea</i>	0.20	0.22	0.56%	0.79
autumn sneezeweed	<i>Helenium autumnale</i>	0.13	0.15	0.36%	5.97
marsh milkweed	<i>Asclepias incarnata</i>	0.11	0.12	0.32%	0.20
hoop nectarines	<i>Bidens frondosa</i>	0.11	0.12	0.31%	0.20
Canada anemone	<i>Anemone canadensis</i>	0.07	0.08	0.19%	0.20
obedient plant	<i>Physostegia virginiana</i>	0.07	0.08	0.21%	0.30
tail coneflower	<i>Rudbeckia laciniata</i>	0.07	0.08	0.21%	0.37
Symphyotrichum					
New England aster	<i>novae-angliae</i>	0.07	0.08	0.19%	1.58
flat topped aster	<i>Doszestina umbellata</i>	0.06	0.07	0.17%	1.50
spotted Joe pye weed	<i>Eutrochium maculatum</i>	0.06	0.07	0.18%	2.19
blue vervain	<i>Verbena hastata</i>	0.05	0.06	0.15%	1.85
Forbs Subtotal		1.00	1.12	2.85%	15.13
Oats	<i>Avena sativa</i>	25.00	28.02	71.43%	11.14
Cover Crop Subtotal		25.00	28.02	71.43%	11.14
Total		35.00	39.23	100.00%	180.85

Purpose: Stormwater pond edges, temporarily flooded dry ponds, and temporarily flooded ditch bottoms.  
 Planting Area: Tallgrass Aspen Parklands, Prairie Parkland, and Eastern Broadleaf Forest Provinces. MNDOT Districts 2 West, 3B, 4, Metro, 6, 7 & 8.

**Carlson McCain**  
 ENVIRONMENTAL - ENGINEERING - SURVEYING  
 3890 Pheasant Ridge Dr. NE #100, Blaine, MN  
 Phone: 763-489-7900 Fax: 763-489-7959

**LANDSCAPE PLAN**  
**LAKE ELMO SHOPPES**  
 Lake Elmo, Minnesota

**MFC PROPERTIES CORP.**  
 3460 Washington Drive, Suite 100  
 Eagan, MN 55122

**REVISIONS**

1.	04/26/17 PER CITY COMMENT	
2.		
3.		
4.		
5.		

DRAWN BY: SDB  
 DESIGNED BY: SDB  
 ISSUE DATE: 02/03/17

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed Landscape Architect under the laws of the State of Minnesota

Name: James A. Kalkes RLA  
 Signature: *James A. Kalkes*  
 Date: 05/24/17 License #: 45071

L1 of 2

CALL BEFORE YOU DIG

Know what's below.  
 Call before you dig.

**GENERAL NOTES:**

- LANDSCAPE CONTRACTOR TO VERIFY ALL UTILITY LOCATIONS ON THE PROPERTY WITH THE GENERAL CONTRACTOR AND BY GOPHER STATE ONE CALL PRIOR TO STAKING PLANT LOCATIONS.
- COORDINATE THE PHASES OF CONSTRUCTION AND PLANTING INSTALLATION WITH OTHER CONTRACTORS WORKING ON THE SITE.
- WHERE EXISTING TREES AND/OR SIGNIFICANT SHRUB MASSINGS ARE FOUND ON THE SITE WHETHER SHOWN ON THE DRAWINGS OR NOT, THEY SHALL BE PROTECTED AND SAVED UNLESS NOTED TO BE REMOVED AND/OR ARE WITHIN THE GRADING LIMITS. ANY QUESTION REGARDING WHETHER PLANT MATERIAL SHOULD OR SHOULD NOT REMAIN SHOULD BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO REMOVAL.
- ALL EXISTING TREES TO REMAIN SHALL BE FERTILIZED AND PRUNED TO REMOVE DEAD WOOD AND DAMAGED OR RUBBING BRANCHES.
- BB TREES AND SHRUBS ARE BALLED AND BURLAPPED.
- NO PLANT MATERIAL SUBSTITUTIONS WILL BE ACCEPTED UNLESS APPROVAL IS REQUESTED OF THE LANDSCAPE ARCHITECT BY THE LANDSCAPE CONTRACTOR PRIOR TO SUBMISSION OF BID AND/OR QUOTATION.
- ALL PLANT MATERIAL SHALL COMPLY WITH THE LATEST ADDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, AMERICAN ASSOCIATION OF NURSERYMEN.
- THE CONTRACTOR IS RESPONSIBLE FOR ON-GOING MAINTENANCE OF ALL NEWLY INSTALLED MATERIALS UNTIL TIME OF OWNER ACCEPTANCE. ANY ACTS OF VANDALISM OR DAMAGE WHICH MAY OCCUR PRIOR TO OWNER ACCEPTANCE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR SHALL PROVIDE A WRITTEN REQUEST FOR THE OWNER ACCEPTANCE INSPECTION.
- WARRANTY FOR THE LANDSCAPE MATERIALS SHALL BEGIN ON THE DATE OF ACCEPTANCE BY THE LANDSCAPE ARCHITECT AFTER THE COMPLETION OF PLANTING ALL LANDSCAPE MATERIALS. NO PARTIAL ACCEPTANCE WILL BE CONSIDERED.
- CONTRACTOR SHALL GUARANTEE NEW PLANT MATERIAL THROUGH ONE CALENDAR YEAR FROM THE DATE OF OWNER ACCEPTANCE WITH ALL REPLACEMENTS TO BE PROVIDED AT NO ADDITIONAL COST TO THE OWNER.

**IRRIGATION NOTES:**

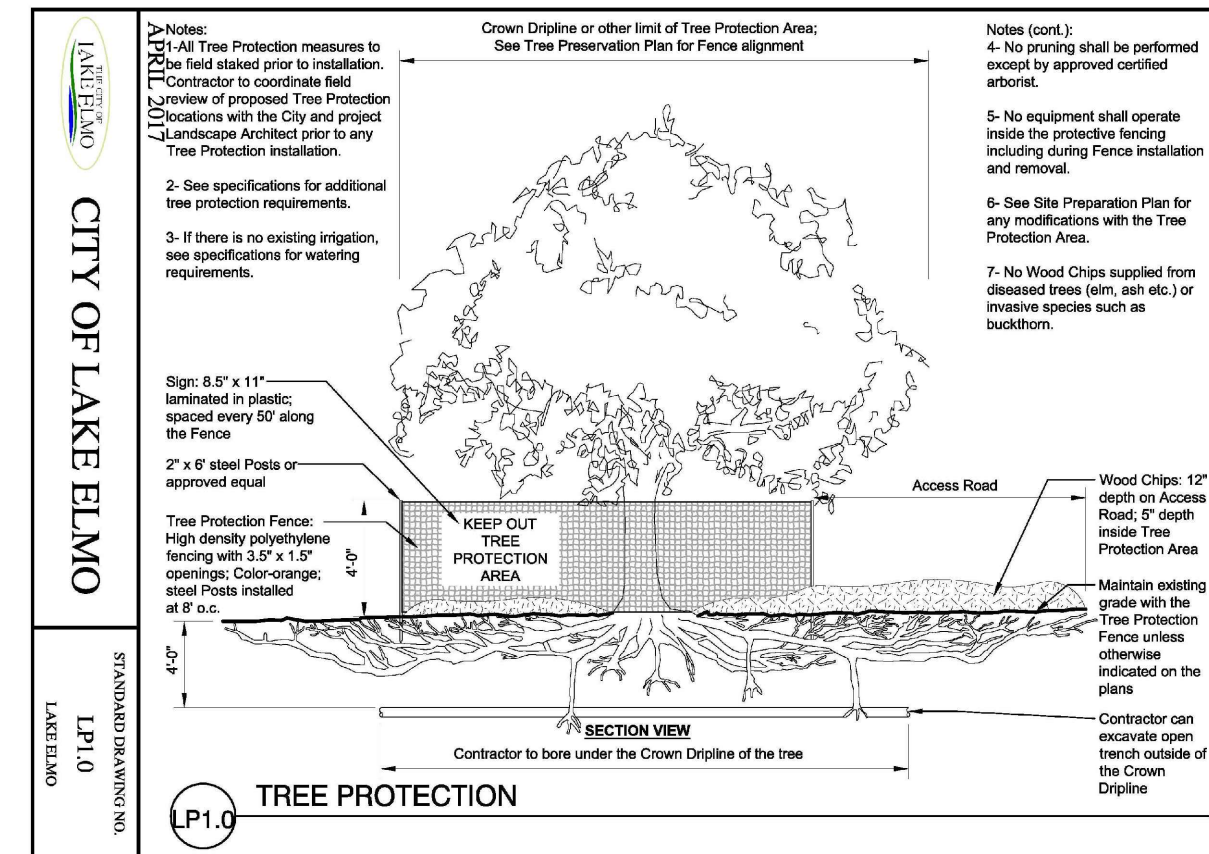
- LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING SHOP DRAWINGS ILLUSTRATING IRRIGATION PLANS AND SPECIFICATIONS AS PART OF THE SCOPE OF WORK WHEN BIDDING. THESE SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO ORDER.
- VERIFY EXISTING/PROPOSED SYSTEM LOCATION.
- UNLESS OTHERWISE NOTED, ALL SOD/SEED AND PLANTING AREAS INDICATED ON THE PLAN SHALL BE 100% IRRIGATED. THE IRRIGATION SHALL INCLUDE THE PUBLIC RIGHT OF WAY BETWEEN THE PROPERTY LINE AND BACK OF CURB WHERE NEW LANDSCAPE IMPROVEMENTS ARE PROPOSED.
- IT SHALL BE THE LANDSCAPE CONTRACTORS RESPONSIBILITY TO INSURE THAT ALL SODDED/SEEDED AND PLANTED AREAS ARE IRRIGATED PROPERLY, INCLUDING THOSE AREAS DIRECTLY AROUND AND ABUTTING BUILDING FOUNDATION.
- THE LANDSCAPE CONTRACTOR SHALL PROVIDE THE OWNER WITH A WATERING/LAWN IRRIGATION SCHEDULE APPROPRIATE TO THE PROJECT SITE CONDITIONS AND TO PLANT MATERIAL GROWTH REQUIREMENTS.

**PLANTING SPECIFICATIONS:**

- PLANTING BED PREPARATION:** ALL MASS PLANTING BEDS SHALL BE TILLED TO A MINIMUM DEPTH OF 10". AMENDMENTS SHALL BE APPLIED AFTER CULTIVATION.
- BACKFILL SOIL:** USE SOIL EXCAVATED FROM PLANTING HOLES & PROVIDE AMENDMENTS. REMOVE ALL DEBRIS AND ROCKS LARGER THAN 3" IN DIA.
- FERTILIZATION:** IT IS RECOMMENDED THAT NEWLY PLANTED TREES AND SHRUBS SHOULD NOT BE FERTILIZED THE FIRST YEAR. IF NECESSARY, BEGIN FERTILIZING TREES AND SHRUBS THE SECOND YEAR AFTER ESTABLISHMENT AT A RATIO AND RATE BASED ON A SOIL TEST.
- MULCH MATERIAL:** AS SPECIFIED ON LANDSCAPE PLANS. MASS MULCH ALL PLANTING BEDS TO 3" DEPTH OVER FIBER MAT WEED BARRIER. ALL PERRENIAL PLANTING BEDS TO RECEIVE 3" DEEP SHREDDED HARDWOOD MULCH WITH NO FIBER WEED MAT BARRIER. ALL DECIDUOUS AND EVERGREEN TREES TO RECEIVE 6" DEEP SHREDDED HARDWOOD MULCH WITH NO MULCH IN DIRECT CONTACT WITH TREE TRUNK.
- TREE STAKING:** IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO STAKE AND/OR GUY THE TREES ACCORDING TO THE DETAILS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO TAKE EVERY STEP NECESSARY TO MAINTAIN THE TREES AND SHRUBS ON AN UPRIGHT AND PLUMB CONDITION AT ALL TIMES UNTIL THE END OF THE PLANT GUARANTEE PERIOD, ESPECIALLY WHERE VANDALISM, SOIL OR WIND CONDITIONS ARE A PROBLEM.
- TREE WRAPPING:** WRAPPING MATERIAL SHALL BE QUALITY, HEAVY WATERPROOF CREPE PAPER MANUFACTURED FOR THIS PURPOSE. WRAP ALL DECIDUOUS TREES IN THE FALL PRIOR TO 12-1 AND REMOVE ALL WRAPPING BY 5-1.
- RODENT PROTECTION:** PROVIDE ON ALL TREES, EXCEPT SPRUCE UNLESS OTHERWISE NOTED.
- PLANTING PLAN:** ALL PROPOSED PLANTS SHALL BE LOCATED CAREFULLY AS SHOWN ON THE PLANS. THE PLAN TAKES PRECEDENCE OVER THE NOTES. RESPECT STATED DIMENSIONS. DO NOT SCALE DRAWINGS.
- EDGING:** EDGING SHALL BE 4" P.V.C COMMERCIAL GRADE EDGING WITH THREE (3) METAL ANCHOR STAKES PER 20 FOOT SECTION, UNLESS OTHERWISE NOTED ON THE PLAN. ALL MASS PLANTING BEDS SHALL HAVE EDGING PLACED BETWEEN MULCH AND ANY ADJACENT TURF AREAS.

**PLANTING NOTES:**

- NO PLANTING TO BE INSTALLED UNTIL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
- ALL PLANT MATERIAL LOCATIONS MUST BE REVIEWED AND APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO ANY AND ALL DIGGING.
- IF THE LANDSCAPE CONTRACTOR PERCEIVES AND DEFICIENCIES IN THE PLANT SELECTIONS, SOIL CONDITIONS, OR ANY OTHER SITE CONDITION WHICH MIGHT NEGATIVELY AFFECT PLANT MATERIAL ESTABLISHMENT, SURVIVAL OR GUARANTEE, THEY SHALL BRING THESE DEFICIENCIES TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- ADJUSTMENTS IN THE LOCATION OF PROPOSED PLANT MATERIAL MAY BE NEEDED IN THE FIELD. SHOULD AN ADJUSTMENT BE ADVISED, THE LANDSCAPE ARCHITECT MUST BE NOTIFIED.
- ALL PLANTS TO BE INSTALLED PER PLANTING DETAILS.
- ONE SHRUB PER TYPE AND SIZE IN EACH PLANTING BED AND EVERY TREE SHOULD BE CLEARLY IDENTIFIED (COMMON OR LATIN NOMENCLATURE) WITH A PLASTIC TAG WHICH SHALL NOT BE REMOVED PRIOR TO OWNER ACCEPTANCE.
- WHERE SOD/SEED ABUTS PAVED SURFACES, FINISHED GRADE OF SEED/SOD SHALL BE HELD 1" BELOW SURFACE ELEVATION OF TRAIL, SLAB, CURB, ETC.
- SOD SHALL BE LAID PARALLEL TO THE CONTOURS AND SHALL HAVE STAGGERED JOINTS. ON SLOPES STEEPER THAN 3 : 1 OR IN DRAINAGE SWALES, THE SOD SHALL BE STAKED TO THE GROUND.
- SEED ALL AREAS DISTURBED DUE TO GRADING OTHER THAN THOSE TO RECEIVE SOD.
- REPAIR ALL DAMAGE TO PROPERTY FROM PLANTING OPERATORS AT NO COST TO THE OWNER.



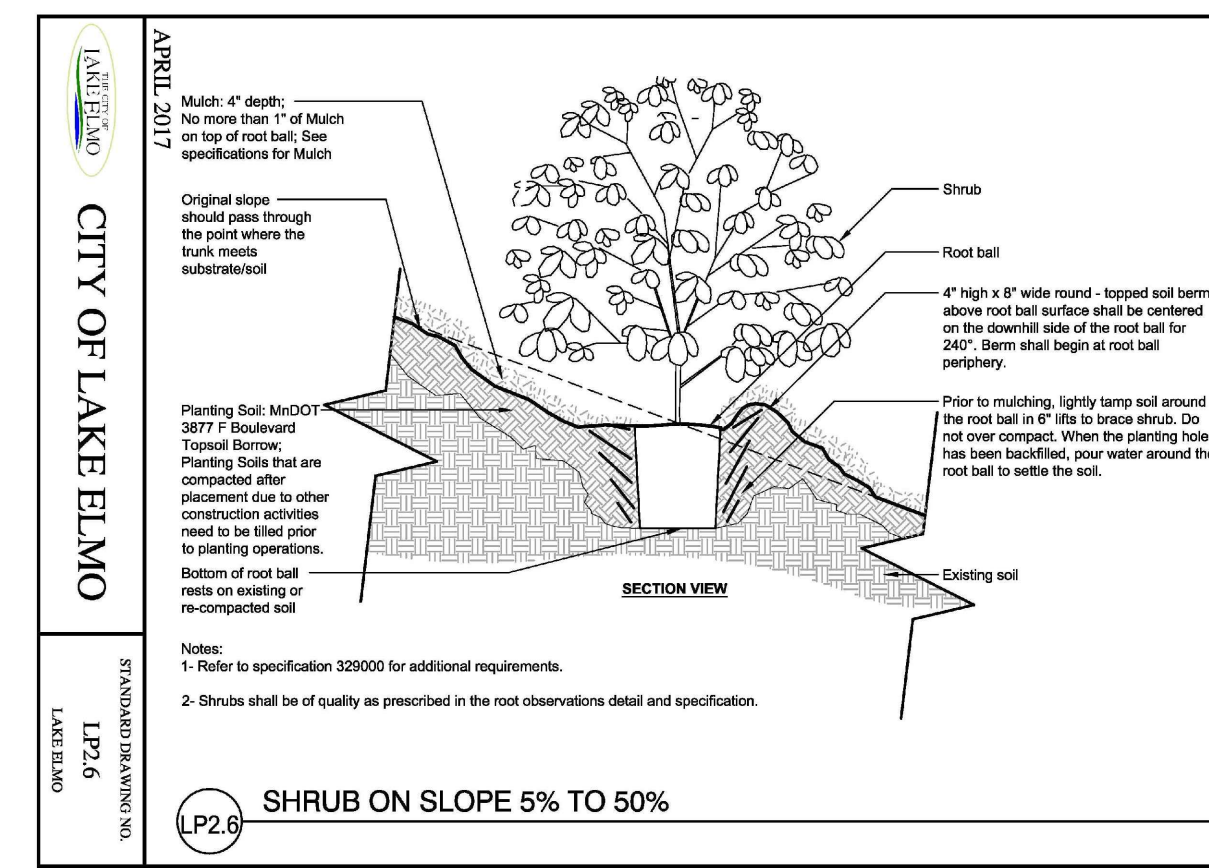
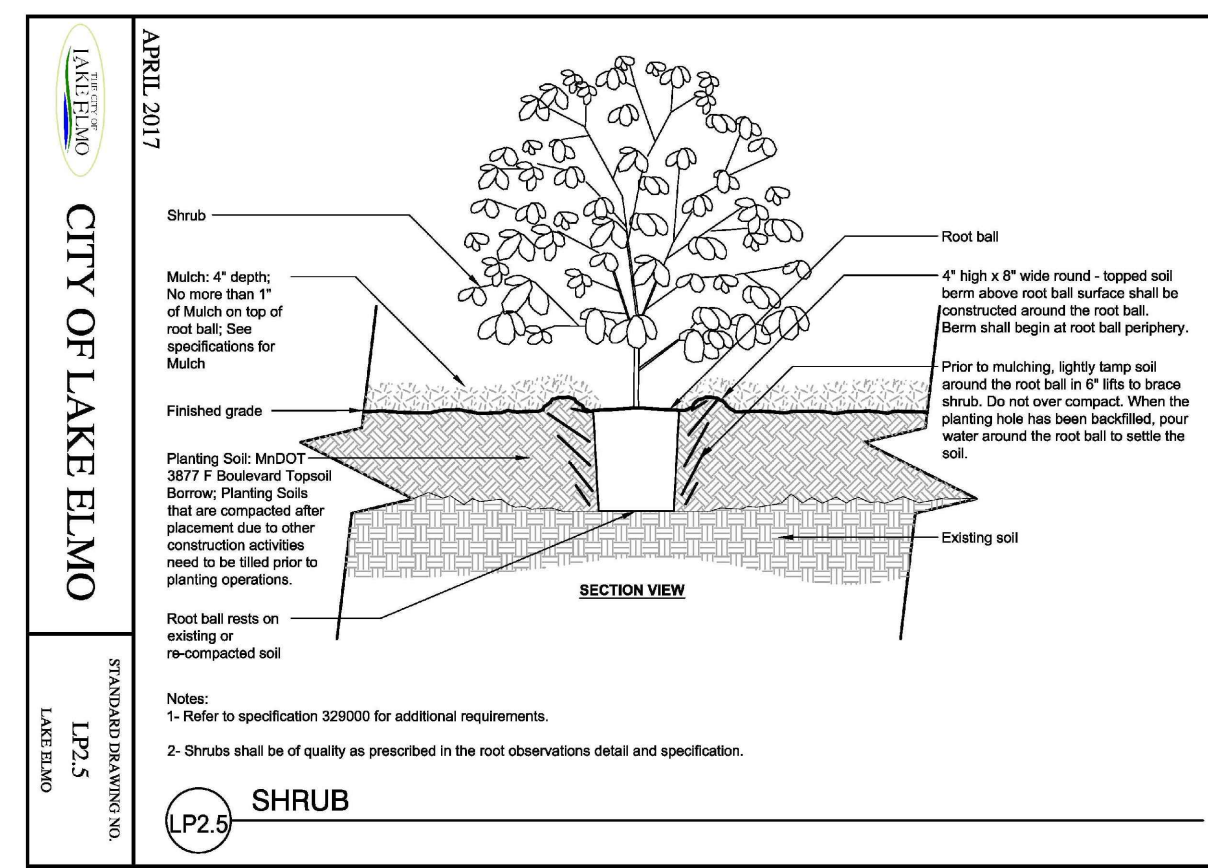
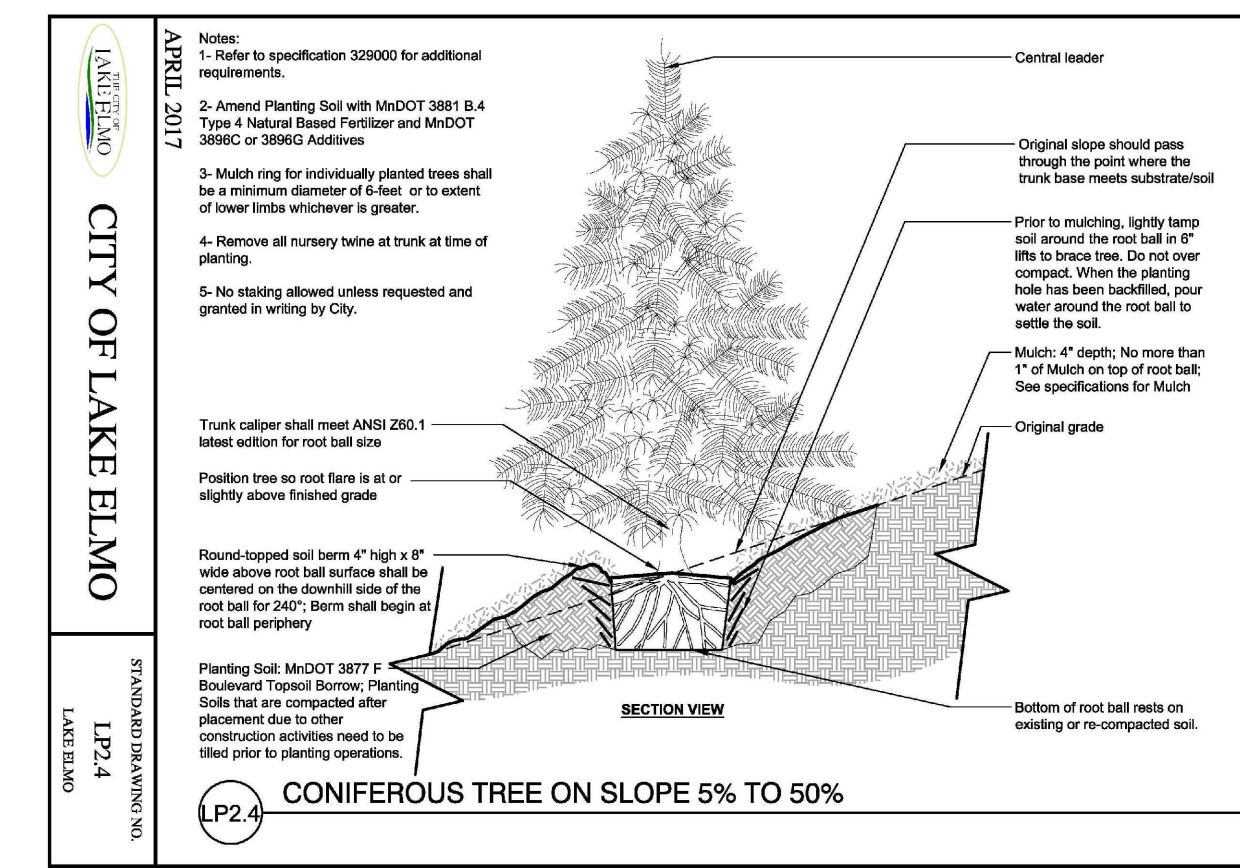
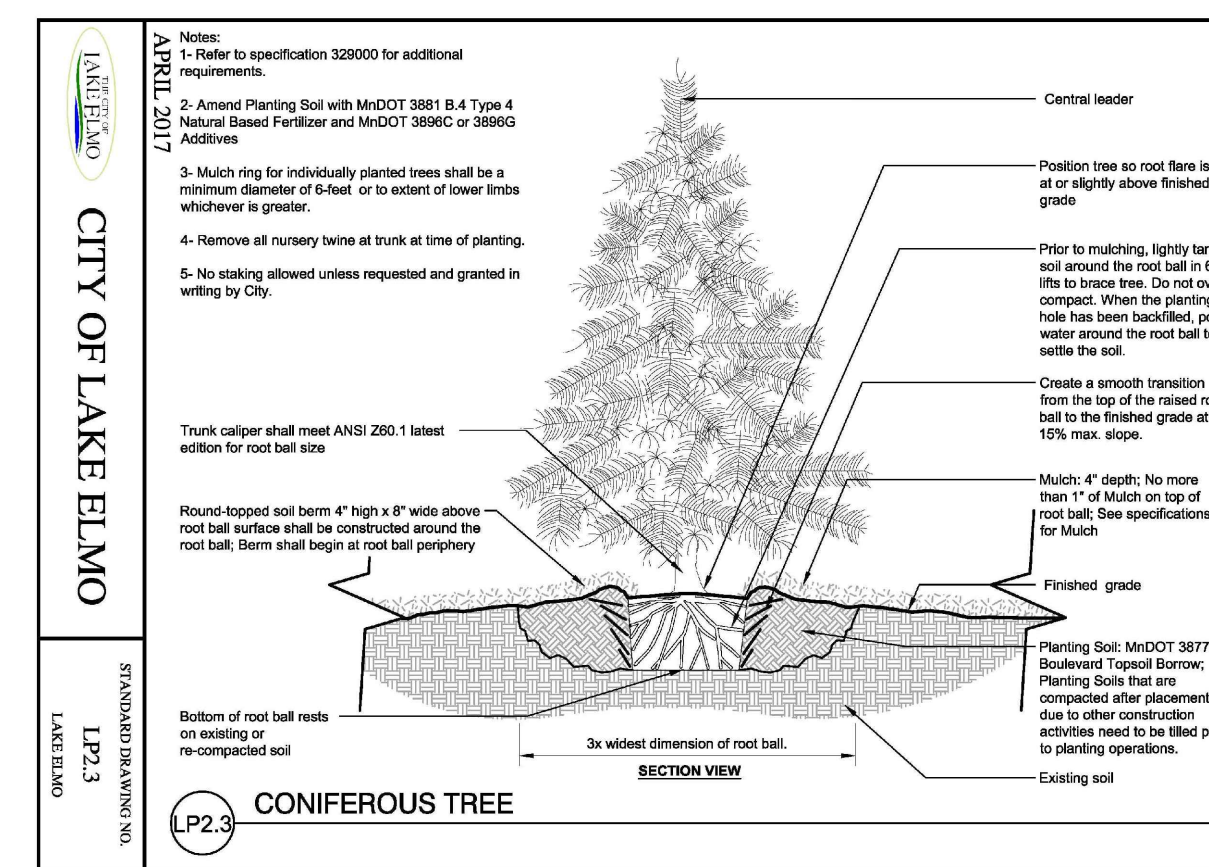
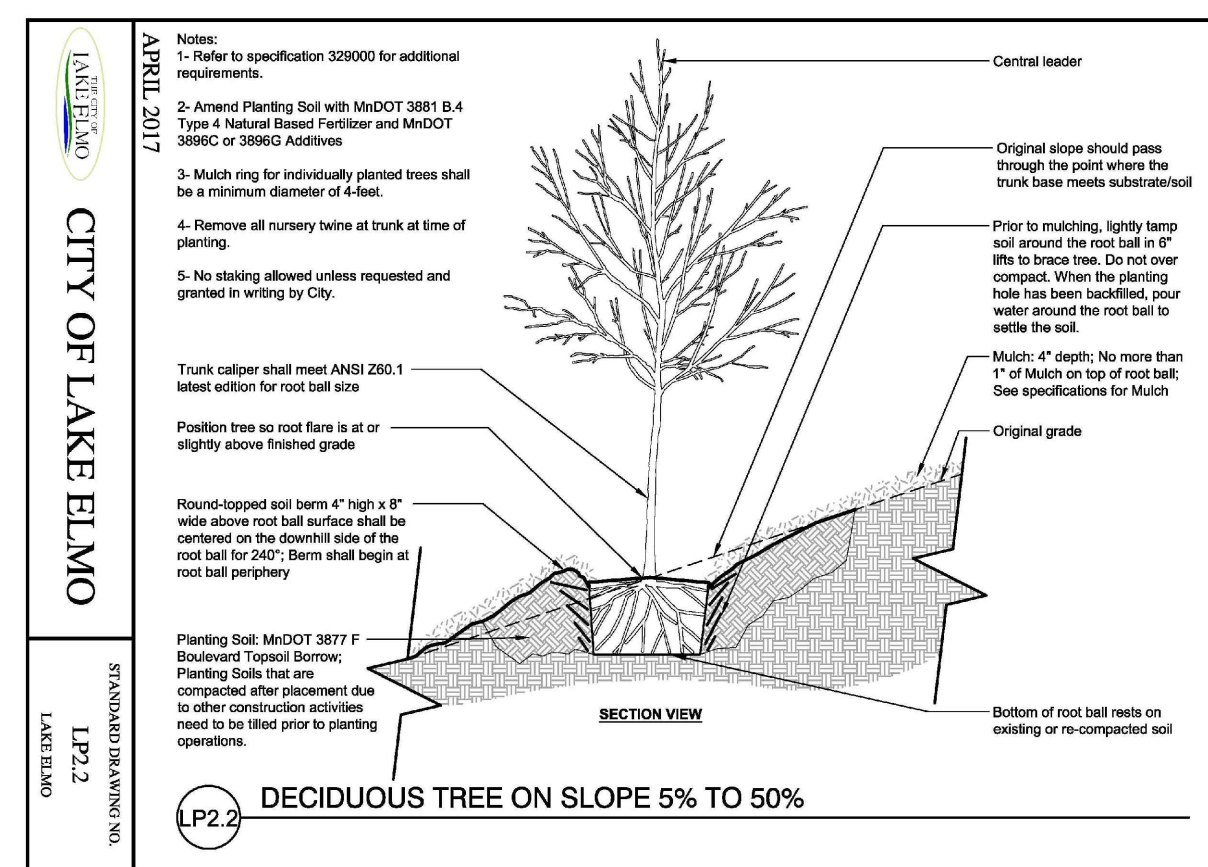
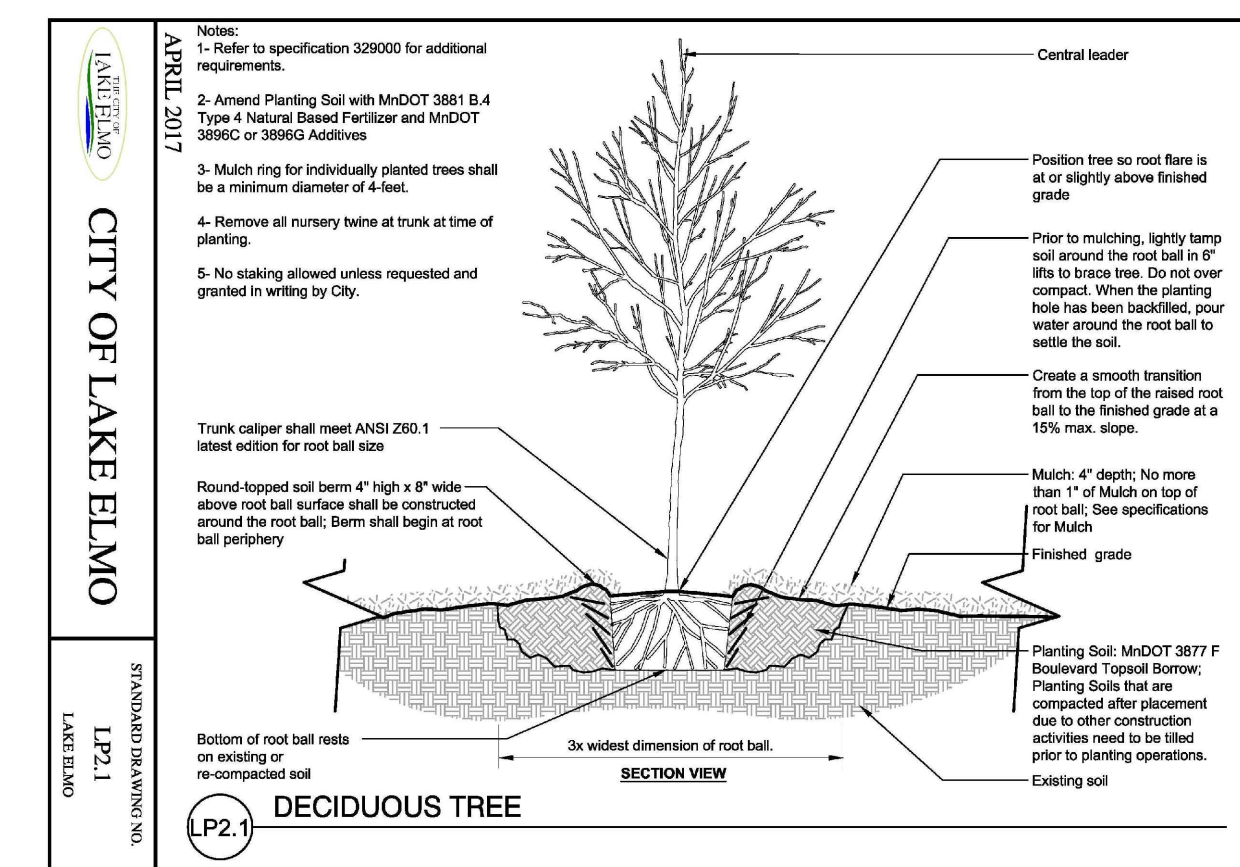
**STANDARD PLAN NOTES**  
LANDSCAPE PLANS

- Contractor must contact Gopher State One Call 811 ([www.gopherstateonecall.org](http://www.gopherstateonecall.org)) prior to tree staking and planting operations to verify underground utilities. Where private utilities exist on-site the Contractor is required to have those located as well.
- Plant materials shall meet American Standard for Nursery Stock: ANSI Z60.1, latest edition.
- No plant substitutions shall be made without the prior written authorization from the City.
- All tree locations to be field staked prior to installation. Contractor to coordinate field review of proposed tree locations with the City and Project Landscape Architect prior to any tree installation.
- All plants shall be planted immediately upon arrival to project site. No plant material is to be left overnight on the project site without being installed unless written approval by City.
- All trees, shrubs, perennials, turf lawn and native seeding to have a two-year warranty beginning upon written acceptance by the City. Defective plants as determined by the City shall be replaced within 30 days of notice during the growing season, and replacement materials shall receive the same two year warranty until plants are successfully established.
- Contractor to protect and maintain all plantings and plant beds, including protection from wildlife, weeding, re-mulching, fertilization, irrigation and all other typical forms of horticultural care until the end of the warranty period as determined and approved by City.
- All plants installed and maintained on City property shall be in accordance with City of Lake Elmo Bee-Safe Resolution No. 2015-13.
- An irrigation system or other water supply adequate to support the specified plant materials shall be provided.

APRIL 2017

CITY OF LAKE ELMO

STANDARD DRAWING NO. LP2.0 LAKE ELMO



**Carlson McCain**  
ENVIRONMENTAL - ENGINEERING - SURVEYING

3890 Pheasant Ridge Dr. NE #100, Blaine, MN  
Phone: 763-489-7900 Fax: 763-489-7959

**LANDSCAPE DETAILS**

**LAKE ELMO SHOPPES**  
Lake Elmo, Minnesota

**MFC PROPERTIES CORP.**  
3460 Washington Drive, Suite 100  
Eagan, MN 55122

**REVISIONS**

1.	04/26/17 PER CITY COMMENT
2.	
3.	
4.	
5.	
6.	

**DRAWN BY:** SDB  
**DESIGNED BY:** SDB  
**ISSUE DATE:** 02/03/17

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed Landscape Architect under the laws of the State of Minnesota

**Name:** James A. Kalkes  
**Signature:** *James A. Kalkes*  
**Date:** 05/24/17 **License #:** 45071

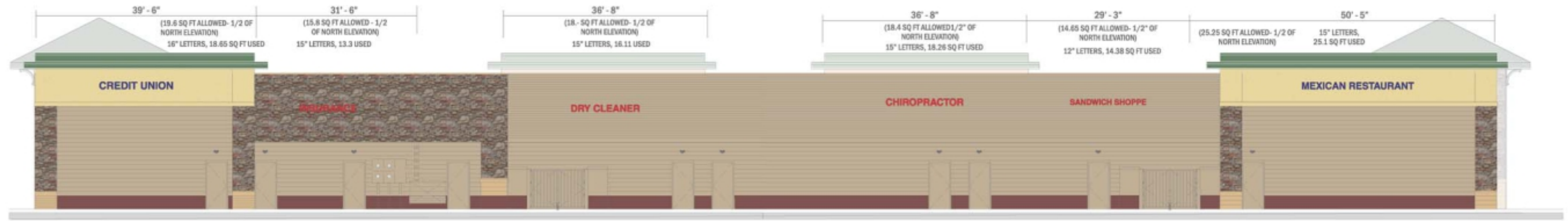
L2 of 2





1X PER LF OF FRONTAGE

1 PROPOSED: NORTH ELEVATION (FRONT), ILLUMINATED CHANNEL LETTERS. 5" RETURNS, 1" TRIMCAP, 3/16" ACRYLIC FACES.  
 1 SCALE: 1"=20'-0"



1/2X SQ FT OF NORTH EL

2 PROPOSED: SOUTH ELEVATION (BACK), ILLUMINATED CHANNEL LETTERS. 5" RETURNS, 1" TRIMCAP, 3/16" ACRYLIC FACES.  
 1 SCALE: 1"=20'-0"



2.5X PER LF OF FRONTAGE

1  
1 PROPOSED: NORTH ELEVATION (FRONT), ILLUMINATED CHANNEL LETTERS. 5" RETURNS, 1" TRIMCAP, 3/16" ACRYLIC FACES.  
SCALE: 1"=20'-0"



2.5X PER LF OF FRONTAGE

2  
1 PROPOSED: SOUTH ELEVATION (BACK), ILLUMINATED CHANNEL LETTERS. 5" RETURNS, 1" TRIMCAP, 3/16" ACRYLIC FACES.  
SCALE: 1"=20'-0"



1.5X PER LF OF FRONTAGE

3  
1 PROPOSED: WEST & EAST ELEVATIONS, ILLUMINATED CHANNEL LETTERS. 5" RETURNS, 1" TRIMCAP, 3/16" ACRYLIC FACES.  
SCALE: 1"=20'-0"

**NOTE:**  
 COLORS, TENANT PANEL  
 CONFIGURATION AND DESIGN  
 ELEMENTS ARE SHOWN ONLY  
 TO REPRESENT PROPOSED  
 SIZES AND DIMENSIONS.  
 ALL ARE SUBJECT TO CHANGE.

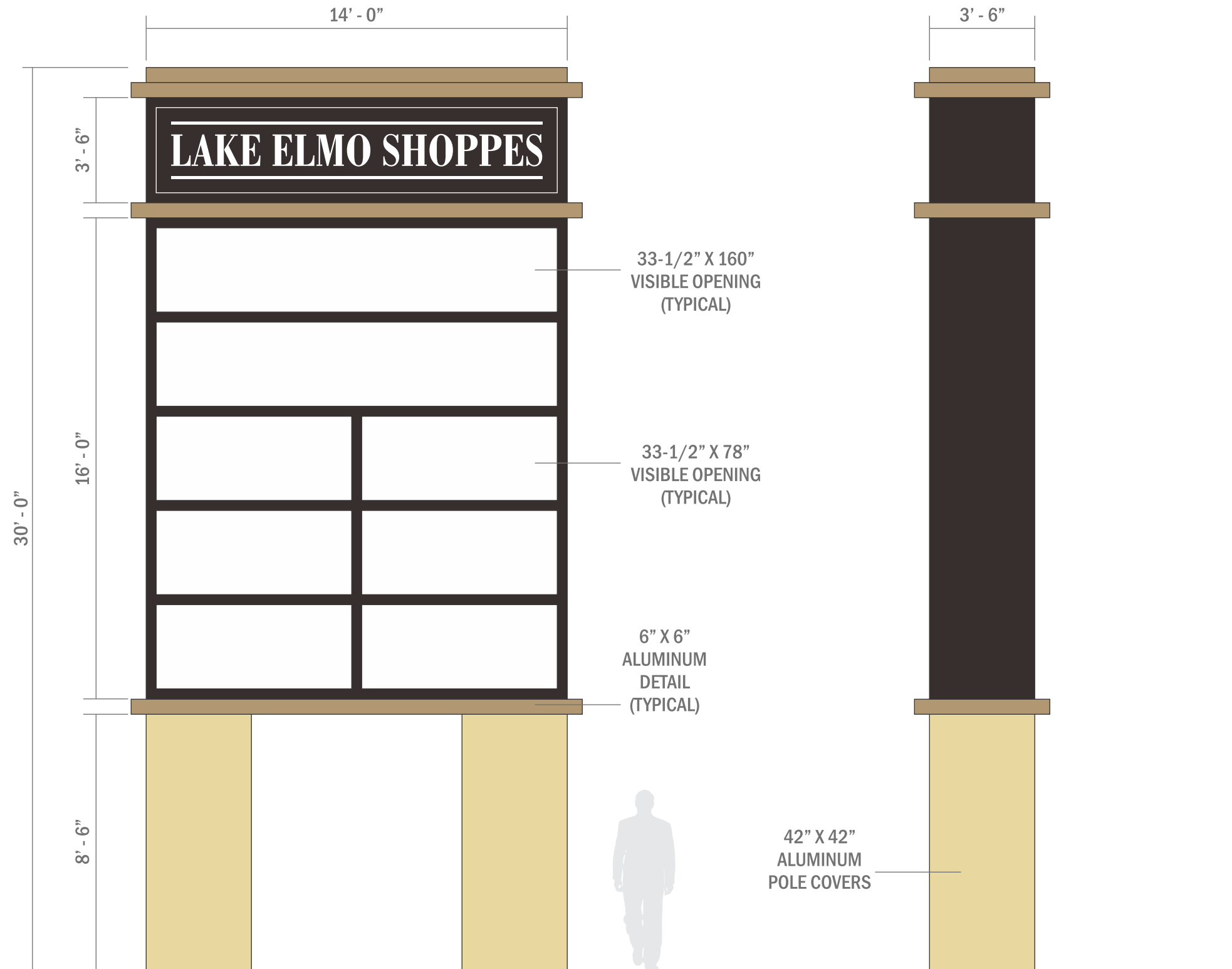


3  
1

PROPOSED: ILLUMINATED MONUMENT SIGN WITH TENANT PANELS

SCALE: 1/2" = 1' - 0"

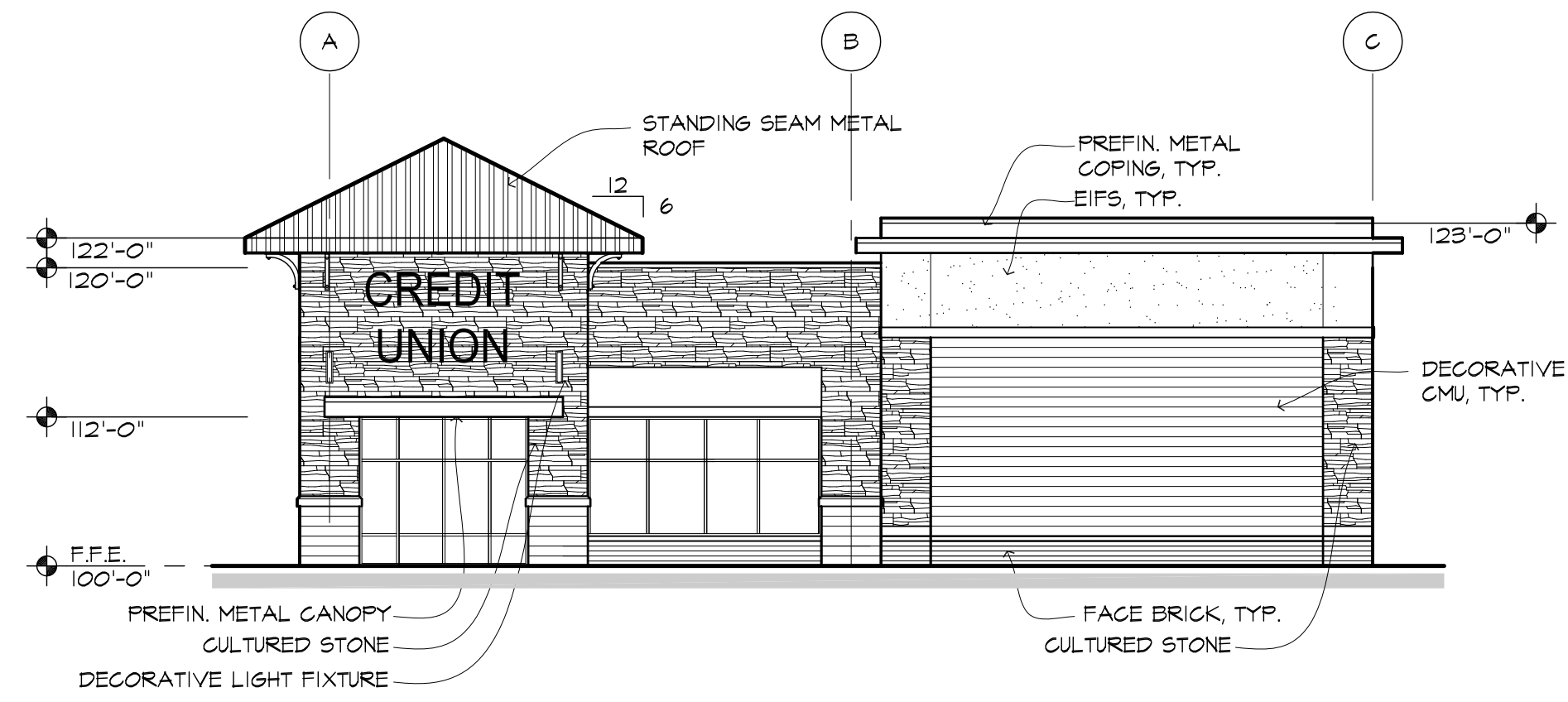
**NOTE:**  
 COLORS, TENANT PANEL  
 CONFIGURATION AND DESIGN  
 ELEMENTS ARE SHOWN ONLY  
 TO REPRESENT PROPOSED  
 SIZES AND DIMENSIONS.  
 ALL ARE SUBJECT TO CHANGE.



4  
1

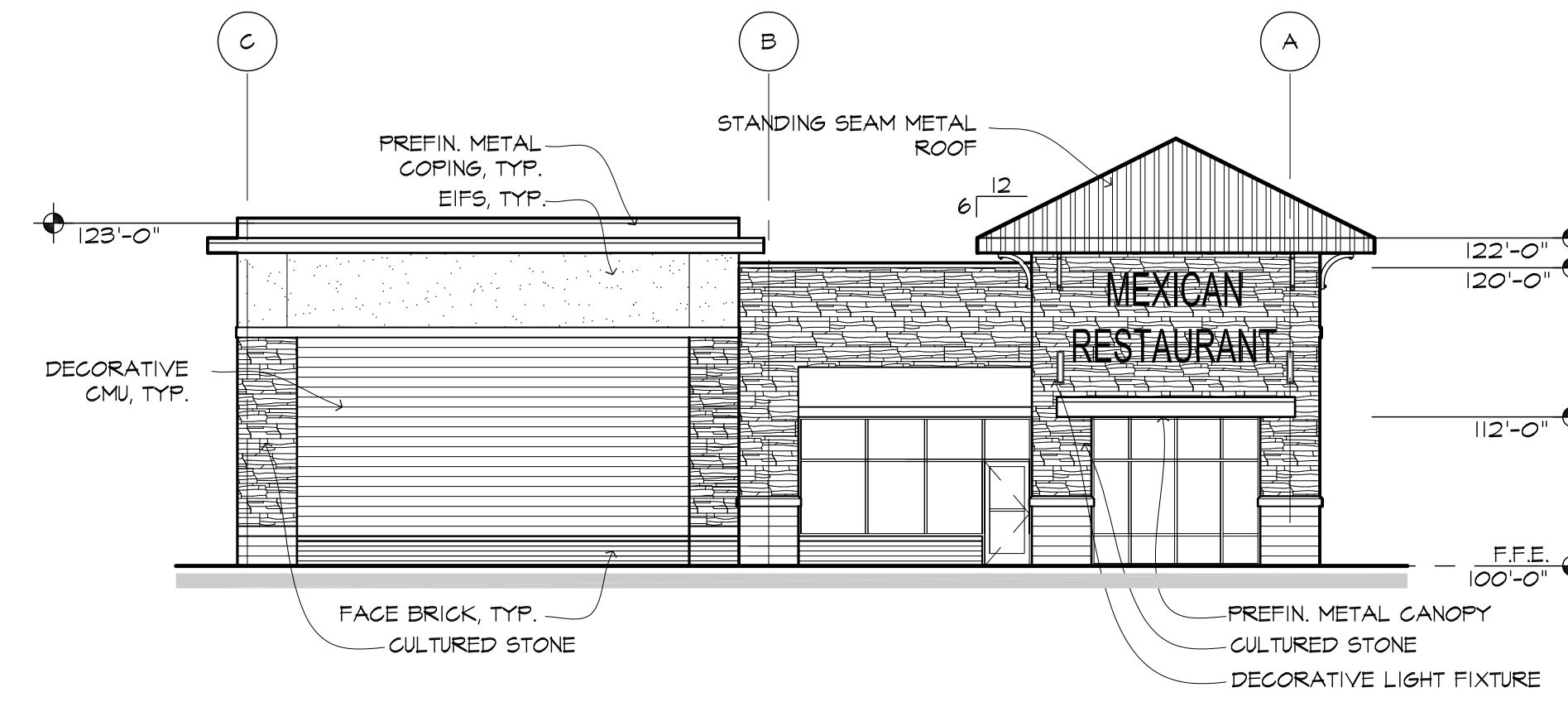
PROPOSED: ILLUMINATED MAIN ID PYLON SIGN WITH TENANT PANELS

SCALE: 1/4" = 1' - 0"



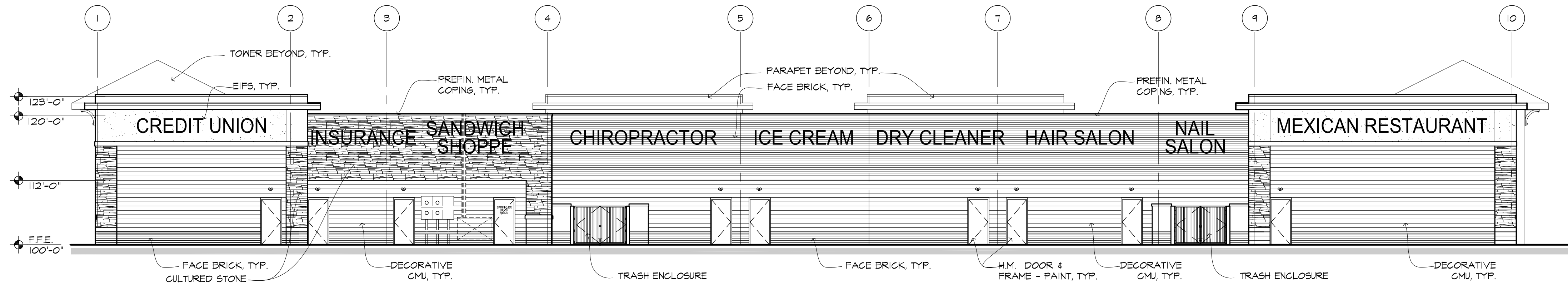
1 WEST ELEVATION

3/32" = 1'-0"



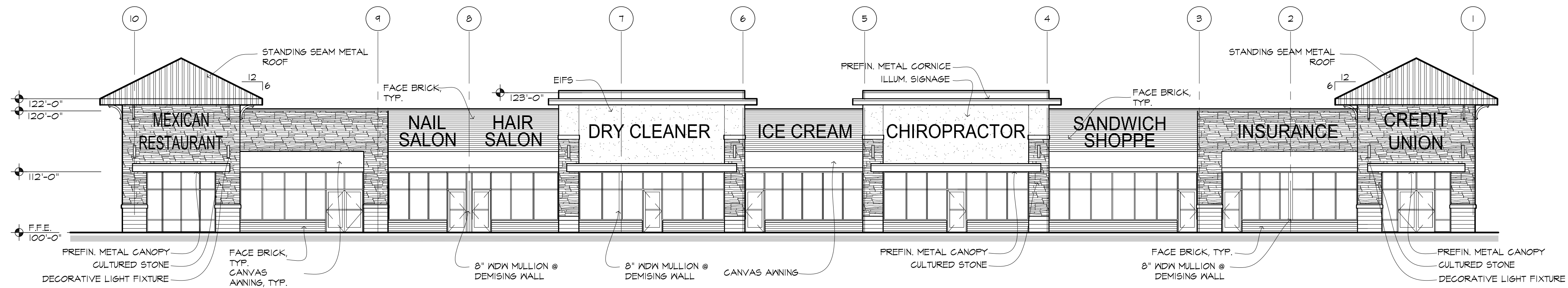
2 EAST ELEVATION

3/32" = 1'-0"



3 SOUTH ELEVATION

3/32" = 1'-0"



4 NORTH ELEVATION

3/32" = 1'-0"

Mark	Revision / Issue	Date
	CITY SUBMITTAL	05/01/17

**PRELIMINARY**  
NOT FOR CONSTRUCTION

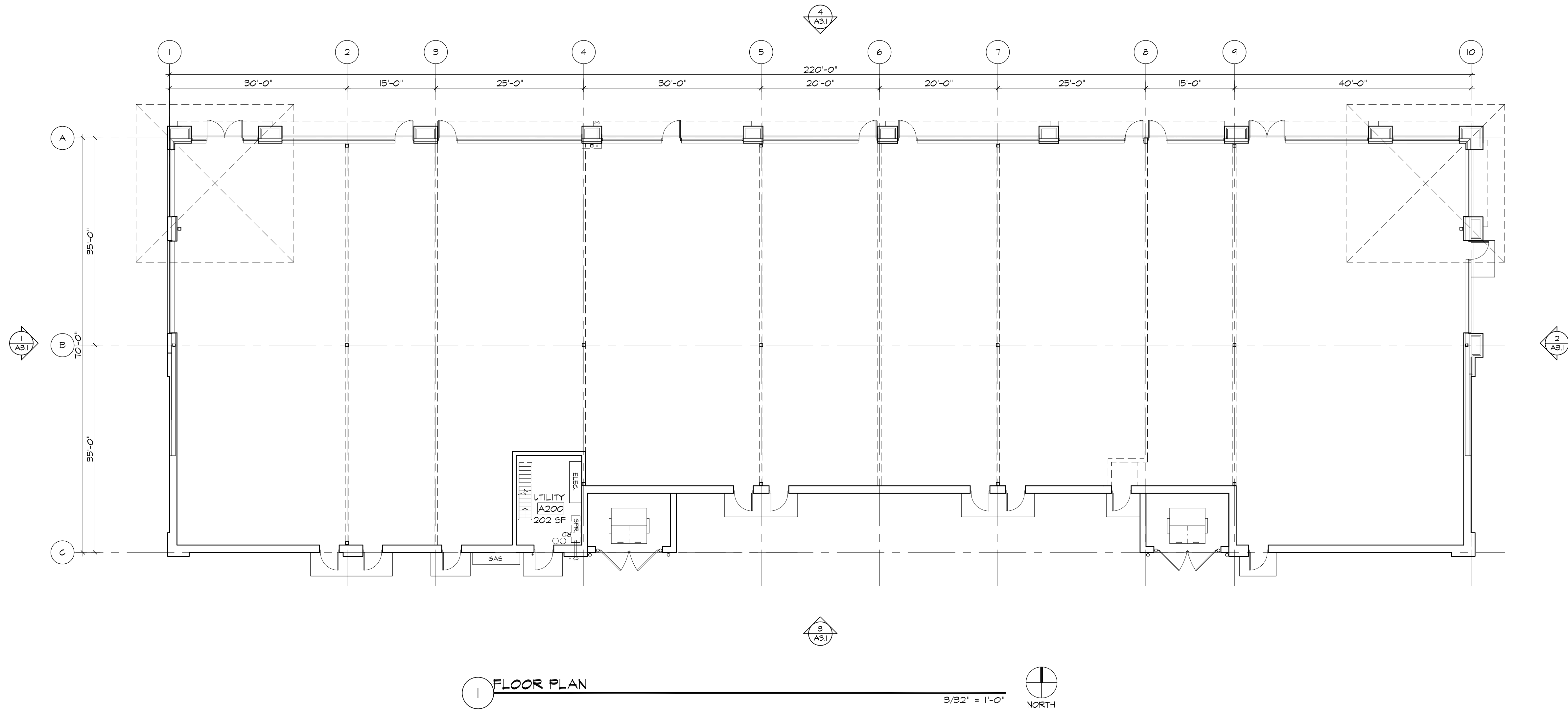
**LAKE ELMO  
SHOPPES**

LAKE ELMO, MN

**PROPOSED EXTERIOR  
ELEVATIONS**

PROJECT NUMBER:	17-1001-01
ISSUED DATE:	05-01-17
DRAWN BY:	MR
CHECKED BY:	KA

**A3.1**



**1 FLOOR PLAN** 3/32" = 1'-0" NORTH

Mark	Revision / Issue	Date
	CITY SUBMITTAL	05/01/17

**PRELIMINARY**  
NOT FOR CONSTRUCTION

**LAKE ELMO  
SHOPPES**

LAKE ELMO, MN

**PROPOSED  
FLOOR PLAN**

PROJECT NUMBER:	17-1001-01
ISSUED DATE:	05-01-17
DRAWN BY:	MR
CHECKED BY:	KA

**A2.1**

# MEMORANDUM

# FOCUS ENGINEERING, inc.

Cara Geheren, P.E. 651.300.4261  
Jack Griffin, P.E. 651.300.4264  
Ryan Stempski, P.E. 651.300.4267  
Chad Isakson, P.E. 651.300.4285

Date: May 31, 2017

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To: Emily Becker, City Planner  
Cc: Stephen Wensman, Planning Director  
From: Jack Griffin, P.E., City Engineer  
Re: Lakewood Crossing 2nd Addition  
Preliminary/Final Plat Review

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An engineering review has been completed for the Lakewood Crossing 2nd Addition Preliminary/Final Plat. The submittal consisted of the following documentation received on May 30, 2017 and prepared by Carlson McCain:

- Lake Elmo Shoppes Site Improvement Plans dated May 24, 2017.
  - Lake Elmo Shoppes Stormwater Management Plans dated April 26, 2017.
  - Lakewood Crossing 2nd Addition Plat dated April 26, 2017.
  - Narrative, not dated.
- 

**STATUS/FINDINGS:** Following comments should be incorporated as conditions of Preliminary/Final Plat approval.

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## FINAL PLAT: LAKEWOOD CROSSING 2ND ADDITION

- The Final Plat shall not be recorded until final construction plan approval is granted and all easements as requested by the City Engineer and Public Works department are documented on the Final Plat.
- No construction for Lakewood Crossing 2nd Addition may begin until the applicant has received City Engineer approval for the Final Construction Plans; the applicant has obtained and submitted to the City applicable permits, easements and permissions needed for the project; and a preconstruction meeting has been held by the City's engineering department.
- Final Construction Plans and Specifications must be prepared for any phased site improvements and submitted for review and approval by the City Engineer prior to construction. The Final Construction Plans and Specifications must be prepared in accordance with the City Engineering Design Standards Manual using City details, plan notes and specifications and meeting City Engineering Design Guidelines.
- The preliminary plans must be updated and the final construction plans must include a right turn lane along eastbound Hudson Boulevard and any additional fire hydrants as required the Fire Chief/Building Official.
- The preliminary/final construction plans must be updated to include all necessary drainage and utility easements as required for the public sanitary sewer and watermain/hydrants.
- There are no Outlots as part of this Plat to be dedicated to the City.
- Prior to the start of construction a Stormwater Maintenance and Easement Agreement in the City's standard form must be executed and recorded with the County.

### *Traffic and Access Management Requirements:*

- Primary access. The shared access location with Kwik Trip has been approved for this site.
- Right turn lane should be required on Hudson Boulevard. There is an existing westbound left turn lane on Hudson Boulevard at the existing access location to this development. However an eastbound right turn should be required as part of this project to handle the increased traffic volumes. Hudson Boulevard is a local

collector roadway and Municipal State Aid route. Hudson Boulevard is expected to receive significant growth in traffic volume as the I94 corridor develops. The road is considered to be a major collector for serving the area but it is the goal of the City to maintain the road as 2-lanes. In order to achieve that goal left and right turn lanes will need to be implemented throughout the corridor to facilitate the turning movements for the developing areas while maintaining the mobility of the through traffic.

- Secondary access. The site does not have sufficient frontage along Hudson Boulevard to accommodate a secondary access. The secondary access shown on the concept plans approximately 250 feet to the west of the shared access has therefore been eliminated as required by staff. The Access Management Guidelines per the City's Comprehensive Transportation Plan requires access spacing of 1/8 mile (660 feet) for full access intersections and commercial driveways along Hudson Boulevard. A shared access driveway was planned as part of the Lakewood Crossing 1st Addition to allow access to the proposed development area while maintaining the required access spacing guidelines.
- Future secondary access potential. A second access location could be planned along the south side of Hudson Boulevard that would align with the property to the west of Lakewood 2 Addition with this parcel (PID No. 3402921440004) coordinating and sharing access with Lakewood 2nd Addition. A potential future access connection to the adjacent westerly property has been shown on the site plan.
- Right-in/Right-out access locations cannot be allowed along Hudson Boulevard since the roadway does not include raised center medians to prohibit left turning movements from the site. There currently are no plans for a center raised median along Hudson Boulevard.

#### *Grading, Drainage & Erosion Control Plan:*

- Governing Specifications and Plan Details for grading and erosion control must be in accordance with the City of Lake Elmo standard specifications.
- Retaining walls that exceed 4 feet in height must have a design submitted and certified by an engineer licensed in the state of Minnesota.

#### *Utility Plans:*

- Connection to existing sanitary sewer stub. The project proposes to connect to the existing sanitary sewer stub located in the northeast corner of the property and is extended throughout the development to connect 3 commercial buildings. Sanitary sewer is extended along the northwest property line and stubbed to the westerly adjacent plat limits as required by City policy. A drainage and utility easement is shown on the plat over the portion of sanitary sewer to be owned and maintained by the City.
- Connection to existing watermain stub. The project proposes to connect to an existing 8-inch watermain located in the northeast corner of the property and is extended along the northwest property line and stubbed to the westerly adjacent plat limits as required by City policy. A service stub is shown for connection for Lot 3. A lateral 6-inch watermain to the south of the development is also proposed for the placement of a fire hydrant and the connection of two additional buildings. A drainage and utility easement is shown on the plat over the portion of watermain/hydrant to be owned and maintained by the City.
- Fire Hydrant locations. Additional fire hydrants may be required based on review by the Fire Chief/Building Official. All watermain and fire hydrants required for the project are to be owned and maintained by the City and will require the necessary easements outlined below.
- Drainage and utility easements are required over all public sanitary sewer and watermain not located on City Outlots and right-of-way, minimum 30-feet in width, 15 feet from centerline on each side of pipe (including 15 feet from all sides of a fire hydrant). Drainage and utility easements must be provided on the plat or in the City's standard form of easement agreement.

#### *Stormwater Management:*

- The site plan is subject to a storm water management plan meeting State, VBWD and City rules and regulations. A VBWD permit has been obtained for the improvements.
- Stormwater runoff will be routed through two underground parking lot storm chamber systems that will provide infiltration and retention in addition to one above ground infiltration basin. The above ground infiltration basin will be obtained by converting an existing storm water pond to an infiltration basin.



Pretreatment for the infiltration systems will be provided by oversized sump manholes equipped with scour prevention devices.

- Due to the proximity of the site to the Kwik Trip Service Station the applicant should review with MPCA if infiltration practices will be allowed, in particular the planned conversion of the existing storm water pond to a storm water infiltration basin directly east and adjacent to the service station.
- The proposed storm water facilities will be privately owned and maintained. Prior to the start of onsite construction a Stormwater Maintenance and Easement Agreement in the City's standard form must be executed and recorded with the County. The agreement shall provide a maintenance plan defining the maintenance responsibilities for the private owner, the type of maintenance and the maintenance intervals.
- Written landowner permission may be required for any off-site storm water discharges to adjacent properties to avoid negative impacts to downstream properties.

## COMPREHENSIVE SIGN PLAN NARRATIVE

Lake Elmo Shoppes is proposed to be an upscale neighborhood shopping and dining experience. Signage for Lake Elmo Shoppes should be allocated and designed to encompass a professional, first class look and feel. When potential tenants deliberate on signing leases and joining the Lake Elmo business community, they must also have the confidence that their brand will be properly showcased and seen by the vast numbers of commuters and the local traffic patterns. Our signage goals are to provide consistency in size and construction materials, while allowing each tenant the flexibility and square footage to showcase their building storefronts, brand names, and individual site locations as best as possible. Lake Elmo Shoppes is proposing wall signage and two (2) ground signs.

### **Wall Signs:**

As we began to develop the sign criteria for the various buildings at the Lake Elmo Shoppes, we reviewed what the current sign code allows, which is 1 square foot of signage per 1 lineal foot of storefront on the front elevations and .5 square foot of signage per 1 lineal foot of frontage for the rear elevation. Wall signs sized to code are represented on the attached Exhibit 1 "Sign Per Code". As you can see on Exhibit 1, at these ratios the signs look small on the storefront and do not "fit" the façade. The signs look dwarfed on these elevations and certainly do not give the look or exposure that tenants would consider adequate. Regarding the even smaller signs on the back, the Tenants would pass on adding signage on the rear of the property as the cost and readability would make for an unworthy investment.

Exhibit 2 "Comprehensive Sign Plan" represents a 2.5 square feet of signage per 1 lineal foot of storefront on the front and rear elevations, and a 1.5 square feet of signage per 1 lineal foot on the west and east elevations. As you will see, these signs look much more proportional and "fit" the elevations much better. These ratios will also create consistency with any size proportion issues that arise with future tenants and their short and long business names. The north elevation will now be visible from Hudson Blvd., and tenant's will be able to showcase their place of business. The rear signage is really designed to capture the traffic from the westbound on-ramp traffic to I-94. With this increased size, it will now be readable and will help in the promotion and success of the Lake Elmo Shoppes retail and restaurant businesses. The 1.5 square feet of signage per 1 lineal foot on the west and east elevations will allow those tenants the opportunity to capture additional angles of east and west traffic and stop light patterns as commuters enter the freeway as well as from the adjacent Kwik Trip.

The intention of the increased ratios is to develop a uniform criteria that creates a fair amount of square footage allowance for all tenants. Since there is uncertainty of what new businesses will occupy each space at this time, utilizing this new ratio implements tenant fairness while ensuring a professional look. This will also address the variety and length of actual store names, their corporate sign regulations, and desired letter sizes that tenants will be required or want to achieve with their space. This provides flexibility and approval to enhance their investment with the specified ratio, provided that their identity and look is approved by the Landlord.

**Ground Sign #1:**

Ground Sign #1 is an attractive 10'x12' multi-tenant, sign to be located in the northeastern access and will face Hudson Boulevard. As the two main retail buildings are set back up to 400' from Hudson Boulevard, this monument will allow potential customers to know which businesses are in the shopping center. Ground Sign #1 will help in the promotion and success of these Lake Elmo businesses.

**Ground Sign #2:**

Ground Sign #2 is an attractive 14'x30' multi-tenant sign located in the southwest corner of the development and is intended to provide a signage opportunity to the over 100,000 VPD on I-94. Ground Sign #2 will have print that is large enough for the traveling public to be able to notice what businesses are in Lake Elmo Shoppes which will further enhance the promotion and success of these Lake Elmo businesses.

Lake Elmo Shoppes has 22' high, prominent storefronts and proportionate signage on these storefronts is necessary both from the tenants branding perspective as well as the building look. We believe this Comprehensive Sign Plan overall enhances the quality and effectiveness of the shopping and dining experience.

**EXHIBIT A**  
**SIGN CRITERIA**

**Lake Elmo Shoppes, Lake Elmo, MN 55042**

Tenant agrees to be bound and comply with the Sign Criteria as follows:

1. EXTERIOR SIGNAGE:

- A. Tenant shall be required to identify its Leased Premises with an exterior sign.
- B. Tenant's signs shall be retail store identity signs only with restricted copy to designate the Tenant's name, product, or service.
- C. Each Tenant will be allocated an area on the exterior sign band of the building.
- D. Sign size and placement will be restricted to the sign band. The beginning and end of the Tenant's sign shall not be nearer than 18" to outside edges of the Tenant's allocated sign area, and determined by Lessor as per Exhibit G. All signs shall contain individual letters. All signs shall be lighted, and all transformers will be installed inside the Tenant's Leased Premises. All exterior signs shall be located within the designated signing area. Any drilling or boring to be done through the exterior wall of the building shall be performed in accordance with all architecturally accepted methods and every precaution shall be taken to assure that these areas be sufficiently waterproofed. All illuminated sign letters shall be channel type construction, plastic faces attached with trim cap. All illuminated sign letters must be LED lit. Colors need to be approved by the Landlord, raceways behind the parapet wall are required on all signage above the roof line and must be installed in a manner as to not void the roof warranty.
- E. Each Tenant is allowed (1) sign on the Front of the Building (North Elevation) and (1) on the Back of the Building (South Elevation).
- F. The total sign surface area of all wall signs on a façade shall not exceed 2.5 square feet of sign area per 1 lineal feet of storefront leased on the front elevation.
- G. Each end cap Tenant is allowed (1) additional signage area that will be utilized to face the west or east elevations for a maximum total of (3) signs for those Tenant's only. End cap tenants are allowed (1) added sign that is 1.5 sf per lf of leased frontage on that elevation.
- H. Awnings are acceptable with Landlord approval. Awnings cannot include logos or copy.
- I. The use of predominantly decorative sculpture, coat of arms, shields or other such logos requires special approval by Landlord
- J. Landlord expressly reserves the right to deviate from this sign criteria when dealing with other tenants.

2. GROUND SIGN #1:

- A. (1) freestanding multi-tenant ground sign shall be allowed in the northeast corner of the development providing signage to Hudson Boulevard.
- B. Ground sign is limited to a maximum height of 12.5'.
- C. Landlord will allocate which tenants shall be on the ground sign and allocate the amount of space to the individual tenants.
- D. Landlord expressly reserves the right to deviate from this sign criteria when dealing with other tenants.
- E. Freestanding sign will incorporate colors and/or decorative design similarities to building façade as determined by Landlord.

3. GROUND SIGN #2:

- A. (1) freestanding multi-tenant ground sign shall be allowed in the southwest corner of the development providing signage to I-94.
- B. Freestanding sign is limited to a maximum height of 30'
- C. Landlord will allocate which tenants shall be on the ground sign and allocate the amount of space to the individual tenants.
- D. Landlord expressly reserves the right to deviate from this sign criteria when dealing with other tenants.
- E. Freestanding sign will incorporate colors and/or decorative design similarities to building façade as determined by Landlord.

4. PROHIBITED SIGNS:

The following types of signs or sign components shall be PROHIBITED.

- A. Signs employing exposed raceways, ballast boxes or transformers.
- B. Signs employing moving or flashing lights.
- C. Signs, letters, symbols, or identification of any nature painted directly on the surface of the exterior to Leased Premises.
- D. Signs employing unedged or uncapped plastic letters or letters with no returns or exposed fastenings.
- E. Cloth, wood, paper or cardboard signs, stickers, decals or painted signs around or on exterior surfaces (doors and/or windows of the Leased Premises).
- F. Rooftop signs.
- G. Signs employing noise-making devices or components.
- H. Signs exhibiting the names, stamps, or decals of the sign manufacturer or installer.
- I. Signs prohibited by city ordinance.

5. SIGN APPROVALS:

Procedure for obtaining Landlord's approval of sign drawings

- A. Tenant shall submit one (1) set in electronic format (.pdf or .jpg) including a color photo rendering and specifications to Landlord for all proposed sign work.
- B. The drawings shall clearly show location of sign onto fascia of building, graphics, color and construction and attachment details
- C. The Landlord shall reply to Tenant with "Approved", "Approved as Noted" or "Disapproved". In no event shall erection of any sign take place without the written approval of the Landlord. Sign drawings that have been disapproved are to be redesigned and resubmitted to Landlord for approval within (7) days of receipt by Tenant. After the sign has been approved by Landlord, Tenant shall also require the approval of the City of Lake Elmo prior to the erecting said sign if said approval is required or requested by said City. Tenant is responsible for receiving and paying for all fees associated with the installation of this sign, including ALL permits.
- D. Approval of store design drawings or working drawings and specifications for Tenant's Leased Premises does not constitute approval of any sign work

6. GENERAL TENANT REQUIREMENTS

- A. The furnishing and installation of signage and all expense incurred shall be the sole responsibility of the Tenant.
- B. Sign construction is to be completed according to the instructions contained within this criteria
- C. All exterior signage installation must be performed utilizing mechanical platforms on the outside of the building. No ladders may be used in connection with said installation
- D. A representative of Landlord must be present prior to the installation of any exterior signage in order to supervise same
- E. Tenant shall, at its own expense, install and maintain a time clock that will cause its exterior signage to be fully illuminated at times provided in the Lease.

**From:** [Michael Bent](#)  
**To:** [Emily Becker](#)  
**Cc:** [Greg Malmquist](#)  
**Subject:** Lake Elmo Shoppes Site Plan Review - Revisions  
**Date:** Thursday, May 11, 2017 9:10:20 AM

---

Hey Emily

Greg and I have reviewed the utility plan for the Lake Elmo Shoppes project. Below are revisions to the comments I sent to you previously and refer specifically to page C6:

1. The apparatus access roads look pretty good around the entire site with the following exceptions:
  - a. The west end of the proposed building on Lot 2 indicates a Drive Thru lane and then a lane to the west of the drive thru lane, the plan does show a width for this lane, but it indicates a 16' width (I missed it), please have the developer widen this to 20' as the apparatus access width is set at a minimum 20' wide. It doesn't make any sense to choke the width down to 16' and then widen it to the required width beyond and behind the buildings.
2. The fire protection water service is shown on the south side of both buildings. The Fire Department Connections for both of the proposed buildings will be installed on the north (front) of the buildings. A fire hydrant will need to be located on the west end island of the building on lot 2 and the east end island of the building on lot 1, in line with the proposed hydrant in the island at the northwest corner of lot 1. (totaling 3 hydrants on the north side of both buildings.)
3. No other hydrants are shown on the plans, another fire hydrant needs to be added in the vicinity of the west end of the building proposed to the north, my previous comment regarding the addition of a hydrant on the east end of the building to the north can be removed, there is an existing hydrant on the Kwik Trip property that will suffice for coverage.

Let me know if you have any questions.

Michael Bent  
Building Official  
City of Lake Elmo  
651-747-3910  
[mbent@lakeelmo.org](mailto:mbent@lakeelmo.org)



June 1, 2017

Steven Wensman  
Planning Director, City of Lake Elmo  
3800 Laverne Avenue N.  
Lake Elmo, MN 55042

SUBJECT: P17-020A Lakewood Crossing  
NW quad I-94 and Keats Ave  
Lake Elmo, Washington County  
Control Section 8204

Dear Mr. Wensman:

The Minnesota Department of Transportation (MnDOT) has reviewed the above referenced plat in compliance with Minnesota Statute 505.03, subdivision 2, Plats. All MnDOT's comments from the previous March 2017 letter still apply. Before any further development, please address the following issues:

***Design***

Grading is proposed up to the existing freeway fence and MnDOT property line. Ensure that the proposed grading stays with the limits of the development and that a note is included to protect the existing fence during construction.

For questions regarding these comments please contact Nancy Jacobson 651.234.7647 or [Nancy.L.Jacobson@state.mn.us](mailto:Nancy.L.Jacobson@state.mn.us)

***Right of way***

Hudson Blvd has been turned back to the city of Lake Elmo in this area. The MnDOT plats referenced in this preliminary plat do not show this release. Our current right of way can be seen on RW Map No. 180-12D.

For questions regarding these comments please contact Ashley Roup at (651)-234-7558 or [Ashley.Roup@state.mn.us](mailto:Ashley.Roup@state.mn.us)

***Water Resources***

A drainage permit is required. We would like to review the computations and plans as a check that the proposed development maintains or reduces drainage rates to MnDOT right of way when comparing the existing to the proposed condition.

As part of a drainage permit submittal, the City or project developer will need to submit existing and proposed hydraulic computations for both 10 and 100 year rainfall events verifying that all existing drainage patterns, ponding, and systems affecting MnDOT right of way will be perpetuated.



**CITY OF LAKE ELMO  
WASHINGTON COUNTY, MINNESOTA**

**RESOLUTION NO. 2017-063**

*A RESOLUTION APPROVING LAKEWOOD CROSSING 2<sup>ND</sup> ADDITION PRELIMINARY AND  
FINAL PLAT AND PLANNED UNIT DEVELOPMENT PLANS*

**WHEREAS**, CM Properties 94 LP, c/o MFC Properties, 3460 Washington Drive, Suite 100, Eagan, MN 55112 (“Applicant”) has submitted an application to the City of Lake Elmo (“City”) for approval of Preliminary and Final Plat and Planned Unit Development (PUD) Plans for Lakewood Crossing 2<sup>nd</sup> Addition; and

**WHEREAS**, the Lake Elmo Planning Commission held a Public Hearing on March 13, 2017 to consider the PUD Concept Plan for Lakewood Crossing 2<sup>nd</sup> Addition; and

**WHEREAS**, the Lake Elmo City Council approved the Lakewood Crossing 2<sup>nd</sup> Addition Concept Plan on March 21, 2017, and

**WHEREAS**, The Planning Commission held a public hearing and considered the Lakewood Crossing 2<sup>nd</sup> Addition Preliminary and Final Plat and PUD Plans on June 12, 2017 and recommended approval subject to a number of conditions; and

**WHEREAS**, the City Council reviewed the recommendation of the Planning Commission and the proposed Lakewood Crossing 2<sup>nd</sup> Addition Preliminary and Final Plat and PUD Plans at a meeting on June 20, 2017; and

**NOW, THEREFORE**, based upon the testimony elicited and information received, the City Council makes the following:

**FINDINGS**

1. That the Applicant has submitted all application requirements outlined in Sections 153.07; 153.08; and 154.759 for Preliminary and Final Plat and PUD Plans.
2. That the Lakewood Crossing 2<sup>nd</sup> Addition Preliminary Plat and PUD Plans meets at least one or more of the objectives outlined in Section 154.751 of the Zoning Code.
3. That the Lakewood Crossing 2<sup>nd</sup> Addition Preliminary and Final Plat and PUD Plans propose to enhance the site and retail shopping and dining experience with enhanced architectural materials and site furnishings.

4. That the Lakewood Crossing 2<sup>nd</sup> Addition Preliminary and Final Plat and PUD Plans is consistent with the Lake Elmo Comprehensive Plan and the Future Land Use Map for this area.
5. That the Lakewood Crossing 2<sup>nd</sup> Addition Preliminary and Final Plat and PUD Plans generally comply with the City's Commercial zoning district.
6. That the Lakewood Crossing 2<sup>nd</sup> Addition Preliminary and Final Plat and PUD Plans complies with the City's Subdivision Ordinance.
7. That the Lakewood Crossing 2<sup>nd</sup> Addition Preliminary and Final Plat meets other City zoning ordinances, such as landscaping, tree preservation, erosion and sediment control, and other ordinances, except where noted in the conditions of approval, Staff report to the Planning Commission dated June 12, 2017 or attachments thereto.
8. That the Applicant has requested the following PUD flexibility from the City's Zoning Code:
  - a. Allowing medical facilities, drive-throughs, and outdoor dining as a permitted, rather than conditional, use.
  - b. Allowing a minimum lot width of 18 feet for Lot 2.
  - c. Allowing a maximum impervious surface of 80% for Lot 2, with a total of 75% impervious surface.
  - d. Allowing flexibility from the minimum parking lot setbacks from property lines, allowing a zero foot setback between three lots and an 8.7 foot setback from the lot to the east.
  - e. Allowing flexibility from certain Lake Elmo Design Guidelines and Standards Manual as detailed in the Staff Report to the Planning Commission dated June 12, 2017.
  - f. To waive off-street loading requirements.
  - g. To waive requirement PUD requirement for 20% protected open space, as other site amenities are provided within the development.
  - h. Approval of a Comprehensive Sign Plan which allows 2.5 square feet of wall signage per one (1) lineal foot of store front on the front and rear elevations; 1.5 square feet of wall signage per one (1) lineal foot on building on the west and east elevations; a 10' X 12' multi-tenant ground sign near the northeastern corner of the site; and a 14' X 30' multi-tenant monument sign located in the southwest corner of the site.
9. That the Lakewood Crossing 2<sup>nd</sup> Addition Preliminary and Final Plat and PUD Plans is consistent with the City's engineering standards provided the plans are updated to address the City Engineer's comments documented in a letter dated May 31, 2017. The Landscape Plan shall include additional landscaping along the Keats Ave N to WB I-94 ramp.
10. That the Lakewood Crossing 2<sup>nd</sup> Addition Preliminary and Final Plat and PUD Plans will require a relatively minor amount of city services and will not create a significant burden on the City.
11. That the Lakewood Crossing 2<sup>nd</sup> Addition Preliminary and Final Plat and PUD Plans will not conflict with nearby land uses.

12. The Applicant will be extending municipal sewer and water to the westerly property (PID# 34.029.21.44.0004), providing benefit to the City.

### **CONCLUSIONS AND DECISION**

Based on the foregoing, the Applicants' application for Preliminary and Final Plat and PUD Plans, subject to the following conditions of approval:

1. The Applicant shall address all of the comments outlined in the City Engineer memorandum dated May 31, 2017.
2. The Final Plat shall not be recorded until final construction plan approval is granted and all easements as requested by the City Engineer and Public Works department are recorded on the Final Plat.
3. The Applicant shall obtain all necessary permits including but not limited to all applicable city permits (building, grading, sign, etc.), NPDES/SWPPP permits, and Valley Branch Watershed District approval.
4. The Applicant should review with the MPCA if infiltration practices will be allowed, in particular the planned conversion of the existing storm water pond to a storm water infiltration basin directly east and adjacent to the service station.
5. The Applicant shall address all comments outlined in the Minnesota Department of Transportation memorandum dated June 1, 2017.
6. The Applicant shall be required to extend sanitary sewer and municipal water service to the westerly adjacent property.
7. Stormwater facilities shall be privately owned and maintained. A Stormwater Maintenance and Easement Agreement in the City's standard form must be executed and recorded.
8. A right turn lane on Hudson Boulevard shall be constructed at the Applicant to handle increased traffic volumes.
9. The Applicant shall amend the proposed Landscape Plan to comply with City standards and obtain approval by the City.
10. The Applicant shall provide financial security for 125% of landscaping materials with a Site Work Agreement.
11. The Applicant shall provide further information on the proposed drive-through locations including but not limited to exact location of speakers, windows, and refuse receptacles as well as drive-through canopy detail.
12. The Applicant shall address all of the recommendations to improve the drive-through operations as outlined in the Technical Memorandum prepared by Spack Consulting dated April 12, 2017.
13. The Applicant shall submit a photometric plan, and all lighting must meet requirements of Sections 150.035-150.038 of the City Code.
14. The Applicant shall pay a park dedication fee of \$4500 per acre, totaling \$17,190, in lieu of required parkland.

15. The Applicant shall address all comments outlined in the Fire Chief and Building Official email memos dated May 10, 2017 and May 11, 2017. Specifically, the building on Lot 3 shall be sprinklered; fire hydrant locations shall be added according to comments; and the drive aisle width to the west of the drive-through lane on Lot 2 shall be widened to 20 feet. The Applicant shall submit a plan and obtain approval from the Building Official and Fire Chief for the location of hydrants and No Parking and Fire Lane signs.
16. A Reciprocal Easement and Operating Agreement provided shared parking access across all lots shall be provided, approved by the City, and recorded.
17. Tables cannot block a public sidewalk or other walkway needed for pedestrian circulation. Minimum of 5 ft. of sidewalk must remain open
18. Mechanical rooftop equipment must be screened.
19. The Site Plan shall be updated to include lines that clearly delineate two aisles on the lanes to the south of Lots 1 and 2 and the east side of Lot 1.
20. The Applicant shall provide additional striping and/or a stop sign to facilitate traffic circulation in the area where access is shared with Kwik Trip.
21. Permitted medical facilities shall be limited to non-urgent facilities, including but not limited to clinics, eye doctors, and dental facilities.
22. The Applicant shall add site amenities such as benches, trash receptacles, decorative lighting, and signage compliant with the Lake Elmo Design Guidelines and Standards Manual to the site plan.
23. Fencing shall be provided enclosing all outdoor dining areas.

Passed and duly adopted this 20th day of June, 2017 by the City Council of the City of Lake Elmo, Minnesota.

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Mike Pearson, Mayor

ATTEST:

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Julie Johnson, City Clerk