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NOTICE OF MEETING

The City of Lake Elmo
Planning Commission will conduct a meeting on
Monday, July 14, 2014 at 7:00 p.m.

AGENDA

1. Pledge of Allegiance
2. Approve Agenda
3. Approve Minutes
 - a. June 30, 2014
4. Public Hearing
 - a. PRELIMINARY PLAT, FINAL PLAT AND CONDITIONAL USE PERMIT – CM PROPERTIES 94 LP AND KWIK TRIP, INC. The Planning Commission will hold a public hearing to consider an application from CM Properties 94 LP and Kwik Trip, Inc. for a Preliminary and Final Plat for a gasoline station in stage 1 of the I-94 corridor. The request also includes a conditional use permit, as gasoline stations and car washes are conditional uses in the Commercial zoning district.
 - b. COMPREHENSIVE PLAN AMENDMENT – ELIMINATE RAD-ALT LAND USE CATEGORY. The Planning Commission will hold a public hearing to consider amending the City’s Comprehensive Plan to eliminate the RAD-Alt (Rural Area Development Alternative Density) land use category from the Future Land Use Map and as referenced in other portions of the Comprehensive Plan.
 - c. ZONING MAP AMENDMENT – PERFECTING AMENDMENTS. The Planning Commission will hold a public hearing to consider a Zoning Map Amendment to change the zoning for several properties guided for pending development, as well as other clean-up or perfecting amendments.
5. Business Items
 - a. None
6. Updates
 - a. City Council Updates – July 1, 2014 meeting:
 - i. Hammes Shoreland Variance was withdrawn by the applicant.
 - ii. Hunter’s Crossing Preliminary Plat passed with 15 conditions of approval.

iii. Eagle Point Medical Center Preliminary and Final Development plans passed with 7 conditions of approval.

iv. Hammes Estates Preliminary Plat passed with 17 conditions of approval.

b. Staff Updates

i. Upcoming Meetings:

- July 28, 2014
- August 11, 2014

c. Commission Concerns

7. Adjourn



**City of Lake Elmo
Planning Commission Meeting
Minutes of June 30, 2014**

Chairman Williams called to order the meeting of the Lake Elmo Planning Commission at 7:00 p.m.

COMMISSIONERS PRESENT: Williams, Dodson, Larson, Dorschner and Lundgren.

COMMISSIONERS ABSENT: Kreimer, Haggard and Morreale;

STAFF PRESENT: Community Development Director Klatt, City Planner Johnson and City Administrator Zuleger.

Approve Agenda:

The agenda was accepted as presented.

Approve Minutes: June 23, 2014

M/S/P: Williams/Lundgren, move to approve the minutes as amended, **Vote: 5-0, motion carried.**

Public Hearing: Easton Village – Preliminary Plat

Klatt started his presentation of the Preliminary Plat. He noted that the plat includes 217 single family lots and includes a significant portion of the planned Village Parkway minor collector road. He noted that there is also an access proposed to Manning Ave., which staff is recommending be temporary until the collector road connects to 30th Street. Klatt noted that the Minor/Lampert residential home and Northern Natural Gas service station is access by a private driveway that crosses the Union Pacific rail corridor.

Klatt noted that the Planning Commission reviewed the Sketch Plan in January in 2014. The plat has been significantly revised in response to the initial review. The changes included a more grid pattern for the streets, less cul-de-sacs and improved pedestrian facilities. Klatt then discussed the review comments that arose from the Park Commissions review of the Sketch Plan.

Regarding the planned buffers that are guided in the Comprehensive Plan, Klatt noted that the applicants are proposing to use significant portions of the greenbelt for stormwater management. Klatt noted how sewer was expected to be extended as part of the Easton Village Sanitary Sewer project. It should be noted that there is currently a

collaborative effort by multiple private parties to extend sanitary sewer up the eastern side of the Village to the northern portions of the Village.

Klatt mentioned how the AUAR relates to the review of the Preliminary Plat.

Regarding density, Klatt noted that the net density is 2.5 units per acre. Most of the lots are 65' or 75' in width. All of the lots meet the City's minimum size requirements for the Urban Low Density Residential (LDR) zoning.

The most critical aspect of review for the proposed plat is storm water management. Due to the significant drainage issues related to this location, it will be imperative to provide an effective storm water management system. The applicant still needs to get approval from the Valley Branch Watershed District.

Klatt moved on to discuss Village Parkway. He noted that the design of Village Parkway is different than 5th St., as it does not have a center planted median. Klatt shared the reasons why a different design is being considered by the City.

Klatt moved on to highlight important issues surrounding the Lake Elmo Airport. He noted that the Metropolitan Airport Commission has submitted a comment letter to the City. The most important consideration related to the airport relates to the design of the storm water facilities. They will need to be designed in a way that does not attract waterfowl.

To wrap up the key issues of review, Klatt discussed the proposed connection to Manning Ave. He noted that staff is recommending that the access to Manning Ave. be temporary. The reason for this recommendation is that if the connection is permanent, then more traffic will be directed to a portion of the neighborhood that is intended to serve local residential traffic.

Klatt noted that staff is recommending approval of the preliminary plat with 21 conditions of approval. Klatt discussed the proposed findings as shown in the Staff Report. After showing the staff recommendation and proposed findings and conditions, Klatt presented the plat and other preliminary plans to the Planning Commission.

Dodson asked if the staff and the applicants explored aligning Village Parkway along the natural gasline easement. Klatt noted that they did explore the possibility, but the land uses to the south of this area made it difficult to bring the collector road through. Dodson asked about the proposed parkland dedication and Condition #11. Klatt explained why it is difficult at this time to make more firm plans for the site north of the railroad tracks. Dodson asked about Outlot G and the pedestrian crossing of Village Parkway.

Williams asked about Condition #4. Williams asked if there was any trail connections proposed to a potential future trail on Manning Ave., as the County has shown the possibility of trails along this corridor.

Tom Wolter, Easton Village, LLC, introduced himself and welcomed any questions. Williams asked about the mass grading of the site, and asked if all the area will be properly settled after the grading. Wolter noted that all the building pads will be certified by a soils engineer. Williams asked the applicant if making the access to Manning temporary is acceptable. Wolter noted that they would like to discuss the timing of the closure with City and County staff, but they are open to making it temporary.

Dodson asked about the High Water level of one of the ponds. He asked about the trail location along blocks 4 and 5 near outlot B. The applicants noted that they are planning a sledding hill in Outlot B.

Williams asked about the infiltration basin in Outlot B. Todd Erickson, project engineer, explained the design of storm water ponds in Outlot B. Dorschner asked about the safety components of the pond. Erickson stated that there is a safety bench planned, which is typical for a pond of this depth.

Public Hearing opened at 8:17pm.

Susan Dunn, 11018 Upper 33rd St. N., shared some of the guiding principles of the Village Master Plan and noted that she is in opposition to the proposed plat. She shared some thoughts about safety related to the railroad tracks, the airport, and the Manning Ave. corridor. She stated that she does not support the proposed development as proposed.

The City did receive written comments from the Metropolitan Airport Commission. These comments were entered into the record.

Public Hearing closed at 8:22 pm.

Williams asked if the applicant is seeking parkland dedication for Outlot K. The applicants noted that they are seeking credit for Outlot K. Williams asked how staff is calculating dedication for trail areas. Klatt stated that the City requires 30 feet of width for a trail to accommodate for maintenance.

There was a general discussion about railroad noise. Klatt explained the guidance of the AUAR regarding railroad noise.

There was a discussion about the level of parkland dedication and the labeling of the outlots. Staff noted that the outlots and calculations have been consistent.

Lundgren is concerned about the number of conditions of approval. Erickson noted that the number of conditions is fairly typical in his experience. He also noted the importance of the window of opportunity of the construction season. Klatt spoke to the City's recommendation and explained the reason for conditions and approval.

Williams spoke about the design of Village Parkway. Klatt explained that the volume of cars drives how the roadway is designed. In this case, staff is recommending no center median because the cost does not justify it based on many factors.

Williams spoke about the draft findings. He noted that finding #3 is not accurate. Williams stated that he is in favor of recommending approval of the plat with conditions, but not based on finding number 3. Condition # 9 shall include "for the storm water management system". Combine findings 4 and 6 and include the language "with the conditions noted in the Engineer's report dated 6/26/14".

There was a discussion about the design of Village Parkway. Initially the Commission was inclined to add a landscaped center median as a condition of approval. After hearing from staff regarding the design, costs and the right-of-way width needed, the Planning Commission decided not to include the planted median.

M/S/P: Dorschner/Dodson, move to recommend approval of the Preliminary Plat of Easton Village with the amendments to condition 9 and combining findings 3 and 5.
Vote: 5-0, motion carried unanimously.

Public Hearing: Village Preserve – Preliminary Plat

Johnson presented an overview of a preliminary plat to be called Village Preserve, located in the northern portion of the Village Planning Area. He discussed the future land use plan for this area and reviewed the overall lot layout and street configuration. Johnson reviewed a summary of the specific comments from staff, and presented the critical issues that were identified, which included stormwater management, park land dedication, and sanitary sewer. He stated that staff is recommending approval of the preliminary plat with 12 conditions of approval.

Dodson questioned how much of the storm water was currently being directed downstream and how this would change with the proposed development. Johnson noted that the developer cannot increase the volume of water leaving the site post development under City and watershed district reviews. Johnson also stated that the City will be working with developers and the watershed district to identify options for diverting water to other watershed basins.

The Commission asked general questions about the future road connections, the density of the subdivision, the proposed trail connections and the ownership of the storm water ponds.

Dave Gonyea commented that there is only a very small portion of the existing overall storm water being directed north to another watershed basin. He noted that the Park Commission asked that the trails around Outlot D allow for the maximum potential use of the land until a final plan for the park can be implemented.

Public Hearing opened at 10:07pm.

No one spoke

Public Hearing closed at 10:08 pm.

Williams asked that condition number 5 be revised to specify that the condition apply to the storm water management plan. He also asked that condition 13 be added to require that all storm water outlots be dedicated to the City. Williams requested that findings 4 and 5 note that the plat meets the City's requirements with the exceptions noted in the staff report and the comments made.

M/S/P: Lundgren/Larson, move to recommend approval of the Preliminary Plat of Village Preserve with the 12 conditions drafted by staff with the findings of fact listed in the staff report with modifications to finding 4 and adding condition 13. **Vote: 5-0, motion carried unanimously.**

Public Hearing: Comprehensive Plan – Holliday Property

Johnson stated that the City has received an application for a Comprehensive Plan amendment to change the future land use designation of property located at the intersection of CSAH 15 and 30th Street from RAD-Rural Area Development to V-LDR-Village Urban Low Density Residential. He reviewed an aerial image showing the current site conditions and reviewed other details concerning the existing and proposed land use for the property. Johnson described the land use designations that the Comprehensive Plan guide the subject site and the property to the north.

Johnson summarized the Staff review comments, focusing on the Staff findings that support approval of the Comprehensive Plan Amendment. He noted that the amendment would facilitate the construction of the Village Parkway across a property that would otherwise have very limited development potential. The staff recommendation includes a condition that the parcel to the west of the subject property be screened with year-round plantings.

Dave Gonyea stated that the amendment is critical for moving their proposed sketch plan forward on this property and the adjacent property to the north.

Public Hearing opened at 10:32 pm.

Public Hearing closed at 10:32 pm.

M/S/P: Dodson/Dorschner, move to recommend approval of the request to amend the City's future land use map and to change the future land use dedication from RAD to V-LDR with the conditions recommended by staff. **Vote: 5-0, motion carried.**

Business Item: Village Preserve South – Sketch Plan Review

Johnson presented an overview of a proposed sketch plan for a residential subdivision to be called Village Preserve South. The site is located within the southern portion of the Village Planning area and is within an area guided for future sewer service. Johnson discussed the existing site conditions and reviewed the proposed subdivision layout with the Commission and other details concerning the proposed subdivision. He reviewed the key issues with the sketch plan that have been identified by Staff and summarize the review comments identified in the staff report to the Commission.

Dorschner commented that the proposed bump out/cul-de-sac with direct access to the parkway may be problematic. Johnson noted that this connection will be subject to further review and comment by the City Engineer and will need to work within the overall geometrics of the Village Parkway. Williams noted that the first intersection north of 30th Street may be too close to 30th Street. Johnson replied that the City Engineer has found the proposed spacing to be acceptable given the design parameters of the parkway.

Williams questioned why the parkway was intersecting 30th Street at a one-way no entrance road to the South. Johnson stated that there are advantages to lining up intersections even if one of the roads is a very low volume roadway, and would be considered a safer alternative to off-setting intersections. Dodson asked if the City could take a look at whether or not there would be advantages to having the parkway follow a more rigid North/South alignment. Johnson replied that the City could look into this.

Williams questioned whether or not the City's ordinances would allow the crediting of park land dedication for a remote site. Johnson stated that staff would look into this with the City Attorney. Williams expressed concerns about the reduction in the green belt buffer along 30th Street, the need for watershed district review of the storm water system, the location of the intersection of the parkway with 30th Street, and the screening the McLeod property to the west.

Dodson stated that he liked that most of the lots do not abut another residential lot in the rear yard, but would like to see modifications to the bump-out lots.

Larson questioned whether or not the proposed trails could eventually connect to a trail or sidewalk system on 30th Street. Johnson noted that the City has looked into the possibility of placing trails along 30th Street, but cited limited right-of-way as a potential barrier to this occurring.

Updates and Concerns

Council Updates – None

Staff Updates

1. Upcoming Meetings
 - a. July 14, 2014
 - b. July 28, 2014

Commission Concerns –

Dodson questioned how binding the proposed findings would be in cases where conditions of approval. Staff noted that the findings should line up with the conditions of approval and the staff needs to be clear about them. Klatt noted that the findings need to support the decision made and the information distributed.

Dorschner suggested an ordinance to deal with railroad setbacks and safety issues and possibly require some berms based on feedback from professionals.

Dorschner asked about site visits, done like a workshop. Lundgren supported. There was a general discussion concerning the best manner for Planning Commissioners to visit development sites. Johnson stated that staff has name badges and that might be an option.

Meeting adjourned at 11:16 pm

Respectfully submitted,

Joan Ziertman
Planning Program Assistant



PLANNING COMMISSION
DATE: 7/14/2014
AGENDA ITEM: 4A – PUBLIC HEARING
CASE # 2014-37

ITEM: Kwik Trip Gasoline Station – Preliminary Plat, Final Plat and Conditional Use Permit

SUBMITTED BY: Nick Johnson, City Planner

REVIEWED BY: Kyle Klatt, Community Development Director
Jack Griffin, City Engineer
Ann Pung-Terwedo, Washington County
Greg Malmquist, Fire Chief
Stephen Mastey, Landscape Architecture, Inc.

SUMMARY AND ACTION REQUESTED:

The Planning Commission is being asked to consider a request from CM Properties 94 LP and Kwik Trip, Inc. to plat a portion of a property located immediately west of Keats Avenue (CSAH 19) and immediately south of Hudson Boulevard North within Stage 1 of the I-94 Corridor Planning Area with the intent of constructing a Kwik Trip gasoline station. In addition to the platting application, the request includes application for a Conditional Use Permit, as gasoline stations and car washes require a conditional use permit in the Commercial zoning district. Staff is recommending approval of the request subject to compliance with 8 conditions as noted in this report.

GENERAL INFORMATION

Applicant: CM Properties 94 Limited Partnership (Bruce Miller); 3470 Washington Drive, Suite 102, Eagan, MN 55112 and Kwik Trip, Inc. (Scott Teigen); PO Box 2107, La Crosse, WI 54603.

Property Owners: CM Properties 94 LP (Bruce Miller); 3470 Washington Drive, Suite 102, Eagan, MN 55112

Location: Part of Sections 34, Township 29 North, Range 21 West in Lake Elmo, immediately west of Keats Avenue North (CSAH 19), immediately south of Hudson Boulevard North and immediately north of Interstate Highway 94. PID Number: 34.029.21.44.0007.

Request: Application for preliminary plat, final plat and conditional use permit approval of a gasoline station in Stage 1 of the I-94 Corridor Planning Area.

Existing Land Use and Zoning: Vacant agricultural land. Current Zoning: RT – Rural Development Transitional Zoning District; Proposed Zoning: C – Commercial District

Surrounding Land Use and Zoning: North – vacant/agricultural land, guided for C - Commercial; west – vacant/agricultural land, guided for C- Commercial;

south – Interstate Highway 94; east – Keats Avenue North (CSAH 19)

Comprehensive Plan: Commercial

History: Utilities (sewer and water) were extended to the subject property as part of the Section 34 Public Utility Project, which is now complete.

Deadline for Action: Application Complete – 5/28/2014
60 Day Deadline – 7/28/14
Extension Letter Mailed – No
120 Day Deadline – 9/26/14

Applicable Regulations: Chapter 153 – Subdivision Regulations
§154.106 Conditional Use Permits
Article 12 – Commercial Districts (C)
Article 7 – Specific Development Standards (§154.305)
§150.270 Storm Water, Erosion, and Sediment Control
§150.035 Lighting, Glare Control and Exterior Lighting Standards

REQUEST DETAILS

The City of Lake Elmo has received a request from CM Properties 94 Limited Partnership and Kwik Trip, Inc. for a preliminary and final plat to subdivide 5.87 acres of land located within Stage 1 of the I-94 Corridor Planning Area into 2 lots with the intention of constructing a Kwik Trip gasoline station. The proposed plat would be located on property currently owned by CM Properties 94 Limited Partnership, and would be located immediately west of Keats Avenue (CSAH 19), immediately south of Hudson Boulevard, and immediately north of Interstate Highway 94. The parcel has historically been used for agricultural purposes.

The proposed plat and conditional use permit application have been developed in response to the City's Comprehensive Plan, which identifies the applicant's property for Commercial development. The proposed Kwik Trip would be the first commercial development in the eastern portion of Stage 1 of the I-94 Corridor Planning Area since the adoption of the City's current Land Use Plan.

In terms of access, the proposed site plan shows two connections to Hudson Blvd. along the northern boundary of the site. To account for the appropriate access spacing necessary for Hudson Blvd., the applicants are showing the western access to be a full access, supporting all turning movements, while the eastern access shall serve only as a right-out exit to Hudson. The City Engineer has reviewed the proposed access locations and found them to be acceptable with the proposed access spacing.

PLANNING AND ZONING ISSUES

The proposed Kwik Trip site is guided for Commercial development in the City's Comprehensive Plan. The overall subdivision plan has therefore been prepared in order to comply with the district standards for the Commercial zoning district in terms of lot size, lot widths, building setbacks, and other design criteria. It should be noted that while a neighborhood convenience store is a permitted use in the Commercial zoning district, gasoline stations are considered a conditional use. In addition, car washes, while accessory to the gasoline station use, also are a conditional use. For these reasons,

the applicant must apply for a conditional use permit for approval of the gasoline station and car wash land uses.

The proposed plat would subdivide the subject property into two parcels; one parcel (Lot 1, Block 1) for the proposed Kwik Trip and an outlot (Outlot A) for a future commercial development adjacent to the proposed Kwik Trip. The proposed size of the site (Lot 1) for the Kwik Trip is 89,355 square feet, or 2.05 acres. The remaining area for Outlot A would be 3.82 acres, which will likely be used for future commercial development and additional storm water management. It should be noted that the applicant is requesting to proceed with Preliminary Plat and Final Plat concurrently. From staff's perspective, if the applicant is meeting all the submission requirements for both preliminary and final plat, and the proposed plat is limited in scope and scale, then proceeding through both steps concurrently is acceptable. To better define what constitutes a project of limited scope or scale in staff's judgment, a limited scale plat would involve subdivision of a limited number of parcels without the need for major infrastructure improvements (utility extensions, new public roads, etc.). In this context, the proposed plat is of limited scale and scope according to staff.

In reviewing the applicable requirements from the City's zoning and subdivision regulations, staff has found that the proposed project is in conformance with these requirements. More specifically, staff reviewed the proposed gasoline station in light of the following:

- **Lot Size.** The proposed lot being platted for the commercial use is 89,355 square feet, which meets the minimum lot size requirement of 20,000 square feet per the Commercial zoning district.
- **Building Setback Requirements.** The proposed gasoline station/convenience store meets the required building setbacks (front: 30 feet, corner side yard: 25 feet, interior side yard: 10 feet and rear yard: 10 feet) for the Commercial district. In addition, the car wash meets the 10-foot setback requirement for accessory buildings.
- **Parking Setbacks.** The proposed parking stalls all meet the required parking setback requirements per the Commercial zoning district.
- **Impervious Surface.** The maximum amount of impervious surface in the Commercial zoning district is 75%. The proposed project complies with this requirement, as there is 62,010 square feet of pervious surface proposed, which is 69% of lot area.
- **Parking Stalls.** Per the City's off-street parking requirements, gasoline stations are required to have one parking stall for every 250 square feet of gross floor area used for sales. Using this calculation, staff determined that 23 parking stalls would be required. The applicants are proposing 28 standard and 2 handicap parking stalls, easily meeting the City's parking requirement.
- **Specific Development Standards – Gasoline Stations and Car Washes.** The City adopted specific development standards (§154.305) for gasoline stations and car washed, both of which are included in this proposal. In reviewing these standards, staff found that the application was in compliance with the specific development standards related to these two uses, including the following:
 - **Access to a Collector.** The proposed gas station meets the minimum lot size requirements and has access to a collector street (Hudson Blvd.).

- **Canopies.** The proposed canopy meets the required setback of 20 feet from all property lines. In addition the canopy columns reflect the design and building materials of the principal building.
- **Outdoor Displays.** Proposed outdoor merchandise is either located under the canopy or adjacent to the principal structure, meeting all setback requirements.
- **Car Wash Enclosure.** The proposed car wash has doors that enclose the car wash while in operation.
- **Sounds from Speakers.** The car wash area is not within close proximity to any residential districts or properties. Any sounds from speakers related to the car wash should not negatively impact any residential properties.

Based on Staff's review of the Site Plan and Supporting Plans for the proposed gasoline station and car wash, the applicant has demonstrated compliance with all applicable Zoning Code requirements related to gasoline stations in Commercial districts.

In terms of parkland dedication, the Subdivision Ordinance requires any property being developed in Commercial districts to pay a fee as determined by Resolution by the City Council. Currently, the fee for parkland dedication for commercial properties is \$4,500 per acre of land subdivided for commercial use. In this case, the applicant is proposing to subdivide 2.05 acres of land for commercial use. The remaining land is being left in outlot. Using the \$4,500 fee per acre, the applicant will be required to provide \$9,225 (2.05 acres x \$4,500 per acre = \$9,225) to the City to comply with the City's parkland dedication requirement. Staff would recommend that this fee be provided to the City in advance of releasing the final plat for recording (Condition #4).

Finally, as the gasoline station and car wash is considered a conditional use in the Commercial zoning district, the applicant has applied for a conditional use permit (CUP). Per the procedure described in Article 3 of the Zoning Code (§154.106), the City is required to make findings related to the proposed conditional use. Generally speaking, these findings are required to ensure that no negative impacts to the adjacent properties or broader community are associated with the proposed use. Staff reviewed all 12 required findings per the procedure for CUPs and found that the proposed use meets all the criteria to be granted a conditional use permit. As part of the draft findings for recommending approval of the request, staff has included the required findings that relate to the conditional use permit.

REVIEW AND ANALYSIS

City Staff has reviewed the proposed plat and conditional use permit, which has gone through multiple versions in advance of the formal application being accepted as complete by the City. As currently submitted, the proposed plat will meet all applicable City requirements for conditional approval, and any deficiencies or additional work that is needed is identified in the recommended conditions of approval.

Also, the City has received a detailed list of comments from the City Engineer and Washington County concerning the proposed gasoline station, in addition to informal review by the City's Fire Chief and Building Official. The written review comments from the City Engineer and Washington County are attached for consideration by the Planning Commission.

In addition to the general comments that have been provided in the preceding sections of this report, Staff would like the Planning Commission to consider the following discussion areas as well:

- **Comprehensive Plan.** The proposed subdivision and commercial use is consistent with the Lake Elmo Comprehensive Plan for this area. The parcel is within Stage 1 of the I-94 Corridor Planning Area and has access to City sanitary sewer and water.
- **Zoning.** The proposed zoning for the Lakewood Crossing site will be C – Commercial. The submitted development plans demonstrate compliance with the City’s Commercial zoning district standards. Neighborhood Convenience Store is a permitted use in the Commercial zoning district, while gasoline station and car wash are conditional uses. The applicants have applied for a conditional use permit in conjunction with the platting request.
- **Subdivision Requirements.** The City’s Subdivision Ordinance includes a fairly lengthy list of standards that must be met by all new subdivisions, and include requirements for blocks, lots, easements, erosion and sediment control, drainage systems, monuments, sanitary sewer and water facilities, streets, and other aspects of the plans. Staff, as well as the City Engineer, have not identified any existing conflicts with the City’s Subdivision Ordinance.
- **Access.** Staff has the following review comments related to access:
 - **Eastern Right-Out Only Access.** To provide access to the proposed gasoline station, the applicant is proposing two access locations (west and east) along Hudson Boulevard. The City Engineer reviewed the proposed access locations and found the spacing to be acceptable. However, staff is recommending that the eastern access be permitted only as a right-out only. The applicants have updated their plans to reflect this design. After reviewing the proposed right-out eastern access, staff and Washington County found that the proposed right turn is too sharp, necessitating a different design or an acceleration lane on Hudson. Staff is comfortable working with the applicant to arrive at the best suited design for the right-out eastern access (Condition #1b).
 - **Left Turn Lane on Hudson Blvd.** In addition to the right-out access (eastern access), City staff also requested a left turn lane to serve the full western access of the gas station. It is important to note that Hudson Boulevard is classified as a collector road and it anticipated to serve a substantial amount of traffic as the I-94 Corridor develops. The City does not plan to expand Hudson to a 4-lane roadway. In addition, the western full access will also be designed to serve Outlot A in addition to the gasoline station. For these reasons, staff recommended a left turn lane on westbound Hudson Blvd for the full shared access between the proposed gasoline station and Outlot A. The applicants have provided the left turn lane to respond to the staff request. However, in reviewing the proposed design, the City Engineer found that the taper is too abrupt for a 50mph design speed. Staff is recommending that the taper be enlarged to allow for a more appropriate design for the existing conditions of Hudson Boulevard (Condition #1a).
 - **Western Shared Access.** Finally, staff is also recommending that the western shared full access be expanded to 40 feet in width (Condition #1c), which would allow for both right and left turn lanes to exit the site. The current width as proposed is approximately 36 feet. The requested expansion would include approximately 4 feet of additional width to allow better circulation in and out of the proposed gasoline station, as well as whatever future use is located on Outlot A. In addition to the

requested modification to the design of the western access drive, staff is also recommending that the applicant provide an access easement along the portion of shared access driveway that is located on the Kwik Trip site (Condition #2). The goal is to ensure that the future use on Outlot A has dedicated full access to the western driveway.

- **Landscaping.** The applicant has submitted a Landscape Plan (Sheet L1) as part of the proposed gasoline station. The City's landscaping provisions (§154.258) require 1 tree per 50 feet of street frontage. In addition to street plantings, the City's requirements include 5 trees per developed acre. Given these requirements, staff calculated that 15 street trees are required (based on approximately 780 feet of street frontage on Hudson and Keats) and 10 interior trees are required (5 trees per acre). The total required amount of trees per staff's calculation is 25 trees. The applicants are proposing 25 trees of both deciduous and coniferous varieties, meeting the City's requirement. It should be noted that the coniferous trees are being utilized to screen the trash or refuse area. In addition to the trees, the applicants are proposing a variety of shrubs and planted beds. Finally, staff has distributed the Landscape Plan to the City's Landscape Consultant, Stephen Mastey. Staff would recommend as a condition of approval (Condition #5) that the Landscape Plan be reviewed by the Landscape Consultant. In addition to review, staff would recommend that any modifications related to plant species or location be incorporated in the Final Landscape Plan prior to issuance of the building permit.
- **Theming and Branding.** As part of the improvements proposed for the gasoline station, the applicants are proposing to install decorative fencing that is consistent with the design recommended by the City's Theming and Branding Study. The decorative fencing is a white three-rail fence that is often used on agricultural properties for horses. The applicants are proposing to install this fencing along the northeastern portions of the property, which should be visible from Keats Ave. N. (CSAH 19).
- **Architectural Design Review.** City staff reviewed the architectural plans and architectural renderings of the proposed Kwik Trip gasoline station and car wash for consistency with the Lake Elmo Design Guidelines and Standards Manual (Attachment #11). Overall, Staff finds that the proposed building meets the guidelines and standards for Commercial development contained within the manual and would offer the following review comments:
 - **Building Materials.** The proposed building is chiefly constructed of red modular brick. Accent materials include limestone and tan brick soldier course. The proposed building materials meet the City standards for building materials in the Commercial district.
 - **Streetscape.** A significant portion of the proposed landscaping are located along the public right-of-way for Hudson Boulevard and Keats Avenue (CSAH 19). In addition, the applicants are proposing to install a three-rail decorative fence that is consistent with the City's Branding and Theming Study. In staff's judgment, the proposed design will be contributing to a positive streetscape.
 - **Storage Areas.** The Manual requires that trash collection area be located out of the view of the public right-of-way. The proposed site plan indicated that the trash

collection area will be enclosed and located in the far eastern portion of the site. The proposed trash collection area meets the intent of the standard.

- **Entry.** The proposed convenience store has accent treatments of both limestone and tan brick soldier course around the entryway. In addition, the peaked roof at the entry provides an additional architectural feature.
- **Photometric Plan.** To demonstrate compliance with the City's sign and exterior lighting ordinances, the applicant has submitted a Photometric Plan. The Photometric Plan includes information about the lighting fixtures proposed for the site. In addition, it provides information about the light intensity, measuring the foot candles across the entire site and beyond. Based on the plan, the applicants are proposing pole lights that are 18 feet in height and that include a 90 degree cutoff. In addition, the proposed lighting internal to the canopy are recessed LEDs. From staff's review of the photometric plan, the proposed lighting is consistent with the City's ordinance, as the plan does an effective job of limiting light from being directed outside of the property. Generally speaking, the foot-candle reading at or near the property boundaries are all very low and are consistent with the ordinance. In addition, the 18-foot pole lights do not exceed the maximum height allowed for such lighting (30 feet maximum height for lights in non-residential districts with cutoff 90 degrees or less). When City staff reviews the building permit for the proposed gasoline station, a more in-depth review of the proposed lighting will be completed to ensure conformance to the City's ordinance.
- **Signage Plan.** As part of the plat and conditional use permit applications, the applicants have submitted a signage plan. The signage plan includes multiple sign types, including wall signs, canopy signs and a monument or ground sign. From a high level review perspective, it appear that the proposed signage will meet the City's Sign Ordinance. However, the applicant will be required to submit sign permits for the proposed signage, at which time staff will conduct a more in-depth review. Staff is recommending that the applicant submit sign permits for any proposed signage (Condition #6)
- **City Engineer Review.** The City Engineer has provided the Planning Department with a detailed comment letter (Attachment #8) as a summary of his review of the proposed gasoline station. The majority of the Engineer's comments relate to traffic and access management. In addition, the Engineer identifies other aspects of the proposed plan that currently do not meet City Engineering Standard, including utilities, grading and erosion control. As a condition of approval, staff is recommending that all modification requested by the City Engineer in his memo dated 7/9/14 be incorporated into Final Plans prior to the approval of the building permit (Condition #1).
- **Fire Department Review.** The Fire Chief has reviewed the Utility Plan for the proposed gasoline station and found the proposed hydrant location to be acceptable. Additional review by the Fire Chief will be required upon the submission of the building permit.
- **Washington County Review.** County Staff has reviewed the Lakewood Crossing plat and proposed gasoline station, providing comments in a written memorandum dated 7/9/14 (Attachment #9). In the memo, they note that no additional right-of-way for Keats Ave. N. is required as part of the proposed plat. In addition, the County recommends that the City monitor the traffic situation along the Hudson Boulevard corridor with the possibility that the

eastern right-out only access be removed at some point in the future. To account for this consideration, the City Engineer has recommended that a center median be installed adjacent to the right-out only access at some point in the future as opposed to eliminating the access. Finally, the memo from Washington County also includes notification that the Hudson Blvd. corridor in Lake Elmo is currently being considered as one of the alternate alignments (D2 Alignment) for the Gateway Corridor transit project to be constructed at some point in the future. City staff have also notified the applicants of this possibility in previous meetings.

- **MnDOT Review.** The Minnesota Department of Transportation owns a substantial amount of right-of-way surrounding the proposed gasoline station site. In a review email dated 7/10/14, MnDOT noted that the applicant may not include any ponding within the MnDOT right-of-way. In addition, any work affecting or impacting the MnDOT right-of-way will require a permit. Staff is recommending that the applicant obtain necessary permits from MnDOT for any work impacting the MnDOT right-of-way (Condition #8)
- **Watershed Districts.** The project area lies within the Valley Branch Watershed District (VBWD). It should be noted that the developer must meet all the rules of the Wetland Conservation Act and VBWD and will need to secure permits from the VBWD in order to proceed with the development as planned (Condition #3).

Based on the above Staff report and analysis, Staff is recommending approval of the preliminary plat, final plat and conditional use permit with 8 conditions of approval. The recommended conditions are as follows:

Recommended Conditions of Approval:

- 1) All required modifications to the plans as requested by the City Engineer in a review letter dated July 9, 2014 shall be incorporated into the plans prior to the approval of the building permit for the gasoline station. Required modifications include, but are not limited to, the following:
 - a. The design of the west-bound left turn lane proposed for the southwest access on Hudson Boulevard must be revised to incorporate a 50:1 taper to account for the 50mph design speed of the road as suggested by the City Engineer and the supporting consultant.
 - b. The design of the northeast right-out only access must be revised per the recommendation of the City Engineer and Washington County. The design of the northeast access must be approved by the City prior to the approval of the building permit.
 - c. The width of the western full access road must be modified to 40 feet to allow for one inbound lane (16 feet wide) and two outbound lanes (left and right turn lanes, 12 feet wide).
- 2) The applicant shall provide for an access easement over the entire portion of the shared western driveway located on Lot 1, Block 1 to provide dedicated access for Outlot A.

- 3) The developer shall follow all of the rules and regulations spelled out in the Wetland Conservation Act, and shall acquire the needed permits from Valley Branch Watershed District prior to the commencement of any grading or development activity on the site.
- 4) The applicant shall enter into a maintenance agreement with the City concerning the storm water infiltration areas prior to the issuance of a building permit for the gasoline station.
- 5) The applicant shall pay a parkland dedication fee in the amount of \$9,225.00 prior to the final plat being released for recording.
- 6) The Landscape Plan shall be reviewed by the City's Landscape Consultant. Any modifications requested by the Landscape Consultant shall be incorporated into the Final Landscape Plan prior to the approval of the building permit for the gasoline station.
- 7) The applicant shall secure a sign permit for all signage associated with the proposed gasoline station.
- 8) The applicant shall secure any necessary MnDOT permits for any work impacting the MnDOT right-of-way.

DRAFT FINDINGS

Staff is recommending that the Planning Commission consider the following findings with regards to the proposed Lakewood Crossing preliminary plat, final plat and conditional use permit:

- That the Lakewood Crossing preliminary and final plat is consistent with the Lake Elmo Comprehensive Plan and the Future Land Use Map for this area.
- That the Lakewood Crossing preliminary and final plat complies with the City's C – Commercial zoning district.
- That the Lakewood Crossing preliminary and final plat complies with the City's subdivision ordinance.
- That the Lakewood Crossing preliminary and final plat meets other City zoning ordinances, such as landscaping, erosion and sediment control, and other ordinances.
- That the Lakewood Crossing preliminary and final plat is consistent with the City's engineering standards provided the plans are updated to address the City Engineer's comments documented in a letter July 9, 2014.
- That the proposed architectural design of the gasoline station, canopy and car wash is consistent with the Lake Elmo Design Guidelines and Standards Manual.
- That the proposed use will not be detrimental to or endanger the public health, safety, comfort, convenience or general welfare of the neighborhood or City.
- That the use or development conforms to the City of Lake Elmo Comprehensive Plan.
- That the use or development is compatible with the existing neighborhood.
- That the proposed use meets all specific development standards for such use listed in Article 7 of the Zoning Code.

- That the proposed use will be designed, constructed, operated and maintained so as to be compatible in appearance with the existing or intended character of the general vicinity and will not change the essential character of the area.
- That the proposed use will not be hazardous or create a nuisance as defined under the Zoning Code to existing or future neighboring structures.
- That the proposed use will be served adequately by essential public facilities and services, including streets, police and fire protection, drainage structures, refuse disposal, water and sewer systems.
- That the proposed use will not create excessive additional requirements at public cost for public facilities and services and will not be detrimental to the economic welfare of the community.
- That the proposed use will not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons, property or the general welfare because of excessive production of traffic, noise, smoke, fumes, glare or odors.
- That vehicular approaches to the property will not create traffic congestion or interfere with traffic on surrounding public thoroughfares.
- That the proposed use will not result in the destruction, loss or damage of a natural or scenic feature of major importance.

RECOMMENDATION:

Staff recommends that the Planning Commission recommend approval of the Lakewood Crossing Preliminary Plat and Final Plat. In addition, staff recommends that the Planning Commission recommend approval of the Conditional Use Permit for the proposed Kwik Trip gasoline station and car wash. Staff is recommending approval of the requests subject to 8 conditions of approval as listed in the Staff report. Suggested motion:

“Move to recommend approval of the Lakewood Crossing Preliminary and Final Plat and Conditional Use Permit with the 8 conditions of approval as drafted by Staff based on the findings of fact listed in the Staff Report.”

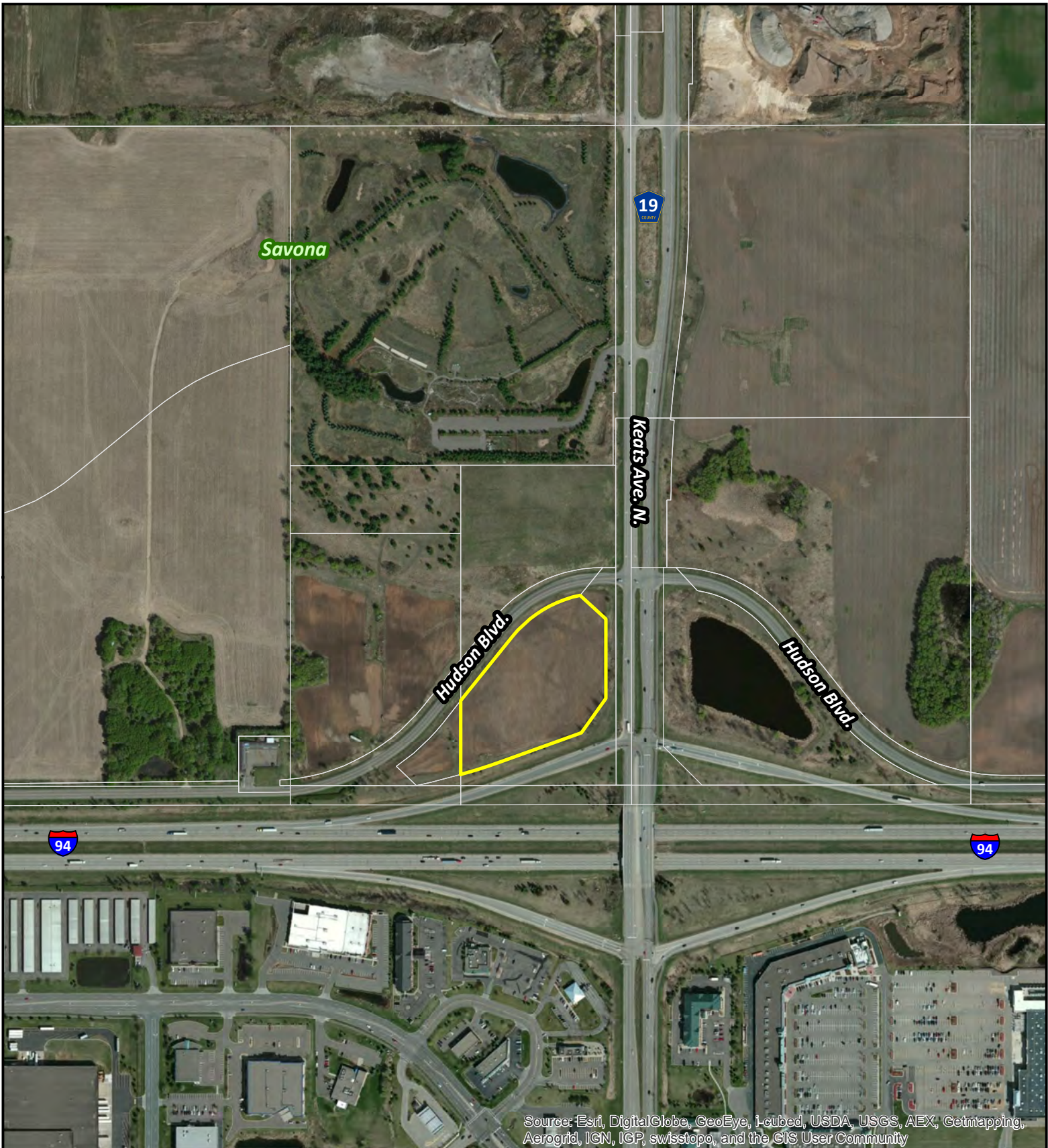
ATTACHMENTS:

1. Location Map
2. Application Forms and Narrative
3. Lakewood Crossing Preliminary and Final Plat
4. Kwik Trip Site Plan and Supporting Plans (12 sheets)
5. Kwik Trip Architectural Plans
6. Kwik Trip Architectural Renderings
7. Kwik Trip Signage Plan
8. City Engineer Review Memorandum, dated 7/9/14

- 9. Washington County Review Memorandum, dated 7/9/14
- 10. MnDOT Review Email, dated 7/10/14
- 11. Design Guidelines and Standards - Commercial

ORDER OF BUSINESS:

- IntroductionPlanning Staff
- Report by StaffPlanning Staff
- Questions from the Commission Chair & Commission Members
- Open the Public HearingChair
- Close the Public Hearing.....Chair
- Discussion by the Commission Chair & Commission Members
- Action by the Commission Chair & Commission Members



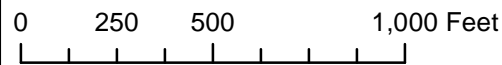
Site Plan: 34.029.21.44.0007



Data Source: Washington County, MN
7-10-2014



34.029.21.44.0007
(Proposed Kwik Trip Site)



1"=500'



Date Received: _____
Received By: _____
LU File #: _____



651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

PRELIMINARY PLAT APPLICATION

Applicant: CM PROPERTIES 94, LP
Address: 3470 WASHINGTON DRIVE, SUITE 102, ENGAN, MN 55122
Phone #: 651-452-3303
Email Address: BRILLOR@MFCPROPERTIES.COM

Fee Owner: _____
Address: SAME
Phone #: _____
Email Address: _____

Property Location (Address and Complete (long) Legal Description):
NWQ OF I-94/KENT AVENUE - SEE ATTACHED SURVEY FOR LEGAL

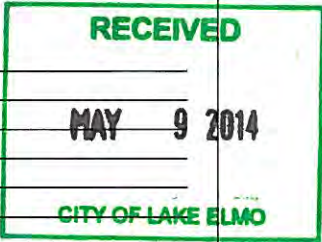
General information of proposed subdivision:
1 LOT, 2 OUTLOT COMMERCIAL SUBDIVISION TO ACCOMMODATE A KWIK TRIP.

Conducted pre-application meeting with Staff? Yes No

In signing this application, I hereby acknowledge that I have read and fully understand the applicable provisions of the Zoning Ordinance and current administrative procedures. I further acknowledge the fee explanation as outlined in the application procedures and hereby agree to pay all statements received from the City pertaining to additional application expense.

Signature of applicant: [Signature] Date: 4-7-14

| |
|--|
| City Use Only |
| Planning: Zoning District: _____ Date: _____ |
| Reviewed by: _____ Date: _____ |
| Subject to the following conditions: _____ |
| Engineering: Reviewed by: _____ Date: _____ |
| Subject to the following conditions: _____ |





Lake Elmo City Hall
651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

AFFIRMATION OF SUFFICIENT INTEREST

I hereby affirm that I am the fee title owner of the below described property or that I have written authorization from the owner to pursue the described action.

Name of applicant CM Properties 94, LP
(Please Print)

Street address/legal description of subject property NWQ of I-94/KUMS
AVENUE. SEE ATTACHED SURVEY FOR LEGAL.

[Signature]
Signature

4-7-14
Date

If you are not the fee owner, attach another copy of this form which has been completed by the fee owner or a copy of your authorization to pursue this action.

If a corporation is fee title holder, attach a copy of the resolution of the Board of Directors authorizing this action.

If a joint venture or partnership is the fee owner, attach a copy of agreement authorizing this action on behalf of the joint venture or partnership.



Lake Elmo City Hall
651-747-3900
3800 Lavene Avenue North
Lake Elmo, MN 55042

ACKNOWLEDGEMENT OF RESPONSIBILITY

This is to certify that I am making application for the described action by the City and that I am responsible for complying with all City requirements with regard to this request. This application should be processed in my name and I am the party whom the City should contact regarding any matter pertaining to this application.

I have read and understand the instructions supplied for processing this application. The documents and/or information I have submitted are true and correct to the best of my knowledge. I will keep myself informed of the deadlines for submission of material and of the progress of this application.

I understand that this application may be reviewed by City staff and consultants. I further understand that additional information, including, but not limited to, traffic analysis and expert testimony may be required for review of this application. I agree to pay to the City upon demand, expenses, determined by the City, that the City incurs in reviewing this application and shall provide an escrow deposit to the City in an amount to be determined by the City. Said expenses shall include, but are not limited to, staff time, engineering, legal expenses and other consultant expenses.

I agree to allow access by City personnel to the property for purposes of review of my application.

Signature of applicant *[Handwritten Signature]* Date 4-7-14

Name of applicant CM PROPERTIES 94, LP Phone 651-452-3303
(Please Print)

Name and address of Contact (if other than applicant) _____

Bruce Milow

3470 Wisconsin Drive, Suite 102

Edina, MN 55122

Date Received: _____
Received By: _____
Permit #: _____



651-747-3900
3800 Lavene Avenue North
Lake Elmo, MN 55042

LAND USE APPLICATION

Comprehensive Plan Zoning District Amend Zoning Text Amend Variance*(see below) Zoning Appeal

Conditional Use Permit (C.U.P.) Flood Plain C.U.P. Interim Use Permit (I.U.P.) Excavating/Grading

Lot Line Adjustment Minor Subdivision

Applicant: KWIK TRIP - BRADFORD FRY

Address: PO BOX 2107 LACROSSE WI 54603

Phone # 608 793 6414

Email Address: bfry@kwiktrip.com

Fee Owner: _____

Address: _____

Phone # _____

Email Address: _____

Property Location (Address and Complete (long) Legal Description): LOT 1 BLOCK 1

Detailed Reason for Request: CONSTRUCTION OF C-STONE W/ CANVASIT AND FUEL

*Variance Requests: As outlined in Section 301.060 C. of the Lake Elmo Municipal Code, the applicant must demonstrate practical difficulties before a variance can be granted. The practical difficulties related to this application are as follows:

In signing this application, I hereby acknowledge that I have read and fully understand the applicable provisions of the Zoning ordinance and current administrative procedures. I further acknowledge the fee explanation as outlined in the application procedures and hereby agree to pay all statements received from the City pertaining to additional application expense.

Signature of applicant: [Signature] Date: 2014-05-01

| |
|--|
| City Use Only |
| Planning: Zoning District: _____ Date: _____ |
| Reviewed by: _____ Date: _____ |
| Subject to the following conditions: _____ |
| Engineering: Reviewed by: _____ Date: _____ |
| Subject to the following conditions: _____ |



City of Lake Elmo
Nick Johnson
3800 Laverne Avenue North
Lake Elmo, MN 55042

May 1, 2014

Mr. Johnson:

This letter is intended to accompany our submittal for our application to the City of Lake Elmo for the requested Conditional Use Permit for our proposed project at the southeast quadrant of Hudson Boulevard and Keats Avenue North. Included in the submittal is the Land Use Application along with the required civil and architectural plans for Lot 1, Block 1

Kwik Trip, Inc. is proposing the construction of a convenience store, carwash, and fueling facility at the currently undeveloped site. Attached are 50 copies (full, 11x17) of the existing conditions plan, site plan, keynote plan, grade and erosion control plan, utility plan, SWPPP, details, landscape plan, photometric plan, building floor plan and elevation, carwash floor plan and elevation, canopy elevation. The storm water management calculations are also attached.

The proposed method of operation for this development will be consistent with that for our existing convenience stores within the area. The requested hours of operation will be 24 hours for all uses. The type of products that will be sold will be similar to that of our existing stores: carwash, gasoline, in line diesel, E-85, groceries, bakery and dairy, hot and cold food and beverages, tobacco products, lotto, convenience store merchandise, ice, and propane. The outside merchandising of products is being requested next to the store (ice and propane) and miscellaneous merchandising under the gas canopy. The proposed store is projected to have between 15-20 full and part time employees, with 2-8 on staff at any given time.

The proposed architectural plan will consist of a brick facade with standing seam metal roof. The building and canopy fascia will tie in with franchise colors. The project will be constructed to the specifications set forth by the USGBC in order to be considered as a LEED "Green Build" project.

Kwik Trip would be happy to provide any additional information or answer any questions or concerns the City Lake Elmo may have with our submission. Please feel free to call Scott Teigen (715 236 6626) or myself with any questions.

Sincerely,

Bradford Fry, PE
Store Engineering
608 793 6414



Our Mission: "To serve our customers and community more effectively than anyone else by treating our customers, co-workers and suppliers as we, personally, would like to be treated, and to make a difference in someone's life."

Kwik Trip & Kwik Star Stores • Tobacco Outlet Plus • Convenience Transportation, LLC • Hearty Platter Restaurants & Cafés



1. Land Use Application along with a check for \$1050 is attached
2. A. Seller: Bruce Miller, CM Properties 94 Limited Partnership, 3470 Washington Dr, Suite 102, Eagan, MN 55112
Contract applicant: Bradford Fry, Kwik Trip, 1626 Oak St, La Crosse, WI 54601
Engineer: Joe Radach, Carlson McCain, 248 Apollo Dr, Suite 100, Lino Lakes, MN 55014
B. Lot 1, Block 1, Lakewood Crossing (to be recorded), Zone C, 75,454 sf
C. The current property is a green field and is undeveloped
D. The proposed use of the property will be for the construction and operation of a convenience store with fuel and a double bay carwash. The site will have access into the development (one shared with future development) and 2 detention ponds (sediment and infiltration). We operate our facilities 24 hours a day, 7 days a week with approximately 15 to 20 employees.
E. The proposed use will provide fueling and carwash services to the community as well as a large array of products within the store. Many of the products sold in the store are created by our kitchens, dairy, bakery, and commissary located in La Crosse, WI. The development will have adequate access with 2 driveways, and sufficient parking in front of and along side the convenience store.
3. A copy of the current title commitment is attached
4. Address labels are attached from Washington County
5. 50 copies of the requested plan sets are attached
6. 50 copies of the landscape plan are attached
7. 50 copies of the architectural plans for the convenience store, carwash, and canopy

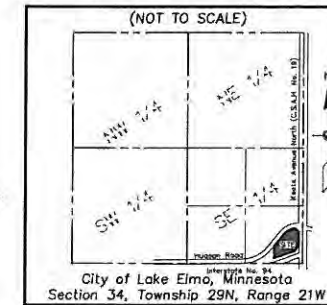


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Kwik Trip & Kwik Star Stores • Tobacco Outlet Plus • Convenience Transportation, LLC • Hearty Platter Restaurants & Cafés

LAKWOOD CROSSING

VICINITY MAP



SITE DATA

| | |
|---|------------|
| TOTAL SITE AREA | ±5.87 AC. |
| TOTAL OUTLOT AREA | ±3.82 AC. |
| TOTAL LOT AREA | ±2.05 AC. |
| LOT 1 | ±89,355 SF |
| TOTAL NUMBER OF LOTS (including Outlot) | 2 |
| EXISTING ZONING | RT |
| UTILITIES | AVAILABLE |
| MINIMUM RT ZONING SETBACK DATA: | |
| FRONT | 30 FT. |
| SIDE | 10 FT. |
| SIDE CORNER | 25 FT. |
| REAR | 40 FT. |

PARCEL DESCRIPTION

(Per Title Commitment File No. K1M00248, dated February 05, 2014 at 4:30 P.M. prepared by The Title Company, Inc., Issuing agent for Chicago Title Insurance Company)

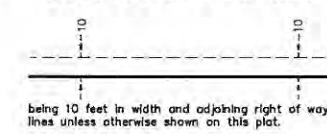
Part of the East half of the Southeast Quarter of the Southeast Quarter of Section 34, Township 29, Range 21, Washington County, Minnesota, described as follows: Beginning at a point on the West line of said East half of the Southeast Quarter of the Southeast Quarter of Section 34, distant 117.99 feet North of the Southwest corner thereof, said point also being in the boundary of Parcel 33, Minnesota Department of Transportation Right of Way Plat Numbered 82-43, according to the recorded plat thereof; thence North 0 degrees 50 minutes 36 seconds West, assumed basis for bearings, along said West line 286.11 feet to said Parcel 33 boundary; thence North 38 degrees 02 minutes 20 seconds East, along said boundary, 321.21 feet; thence Northwesterly, along said boundary, on a tangential curve concave to the Southeast, central angle 39 degrees 48 minutes 22 seconds and radius 445.22 feet, 311.40 feet; thence South 48 degrees 21 minutes 21 seconds East, along said boundary, 135.19 feet; thence South 0 degrees 53 minutes 13 seconds East, along said boundary, 308.81 feet; thence South 34 degrees 19 minutes 58 seconds West, along said boundary, 163.40 feet; thence South 69 degrees 32 minutes 54 seconds West, along said boundary, 314.82 feet; thence Southwesterly, along said boundary, on a tangential curve concave to the Northwest, central angle 3 degrees 44 minutes 14 seconds and radius 2784.79 feet, 180.34 feet to the point of beginning.

(Abstract Property)

GENERAL NOTES

- The field work for this survey was completed on February 19, 2014.
- Bearings shown are based on the East line of the Southeast Quarter of Section 34, Township 29N, Range 21W which is assumed to have a bearing of North 00 degrees 04 minutes 05 seconds West.
- Surveyed property address - unassigned, Lake Elmo, MN.
- Above ground utilities have been field located as shown. All underground locations shown hereon are APPROXIMATE. Prior to any excavations or digging, contact Gopher State One Call for an on-site location (651-454-0002).
- The temporary easement per Document No. 3953750 expires on December 31, 2014 and is not shown hereon.

PROPOSED DRAINAGE AND UTILITY EASEMENTS ARE SHOWN THUS:



being 10 feet in width and adjoining right of way lines unless otherwise shown on this plat.

LEGEND

| | | | |
|---|---|--------|---|
| ⊙ | - Denotes found Washington County Section Corner Monument, as noted | — | - Denotes Overhead Utilities |
| ● | - Denotes Found Iron Pipe, as noted | — | - Denotes Underground Fiber Optic |
| ○ | - Denotes Set Iron Pipe, Marked with RLS 40361 | — FM — | - Denotes Forcemain |
| * | - Denotes Light Pole | — | - Denotes Watermain |
| ⊙ | - Denotes Sanitary Manhole | — | - Denotes Sanitary Sewer |
| ⊙ | - Denotes Miscellaneous Sign | — | - Denotes Underground Gas |
| ⊙ | - Denotes Fire Hydrant | — | - Denotes Storm Sewer |
| ⊙ | - Denotes Gate Valve | — | - Denotes access control dedicated per MINNESOTA DEPARTMENT OF TRANSPORTATION RIGHT OF WAY PLAT NO. 82-43 |
| ⊙ | - Denotes Utility Pole | — | - Denotes Existing Fence as noted |
| ⊙ | - Denotes Service | — | - Denotes Existing 1 Ft. Contour |
| ⊙ | - Denotes Guy Wire | — | - Denotes Existing 5 Ft. Contour |
| | | (Deed) | - Denotes record distance or direction as described in the legal description |

BENCHMARK

1. MNDOT Brass Disk in the Southeast end of Bridge Rd., Station Name 8282 AF Elevation = 974.47



Carlson
McCain
ENVIRONMENTAL - ENGINEERING - SURVEYING
248 Apollo Dr, Suite 100, Lino Lakes, MN 55014
Phone: 763-489-7900 Fax: 763-489-7959

PRELIMINARY PLAT
LAKWOOD CROSSING
Lake Elmo, Minnesota

CM Properties 94
Limited Partnership
3470 Washington Drive
Suite 102
Eagan, MN 55122

REVISIONS

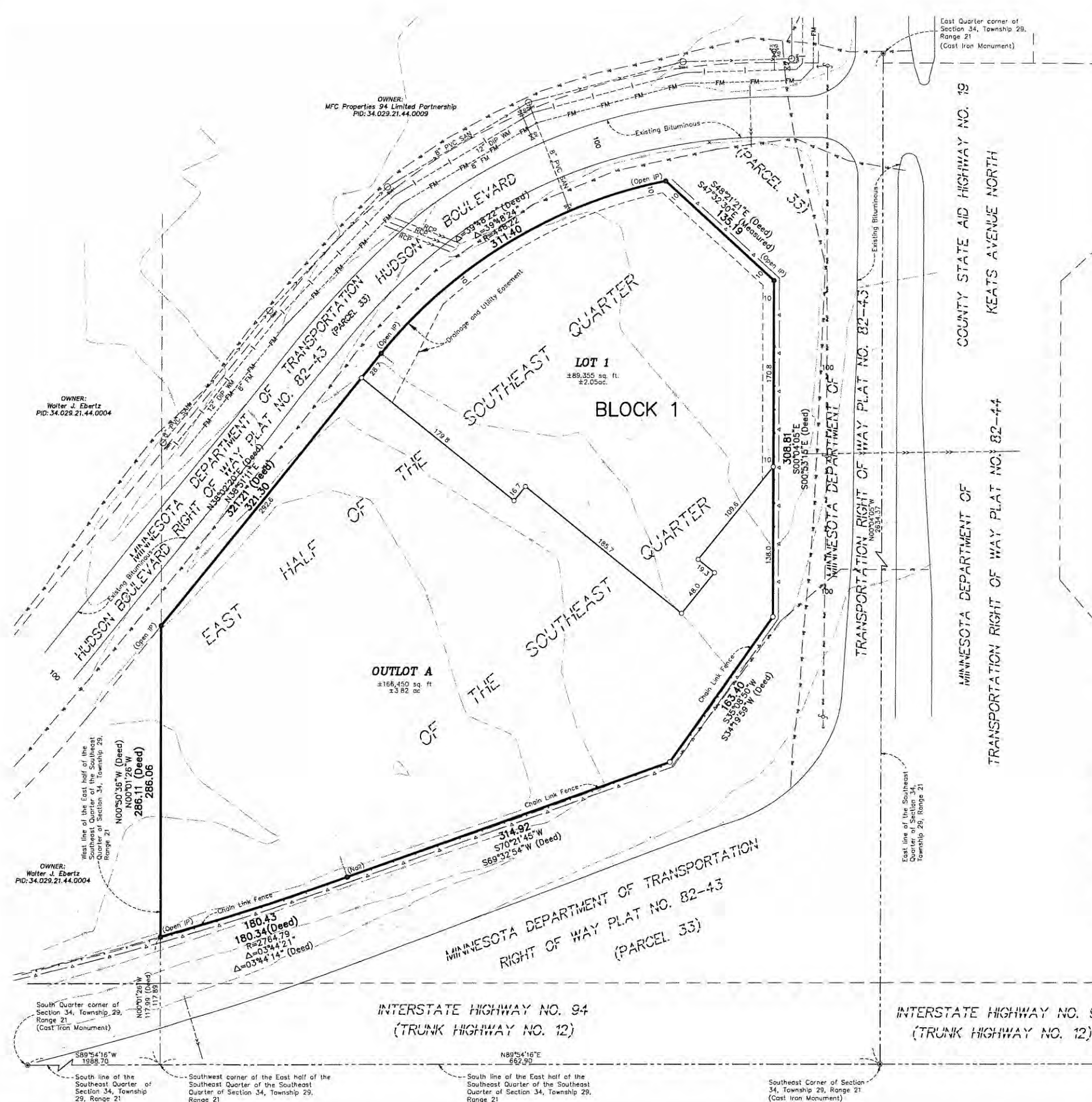
| | |
|----|-----------------------|
| 1. | City Comments-6/27/14 |
| 2. | |
| 3. | |
| 4. | |
| 5. | |
| 6. | |

DRAWN BY: PJB
ISSUE DATE: 05/09/14
FILE NO: 811

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Name: Thomas R. Balluff
Signature: *Thomas R. Balluff*
Date: 05/09/14 License #: 40361

1 of 1



OWNER:
MFC Properties 94 Limited Partnership
PID: 34.029.21.44.0009

OWNER:
Walter J. Ebertz
PID: 34.029.21.44.0004

OWNER:
Walter J. Ebertz
PID: 34.029.21.44.0004

South Quarter corner of Section 34, Township 29, Range 21 (Cast Iron Monument)

South line of the Southeast Quarter of Section 34, Township 29, Range 21

Southwest corner of the East half of the Southeast Quarter of Section 34, Township 29, Range 21

South line of the East half of the Southeast Quarter of Section 34, Township 29, Range 21

Southeast Corner of Section 34, Township 29, Range 21 (Cast Iron Monument)

East Quarter corner of Section 34, Township 29, Range 21 (Cast Iron Monument)

East line of the Southeast Quarter of Section 34, Township 29, Range 21

LAKEWOOD CROSSING

KNOW ALL BY THESE PRESENTS: That CM Properties 94 Limited Partnership, a Minnesota limited partnership, owner of the following described property situated in the County of Washington, State of Minnesota, to wit:

Part of the East half of the Southeast Quarter of the Southeast Quarter of Section 34, Township 29, Range 21, Washington County, Minnesota, described as follows: Beginning at a point on the West line of said East half of the Southeast Quarter of the Southeast Quarter of Section 34, distant 117.99 feet North of the Southwest corner thereof, said point also being in the boundary of Parcel 33, Minnesota Department of Transportation Right of Way Plat Numbered 82-43, according to the recorded plats thereof; thence North 0 degrees 50 minutes 36 seconds West, assumed basis for bearings, along said West line 286.11 feet to said Parcel 33 boundary; thence North 38 degrees 02 minutes 20 seconds East, along said boundary, 321.21 feet; thence Northeasterly, along said boundary, on a tangential curve concave to the Southeast, central angle 39 degrees 48 minutes 22 seconds and radius 448.22 feet, 311.40 feet; thence South 48 degrees 21 minutes 21 seconds East, along said boundary, 135.19 feet; thence South 0 degrees 53 minutes 15 seconds East, along said boundary, 308.81 feet; thence South 34 degrees 19 minutes 59 seconds West, along said boundary, 163.40 feet; thence South 69 degrees 32 minutes 54 seconds West, along said boundary, 314.92 feet; thence Southwesterly, along said boundary, on a tangential curve concave to the Northwest, central angle 3 degrees 44 minutes 14 seconds and radius 2764.79 feet, 180.34 feet to the point of beginning.

Has caused the same to be surveyed and platted as LAKEWOOD CROSSING and does hereby dedicate to the public for public use forever the easements created by this plat for drainage and utility purposes only.

In witness whereof said CM Properties 94 Limited Partnership, a Minnesota limited partnership has caused these presents to be signed by its proper partner this _____ day of _____, 20____.

CM Properties 94 Limited Partnership

Martin F. Colon, General Partner

STATE OF _____
COUNTY OF _____

This instrument was acknowledged before me on _____ by Martin F. Colon, General Partner of CM Properties 94 Limited Partnership, a Minnesota limited partnership on behalf of the partnership.

Notary Public, _____
My commission expires _____

I, Thomas R. Balluff, do hereby certify that I have surveyed and platted or directly supervised the survey and platting of the property described on this plat as LAKEWOOD CROSSING; that I am a duly licensed Land Surveyor in the State of Minnesota; that this plat is a correct representation of the boundary survey, that all mathematical data and labels are correctly designated on this plat; that all monuments depicted on the plat have been or will be correctly set within one year as indicated on the plat; that all water boundaries and wet lands as defined in MS 505.01, Subd. 3 existing as of the date of this certification are shown and labeled on the plat, and that all public ways are shown and labeled on the plat.

Dated this _____ day of _____, 20____.

Thomas R. Balluff, Licensed Land Surveyor
Minnesota License No. 40361

STATE OF _____
COUNTY OF _____

This instrument was acknowledged before me on _____ by Thomas R. Balluff, Licensed Land Surveyor.

Notary Public, _____
My commission expires _____

CITY OF LAKE ELMO PLANNING COMMISSION

Approved by the Planning Commission of the City of Lake Elmo, Minnesota, this _____ day of _____, 20____.

Signed: _____, Chairperson Signed: _____, Secretary

CITY COUNCIL, CITY OF LAKE ELMO, MINNESOTA

This plat was approved by the City Council of the City of Lake Elmo, Minnesota, this _____ day of _____, 20____, and hereby certifies compliance with all requirements as set forth in Minnesota Statutes, Section 505.03, Subd. 2.

Signed: _____, Mayor Signed: _____, Clerk

COUNTY SURVEYOR

Pursuant to Chapter 820, Laws of Minnesota, 1971, and in accordance with Minnesota Statutes, Section 505.021, Subd. 11, this plat has been reviewed and approved this _____ day of _____, 20____.

By: _____
Washington County Surveyor

COUNTY AUDITOR/TREASURER

Pursuant to Minnesota Statutes, Section 505.021, Subd. 9, taxes payable in the year 20____ on the land hereinbefore described have been paid. Also, pursuant to Minnesota Statutes, Section 272.12, there are no delinquent taxes and transfer entered this _____ day of _____, 20____.

By: _____
Washington County Auditor/Treasurer

COUNTY RECORDER

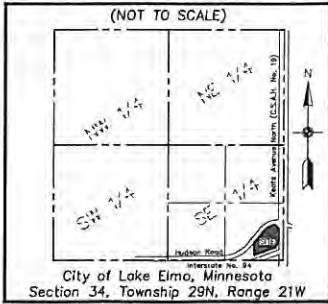
Document Number _____

I hereby certify that this instrument was recorded in the Office of the County Recorder for record on this _____ day of _____, 20____, at _____ o'clock _____ M., and was duly recorded in Washington County Records.

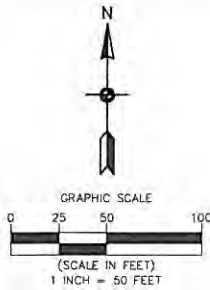
By: _____
Washington County Recorder

By: _____
Deputy

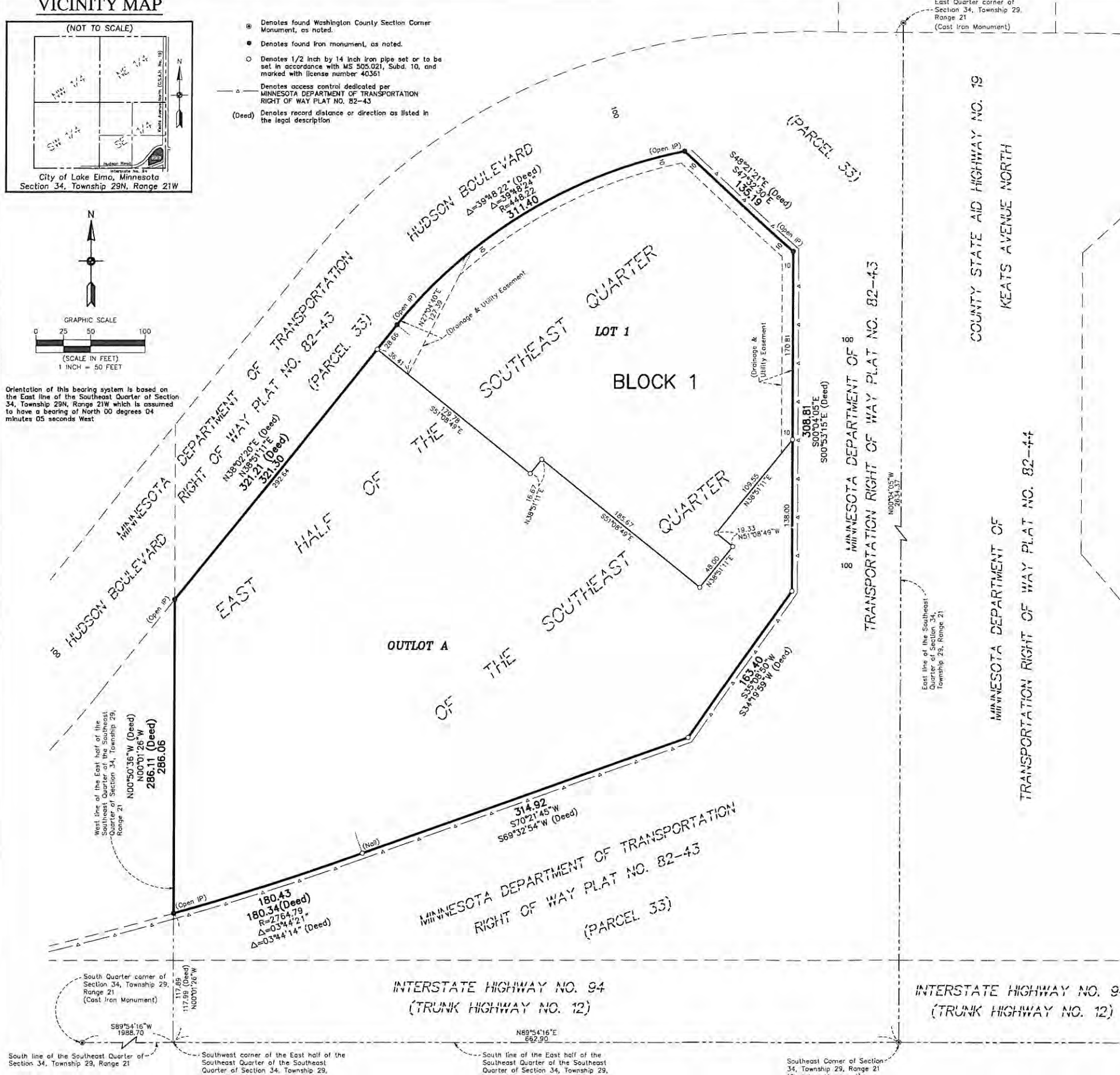
VICINITY MAP



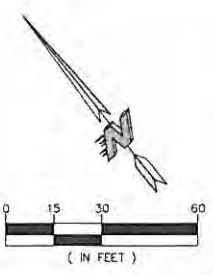
- ⊙ Denotes found Washington County Section Corner Monument, as noted.
- Denotes found iron monument, as noted.
- Denotes 1/2 inch by 1/4 inch iron pipe set or to be set in accordance with MS 505.021, Subd. 10, and marked with license number 40361.
- △ Denotes access control dedicated per MINNESOTA DEPARTMENT OF TRANSPORTATION RIGHT OF WAY PLAT NO. 82-43
- (Dead) Denotes record distance or direction as listed in the legal description



Orientation of this bearing system is based on the East line of the Southeast Quarter of Section 34, Township 29N, Range 21W which is required to have a bearing of North 00 degrees 04 minutes 05 seconds West



Carlson McCain
ENVIRONMENTAL • ENGINEERING • SURVEYING



| BENCHMARKS | |
|------------|---|
| 1. | MN/DOT GSD STATION NO. 33428 TOP OF BENCHMARK DISK ELEVATION = 974.47 (NAVD 88) |
| 2. | MN/DOT GSD STATION NO. 33429 TOP OF BENCHMARK DISK ELEVATION = 943.86 (NAVD 88) |



UTILITY PLAN LEGEND

| | EXISTING | PROPOSED |
|--------------------------|----------|----------|
| CURB | --- | --- |
| BITUMINOUS | --- | --- |
| CONCRETE WALK | --- | --- |
| PROPERTY LINE | --- | --- |
| EASEMENT LINE | --- | --- |
| STORM SEWER | --- | --- |
| SANITARY SEWER | --- | --- |
| WATER MAIN | --- | --- |
| UNDERGROUND GAS | --- | --- |
| OVERHEAD ELECTRIC | --- | --- |
| FORCE MAIN | --- | --- |
| UNDERGROUND FIBER OPTICS | --- | --- |
| UNDERGROUND TELEPHONE | --- | --- |
| MANHOLE | ○ | ○ |
| HYDRANT | ○ | ○ |
| GATE VALVE | ○ | ○ |
| CLEANOUT | ○ | ○ |
| POWER POLE | ○ | ○ |
| FENCE | --- | --- |
| RETAINING WALL | --- | --- |

SANITARY SEWER:

1. Pipe: Use solid-core, SDR-35, ASTM D3034 (or approved equal) Polyvinyl Chloride (PVC) Pipe for all designated PVC sanitary sewer services unless otherwise noted. Pipe for services shall meet the requirements of ASTM D2689 for Schedule 40 sanitary sewer shall have push-on joints with molded rubber gaskets meeting the requirements of ASTM D3212 except for service lateral pipe connections to pipes, tees, wyes, and fittings which shall have solvent-cemented joints meeting the requirements of ASTM D2855 and ASTM D3212. Lay of PVC pipe on a continuous granular bed. Installation must comply with ASTM D2321.
2. Cleanouts: Install cleanouts on all sanitary sewer services. The distance between cleanouts in horizontal piping shall not exceed 100 feet for pipes 4-inch and over in size. Cleanouts shall be at the same nominal size as the pipe they serve and concrete and pipe support. Install a meter box frame and solid lid (Newman R-191-A, or approved equal) over all cleanouts, unless otherwise noted.
3. Testing: Pressure test all sanitary sewer lines per the City requirements. Test all flexible sanitary sewer lines for deflection after the sewer line has been installed and backfill has been in place for at least 30 days. No pipe shall exceed a deflection of 0.5% if the test fails, make necessary repairs and retest.
4. Unless other indicated, use reinforced, precast, concrete maintenance holes conforming to ASTM C478, furnished with precast boxes. Sanitary sewer maintenance holes shall be supplied with pre-formed inverts and flexible response screw connections for all lateral lines 3/8" (1 1/2") in diameter or less, unless otherwise indicated. Joints for all vertical maintenance hole sections shall have confined rubber gaskets in accordance with ASTM C923. The inside barrel diameter shall not be less than 48 inches.
5. Install flexible waterproof frame/chimney seals on all sanitary sewer maintenance holes. Use either Manufactured Maintenance Hole Frame/Chimney Seals or Glaslamer Waterproofing Frame/Chimney Seals.
6. Use Neenah Foundry Co. R-1733 coating on all sanitary sewer maintenance holes unless otherwise noted. Covers shall bear the "Sanitary Sewer" label.

WATER DISTRIBUTION SYSTEM:

1. Separation of Water and Sewer: Provide a minimum horizontal separation of 10 feet between all water and sewer lines. Provide a minimum separation of 18 inches at all water line and sewer line crossings.
2. Watermain Depth: Maintain 7 feet of cover over the top of the water lines to the finished grade. Verify elevation of proposed and existing water lines at all utility crossings. Install the water lines at greater depths in order to clear storm sewers, sanitary sewers, or other utilities as required. Include costs to lower water lines in the bid.
3. Disinfection: Disinfect all completed watermain in accordance with AWWA Standard C651-05 and the City specifications. Water lines shall be flushed prior to disinfection, except when tablet method is used.
4. Testing: Pressure test and perform bacteriological tests on all water lines under the supervision of the City Public Works Department. Notify the City of test 24 working hours prior to any testing. Pressurize the waterline to 100 PSI (150 psf) gauge pressure (measured at the point of lowest elevation) by means of a pump connected to the pipe in a satisfactory manner. Maintain the test pressure for a minimum of 2 hours. Do not add water to the system in order to maintain the required pressure during the watermain pressure testing. The test section of pipe is acceptable with a pressure drop of 7 PSI (1 psf) or less. Electrical conductivity testing shall be performed as directed by the City.
5. Use AWWA C151/A21.51 Thickness Class 52 Ductile Iron Pipe (DIP) with push-on joints for all watermain unless otherwise noted. All watermain fittings shall meet the requirements of AWWA C153, latest edition. Girths and nuts shall be "Car-lin" meeting AWWA C111, latest edition.
6. Use mechanical joint restraint devices for joint restraint on all watermain bands having a vertical or horizontal deflection of 22-1/2 degrees or greater, all wyes, stubs, extension, tees, crosses, plugs, and all hydrant valves, and all hydrants in accordance with City requirements. Use "Sarnie 1100 Magslip" manufactured by GEMA Iron Works, Canton, Texas or approved equal, installed in accordance with the manufacturer's recommendations for restraint on Ductile Iron Pipe.
7. At all valve locations which require a 12" or smaller valve, install gate valves which are of the compression resilient seated (CRS) type. Gate valves shall conform to AWWA C213. Install cast iron valve boxes conforming to ASTM A88 of each valve location. Valve boxes shall be the "Bee-Box" style type with 5-1/4" shafts and the word "water" on the lid. Use Tyler B550 with 4-inch drop lid, or approved equal. Valve boxes shall have at least 8" of adjustment above and below finished grade.
8. Copper service pipe shall be Type K, seamless copper water tubing, soft annealed temper and conform to the requirements of ASTM B88 of each valve location. High density polyethylene (HDPE) water service pipe shall conform to ASTM D2737, for "Enduro" Water Service Tubing, PE 3408-200 psi, SDR 7 (PS), as manufactured by Endit Industries.
9. TRACER WIRE: Tracer wire for all non-metallic pipe shall be No. 8 AWG gauge, single or multiple strand plastic coated copper wire. Wire splices shall be made with a mechanical wire proof connection such as "3M GPO" or approved equal. Tracer wire shall be fastened to fire hydrant with "Hydrant Tracer Wire Bracket" as manufactured by Val Products in approved equal.

STORM DRAINAGE:

1. Unless otherwise indicated, use reinforced, precast, concrete maintenance holes and catch basins conforming to ASTM C478, furnished with water stop gaskets and precast boxes. Joints for all precast maintenance hole sections shall have confined rubber gaskets in accordance with ASTM C923. The inside barrel diameter shall not be less than 48 inches, unless otherwise noted.
2. All joints and connections to catch basins or manholes shall be watertight. Use resilient rubber seals, waterstop gaskets, or approved equal. Cement mortar joints are not allowed.
3. Maintenance holes and catch basins in paved areas shall be sumped 0.05 feet. Catch basins in the gutter line shall be sumped 0.17 feet. Elevations shown on the plans represent the sumped elevations.
4. PVC Pipe: Use solid-core, SDR-35, ASTM D3034 Polyvinyl Chloride (PVC) Pipe for designated PVC storm sewer 4 to 10 inches in diameter. Joints for all PVC storm sewer shall have push-on joints with molded rubber gaskets meeting the requirements of ASTM D3212 except for service lateral pipe connections to pipes, tees, wyes, and fittings which shall have solvent-cemented joints meeting the requirements of ASTM D2855 and ASTM D3212. Lay of PVC pipe on a continuous granular bed. Installation must comply with ASTM D2321.
5. RCP: Reinforced concrete pipe (RCP) and fittings shall conform to ASTM C76, Design C, with circular reinforcing for the pipe specified herein. Use Class V RCP for pipes 24" and larger. Use Class V RCP for pipes 18" and smaller. Joints shall be Bureau of Reclamation type R-4, with confined rubber "O" ring gaskets in accordance with ASTM C561.
6. RCP Access: Install a reinforced concrete apron on the free end of all daylight RCP storm sewer pipes. The top three sections (including apron) of all daylight RCP storm sewers with minimum of two lift stations per lift. This requirement applies to both upstream and downstream pipe lifts and outlets. For concrete culverts, lift all joints. This to be used only to hold the pipe sections together, not for pulling the sections tight. Install safety-trap racks on all concrete aprons.
7. Testing: Test all portions of storm sewer that are within 10 feet of buildings, within 10 feet of buried water lines, within 50 feet of water wells, or that pass through soil or water identified as being contaminated. Test of flexible storm sewer lines for deflection after the sewer line has been installed and backfill has been in place for at least 30 days. No pipe shall exceed a deflection of 0.5%. If the test fails, make necessary repairs and retest.
8. Use Neenah castings, or approved equal, of the type indicated in the Storm Sewer Schedule.
9. The minimum depth of cover for building and canopy roof drain leaders within 15 feet, insulate roof drain leaders at locations where the depth of cover is less than 3 feet. Provide a minimum insulation thickness of 2 inches. The insulation must be at least 4 feet wide and centered on the pipe. Install the insulation boards 6 inches above the top of the pipes on mechanically compacted and leveled pipe bedding material. Use high density, closed cell, rigid board equivalent to DOW Styrofoam 40-40 plastic foam insulation.
10. Cleanouts: Install cleanouts on all roof drains in accordance with S.P.S. 382.35 (3)(c)(1). The distance between cleanouts in horizontal piping shall not exceed 100 feet for pipes 10-inches and under in size. Cleanouts shall be at the same nominal size as the pipe they serve. Install a meter box frame and solid lid (Newman R-191-A, or approved equal) over all cleanouts unless otherwise noted.
11. HDPE Requirements:
 - 11.1. Install dual-wall, smooth interior, corrugated high-density polyethylene (HDPE) pipe at locations indicated on the plan.
 - 11.2. HDPE pipe shall conform to the requirements of AASHTO M294 and Section 12 of the AASHTO LRFD Bridge Design Specification.
 - 11.3. All fittings must comply with ASTM Standard D3212.
 - 11.4. Water-tight joints must be used at all connections including structures.
 - 11.5. Lay all HDPE pipe on a continuous granular bed. Installation must comply with ASTM D3212. All sections of the corrugated HDPE pipe shall be coupled in order to provide water tight joints.
 - 11.6. Testing shall be performed in accordance with the City specifications.

STORM SEWER SCHEDULE

| STRUCTURE | NEENAH CASTING OR EQUAL |
|-----------|-------------------------|
| *CBMH-101 | 60" DIA. R-3067-VB |
| CBMH-102 | 48" DIA. R-3067-VB |
| CB-102A | 24" X 36" R-3067-VB |
| *CBMH-103 | 48" DIA. R-3067-VB |
| CBMH-104 | 48" DIA. R-3067-V |
| CBMH-105 | 48" DIA. R-3067-V |
| CBMH-106 | 48" DIA. R-3067-VB |
| CB-107 | 24" X 36" R-3067-VB |

*INSTALL 1BR SNOOT OIL & DEBRIS STOP AS MANUFACTURED BY BMP, INC.

- GENERAL:**
1. **General Specifications:** The latest editions of the Minnesota Department of Transportation "Standard Specifications for Construction", the City Engineers Association of Minnesota (CEAM) Standard Specifications, the City of Lake Elmo Standard Specifications and the Minnesota Plumbing Code.
 2. Comply with the work safety practices specified by the Occupational Safety and Health Administration (OSHA). Comply with all applicable local, state, and federal safety regulations. OSHA prohibits entry into "confined spaces", such as manholes and inlets (see 29CFR Section 1910.146) without undertaking certain specific practices and procedures. Construction safety is solely the responsibility of the Contractor, who is also solely responsible for the means, methods, and sequencing of the construction operations.
 3. Perform all utility work in accordance with State and City requirements.
 4. Contact the City of Lake Elmo's Public Works Department at (651) 747-3940 for inspection of all utility work.
 5. Contact existing sanitary sewer lift by connecting. Contact existing storm sewer lift by either snoutcutting or connecting. Meet all City standards and specifications for the connection. Reconnect sewer after installation. Use water stop gaskets in order to provide watertight seals when penetrating a structure wall with a pipe. Take measures before beginning construction to ensure that service connections do not cut into maintenance access structure joints or pipe barrel joints.
 6. Perform trench excavations for all utilities in accordance with the requirements of OSHA 29 CFR, Part 1926, Subpart R, "Excavations and Trenches" (see also plan).
 7. Coordinate building utility connection locations at 5 ft. out from the proposed building with the interior plumbing contractor prior to construction. Verify water and sewer service locations and elevations with the Mechanical Engineer prior to construction.

8. The locations of existing utilities shown on this plan are from record information and field as-built data. The Engineer does not guarantee that all existing utilities are shown or, if shown, exist in the locations indicated on the plan. It is the Contractor's responsibility to verify the vertical and horizontal location of all existing utilities (including water service lines and appurtenances). Notify the Engineer of any discrepancies.
9. Contact utility companies for locations of all public and private utilities within the work area prior to beginning construction. Contact 811 for locations of existing utilities at least 72 hours (including weekends or holidays) before beginning any construction. Obtain ticket number and meet with representatives of the various utilities at the site. Provide the Owner with the ticket number information.
10. Follow to verify the location of existing underground facilities at a sufficient number of locations in order to assure that no conflict with the proposed work exists and that sufficient clearance is obtainable.
11. Where existing gas, electric, cable, or telephone utilities conflict with the work, coordinate the abandonment, relocation, offset, or support of the existing utilities with the appropriate utility companies. Coordinate new gas meter and gas line installation, electric meter and electric service installation, cable service and telephone service installation with the local utility companies.
12. Arrange for and secure suitable disposal areas off-site. Dispose of all excess soil, waste materials, debris, and all materials not designated for reuse. Waste material and debris include tires, stumps, pipe, concrete, asphaltic concrete, cans, or other waste material from the construction operation. Obtain the rights to any waste area for disposal of unutilized or surplus material either shown or not shown on the plans. All work in disposing of such material shall be considered incidental to the work. All disposal must conform to applicable state waste disposal permit regulations. Obtain all necessary permits at no cost to the Owner.
13. Straight line saw-cut existing bituminous or concrete surfacing at the perimeter of pavement removal areas. Use saws that provide water to the blade. Track, and match all connections to existing bituminous pavement.

14. Relocate overhead power, telephone, and cable lines as required.
15. All materials required for this work shall be new material conforming to the requirements for class, kind, grade, size, quality, and other details specified herein or as shown on the Plans. Do not use recycled or salvaged aggregate, asphaltic pavement, crushed concrete, or scrap asphalt. Unless otherwise indicated, the Contractor shall furnish all required materials.
16. Restore the public right-of-way. Replace any concrete curb and gutter, bituminous pavement, sidewalk, or vegetative cover damaged by the construction activity. Restore damaged turf with sod within the public right-of-way. The work area shown is general and may need to be adjusted in the field.
17. When sawing or drilling concrete or masonry, use saws that provide water to the blade. Do not allow the slurry produced by this process to be trucked outside of the immediate work area or discharged into the sewer system.
18. Adjust all curb stops, valve boxes, maintenance hole castings, catch basin castings, cleanout covers, and similar items to finished grade.
19. Install all pipe with the ASTM identification numbers on the top for inspection. Commence pipe laying at the lowest point in the proposed sewer line. Lay the pipe with the bell end or receiving groove end of the pipe pointing up. When connecting to an existing pipe, uncover the existing pipe in order to allow any adjustments in the proposed line and grade before laying any pipe.
20. Obtain and pay for all permits, tests, inspections, etc. required by agencies that have jurisdiction over the project. The Contractor is responsible for all bonds, letters of credit, or cash warranties related to the work. Excavate and inspect work in accordance with all local and state codes, rules, ordinances, and regulations pertaining to the particular type of work involved.

Kwik Trip
STORES

Kwik Star
STORES

KWIK TRIP, Inc.
P.O. BOX 2107
1626 OAK STREET
LA CROSSE, WI 54602-2107
PH. (608) 781-8988
FAX (608) 781-8960

Carlson McCain
ENVIRONMENTAL - ENGINEERING - SURVEYING
248 Apollo Dr, Suite 100, Lino Lakes, MN 55114
Phone: 763-489-7900 Fax: 763-489-7959

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Name: Joseph T. Radach, P.E.
Signature: _____
Date: 05/09/14 License #: 45889

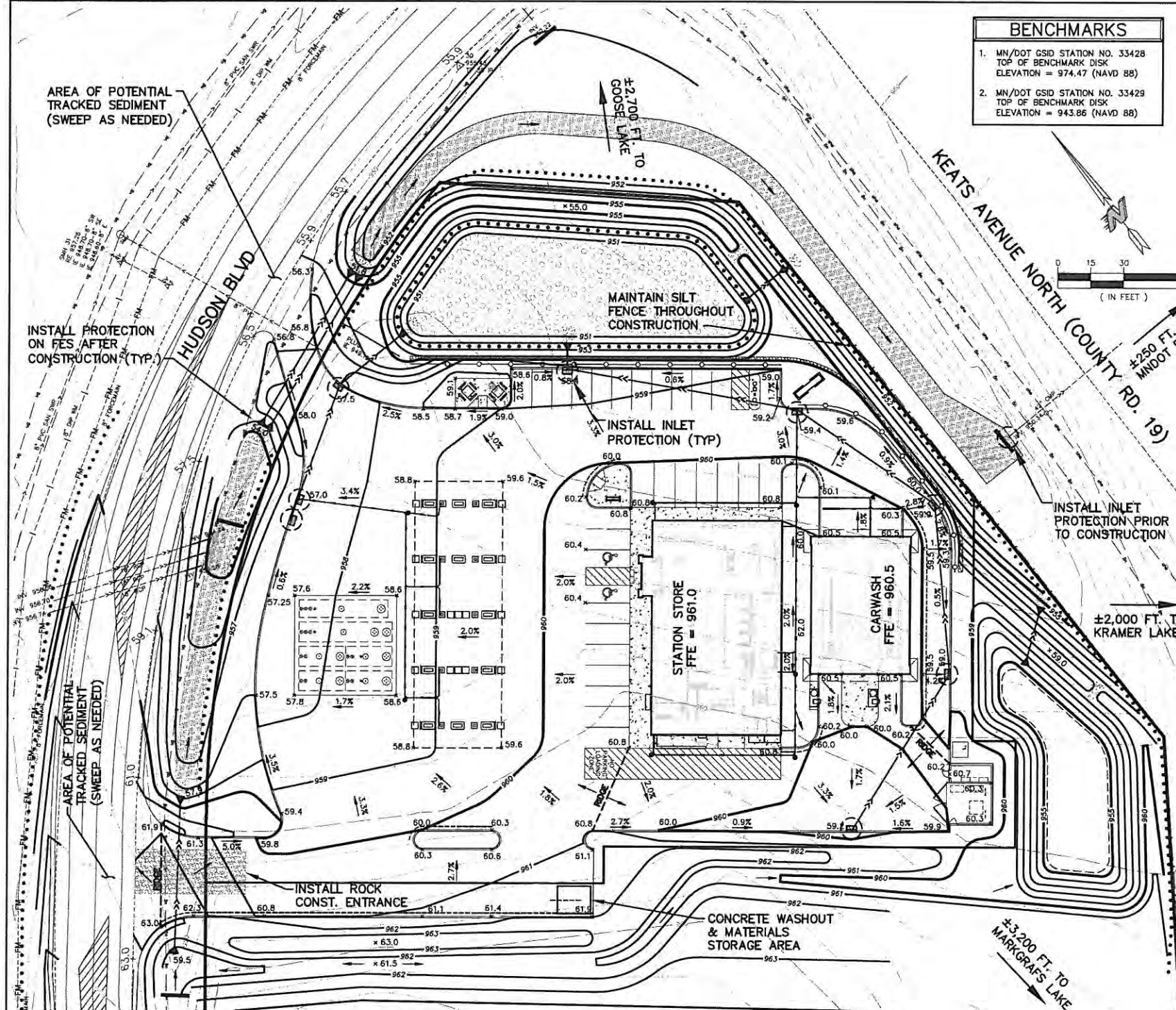
UTILITY PLAN

CONVENIENCE STORE #248 WITH DOUBLE BAY CARWASH

HUDSON BLVD & KEATS AVENUE NORTH
LAKE ELMO, MINNESOTA

| # | DATE | DESCRIPTION |
|---|----------|--------------------|
| 1 | 08/27/14 | Per City comments. |

DRAWN BY: JTR/KMM
SCALE: GRAPHIC
PROJ. NO.: 4982-00
DATE: 2014-05-09
SHEET: SP4



| BENCHMARKS | |
|------------|--|
| 1. | MN/DOT GSID STATION NO. 33428 TOP OF BENCHMARK DISK ELEVATION = 974.47 (NAVD 88) |
| 2. | MN/DOT GSID STATION NO. 33429 TOP OF BENCHMARK DISK ELEVATION = 943.86 (NAVD 88) |

PRIOR TO START OF CONSTRUCTION
THE FOLLOWING STORMWATER POLLUTION PREVENTION MEASURES SHALL BE IMPLEMENTED PRIOR TO CONSTRUCTION. REFER TO GRADING AND EROSION CONTROL PLANS FOR LOCATIONS.

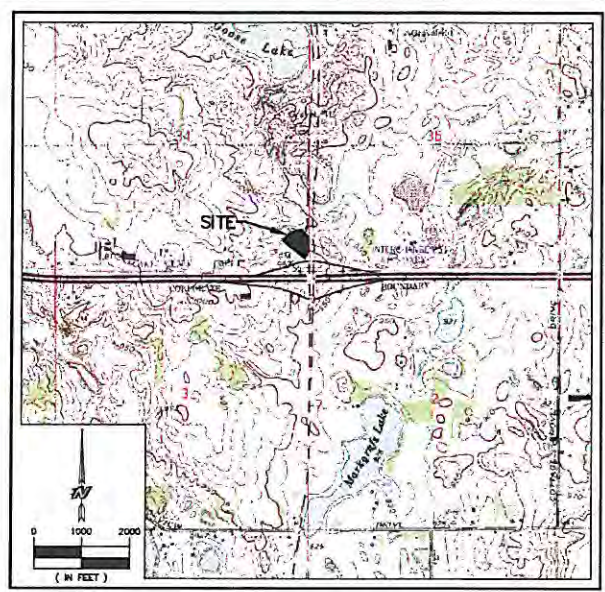
- SILT FENCE**
SILT FENCE SHALL BE INSTALLED AT THE LIMIT OF GRADING ON ANY FILL SLOPE. ADDITIONAL SILT FENCE MAY BE REQUIRED IN CUT SLOPE AREAS. SILT FENCE SHALL ALSO BE INSTALLED AROUND ANY INFILTRATION/FILTRATION PRACTICE.
- ROCK CONSTRUCTION ENTRANCES**
ROCK CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE FIELD ENTRANCES TO THE SITE.
- CATCH BASINS**
ALL CATCH BASINS SHALL BE PROTECTED WITH INLET PROTECTION DEVICES APPROVED BY THE LOCAL GOVERNING UNIT. THESE SHALL INCLUDE, BUT ARE NOT LIMITED TO, WINDO PROTECTION DEVICES, INFRASAFE PROTECTION DEVICES, FILTER FABRIC, BIO ROLLS AND STRAW BALES.

DURING CONSTRUCTION
THE FOLLOWING STORMWATER POLLUTION PREVENTION MEASURES SHALL BE IMPLEMENTED DURING CONSTRUCTION. REFER TO GRADING AND EROSION CONTROL PLANS FOR LOCATIONS.

- PHASED GRADING**
TO THE EXTENT POSSIBLE, GRADING SHALL BE PHASED TO MINIMIZE THE AMOUNT OF DISTURBED AREAS DURING SITE CONSTRUCTION.
- TRACKED SEDIMENT**
ANY SEDIMENT TRACKED FROM THE SITE ONTO THE STREET SHALL BE REMOVED IMMEDIATELY UPON DETECTION. THE ROCK CONSTRUCTION ENTRANCE SHALL BE INSPECTED AND REPAIRED IF INUNDATED WITH SEDIMENT.
- STOCKPILES**
STOCKPILES SHALL BE PLACED IN AN AREA THAT WILL MINIMIZE THE NEED FOR RELOCATION. IF A STOCKPILE WILL REMAIN IN PLACE FOR AN EXTENDED PERIOD OF TIME, STABILIZATION MEASURES SHALL BE IMPLEMENTED, INCLUDING BUT NOT LIMITED TO, SEEDING AND SILT FENCING. TEMPORARY STOCKPILES MUST HAVE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS AND CANNOT BE PLACED IN SURFACE WATERS, INCLUDING STORMWATER CONVEYANCES SUCH AS CURB AND GUTTER SYSTEMS, CONDUITS OR DITCHES.
- TOPSOIL**
UPON GRADING COMPLETION, A MINIMUM OF 4 INCHES OF TOPSOIL SHALL BE PLACED OVER ALL DISTURBED AREAS, EXCLUDING PROPOSED STREETS AND PARKING AREAS.
- RESTORATION**
ALL DISTURBED AREAS NOT ACTIVELY WORKED SHALL BE RESTORED WITH SEED AND MULCH, EROSION CONTROL BLANKET AND/OR SOD WITHIN 7 DAYS.
- SLOPES**
IN ORDER TO MAINTAIN SHEET FLOW AND MINIMIZE RILLS AND/OR CULLES, THERE SHALL BE NO UNBROKEN SLOPE LENGTH OF GREATER THAN 75 FEET FOR SLOPES WITH A GRADE OF 3:1 OR STEEPER.
- DRAINAGE DITCHES**
THE NORMAL WETTED PERIMETER OF ANY TEMPORARY OR PERMANENT DRAINAGE DITCH THAT DRAINS WATER FROM THE SITE, OR DIVERTS WATER AROUND THE SITE, MUST BE STABILIZED WITHIN 200 LINEAL FEET FROM THE PROPERTY EDGE, OR FROM THE POINT OF DISCHARGE TO ANY SURFACE WATER. STABILIZATION MUST BE COMPLETED WITHIN 24 HOURS OF CONNECTING TO A SURFACE WATER.
- PIPE OUTLETS**
PIPE OUTLETS MUST BE PROVIDED WITH TEMPORARY OR PERMANENT ENERGY DISSIPATION WITHIN 24 HOURS OF CONNECTION TO A SURFACE WATER.
- CATCH BASINS**
ALL CATCH BASINS SHALL BE PROTECTED WITH INLET PROTECTION DEVICES APPROVED BY THE LOCAL GOVERNING UNIT. THESE SHALL INCLUDE, BUT ARE NOT LIMITED TO, WINDO PROTECTION DEVICES, INFRASAFE PROTECTION DEVICES, FILTER FABRIC, BIO ROLLS AND STRAW BALES.
- DUST**
CONSTRUCTION DUST SHALL BE CONTAINED TO THE EXTENT POSSIBLE. IF THE SITE BECOMES EXCESSIVELY DUSTY, APPROPRIATE MEASURES SHALL BE TAKEN TO REDUCE DUST BEING TRANSPORTED FROM THE SITE. DUST CONTROL MEASURES INCLUDING, BUT ARE NOT LIMITED TO, WATERING AND CALCIUM CHLORIDE APPLICATION.
- DENATURING**
DENATURING ACTIVITIES SHALL BE CONDUCTED WITH AND APPROVED BY THE LOCAL GOVERNING UNIT. IF THERE WILL BE ANY DENATURING OR BASIN DRAINING THAT MAY HAVE TURBID OR SEDIMENT LADEN DISCHARGE, THE WATER MUST BE DISCHARGED TO A TEMPORARY OR PERMANENT SEDIMENTATION BASIN ON THE PROJECT SITE WHENEVER POSSIBLE. APPROPRIATE BMP'S SHALL BE USED FOR EROSION AND SEDIMENT CONTROL AND ENERGY DISSIPATION.
- CONSTRUCTION MATERIALS AND DEBRIS**
CONSTRUCTION MATERIALS SHALL BE STORED IN AN ORDERLY MANNER AND IN AN AREA THAT WILL MINIMIZE CONFLICTS WITH OTHER CONSTRUCTION ACTIVITIES. CONSTRUCTION DEBRIS SHALL BE CONTAINED IN DUMPSTERS AND REMOVED FROM THE SITE AS NECESSARY.
- CHEMICALS**
CHEMICALS SHALL BE STORED IN A SAFE AREA IN SEALED CONTAINERS WITH THE ORIGINAL LABELING AND MATERIAL SAFETY DATA SHEETS AVAILABLE.
- SPILLS AND CONTAMINATION**
IF FUEL, OIL, OR A HAZARDOUS CHEMICAL IS SPILLED OR DETECTED DURING CONSTRUCTION ACTIVITIES, ALL APPROPRIATE AGENCIES SHALL BE IMMEDIATELY NOTIFIED, INCLUDING, BUT NOT LIMITED TO, THE MINNESOTA DUTY OFFICER AT 800-422-0798.
- CONCRETE WASHOUT AREA**
ALL LIQUID AND SOLID WASTES GENERATED BY CONCRETE WASHOUT OPERATIONS MUST BE CONTAINED IN A LEAK-PROOF CONTAINMENT FACILITY OR IMPERMEABLE LINER. AN IMPERMEABLE COMPACTED CLAY LAYER IS REQUIRED. CONCRETE WASHOUT IN THE AGGREGATE ROAD BASE IS ALLOWED. A SIGN MUST BE INSTALLED AT EACH WASHOUT FACILITY TO DIRECT EQUIPMENT OPERATORS TO THE APPROPRIATE LOCATION.

POST CONSTRUCTION
WHEN THE SITE HAS BEEN COMPLETELY CONSTRUCTED, THE SITE MUST UNDERGO FINAL STABILIZATION. FINAL STABILIZATION OCCURS WHEN ALL OF THE GRADING, INFRASTRUCTURE AND BUILDING ACTIVITIES HAVE BEEN COMPLETED. TO ACHIEVE FINAL STABILIZATION, THE FOLLOWING MEASURES SHALL BE COMPLETED.

- ALL DISTURBED AREAS WITHOUT PERMANENT IMPERVIOUS SURFACES SHALL BE STABILIZED BY A UNIFORM PERENNIAL VEGETATIVE COVER. AREAS NOT REQUIRING SOD OR EROSION CONTROL BLANKET SHALL BE SEEDED AND MULCHED.
- SEDIMENT FROM CONVEYANCES AND TEMPORARY SEDIMENTATION BASINS THAT ARE TO BE USED AS PERMANENT WATER QUALITY MANAGEMENT BASINS SHALL BE CLEANED OUT. SEDIMENTATION BASINS SHALL BE SUFFICIENTLY CLEANED OUT TO RETURN THE BASIN TO DESIGN CAPACITY. SEDIMENT MUST BE STABILIZED TO PREVENT IT FROM BEING WASHED BACK INTO THE BASIN OR CONVEYANCES DISCHARGING OFF-SITE OR TO SURFACE WATERS.
- WHEN STABILIZED VEGETATION HAS BEEN ESTABLISHED OVER 70 PERCENT OF THE PERVIOUS SURFACE AREA, ALL SYNTHETIC TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED. THIS INCLUDES, BUT IS NOT LIMITED TO, SILT FENCE, TREE FENCE AND CATCH BASIN INLET PROTECTION DEVICES.



NOTICE OF TERMINATION
THE PERMITEE(S) MUST SUBMIT A NOTICE OF TERMINATION (NOT) TO THE MPCA WITHIN 30 DAYS AFTER FINAL STABILIZATION IS COMPLETE, OR ANOTHER OWNER/OPERATOR (PERMITEE) HAS ASSUMED CONTROL OVER ALL AREAS OF THE SITE THAT HAVE NOT UNDERGONE FINAL STABILIZATION.

INSPECTIONS & RECORD KEEPING
STORMWATER POLLUTION PREVENTION INSPECTIONS SHALL OCCUR ONCE EVERY SEVEN (7) DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER A RAINFALL EVENT GREATER THAN 0.5 INCHES IN 24 HOURS. INSPECTIONS MAY BE CEASED DURING FROZEN GROUND CONDITIONS WHERE WORK HAS BEEN SUSPENDED DUE TO FROZEN GROUND CONDITIONS. THE REQUIRED INSPECTIONS AND MAINTENANCE MUST TAKE PLACE WITHIN 24 HOURS AFTER RUNOFF OCCURS AT THE SITE OR PRIOR TO RESUMING CONSTRUCTION, WHICHEVER COMES FIRST. DURING THE COURSE OF CONSTRUCTION, IT MAY BE DETERMINED THAT ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES MAY BE NEEDED, OR CERTAIN MEASURES ARE NOT PRACTICAL TO INSTALL. IN THESE CASES, AN AMENDMENT TO THE SWPPP SHALL BE MADE, AND SUPPORTING REASONS SHALL BE DOCUMENTED IN THE SWPPP.

- RECORD NAME OF INSPECTOR AND DATE AND TIME OF INSPECTION.
- RECORD RAINFALL AMOUNT SINCE MOST RECENT INSPECTION.
- INSPECT GRAVEL CONSTRUCTION ENTRANCES FOR SEDIMENTATION. INSPECT ADJACENT STREETS FOR SEDIMENT TRACKING.
- INSPECT SITE FOR EXCESSIVE EROSION AND SEDIMENT ACCUMULATION.
 - INSPECT SILT FENCE AND OTHER TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES FOR EROSION, SEDIMENTATION AND MALFUNCTIONING.
 - INSPECT FLARED END SECTIONS FOR EROSION AND SEDIMENTATION.
 - INSPECT PONDS, INFILTRATION BASINS, TEMPORARY SEDIMENTATION BASINS AND ALL OTHER BMP'S FOR EROSION AND SEDIMENTATION.
 - INSPECT SURFACE WATERS, INCLUDING DRAINAGE DITCHES AND CONVEYANCE SYSTEMS FOR EVIDENCE OF SEDIMENT BEING DEPOSITED BY EROSION.
- INSPECT SITE AND ADJACENT PROPERTIES FOR CONSTRUCTION DEBRIS, TRASH AND SPILLS.
- INSPECT STABILIZED AREAS FOR EROSION.
- RECORD RECOMMENDED REPAIRS, MAINTENANCE AND/OR REPLACEMENTS REQUIRED TO ENSURE EROSION AND SEDIMENTATION CONTROL MEASURES ARE SUFFICIENT.
- RECORD RECOMMENDED AMENDMENTS TO THE SWPPP.
- RECORD REPAIRS, MAINTENANCE AND/OR REPLACEMENTS THAT WERE COMPLETED SINCE THE LAST INSPECTION.

MAINTENANCE
THE OWNER/CONTRACTOR IS RESPONSIBLE FOR THE OPERATION, INSPECTION AND MAINTENANCE OF ALL STORMWATER POLLUTION PREVENTION MEASURES FOR THE DURATION OF THE PROJECT. THE FOLLOWING GUIDELINES SHALL BE USED TO DETERMINE NECESSARY REPAIRS, MAINTENANCE AND/OR REPLACEMENT OF THE EROSION AND SEDIMENTATION CONTROL MEASURES.

- ROCK CONSTRUCTION ENTRANCES SHALL BE REPAIRED OR REPLACED IF THE ROCK BECOMES INUNDATED WITH SEDIMENT AND/OR EXCESSIVE SEDIMENT IS BEING TRACKED FROM THE SITE. SEDIMENT TRACKED ONTO ADJACENT STREETS SHALL BE REMOVED. MEASURES SHALL BE TAKEN IMMEDIATELY UPON DISCOVERY.
- SILT FENCE SHALL BE REPAIRED OR REPLACED WHEN SEDIMENT REACHES 1/3 THE HEIGHT OF THE SILT FENCE, THE SILT FENCE IS DAMAGED AND/OR THE SILT FENCE BECOMES NONFUNCTIONAL. MEASURES SHALL BE TAKEN WITHIN 24 HOURS OF DISCOVERY.
- CATCH BASIN INLET PROTECTION DEVICES SHALL BE CLEANED WHEN SEDIMENT REACHES 1/2 THE HEIGHT OF THE SEDIMENT TRAP AND/OR REPAIRED OR REPLACED IF THE DEVICE BECOMES NONFUNCTIONAL. MEASURES SHALL BE TAKEN WITHIN 72 HOURS OF DISCOVERY.
- FLARED END SECTIONS SHALL BE CLEANED IF DEBRIS IS RESTRICTING FLOW OR IF SEDIMENT HAS ACCUMULATED AT THE OUTLET. IF A FLARED END SECTION BECOMES NONFUNCTIONAL OR DAMAGED, IT SHALL BE REPAIRED OR REPLACED. MEASURES SHALL BE TAKEN WITHIN 72 HOURS OF DISCOVERY.
- IF SEDIMENT IS OBSERVED OFF-SITE OR NEAR SURFACE WATERS, THE SOURCE OF SEDIMENT SHALL BE DETECTED AND ADDITIONAL MEASURES SHALL BE IMPLEMENTED. THE PERMITEE(S) SHALL COORDINATE SEDIMENT RETRIEVAL FROM SURFACE WATERS WITH ALL APPROPRIATE AGENCIES. MEASURES SHALL BE TAKEN WITHIN 7 DAYS OF DISCOVERY.
- PONDS, INFILTRATION BASINS, TEMPORARY SEDIMENTATION BASINS AND ALL OTHER BMP'S SHALL BE CLEANED IF DEBRIS IS PRESENT AND/OR EXCESSIVE SEDIMENTATION HAS OCCURRED. TEMPORARY AND PERMANENT SEDIMENTATION BASINS MUST BE DRAINED AND THE SEDIMENT REMOVED WHEN SEDIMENT HAS FILLED THE BASIN TO 1/2 THE STORAGE VOLUME. NO SEDIMENT SHALL BE ALLOWED TO ACCUMULATE IN INFILTRATION BASINS. MEASURES SHALL BE TAKEN WITHIN 72 HOURS OF DISCOVERY.

QUANTITIES
THE FOLLOWING TABLE PROVIDES ESTIMATED QUANTITIES FOR STORMWATER POLLUTION PREVENTION THROUGHOUT THE PROJECT.

| ITEM | UNIT | ESTIMATED QUANTITY |
|--------------------|------|--------------------|
| ROCK ENTRANCE | EA. | 1 |
| SILT FENCE | LF. | 1,500 |
| INLET PROTECTION | EA. | 12 |
| TURF ESTABLISHMENT | AC. | 1.0 |

GENERAL INFORMATION
THE MINNESOTA'S CONSTRUCTION STORMWATER PERMIT IS AN EXTENSION OF THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM STORMWATER PROGRAM, WHICH IS PART OF THE FEDERAL CLEAN WATER ACT. REGULATED PARTIES MUST DEVELOP A STORMWATER POLLUTION PREVENTION PLAN (SWPPP). THE SWPPP PROVIDES INFORMATION ON THE EXISTING AND PROPOSED SITE CONDITIONS, CONTROL MEASURES FOR STORMWATER POLLUTION PREVENTION BEFORE, DURING AND AFTER CONSTRUCTION, INSPECTION, MAINTENANCE AND INFORMATION RELATED TO THE PERMANENT STORMWATER MANAGEMENT SYSTEM. THE SWPPP SHALL BE KEPT ON SITE AT ALL TIMES DURING ACTIVE CONSTRUCTION.

PROJECT INFORMATION
PROJECT NAME: CONVENIENCE STORE 248 WITH DOUBLE BAY CARWASH.
PROJECT LOCATION: LAKE ELMO, WASHINGTON COUNTY, MINNESOTA
PROJECT OWNER: KWIK TRIP, INC.

RESPONSIBLE PARTIES
THE OWNER MUST IDENTIFY A PERSON KNOWLEDGEABLE AND EXPERIENCED IN THE APPLICATION OF EROSION PREVENTION AND SEDIMENT CONTROL BMP'S WHO WILL OVERSEE THE IMPLEMENTATION OF THE SWPPP, AND THE INSTALLATION, INSPECTION AND MAINTENANCE OF THE EROSION PREVENTION AND SEDIMENT CONTROL BMP'S.

SITE MANAGER: BRAD FRY
TRAINING DOCUMENTATION:
INSPECTION REPRESENTATIVE: BRAD FRY
TRAINING DOCUMENTATION:

EXISTING SITE CONDITIONS
THE SITE IS LOCATED IN THE NORTHWEST QUADRANT OF THE INTERSECTION OF KEATS AVENUE NORTH (COUNTY ROAD 19) AND THE WEST BOUND ON RAMP TO INTERSTATE HIGHWAY 94 IN LAKE ELMO, MINNESOTA. THE SITE IS BOUNDED ON THE SOUTH BY WEST BOUND ON RAMP TO INTERSTATE HIGHWAY 94, ON THE EAST BY KEATS AVENUE, ON THE NORTH BY HUDSON BOULEVARD, AND ON THE WEST BY UNDEVELOPED AGRICULTURAL LAND. THE PROPOSED SITE

LIES WITHIN A PROPOSED COMMERCIAL COMPLEX TOTALING 3.87 ACRES. THERE ARE NO EXISTING IMPERVIOUS SURFACES ONSITE.

THE SITE IS CURRENTLY USED FOR AGRICULTURAL PURPOSES, TYPICALLY PLANTED WITH ROW CROPS. THE SITE HAS A SLOPING TOPOGRAPHY WITH ELEVATIONS RANGING FROM 975 IN THE SOUTHWEST, DOWN TO 951 IN THE EAST. THE MAJORITY OF THE STORMWATER FROM THE SITE DRAINS OVERLAND TO THE NORTHEAST WHERE IT IS COLLECTED IN THE KEATS AVENUE DITCH PRIOR TO DISCHARGING VIA A 42" CMP CULVERT UNDER KEATS AVENUE TO A MANDOT POND LOCATED ON THE EAST SIDE OF KEATS AVENUE. A SMALL PORTION OF THE SITE DRAINS SOUTH TO THE WEST BOUND ON RAMP DITCH WHERE IT DISCHARGES VIA A 30" CMP CULVERT TO THE SOUTH. TO THE NORTHWEST OF THE SITE, ON THE NORTH SIDE OF HUDSON BOULEVARD, THERE IS APPROXIMATELY 80 ACRES OF AGRICULTURAL LAND THAT DRAINS SOUTHWEST TO A DITCH ON THE NORTH SIDE OF HUDSON BOULEVARD. THIS DITCH OUTLETS TO THE SOUTH UNDER HUDSON BOULEVARD TO THE SITE WHERE IT IS COLLECTED IN THE ONSITE DITCH AND DISCHARGES VIA THE 42" CMP CULVERT UNDER KEATS AVENUE. THERE ARE TWO LOCATIONS WHERE THE NORTH SIDE DITCH OF HUDSON BOULEVARD OUTLETS TO THE SOUTH. THE WESTERLY LOCATION OUTLETS VIA THREE 18" RCP CULVERTS. THE EASTERLY LOCATION OUTLETS VIA A SINGLE 18" RCP CULVERT.

PROPOSED SITE CONDITIONS
KWIK TRIP, INC. PLANS ON CONSTRUCTING A CONVENIENCE STORE WITH A GASOLINE FUELING CANOPY, A CARWASH, ASSOCIATED PARKING AND DRIVE AREAS. DURING CONSTRUCTION, APPROXIMATELY 3.5 ACRES WILL BE DISTURBED. AFTER CONSTRUCTION, APPROXIMATELY 1.54 ACRES OF IMPERVIOUS SURFACES WILL EXIST ON SITE. STORMWATER FROM THE SITE WILL BE COLLECTED WITH STORM SEWER. ALL STORMWATER FROM THE SITE WILL ULTIMATELY DISCHARGE ACROSS KEATS AVENUE TO MNDOT'S STORMWATER POND.

SOIL INFORMATION
IN FEBRUARY 2014, BRAUN INTERTEC DRILLED FOUR SOIL BORINGS TO APPROXIMATE NEAR SURFACE SOILS. THESE BORINGS INDICATED ONSITE SOILS CONSIST PRIMARILY OF CLAYEY SAND. THESE SOILS FALL WITHIN THE HYDROLOGIC SOIL GROUP (HSG) "C".

WETLAND CONSIDERATIONS
THERE ARE NO WETLANDS ONSITE.

STORMWATER RECEIVING WATERS
ALL STORMWATER FROM THE SITE WILL ULTIMATELY DISCHARGE ACROSS KEATS AVENUE TO MNDOT'S STORMWATER POND.

SPECIAL/IMPAIRED WATER CONSIDERATIONS
THERE ARE THREE IMPAIRED LAKES LOCATED WITHIN ONE MILE OF THE SITE. MARKGRAFS LAKE IS LOCATED APPROXIMATELY 3,200 FEET SOUTHWEST OF THE SITE. GOOSE LAKE IS LOCATED APPROXIMATELY 2,700 FEET NORTH OF THE SITE. KRAMER LAKE IS LOCATED APPROXIMATELY 2,000 FEET EAST OF THE SITE.

ADDITIONAL BMP'S REQUIRED INCLUDE: SOIL STABILIZATION WITHIN 7 DAYS AFTER CONSTRUCTION ACTIVITY HAS CEASED, TEMPORARY SEDIMENT BASIN FOR COMMON DRAINAGE AREAS OF 5 ACRES OR MORE, A REQUIRED WATER QUALITY VOLUME OF 1 INCH OF RUNOFF FROM NEW IMPERVIOUS SURFACES.

STORMWATER MANAGEMENT PLAN
PER VALLEY BRANCH WATERSHED DISTRICT, THE REQUIRED WATER QUALITY VOLUME FOR THE SITE MUST BE GREATER THAN OR EQUAL TO 1.1 INCHES OF RUNOFF OVER THE NEW IMPERVIOUS AREA OF SITE. THIS EQUATES TO 0.141 ACRE FEET.

THE PROPOSED INFILTRATION BASIN HAS A VOLUME BELOW THE OUTLET OF 0.147 ACRE FEET. THE ANTICIPATED INFILTRATION RATE OF THE BASIN IS 0.25 INCHES PER HOUR. THE MAXIMUM DEPTH OF THE BASIN BELOW THE OUTLET IS PROPOSED TO BE 12 INCHES, WHICH WILL ALLOW THE BASIN TO DRAWDOWN WITHIN 48 HOURS.

A TEMPORARY SEDIMENT BASIN IS PROPOSED IN THE SOUTHERN PORTION OF THE SITE AND WILL COLLECT RUNOFF FROM THE DISTURBED AREA TO THE SOUTH OF THE SITE. WHILE THIS BASIN IS NOT REQUIRED, IT IS BEING CONSTRUCTED FOR FUTURE USE FOR THE FUTURE DEVELOPED LAND TO THE SOUTHWEST.

KWIK TRIP
STORES

KWIK STAR
STORES

KWIK TRIP, Inc.
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1626 OAK STREET
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PH. (608) 781-8988
FAX (608) 781-8960

Carlson McCain
ENVIRONMENTAL - ENGINEERING - SURVEYING
248 Apollo Dr, Suite 100, Lino Lakes, MN 55114
Phone: 763-489-7900 Fax: 763-489-7959

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota

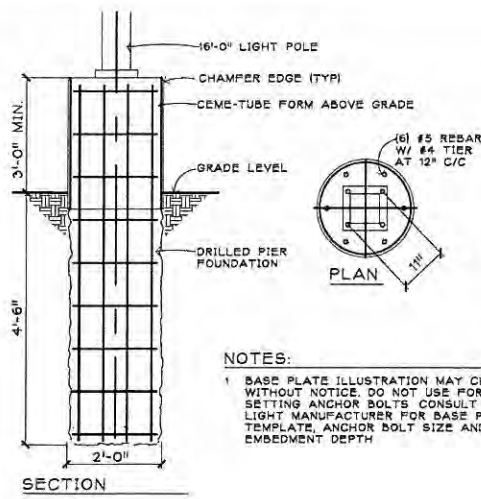
Name: Joseph T. Radach, P.E.
Signature: _____
Date: 05/09/14 License #: 45889

SWPPP

CONVENIENCE STORE #248
WITH DOUBLE BAY CARWASH
HUDSON BLVD & KEATS AVENUE NORTH
LAKE ELMO, MINNESOTA

| # | DATE | DESCRIPTION |
|---|----------|--------------------|
| 1 | 08/27/14 | Per City comments. |
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| | | |
| | | |

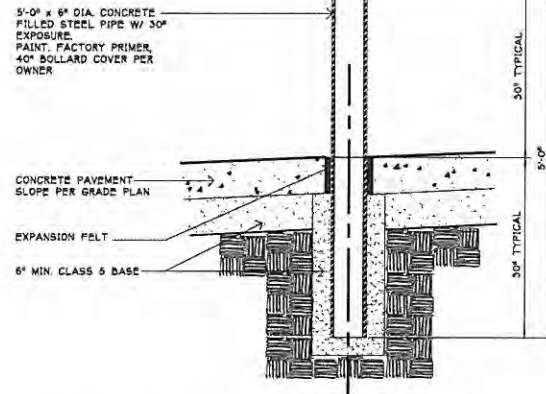
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DATE: 2014-05-09
SHEET: SP5



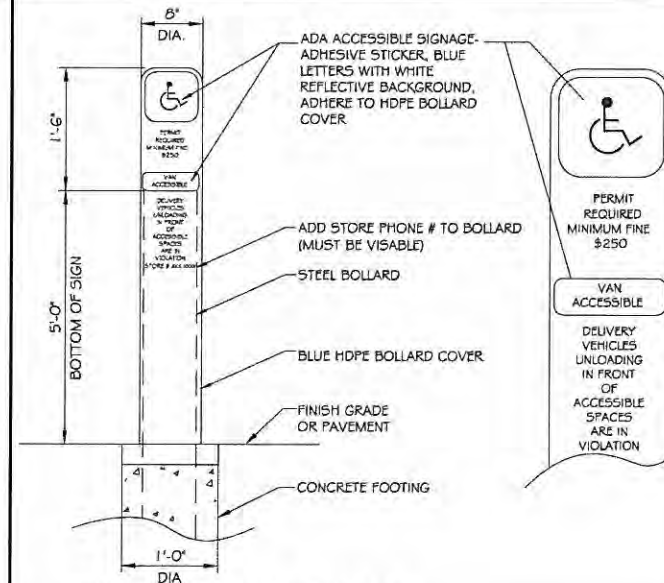
NOTES:
 1. BASE PLATE ILLUSTRATION MAY CHANGE WITHOUT NOTICE. DO NOT USE FOR SETTING ANCHOR BOLTS. CONSULT AREA LIGHT MANUFACTURER FOR BASE PLATE TEMPLATE, ANCHOR BOLT SIZE AND EMBEDMENT DEPTH.

5 | ROUND AREALIGHT FOUNDATION
 SP6 | NO SCALE

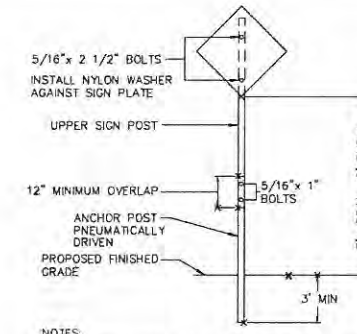
NOTE: SEE ARCHITECTURAL PLANS FOR REMOVABLE BOLLARD LOCATION AND DETAIL.



7 | 6" PIPE BOLLARD - 5'
 SP6 | NO SCALE

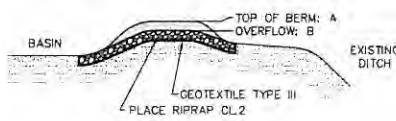
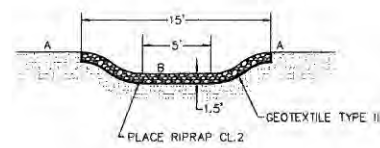


10 | ADA BOLLARD SIGNAGE
 SP8 | NO SCALE



NOTES:
 1. MATERIALS AND INSTALLATION SHALL BE IN ACCORDANCE WITH CITY SPECIFICATIONS FOR MNDOT 2564 TYPE C TRAFFIC SIGNS.
 2. SIGN PANELS-REFLECTIVE SHEETING, VIP DIAMOND GRADE
 3. BOLTS SHALL BE TAMPER PROOF.
 4. FLANGED CHANNEL SIGN POSTS SHALL BE PAINTED GREEN, 3 LB./L.F. AND 7' LONG.
 5. SIGNS INSTALLED IN CONCRETE SHALL HAVE AN APPROVED BREAK-AWAY DEVICE.
 6. EDGE OF SIGN SHALL BE MINIMUM 18" FROM BACK OF CURB.

2 | TRAFFIC SIGN INSTALLATION
 SP8 | NO SCALE

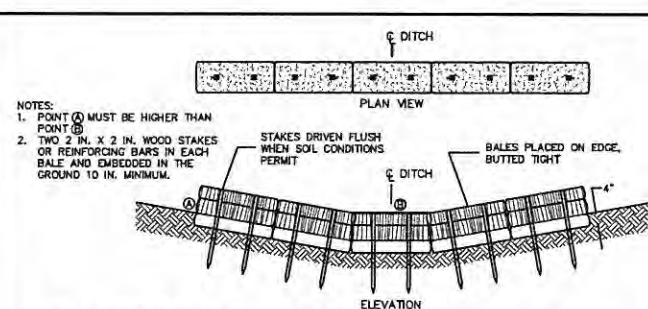


| | A | B |
|-----------------|-------|-------|
| BASIN | 955.0 | 954.5 |
| TEMP. SED. POND | 959.0 | 958.5 |

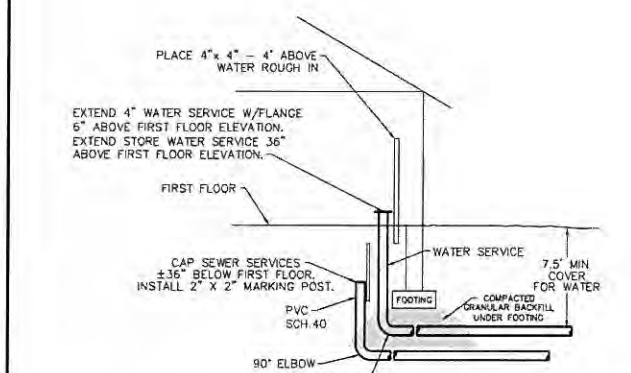
5 | RIPRAP OVERFLOW
 SP7 | NO SCALE

- TURF ESTABLISHMENT SHALL APPLY TO ALL DISTURBED AREAS AND SHALL BE ACCORDING TO MNDOT STANDARD SPECIFICATIONS FOR CONSTRUCTION (2005) EXCEPT AS MODIFIED BELOW.
- TURF ESTABLISHMENT SHALL OCCUR AS SOON AS POSSIBLE BUT IN NO CASE MORE THAN 7 DAYS.
- SEED: MNDOT MIXTURE 250 AT 70 POUNDS PER ACRE.
- DORMANT SEED: SHALL BE APPLIED AT TWICE THE NORMAL RATE AFTER NOVEMBER 1ST.
- MULCH: TYPE 1 AT 2 TONS PER ACRE (DISK ANCHORED).
- FERTILIZER: TYPE 24-12-24 AT 300 POUNDS PER ACRE.

8 | TURF ESTABLISHMENT
 SP6 | NO SCALE



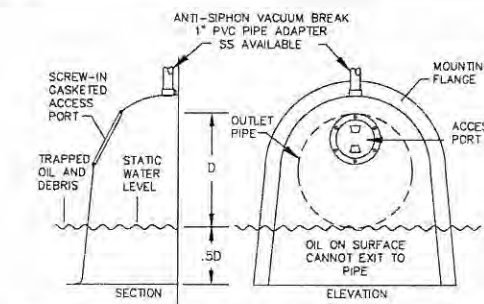
6 | HAY BALE DITCH CHECK
 SP7 | NO SCALE



WATER NOTES: 4" DOMESTIC SERVICE FOR CARWASH SHALL BE EXTENDED VERTICALLY WITH FLANGE SET 6" ABOVE FIRST FLOOR ELEVATION. 2" DOMESTIC SERVICE FOR STORE BUILDING SHALL BE EXTENDED TO 36" ABOVE FINISHED GRADE AND TAPED OFF. ALL SERVICES SHALL BE MARKED WITH A 4" X 4" POST, 4" ABOVE GRADE.

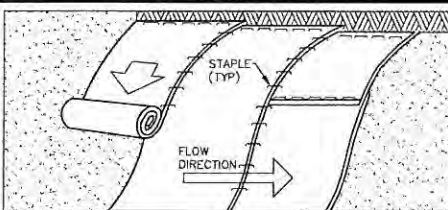
SEWER NOTES: SANITARY SEWER SERVICES FOR CARWASH AND STORE BUILDING SHALL BE EXTENDED VERTICALLY TO APPROXIMATELY 36" BELOW FIRST FLOOR ELEVATION AND CAPPED. ALL SERVICES SHALL BE MARKED WITH A 2" X 2" POST.

3 | SANITARY & WATER SERVICE INSTALLATION
 SP7 | NO SCALE



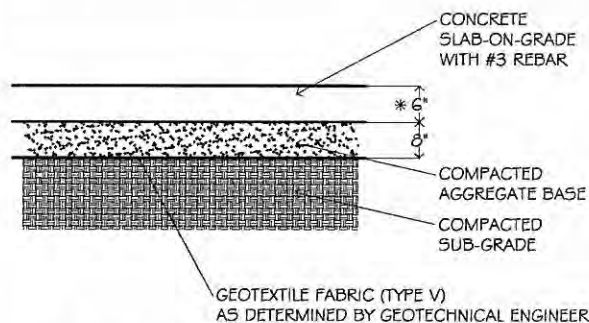
NOTE: WEB DESIGN INFO AT www.bestmp.com

4 | "SNOUT" OIL-WATER-DEBRIS SEPARATOR
 SP7 | NO SCALE



- NOTES:
- PREPARE SOIL BEFORE INSTALLING BLANKETS, INCLUDING APPLICATION OF LIME, FERTILIZER AND/OR SEED.
 - BEGIN AT THE TOP OF THE SLOPE (OR CHANNEL) BY ANCHORING THE BLANKET IN A 6" DEEP X 6" WIDE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING.
 - ROLL THE BLANKETS DOWN (STARTING DOWNSTREAM PROCEEDING UPSTREAM) HORIZONTALLY ACROSS THE SLOPE.
 - THE EDGES OF PARALLEL BLANKETS MUST BE STAPLED WITH A MINIMUM 4" OVERLAP.
 - WHEN BLANKETS MUST BE SPICED DOWN THE SLOPE, PLACE BLANKETS END OVER END (SHINGLE STYLE) WITH MINIMUM 6" OVERLAP. USE A DOUBLE ROW OF STAGGERED STAPLES 4" APART TO SECURE BLANKETS.
 - IN HIGH FLOW CHANNEL APPLICATIONS, A STAPLE CHECK SLOT IS RECOMMENDED AT 30 TO 40 FOOT INTERVALS. USE A ROW OF STAPLES 4" APART OVER ENTIRE WIDTH OF THE CHANNEL. PLACE A SECOND ROW 4" BELOW THE FIRST ROW IN A STAGGERED PATTERN.
 - THE TERMINAL ENDS OF THE BLANKETS MUST BE ANCHORED IN A 6" DEEP X 6" WIDE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING.

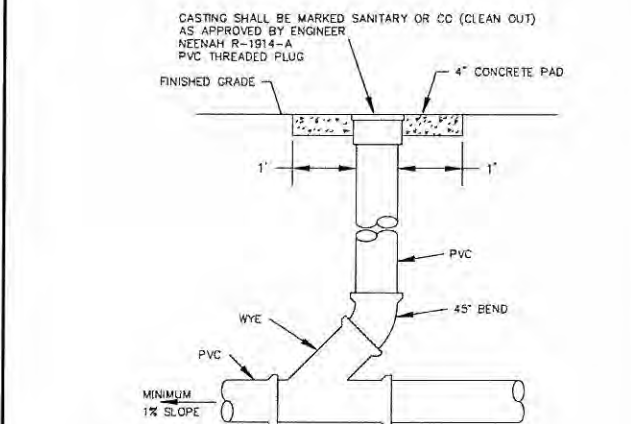
4 | EROSION CONTROL BLANKET
 SP6 | NO SCALE



* HEAVY DUTY INCREASE TO 8" THICK AS NOTED ON PLANS (I.E. OVER TANKS & AT ENTRANCES)

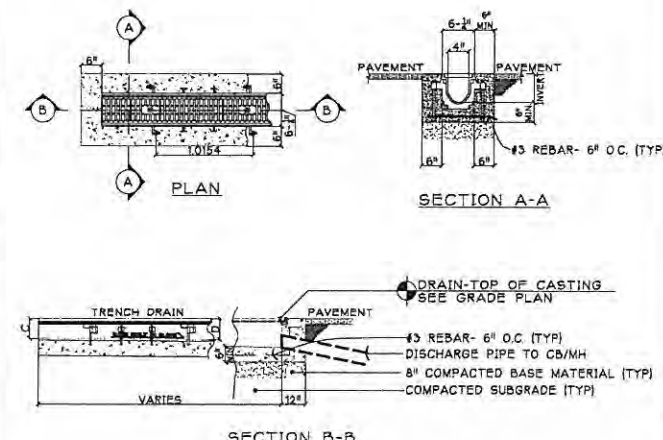
NOTE: PAVEMENT SECTION TO BE VERIFIED W/ GEOTECHNICAL REPORT.

7 | 6" CONCRETE PAVEMENT SECTION (LIGHT DUTY)
 SP8 | NO SCALE



NOTES:
 - ALL JOINTS SHALL BE SOLVENT WELD EXCEPT AS NOTED
 - CLEAN OUTS ARE REQUIRED AT 100' SPACING AND AT CHANGE OF DIRECTION.

1 | SERVICE CLEAN OUT
 SP7 | NO SCALE



3 | Z 886 TRENCH DRAIN DETAIL
 SP8 | NO SCALE

KWIK TRIP STORES

KWIK STAR STORES

KWIK TRIP, Inc.
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 ENVIRONMENTAL - ENGINEERING - SURVEYING
 248 Apollo Dr, Suite 100, Uno Lakes, MN 55014
 Phone: 763-489-7900 Fax: 763-489-7959

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Name: Joseph T. Radach, P.E.
 Signature: *Joseph T. Radach*
 Date: 05/09/14 License #: 45889

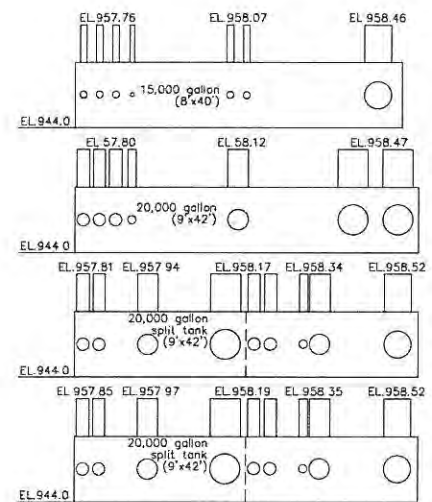
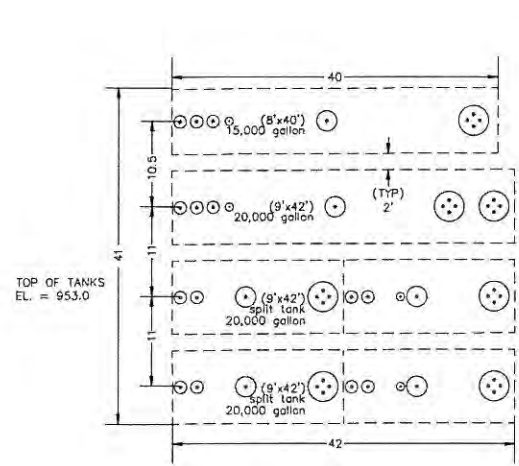
DETAILS

CONVENIENCE STORE #248 WITH DOUBLE BAY CARWASH

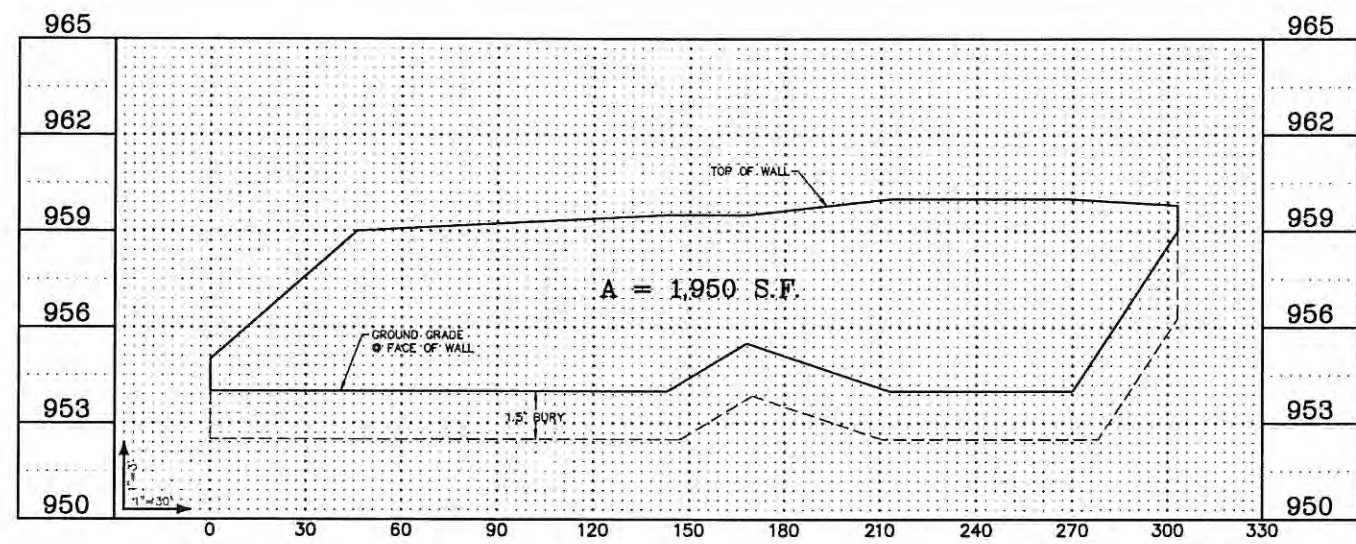
HUDSON BLVD & KEATS AVENUE NORTH LAKE ELMO, MINNESOTA

| # | DATE | DESCRIPTION |
|---|----------|--------------------|
| 1 | 06/27/14 | Per City comments. |

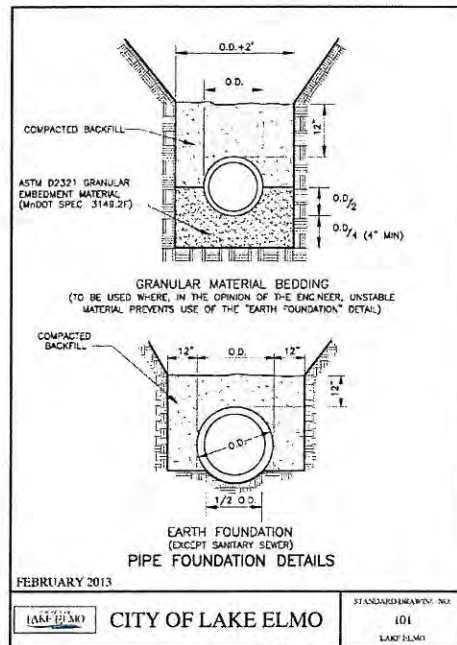
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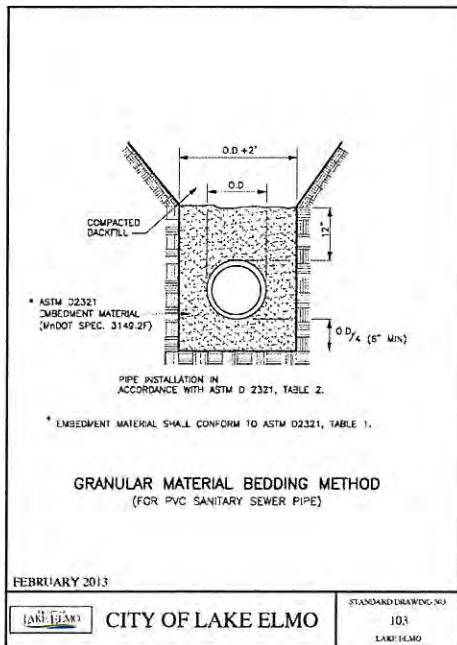
1 TANK COMPLEX DIAGRAM AND ELEVATIONS
SP8 NO SCALE



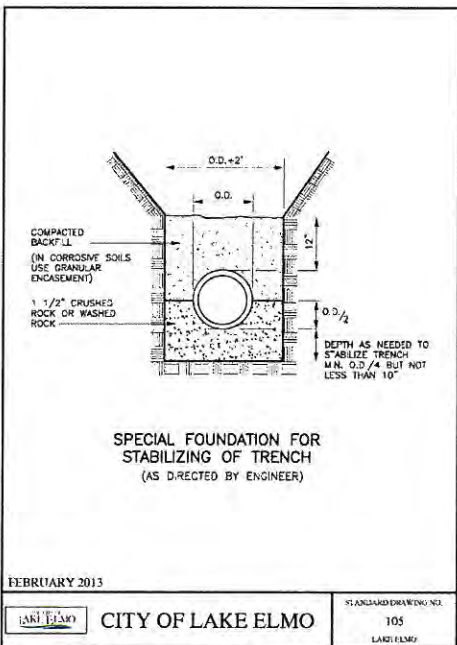
9 RETAINING WALL
SP8 HORIZ: 1"=30'; VERT: 1"=3'



FEBRUARY 2013
CITY OF LAKE ELMO
101
LAKELAND



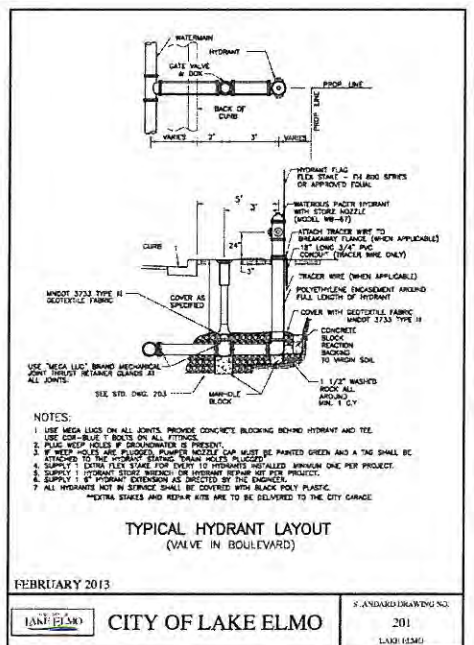
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103
LAKELAND



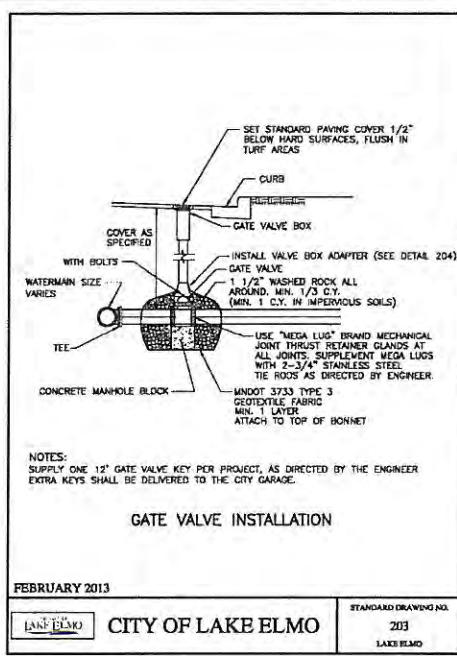
FEBRUARY 2013
CITY OF LAKE ELMO
105
LAKELAND

- ALL WATERMAIN AND ACCESSORIES MUST BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF LAKE ELMO STANDARD SPECIFICATIONS AND DETAILS.
- MANIPULATION OF EXISTING VALVES SHALL BE PERFORMED ONLY BY CITY PERSONNEL.
- WATERMAIN SHALL BE DUCTILE IRON PIPE, CLASS-52.
- ALL FITTINGS SHALL COMPLY WITH CEAM SPEC. 2811.2.1.1. ALL FITTINGS SHALL BE DUCTILE IRON PIPE WITH POCKET/FLANGE ENCASUREMENT. ALL CONNECTIONS SHALL BE INSTALLED UTILIZING COR-BLUE NUTS & BOLTS.
- USE GATE VALVES FOR ALL APPLICATIONS UP THROUGH 12 INCHES.
- GATE VALVES SHALL BE RESILIENT WEDGE AMERICAN FLOW CONTROL SERIES 2500 OR APPROVED EQUAL. GATE VALVES MUST COMPLY WITH CEAM SPEC. 2811.2.0.2.
- USE BUTTERFLY VALVES FOR ALL APPLICATIONS GREATER THAN 12 INCHES.
- BUTTERFLY VALVES SHALL BE KULLER LINGSCAL IR. OR APPROVED EQUAL. BUTTERFLY VALVES SHALL COMPLY WITH CEAM SPEC. 2811.2.0.3.
- BOLTS AND NUTS ON ALL VALVES AND HYDRANTS SHALL BE STAINLESS STEEL.
- ALL HYDRANTS SHALL BE INSTALLED 5.0 FEET BACK OF CURB.
- HYDRANTS SHALL BE WATEROUS "PACER" MODEL W3.67 OR APPROVED EQUAL, FITTED WITH FH 800 SERIES (LEX STAKE AND PAINTED RED).
- HYDRANTS SHALL HAVE TWO OUTLET NOZZLES FOR 2-1/2" (O.D.) HOSE CONNECTIONS AND ONE 4" STORZ NOZZLE.
- THE CURB STOP SERVICE ASSEMBLY SHALL HAVE A MINIMUM 1-FIT ADJUSTMENT RANGE AND SHALL EXTEND 6 INCHES ABOVE FINISHED GRADE FULLY EXTENDED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING WATER TO HOMES AND BUSINESSES WHOSE WATER SUPPLY IS INTERRUPTED DURING THE COURSE OF THE PROJECT.

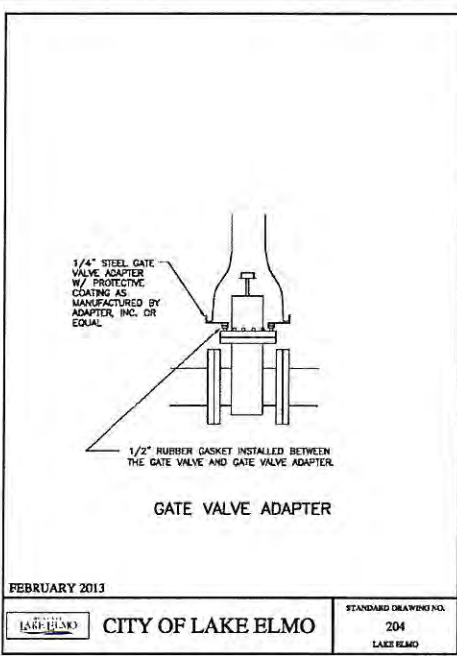
FEBRUARY 2013
CITY OF LAKE ELMO
200A
LAKELAND



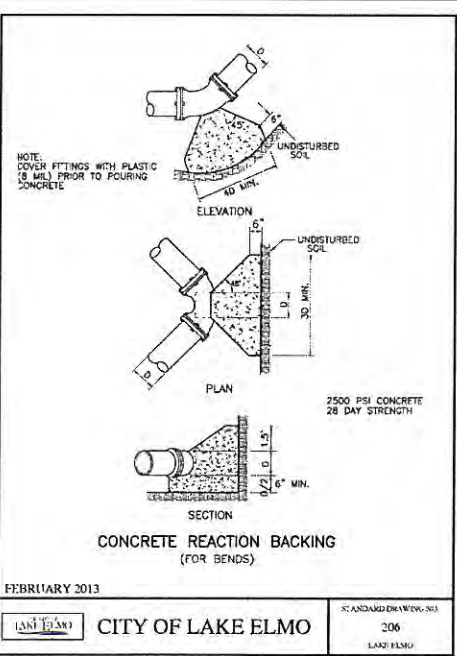
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CITY OF LAKE ELMO
201
LAKELAND



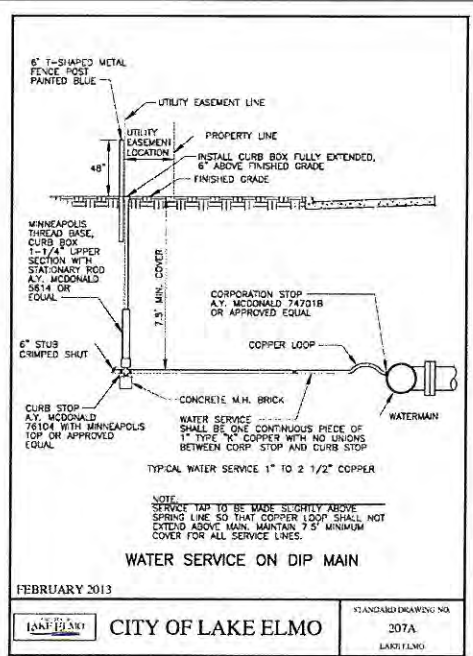
FEBRUARY 2013
CITY OF LAKE ELMO
203
LAKELAND



FEBRUARY 2013
CITY OF LAKE ELMO
204
LAKELAND



FEBRUARY 2013
CITY OF LAKE ELMO
206
LAKELAND



FEBRUARY 2013
CITY OF LAKE ELMO
207A
LAKELAND

- ALL SANITARY SEWER AND ACCESSORIES MUST BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF LAKE ELMO STANDARD SPECIFICATIONS AND DETAILS.
- ALL SANITARY SEWER PVC PIPE SHALL BE INSTALLED ACCORDING TO CITY STANDARD DRAWING 103 "GRANULAR MATERIAL BEDDING METHOD (FOR PVC SANITARY SEWER PIPE)".
- UNLESS NOTED OTHERWISE, ALL SMOOTH WALLED SANITARY SEWER PVC PIPE AND FITTINGS SHALL BE SDR 35 WITH ELASTOMERIC GASKETED JOINTS.
- ALL SANITARY SEWER SERVICES SHALL BE 4-INCH PVC, SCH. 40 OR SDR 26 PIPE.
- SMOOTH WALLED PVC PIPE AND FITTINGS SHALL CONFORM WITH THE REQUIREMENTS OF ASTM D-3034 FOR THE SIZE, STANDARD DIMENSION RATIO (SDR), AND STRENGTH REQUIREMENTS INDICATED ON THE PLANS, SPECIFICATIONS, AND SPECIAL PROVISIONS.
- REINFORCED CONCRETE PIPE AND FITTINGS SHALL CONFORM WITH THE REQUIREMENTS OF MHDOT SPEC. 3226 (REINFORCED CONCRETE PIPE) FOR THE TYPE, SIZE, AND STRENGTH CLASS SPECIFIED HEREIN.
- JOINTS OF MANHOLE RISER SECTIONS SHALL BE TONGUE AND GROOVE WITH RUBBER "O" RING JOINTS PROVIDED ON ALL SANITARY SEWER MANHOLES.
- SANITARY SEWER INLET AND OUTLET PIPES SHALL BE JOINED TO THE MANHOLE WITH A GASKETED, FLEXIBLE, WATER-TIGHT CONNECTION TO ALLOW DIFFERENTIAL SETTLEMENT OF THE PIPE AND MANHOLE TO TAKE PLACE.
- A 1'-0" TO 1'-4" MANHOLE SECTION SHALL BE INSTALLED UNDER THE CONE SECTION TO ALLOW FOR HEIGHT ADJUSTMENT WHENEVER POSSIBLE.
- ALL SERVICE LINE STUBS MUST HAVE A 3/4" HARDWOOD MARKER WITH METAL STRIKE RUNNING FROM THE END OF PIPE TO FINISHED GRADE ELEVATION.
- NO WORKING CONNECTION TO AN EXISTING SANITARY SEWER STUB OR MANHOLE, DIRT AND DEBRIS SHALL BE PREVENTED FROM ENTERING THE EXISTING SEWER BY IMMEDIATELY INSTALLING WATER-TIGHT PLUGS AS NEEDED IN THE EXISTING MANHOLE.

FEBRUARY 2013
CITY OF LAKE ELMO
300A
LAKELAND

KWIK TRIP STORES
KWIK STAR STORES

KWIK TRIP, Inc.
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1626 OAK STREET
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Phone: 763-489-7900 Fax: 763-489-7959

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Name: Joseph T. Radach, P.E.
Signature: _____
Date: 05/09/14 License #: 45689

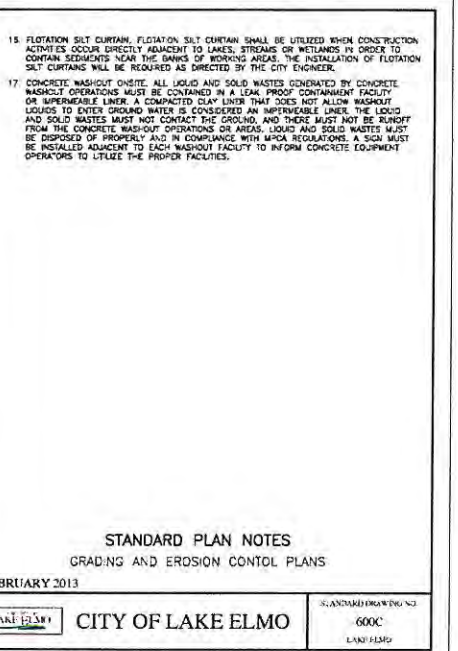
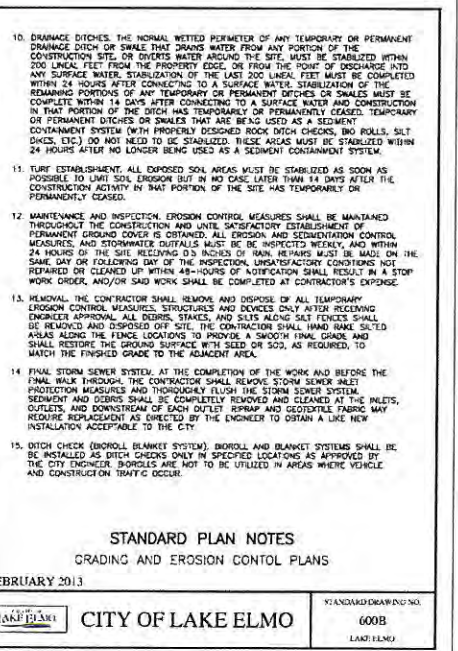
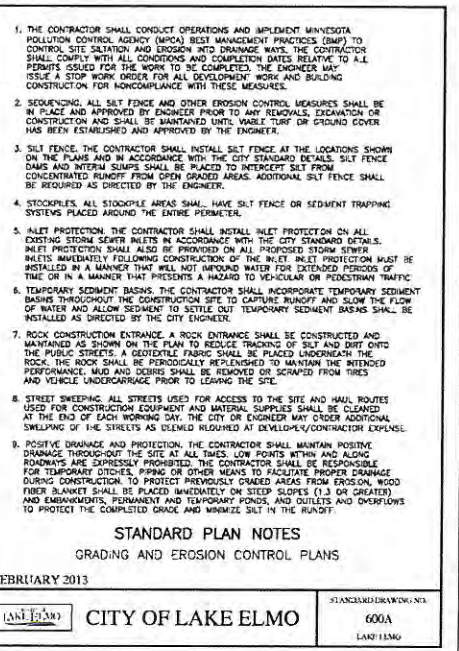
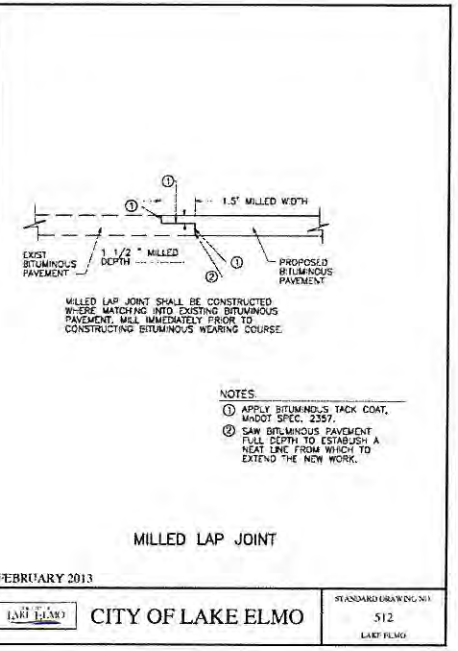
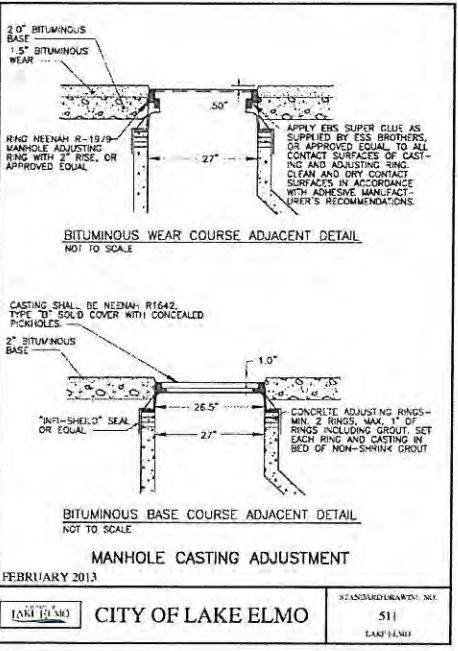
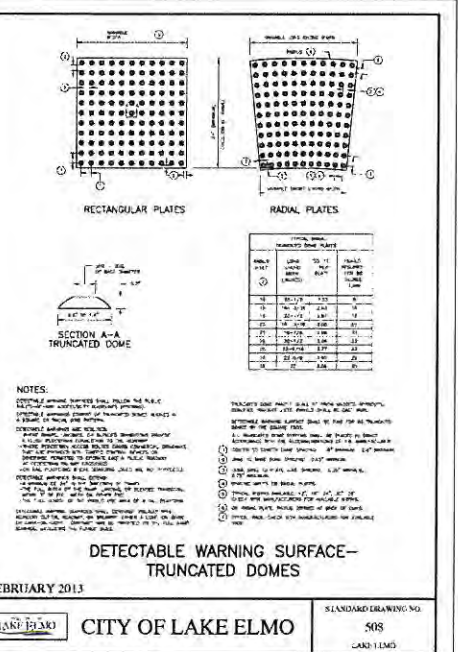
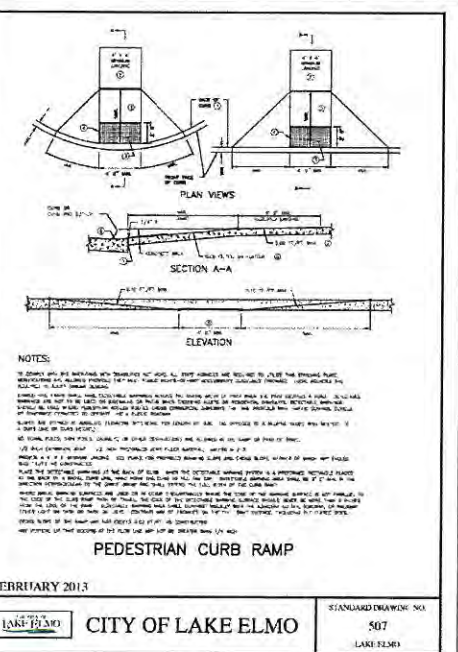
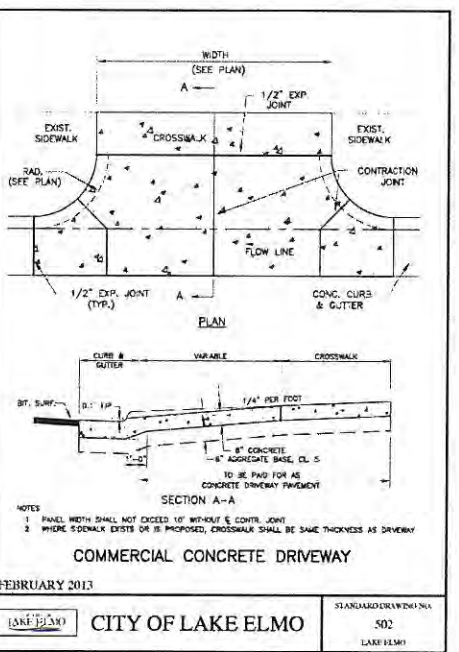
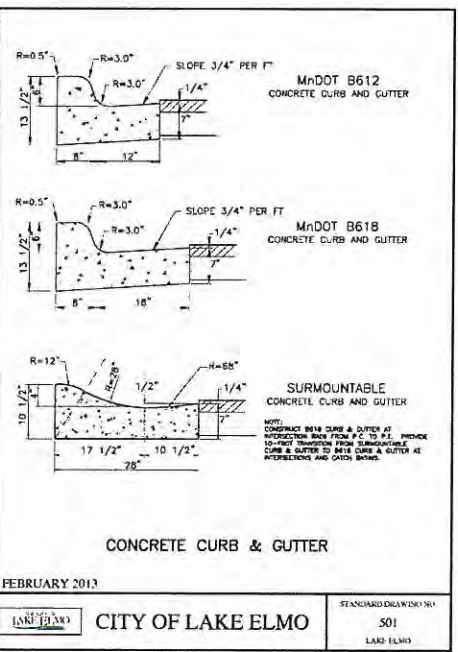
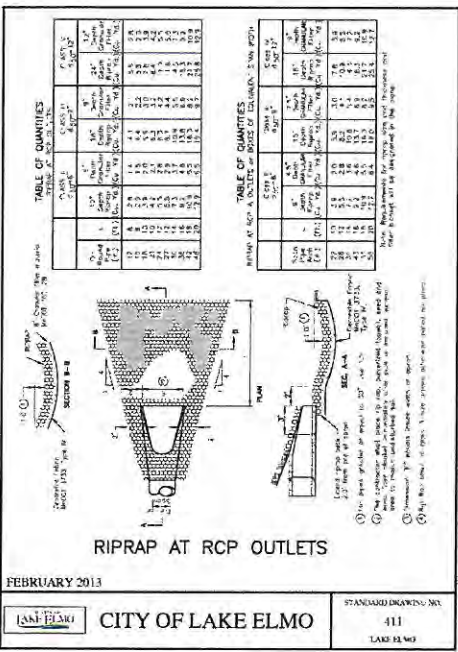
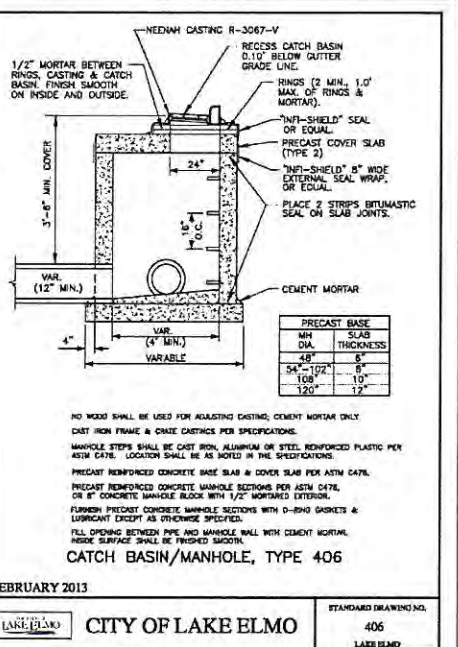
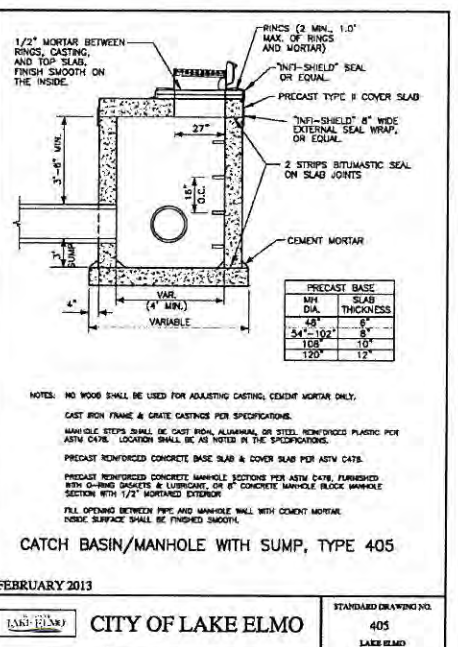
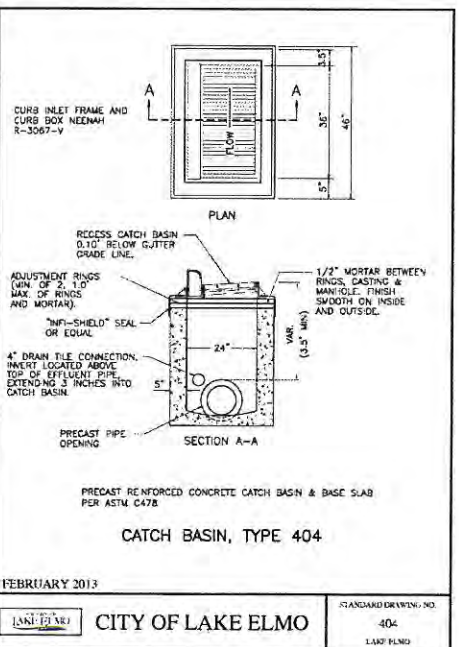
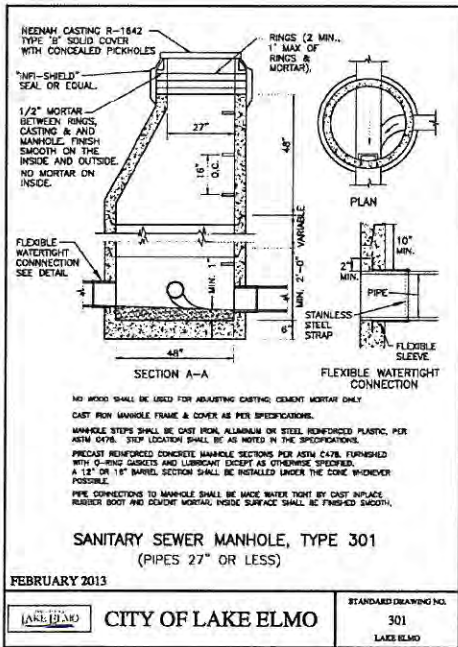
DETAILS

CONVENIENCE STORE #248 WITH DOUBLE BAY CARWASH

HUDSON BLVD & KEATS AVENUE NORTH LAKE ELMO, MINNESOTA

| # | DATE | DESCRIPTION |
|---|----------|-------------------|
| 1 | 08/27/14 | Per City comments |

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SCALE: GRAPHIC
PROJ. NO. 4982-00
DATE: 2014-05-09
SHEET: SP7



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KWIK STAR STORES

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I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota

Name: Joseph T. Radach, P.E.
Signature: [Signature]
Date: 05/09/14 License #: 45889

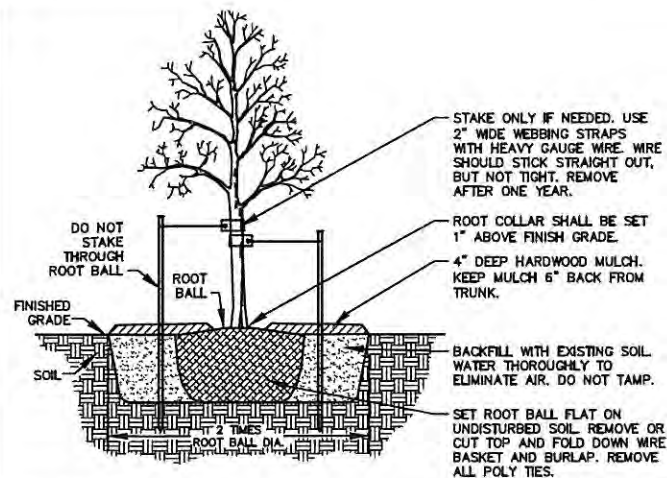
DETAILS

CONVENIENCE STORE #248
WITH DOUBLE BAY CARWASH

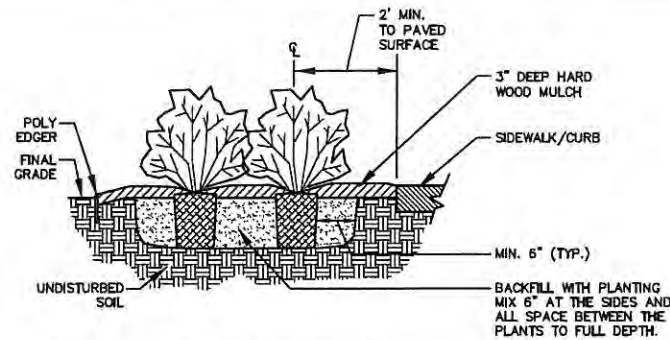
HUDSON BLVD & KEATS AVENUE NORTH
LAKE ELMO, MINNESOTA

| DATE | DESCRIPTION |
|----------|-------------------|
| 09/27/14 | Per City comments |

DRAWN BY: JTR/KMM
SCALE: GRAPHIC
PROJ. NO.: 4982-00
DATE: 2014-05-09
SHEET: SP8



A DECIDUOUS TREE PLANTING DETAIL
NOT TO SCALE



B SHRUB & CONTAINER PLANTING DETAIL
NOT TO SCALE

PLANTING NOTES

1. ALL PLANTS MUST BE HEALTHY, VIGOROUS MATERIAL, FREE OF PESTS AND DISEASE AND BE CONTAINER GROWN OR BALLED AND BURLAPPED AS INDICATED IN THE LANDSCAPE LEGEND.
2. ALL TREES MUST BE STRAIGHT TRUNKED AND FULL HEADED AND MEET ALL REQUIREMENTS SPECIFIED.
3. THE LANDSCAPE ARCHITECT RESERVES THE RIGHT TO REJECT ANY PLANTS WHICH ARE DEEMED UNSATISFACTORY BEFORE, DURING, OR AFTER INSTALLATION.
4. NO SUBSTITUTIONS OF PLANT MATERIAL SHALL BE ACCEPTED UNLESS APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT.
5. ALL PLANTING STOCK SHALL CONFORM TO THE "AMERICAN STANDARD FOR NURSERY STOCK," ANSI-Z60, LATEST EDITION, OF THE AMERICAN ASSOCIATION OF NURSERMEN, INC. AND SHALL CONSTITUTE MINIMUM QUALITY REQUIREMENTS FOR PLANT MATERIALS.
6. EXISTING TREES AND SHRUBS TO REMAIN SHALL BE PROTECTED TO THE DRIP LINE FROM ALL CONSTRUCTION TRAFFIC, STORAGE OF MATERIALS ETC. WITH 4" HT. ORANGE PLASTIC SAFETY FENCING ADEQUATELY SUPPORTED BY STEEL FENCE POSTS 6" O.C. MAXIMUM SPACING.
7. ALL PLANT MATERIAL QUANTITIES, SHAPES OF BEDS AND LOCATIONS SHOWN ARE APPROXIMATE. CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETE COVERAGE OF ALL PLANTING BEDS AT SPACING SHOWN AND ADJUSTED TO CONFORM TO THE EXACT CONDITIONS OF THE SITE. THE LANDSCAPE ARCHITECT SHALL APPROVE THE STAKING LOCATION OF ALL PLANT MATERIALS PRIOR TO INSTALLATION.
8. ALL TREES MUST BE PLANTED, MULCHED, AND STAKED AS SHOWN IN THE DETAILS.
9. ALL PLANTING AREAS MUST BE COMPLETELY MULCHED AS SPECIFIED.
10. MULCH: SHREDDED HARDWOOD MULCH, CLEAN AND FREE OF NOXIOUS WEEDS OR OTHER DELETERIOUS MATERIAL, IN ALL MASS PLANTING BEDS AND FOR TREES, UNLESS INDICATED AS ROCK MULCH ON DRAWINGS. SUBMIT SAMPLE TO LANDSCAPE ARCHITECT PRIOR TO DELIVERY ON-SITE FOR APPROVAL. DELIVER MULCH ON DAY OF INSTALLATION. USE 4" FOR TREES, SHRUB BEDS, AND 3" FOR PERENNIAL/GROUND COVER BEDS, UNLESS OTHERWISE DIRECTED.
11. THE PLAN TAKES PRECEDENCE OVER THE LANDSCAPE LEGEND IF DISCREPANCIES EXIST. THE SPECIFICATIONS TAKE PRECEDENCE OVER THE PLANTING NOTES AND GENERAL NOTES.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MULCHES AND PLANTING SOIL QUANTITIES TO COMPLETE THE WORK SHOWN ON THE PLAN. VERIFY ALL QUANTITIES SHOWN ON THE LANDSCAPE LEGEND.
13. LONG-TERM STORAGE OF MATERIALS OR SUPPLIES ON-SITE WILL NOT BE ALLOWED.
14. THE CONTRACTOR SHALL KEEP PAVEMENTS, PLANTERS AND BUILDINGS CLEAN AND UNSTAINED. ALL PEDESTRIAN AND VEHICLE ACCESS TO BE MAINTAINED THROUGHOUT CONSTRUCTION PERIOD. ALL WASTES SHALL BE PROMPTLY REMOVED FROM THE SITE. ANY PLANT STOCK NOT PLANTED ON DAY OF DELIVERY SHALL BE HELED IN AND WATERED UNTIL INSTALLATION. PLANTS NOT MAINTAINED IN THIS MANNER WILL BE REJECTED. ANY DAMAGE TO EXISTING FACILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE CODES, REGULATIONS, AND PERMITS GOVERNING THE WORK.
16. LOCATE AND VERIFY ALL UTILITIES, INCLUDING IRRIGATION LINES, WITH THE OWNER FOR PROPRIETARY UTILITIES AND GORNER STATE ONE CALL AT 454-0002 (TWIN CITIES METRO AREA) OR 800-252-1166 (GREATER MINNESOTA) 48 HOURS BEFORE DIGGING. CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND REPAIR OF ANY DAMAGES TO SAME. NOTIFY THE LANDSCAPE ARCHITECT OF ANY CONFLICTS TO FACILITATE PLANT RELOCATION.
17. USE ANTI-DESICCANT (W/PRIFUR OR APPROVED EQUAL) ON DECIDUOUS PLANTS MOVED IN LEAF AND FOR EVERGREENS MOVED ANYTIME. APPLY AS PER MANUFACTURER'S INSTRUCTION. ALL EVERGREENS SHALL BE SPRAYED IN THE LATE FALL FOR WINTER PROTECTION THROUGHOUT THE ENTIRE BED BY ROTOTILLING IT INTO THE TOP 12" OF SOIL.
18. PLANTING SOIL FOR TREES, SHRUBS AND GROUND COVERS: FERTILE FRABLE LOAM CONTAINING A LIBERAL AMOUNT OF HUMUS AND CAPABLE OF SUSTAINING VIGOROUS PLANT GROWTH. IT SHALL COMPLY WITH MNDOT SPECIFICATION 3877 TYPE B SELECT TOPSOIL MIXTURE SHALL BE FREE FROM HARDPACK SUBSOIL, STONES, CHEMICALS, NOXIOUS WEEDS, ETC. SOIL MIXTURE SHALL HAVE A PH BETWEEN 6.1 AND 7.5 AND 10-10-10 FERTILIZER AT THE RATE OF 3 POUNDS PER CUBIC YARD. IN PLANTING BEDS INCORPORATE THIS MIXTURE THROUGHOUT THE ENTIRE BED BY ROTOTILLING IT INTO THE TOP 12" OF SOIL.
19. ALL PLANTS SHALL BE GUARANTEED FOR TWO COMPLETE GROWING SEASONS (APRIL 1 - NOVEMBER 1), UNLESS OTHERWISE SPECIFIED. THE GUARANTEE SHALL COVER THE FULL COST OF REPLACEMENT INCLUDING LABOR AND PLANTS.
20. CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 3 DAYS PRIOR TO PLANNED DELIVERY. THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 24 HOURS IN ADVANCE OF BEGINNING PLANT INSTALLATION.
21. SEASONS/TIME OF PLANTING: NOTE: THE CONTRACTOR MAY ELECT TO PLANT IN OFF-SEASONS ENTIRELY AT HIS/HER RISK.
DECIDUOUS POTTED PLANTS:
APRIL 1-JUNE 1; AUG. 21-NOV. 1
EVERGREEN POTTED PLANTS:
APRIL 1-JUNE 1; AUG. 21-NOV. 1
EVERGREEN POTTED PLANTS:
APRIL 1-JUNE 1; AUG. 21-NOV. 1
EVERGREEN POTTED PLANTS:
APRIL 1-MAY 15; AUG. 21-SEPT. 15
22. MAINTENANCE SHALL BEGIN IMMEDIATELY AFTER EACH PORTION OF THE WORK IS IN PLACE. PLANT MATERIAL SHALL BE PROTECTED AND MAINTAINED UNTIL THE INSTALLATION OF THE PLANTS IS COMPLETE. INSPECTION HAS BEEN MADE, AND PLANTINGS ARE ACCEPTED EXCLUSIVE OF THE GUARANTEE. MAINTENANCE SHALL INCLUDE WATERING, CULTIVATING, MULCHING, REMOVAL OF DEAD MATERIALS, RE-SETTING PLANTS TO PROPER GRADE AND KEEPING PLANTS IN A PLUMB POSITION. AFTER ACCEPTANCE, THE OWNER SHALL ASSUME MAINTENANCE RESPONSIBILITIES. HOWEVER, THE CONTRACTOR SHALL CONTINUE TO BE RESPONSIBLE FOR KEEPING THE TREES PLUMB THROUGHOUT THE GUARANTEE PERIOD.
23. ANY PLANT MATERIAL WHICH DIES, TURNS BROWN, OR DEFOLIATES (PRIOR TO TOTAL ACCEPTANCE OF THE WORK) SHALL BE PROMPTLY REMOVED FROM THE SITE AND REPLACED WITH MATERIAL OF THE SAME SPECIES, QUANTITY, AND SIZE AND MEETING ALL LANDSCAPE LEGEND SPECIFICATIONS.
24. WATERING: MAINTAIN A WATERING SCHEDULE WHICH WILL THOROUGHLY WATER ALL PLANTS ONCE A WEEK, IN EXTREMELY HOT, DRY WEATHER. WATER MORE OFTEN AS REQUIRED BY INDICATIONS OF HEAT STRESS SUCH AS WILTING LEAVES. CHECK MOISTURE UNDER MULCH PRIOR TO WATERING TO DETERMINE NEED. CONTRACTOR SHALL MAKE THE NECESSARY ARRANGEMENTS FOR WATER.
25. CONTRACTOR SHALL REQUEST IN WRITING, A FINAL ACCEPTANCE INSPECTION.
26. PROPOSED IRRIGATION SYSTEM TO BE DESIGN BUILD.
27. ALL DISTURBED AREAS TO BE SEEDDED, ADEQUATE TO RECEIVE 4" TOP SOIL, SEED, MULCH, AND WATER UNTIL A HEALTHY STAND OF GRASS IS OBTAINED.

| LANDSCAPE LEGEND | | | | | | |
|-------------------------|------------------------------------|-------------------------|------|-------|------|---------|
| KEY | BOTANICAL NAME | COMMON NAME | SIZE | ROOT | QTY. | REMARKS |
| DECIDUOUS TREES | | | | | | |
| | Acer x freemanii 'Jeffersred' | AUTUMN BLAZE MAPLE | 2.5" | BB | 7 | |
| | Populus tremuloides | QUAKING ASPEN | 2.5" | BB | 6 | |
| | Tilia cordata 'Greenspire' | GREENSPIRE LINDEN | 2.5" | BB | 4 | |
| CONIFEROUS TREES | | | | | | |
| | Picea glauca densata | BLACK HILLS SPRUCE | 6' | HT. | 5 | |
| | Picea pungens | COLORADO SPRUCE | 6' | HT. | 3 | |
| SHRUBS | | | | | | |
| JC | Juniperus chinensis 'Sea Green' | SEA GREEN JUNIPER | #5 | CONT. | 5 | |
| JM | Juniperus x media 'Daub's Frosted' | DAUB'S FROSTED JUNIPER | #5 | CONT. | 15 | |
| PP | Picea pungens 'Globosa' | DWARF BLUE GLOBE SPRUCE | #5 | CONT. | 15 | |
| PO | Physocarpus opulifolius 'Nanus' | DWARF NINEBARK | #5 | CONT. | 9 | |
| PF | Potentilla fruticosa 'Uman' | MANGO TANGO POTENTILLA | #5 | CONT. | 14 | |
| SA | Spiraea albiflora | JAPANESE WHITE SPIREA | #5 | CONT. | 15 | |
| SJ | Spiraea japonica 'Little Princess' | LITTLE PRINCESS SPIREA | #5 | CONT. | 7 | |
| SN | Spiraea japonica 'Neon Flash' | NEON FLASH SPIREA | #5 | CONT. | 14 | |

- NOTES:**
1. SHREDDED HARDWOOD MULCH TYPICAL IN ALL SHRUB AND GROUND COVER AREAS. EDGER TYPICAL WHERE PLANTING BEDS INTERFACE WITH SOD.
 2. PLANTING BED EDGER BLACK DIAMOND EDGER OR EQUAL. FOLLOW OUTLINE OF MATURE PLANT SIZE OR BED LINE AS SHOWN ON PLAN. EASE TRANSITION BETWEEN CURVES. LANDSCAPE ARCHITECT TO APPROVE FINAL BED LAYOUT.
 3. IN STORMWATER AREAS LABELED SEED #1, ESTABLISH PRAIRIE NURSERY'S MOIST MEADOW/RAIN GARDEN MIX. CONTACT PRAIRIE NURSERY FOR QUANTITY REQUIRED. 800-476-9453
 4. IN BASIN AREAS LABELED SEED #2, ESTABLISH PRAIRIE NURSERY'S DETENTION BASIN - BIOSWALE SEED MIX. CONTACT PRAIRIE NURSERY FOR QUANTITY REQUIRED. 800-476-9453
 5. IN ALL OTHER AREAS NOT LABELED SOD, SEED #1 OR SEED #2, PLANT MNDOT SEED MIX 250 (GENERAL ROADSIDE MIX). REFER TO MNDOT SEEDING MANUAL 2014 ADDITION FOR QUANTITY REQUIRED.

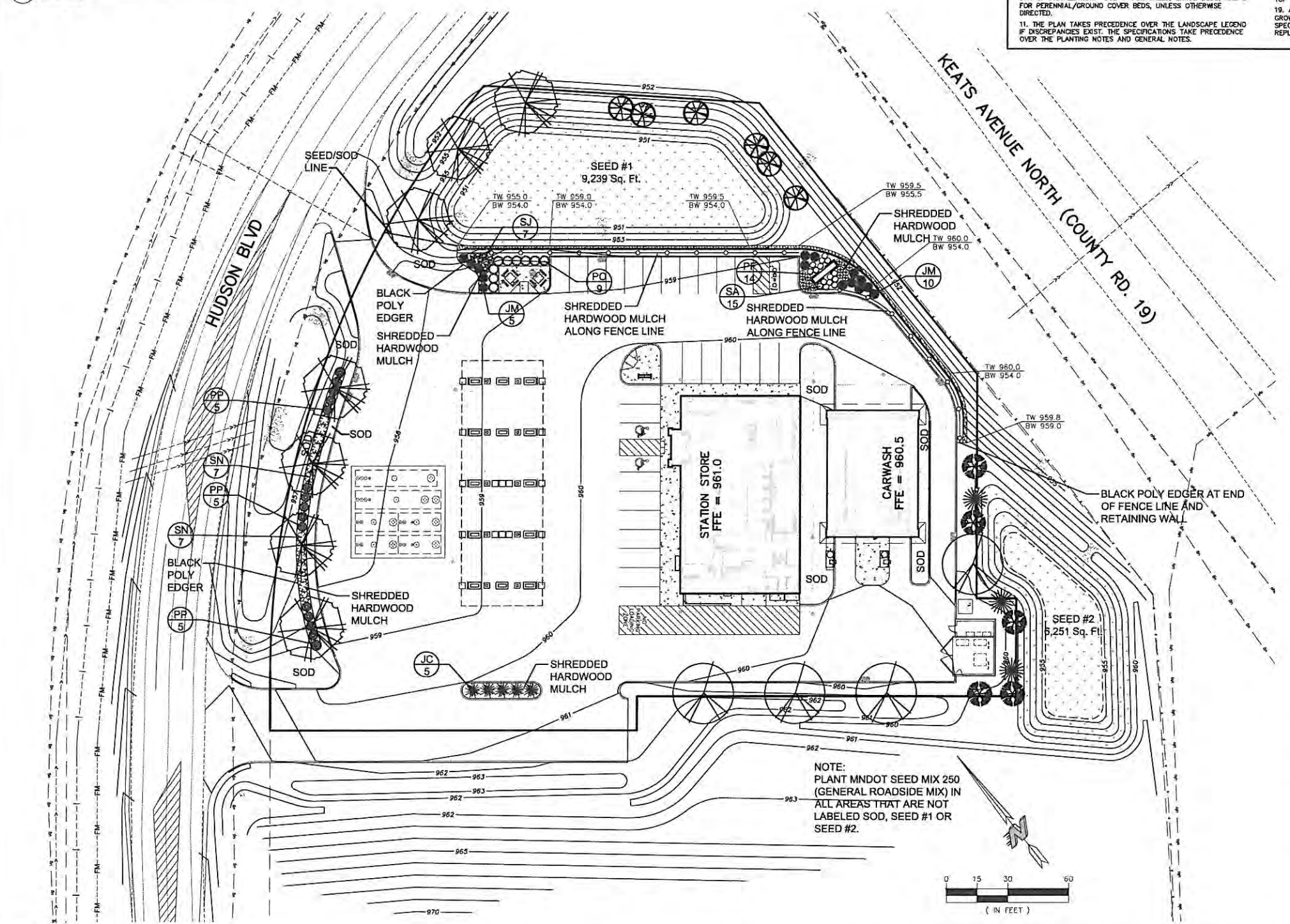
LANDSCAPE REQUIREMENTS:

154.258 C.1: A MIN. OF ONE (1) TREE SHALL BE PLANTED FOR EVERY FIFTY (50) FT. OF STREET FRONTAGE.

340 FT. STREET FRONTAGE ALONG HUDSON BLVD. / 50 = 7 TREES REQ.

154.258 C.2: IN ADDITION TO THE REQUIREMENTS OF C.1 ABOVE, A MINIMUM OF FIVE (5) TREES SHALL BE PLANTED FOR EVERY ONE (1) ACRE OF LAND THAT IS DEVELOPED OR DISTURBED BY DEVELOPMENT ACTIVITY.

3.5 ACRES DISTURBED = 3.5 x 5 = MIN. 18 TREES REQUIRED



Kwik Trip
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Phone: 763-489-7900 Fax: 763-489-7959

FWA
FRIZZELL WINTER ASSOCIATES
LANDSCAPE ARCHITECTS
Prepared by
Frizzell Winter Associates, Landscape Architects

LANDSCAPE PLAN

CONVENIENCE STORE #248 WITH DOUBLE BAY CARWASH

HUDSON BLVD & KEATS AVENUE NORTH
LAKE ELMO, MINNESOTA

| # | DATE | DESCRIPTION |
|---|----------|-------------------|
| 1 | 08/27/14 | Per City Comments |

DRAWN BY: ELF
SCALE: GRAPHIC
PROJ. NO: 4982-00
DATE: 2014-05-05
SHEET: L1

KWIK TRIP

STORES

KWIK STAR

STORES

KWIK TRIP, Inc.
 P.O. BOX 2107
 1600 S. W. 22nd Ave., Ft. Lauderdale, FL 33304
 (954) 578-8100





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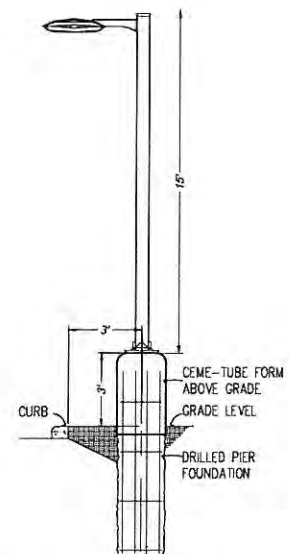
FOOTCANDLE LEVELS ARE MEASURED AT GROUND LEVEL WITHIN THE PROPERTY BOUNDARY AND AT 6' ABOVE GROUND AT THE PROPERTY LINE AND BEYOND.

FIXTURE TYPES:

- A - CREE LIGHTING: CAN-304-SL-RS-06-E-UL-WH-700 MOUNTING HEIGHT - 16'-0"
- B - LED STRIPLIGHT LITHONIA -ZL1-L96-LB840 MOUNTING HEIGHT: SEE ARCHITECTURAL ELEVATIONS
- C - RECESSED LED DOWNLIGHT GOTHAM EVO-41/29-BAR-120-TRW
- TYPE 2B-80 - CREE LIGHTING: ARE-EDG-2MB-DA-08-E-UL-WH-350 WITH BACKLIGHT SHIELD POLE MOUNTED AT A TOTAL HEIGHT OF 18'-0"
- TYPE 2B-120 - CREE LIGHTING: ARE-EDG-2MB-DA-12-E-UL-WH-350 WITH BACKLIGHT SHIELD POLE MOUNTED AT A TOTAL HEIGHT OF 18'-0"
- TYPE 4B-120 - CREE LIGHTING: ARE-EDG-4MB-DA-12-E-UL-WH-350 WITH BACKLIGHT SHIELD POLE MOUNTED AT A TOTAL HEIGHT OF 18'-0"
- TYPE 4-120 - CREE LIGHTING: ARE-EDG-4M-DA-12-E-UL-WH-350 POLE MOUNTED AT A TOTAL HEIGHT OF 18'-0"

FIXTURE SYMBOLS:

- A  RECESSED LED LIGHT MOUNTED UNDER CANOPY
 - B  LED STRIP LIGHT
 - C  RECESSED LED DOWNLIGHT
 -  POLE MOUNTED LED FIXTURE
- TYPES 2 & 4



LOT LIGHT ELEVATION DETAIL
 NOT TO SCALE



PHOTOMETRIC SITE PLAN
 SCALE: 1" = 30'-0"

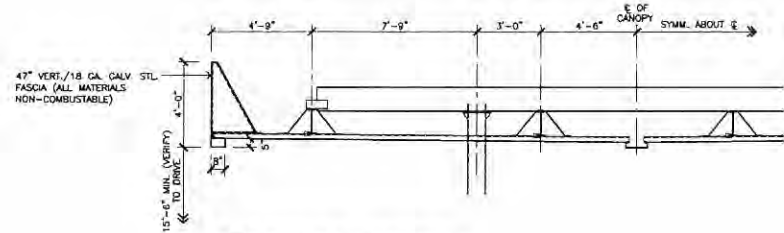
PHOTOMETRIC SITE PLAN

CONVENIENCE STORE #248
 WITH DOUBLE BAY CARWASH

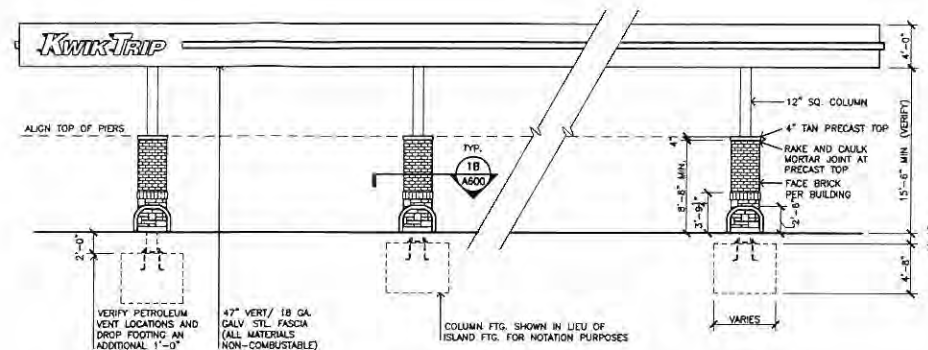
C14063
CZARNECKI ENGINEERING INCORPORATED
 1121 MARLIN COURT, SUITE B - WAUKESHA, WI 53186
 VOICE: (262) 513-2020 FAX: (262) 513-2023
 WEB PAGE: www.cze.com

CZE

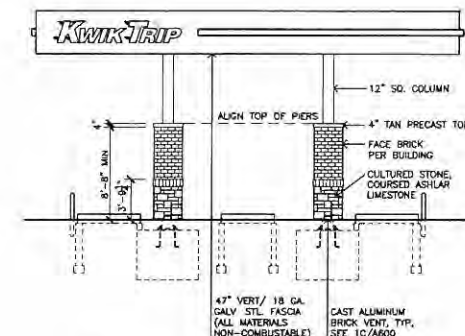
E1-CITY



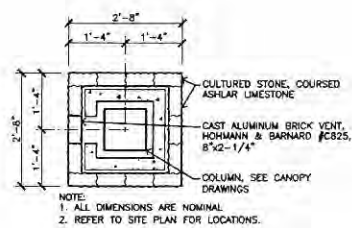
1A CANOPY SECTION
A600 1/4" = 1'-0"



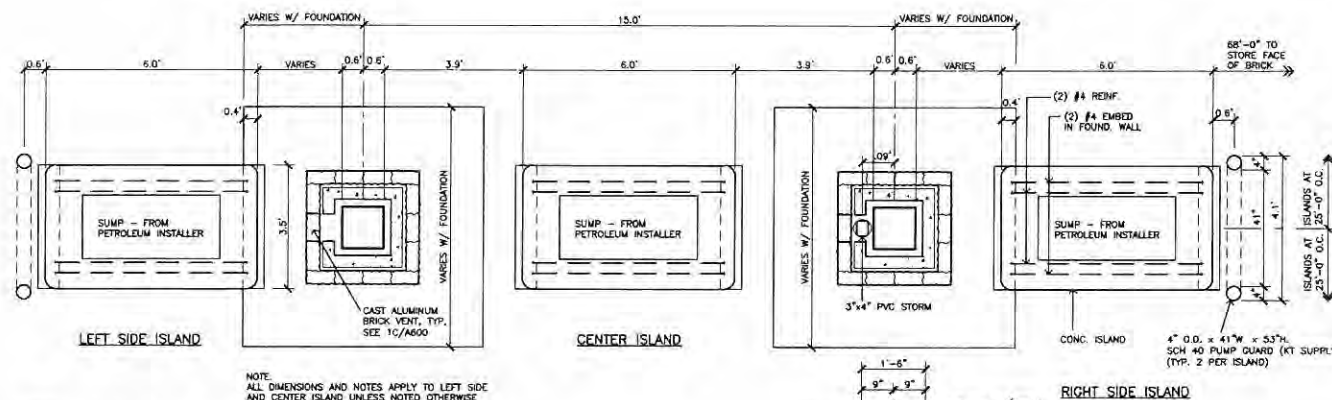
2 END CANOPY ELEVATION
A600 1/8" = 1'-0" (OTHER END SIMILAR)



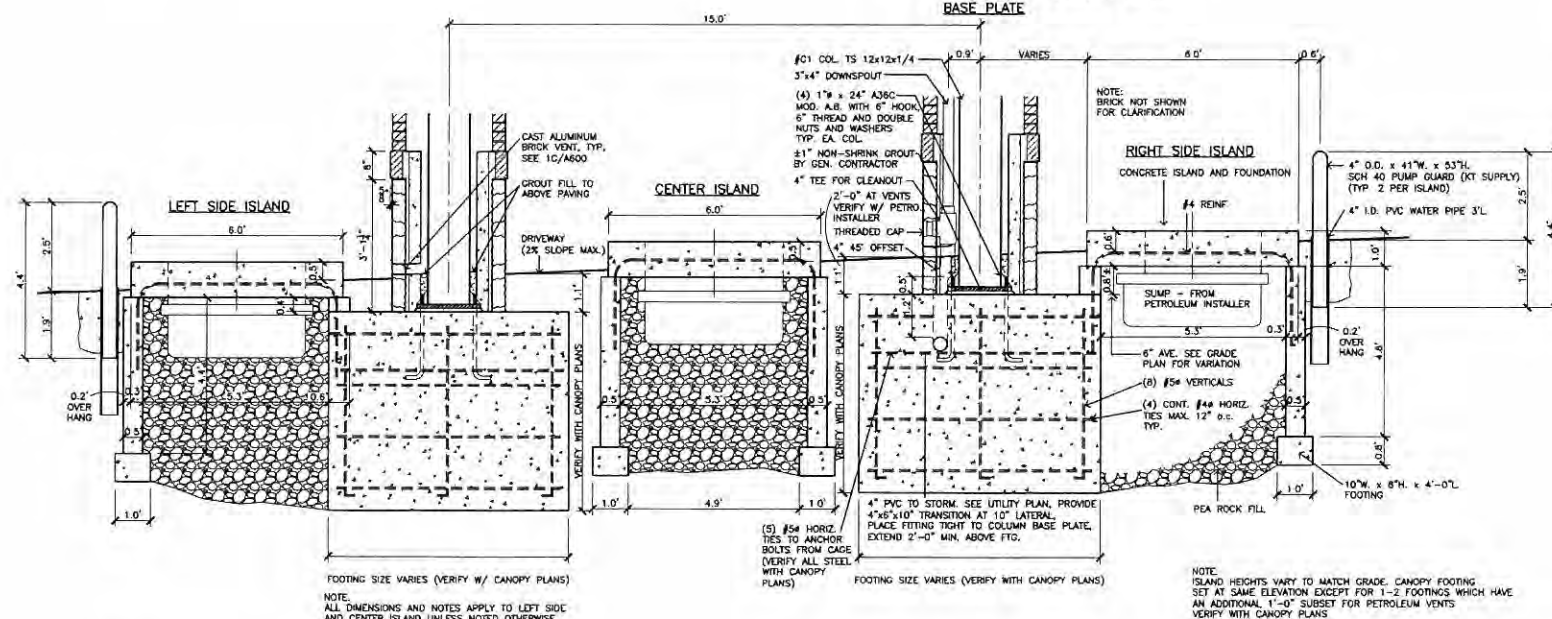
3 SIDE CANOPY ELEVATION
A600 1/8" = 1'-0" (OTHER SIDE SIMILAR)



1B CANOPY COLUMN DETAIL
A600 1/2" = 1'-0"



4 GAS CANOPY COLUMN PLAN
A600 1:10



5 GAS CANOPY COLUMN SECTION
A600 1:10

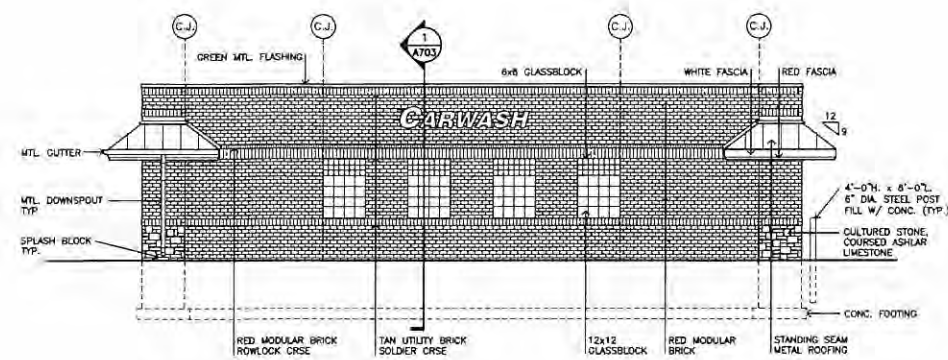
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info@larsonarchitect.com

Kwik
TRIP
STORES
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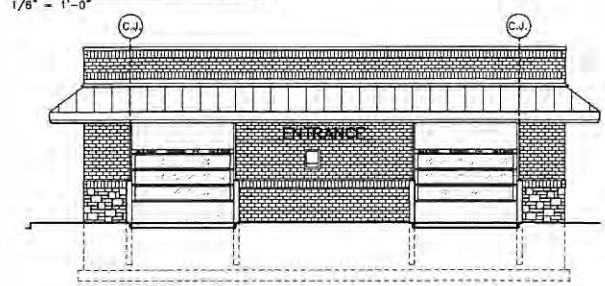
STORE #248
XXX
LAKE ELMO, MN
96' x 58' LH
STANDARD

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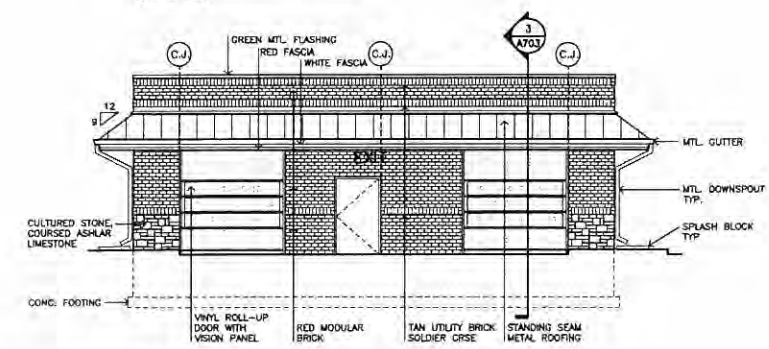
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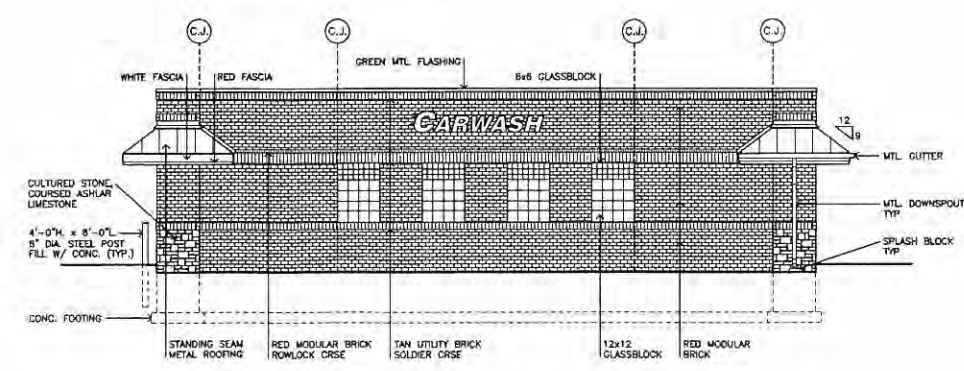
LEFT SIDE ELEVATION
1/8" = 1'-0"



FRONT ELEVATION
1/8" = 1'-0"



BACK ELEVATION
1/8" = 1'-0"



RIGHT SIDE ELEVATION
1/8" = 1'-0"

| EXT. COLOR SCHED. | | |
|-------------------|-----------------|---------------------------|
| MATERIAL | MANUF. | COLOR |
| BOLLARDS | | BLACK GLOSS |
| FASCIA | UNI-CLAD 24 GA. | REGAL RED STONE WHITE |
| METAL ROOF | UNI-CLAD | HEMLOCK GREEN |
| MORTAR | | GREY |
| RED BRICK | SHOUX CITY | CABINET BURGUNDY |
| ROOF CAP CAR WASH | UNI-CLAD | SIERRA TAN |
| STUCCO | TOTAL WALL | MORNING MIST SWRL TEXTURE |
| TAN BRICK | SHOUX CITY | CLEAR BUFF |
| TRASH ENCLOSURE | UNI-CLAD | BURGUNDY |

LARSON
ARCHITECT

200 Mason Street #3
Onalaska, WI 54650
(608) 784-6808
info@larsonarchitect.com

Kwik Trip
STORES

Kwik Star
STORES

STORE #248
XXX
LAKE ELMO, MN
96' x 58' LH
STANDARD

I hereby certify that these plans were prepared by me or registered architects under the laws of the state of Wisconsin.

LARSON ARCHITECT
L.A. Larson, AIA
A000, A100, A101, A102, A103, A104, A105, A106, A107, A108, A109, A110, A111, A112, A113, A114, A115, A116, A117, A118, A119, A120, A121, A122, A123, A124, A125, A126, A127, A128, A129, A130, A131, A132, A133, A134, A135, A136, A137, A138, A139, A140, A141, A142, A143, A144, A145, A146, A147, A148, A149, A150, A151, A152, A153, A154, A155, A156, A157, A158, A159, A160, A161, A162, A163, A164, A165, A166, A167, A168, A169, A170, A171, A172, A173, A174, A175, A176, A177, A178, A179, A180, A181, A182, A183, A184, A185, A186, A187, A188, A189, A190, A191, A192, A193, A194, A195, A196, A197, A198, A199, A200, A201, A202, A203, A204, A205, A206, A207, A208, A209, A210, A211, A212, A213, A214, A215, A216, A217, A218, A219, A220, A221, A222, A223, A224, A225, A226, A227, A228, A229, A230, A231, A232, A233, A234, A235, A236, A237, A238, A239, A240, A241, A242, A243, A244, A245, A246, A247, A248, A249, A250, A251, A252, A253, A254, A255, A256, A257, A258, A259, A260, A261, A262, A263, A264, 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Revised: _____
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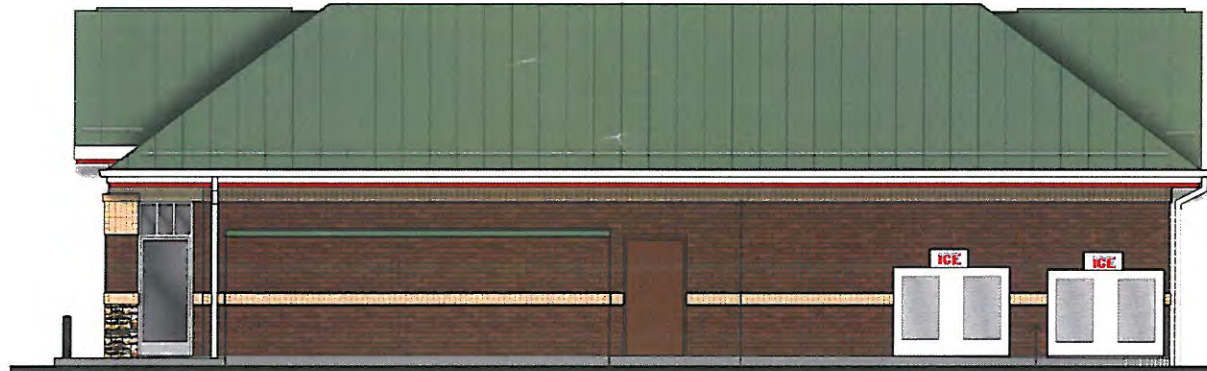
A703



WHITE ALUM. FASCIA | RED ALUM. FASCIA | STANDING SEAM METAL ROOFING

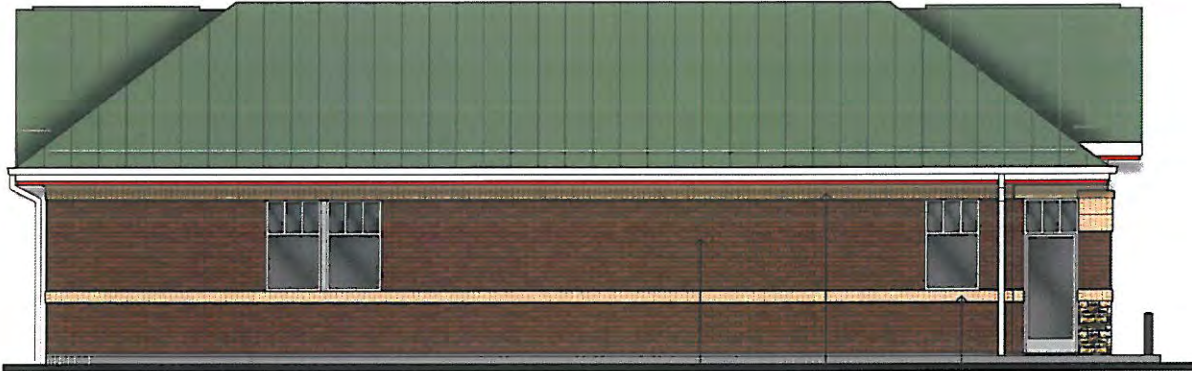
TAN BRICK SOLDIER CRSE AT WINDOWS | RED BRICK | TAN STUCCO | TAN BRICK SOLDIER CRSE

FRONT ELEVATION
1/4" = 1'-0"



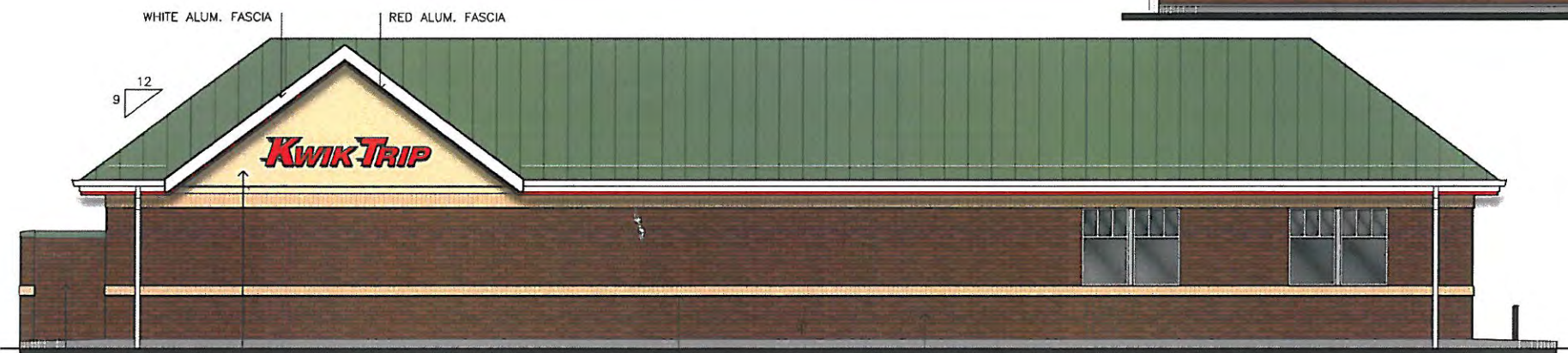
RIGHT SIDE ELEVATION
3/16" = 1'-0"

TAN BRICK SOLDIER CRSE | RED BRICK



RED BRICK | TAN BRICK SOLDIER CRSE | TAN BRICK ROWLOCK CRSE

LEFT SIDE ELEVATION
3/16" = 1'-0"



WHITE ALUM. FASCIA | RED ALUM. FASCIA

TOTE ENCLOSURE | TAN STUCCO | TAN ROWLOCK SOLDIER CRSE | RED BRICK

BACK ELEVATION
1/8" = 1'-0"



200 Mason Street #3
Onalaska, WI 54650
(608) 784 - 6808
info@larsonarchitect.com

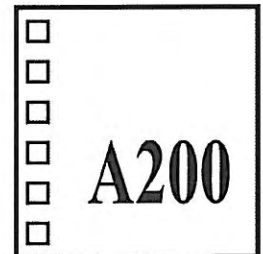


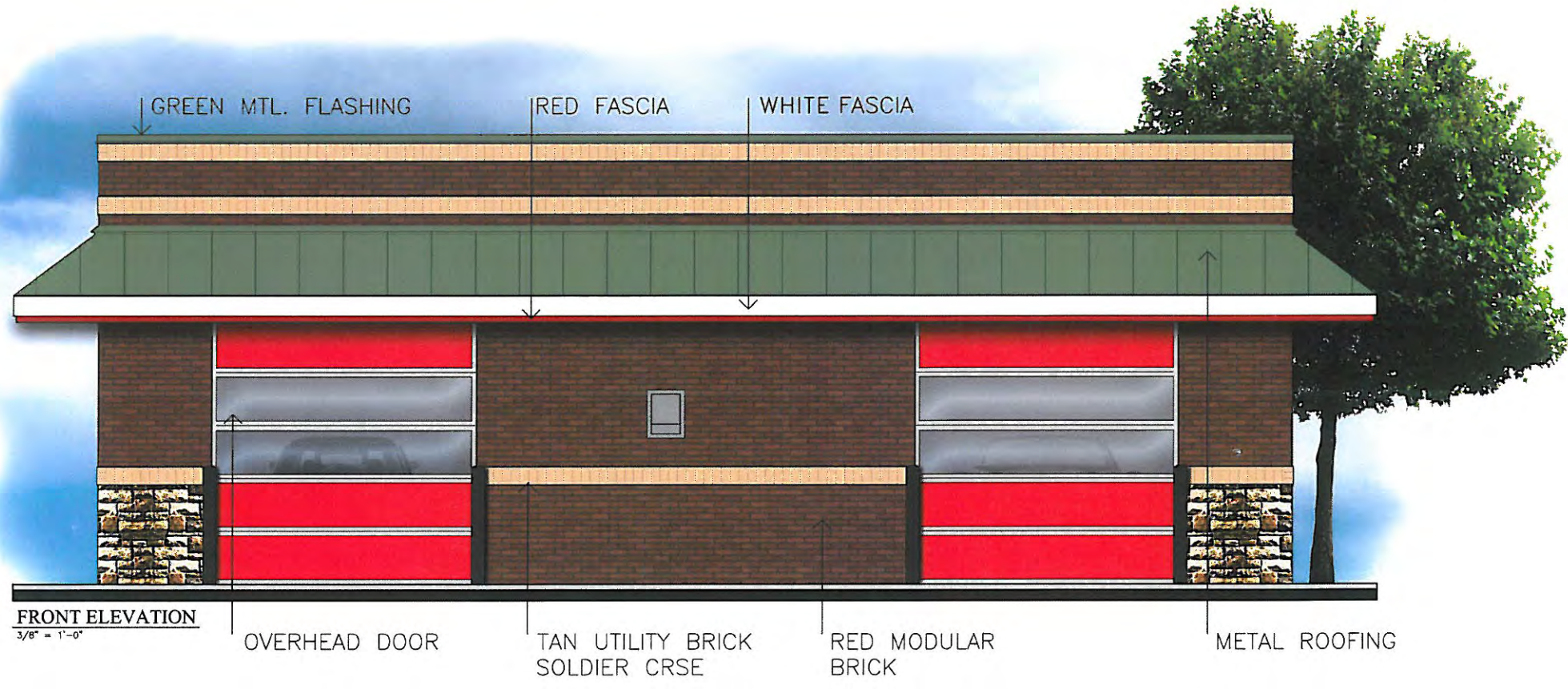
STORES



STORES

96'x58' L

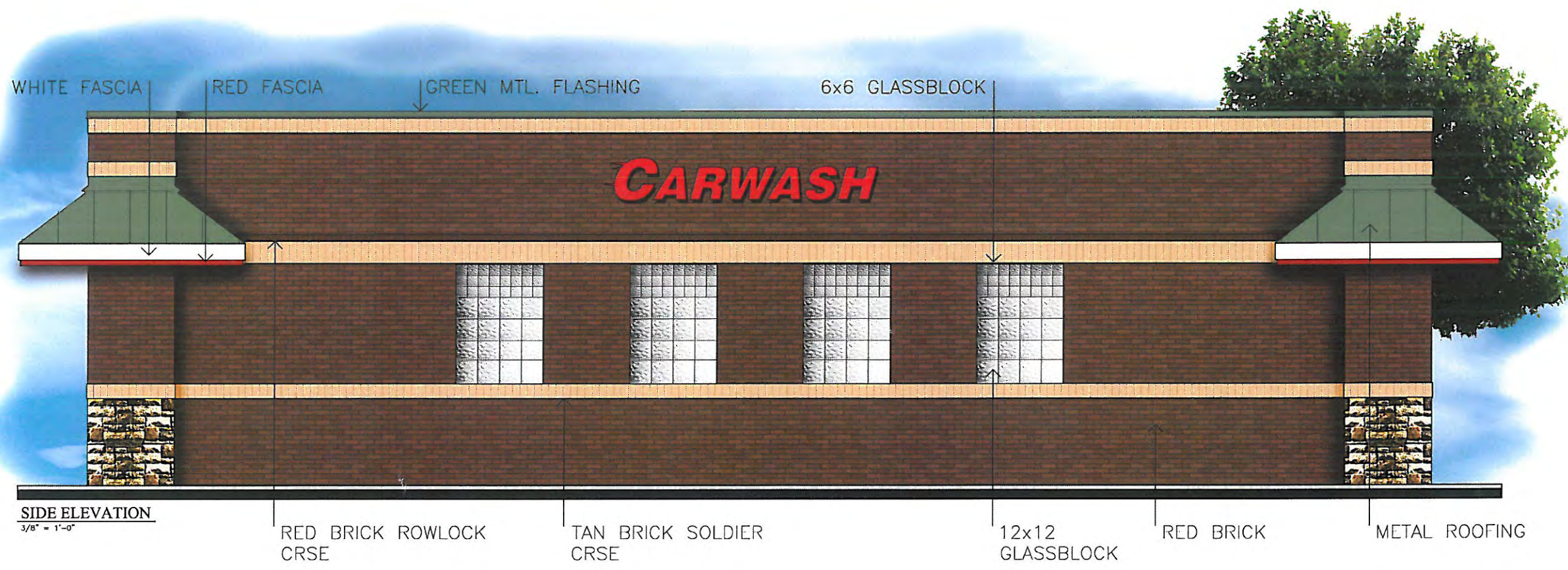




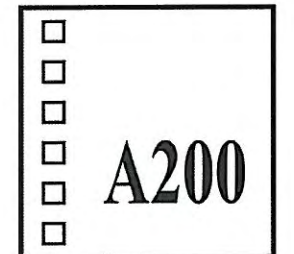
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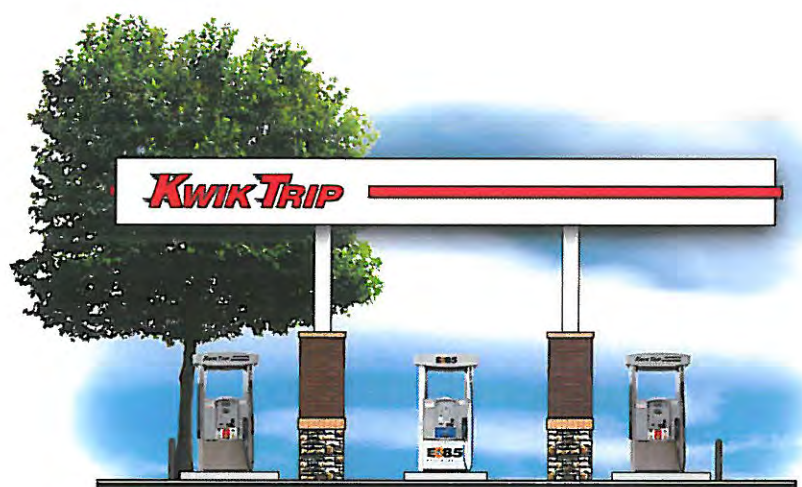


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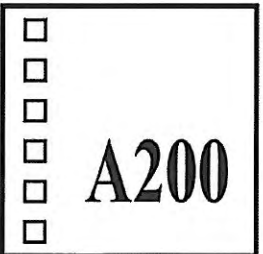
SIDE ELEVATION
3/8" = 1'-0"





Flat Canopy
w/ Brick

10 MPD



Your Choice
TOUCH FREE OR SOFT TOUCH

NON-LIT INFORMATIONAL SIGN
 RED VINYL ON WHITE ALUMINUM
 1'-0"H X 10'-0"W = 10.0 SQ FT

CARWASH INFORMATION SIGNS #05 & #07

SCALE: 3/4" = 1'-0"

#13 KWIK TRIP FREESTANDING MONUMENT SIGN

SEE ATTACHMENT FROM LA CROSSE SIGN CO

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STAR**

STORES

KWIK TRIP, Inc.
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 1626 OAK STREET
 LA CROSSE, WI 54602-2107
 PH. (608) 781-8988
 FAX (608) 781-8960

DIRECTIONAL SIGNAGE

CONVENIENCE STORE #248
 WITH SINGLE CW

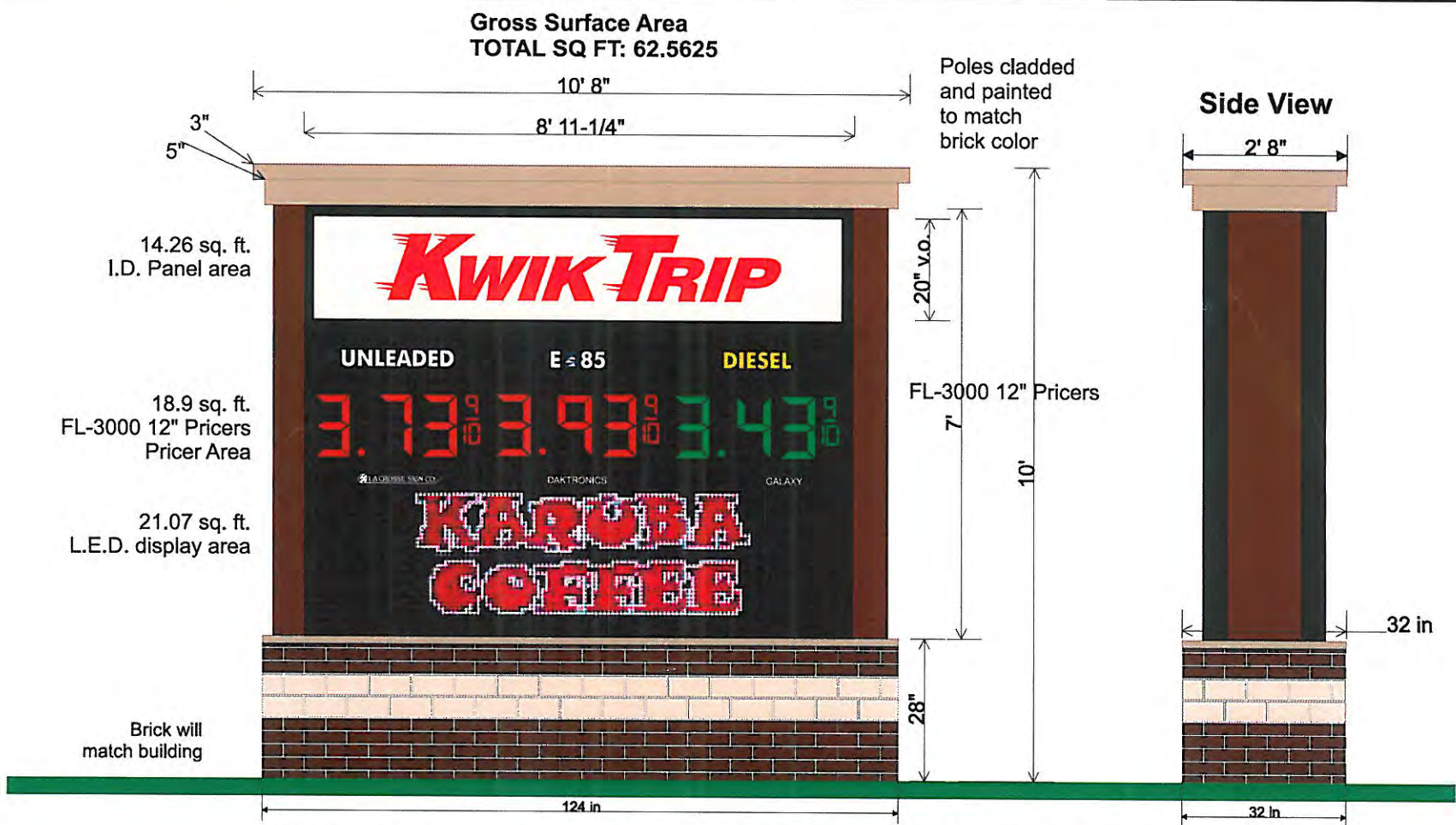
HUDSON BLVD
 LAKE ELMO, MN

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| DRAWN BY | B. DUSEK |
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| DATE | 2014-05-08 |
| SHEET | DS1 |

KWIK TRIP

#248 Lake Elmo, MN



*COLORS ON SKETCH ARE A REPRESENTATION, ACTUAL COLOR MAY DIFFER

LA CROSSE SIGN CO.
MAKE A STATEMENT!

Date: 2-5-2014
Job #: 77597
Artist: Danielle Waas
Sales: Cindy Bluske
Scale: 3/8"=1'

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APPROVED BY: _____

MEMORANDUM

FOCUS ENGINEERING, inc.

Cara Geheren, P.E. 651.300.4261
Jack Griffin, P.E. 651.300.4264
Ryan Stempski, P.E. 651.300.4267
Chad Isakson, P.E. 651.300.4285

Date: July 9, 2014

To: Nick Johnson, City Planner
Cc: Kyle Klatt, Planning Director
From: Jack Griffin, P.E., City Engineer

Re: Lakewood Crossing – Kwik Trip
Preliminary/Final Plat Review

An engineering review has been completed for the Lakewood Crossings – Kwik Trip. Site Plans were received on June 30, 2014. The submittal consisted of the following documentation prepared by Carlson McCain:

- Lakewood Crossing Preliminary Plat dated June 27, 2014.
 - Kwik Trip Store #248 Site Plans dated June 27, 2014.
 - Kwik Trip Store #248 Storm Water Management Plan dated June 27, 2014.
-

Engineering review comments are as follows:

Preliminary Plat / General Comments:

1. The drainage and utility easements must be placed on both the utility plan, Sheet SP4, and the grading plan, Sheet SP3 to demonstrate sufficient easement is being provided. A minimum 15 feet of utility easement is required from the centerline of the proposed watermain utility. It appears additional easement is required.
2. Governing Specifications must be in accordance with the City of Lake Elmo Standard Specifications for General Requirements, including summary of work, project meeting requirements, submittals, regulatory requirements, testing, traffic control, erosion and sedimentation and pollution control, and close-out requirements; and for utility technical specifications. A specification booklet must be created for the project, approved by the City and provided to the contractor for use on the project.
3. Engineering has not reviewed the proposed lighting and landscape improvements.

Site Plans, Traffic and Access Management Requirements:

1. Hudson Boulevard is a local collector roadway and Municipal State Aid route. Hudson Boulevard is expected to receive significant growth in traffic volume as the I94 corridor develops. The road is considered to be a major collector for serving the area but it is the goal of the City to maintain the road as 2-lanes. In order to achieve that goal left and right turn lanes will need to be implemented throughout the corridor to facilitate the turning movements for the developing areas while maintaining the mobility of the through traffic.
2. The Access Management Guidelines per the City's Comprehensive Transportation Plan requires a minimum spacing of 1/8 mile (660 feet) for commercial driveway access along Hudson Boulevard. The proposed westerly access for this site plan appears to sufficiently meet this spacing at ~550 feet. The westerly driveway can be permitted full access if the access is developed as a shared access driveway with Outlot A.
3. The property to the north will also be required to access Hudson Boulevard at this location.
4. The shared access driveway width should be at least 40 feet to accommodate one inbound lane (16 feet wide) and two 12 feet wide outbound lanes (left and right turn lanes).
5. The easterly driveway should be permitted as a right-out-only exit. Future improvements to Hudson Boulevard will likely include the placement of a center raised median to prohibit left turning movements from this site.

6. The right-out-only exit should be revised to increase the site lines for traffic existing the facility. This can be achieved by increasing the intersection angle. The construction of a dedicated acceleration lane could also be considered to address safe egress from the site.
7. The site plan proposes a temporary left turn lane for the westerly driveway access as requested by staff. The pavement markings shall be revised using larger tapers more appropriate for the posted speed limit (see attached TKDA review memorandum and sketch dated June 26, 2014).
8. The plans must be revised to provide greater plan and specification detail for the improvements to Hudson Boulevard. Detailed dimensions shall be placed on the plan sheets indicating start and stop stationing, lane widths, shoulder widths, etc.
9. Shoulder widening details must be dimensioned indicating start and stop stationing, road width increases, pavement section details, and material details. Pavement widening must be completed using minimum practical widths and avoiding tapering at the end points.
10. Specifications and plan notes must be added to further detail the improvements to Hudson Boulevard.
11. All pavement markings shall be shown as epoxy resin with drop-on glass beads per the City's standard specifications.

Grading, Drainage & Erosion Control Plan:

1. City Standard Plan Notes for Grading and Erosion Control, Details 600A-D, must be placed on the Grading and Erosion Control Plan, Sheet SP3. All other conflicting information must be removed. Supplemental notes may be provided if not already addressed by the City standards.
2. Governing Specifications for grading and erosion control must be in accordance with the City of Lake Elmo standard specifications.
3. Retaining walls that exceed 4 feet in height must have a design submitted and certified by an engineer licensed in the state of Minnesota.

Utility Plans:

1. City Standard Plan Notes for Watermain, Detail 200A, for Sanitary Sewer, Detail 300A, and for Storm Sewer, Detail 400A, must be placed on the Utility Plan, Sheet SP4. All other conflicting information must be removed. Supplemental notes may be provided if not already addressed by the City standards.
2. Governing Specifications for utilities must be in accordance with the City of Lake Elmo standard specifications.
3. Add City Standard Detail Plate 210 – Watermain offset to plan sheet SP7. All watermain must be installed at a depth of 7.5 feet or deeper. Shallow watermain with insulation will not be allowed. Watermain offsets will be required when conflicts occur. This appears to be needed at the sanitary sewer crossing point.
4. The water and sewer laterals extending into Outlot A should be realigned to remain parallel to Hudson Boulevard to allow more flexibility to the future extension for this property.
5. The proposed storm sewer system is being constructed entirely on privately owned property and does not conform to the City engineering design standards for storm sewer systems. The storm sewer system should therefore remain privately owned and maintained by the applicant, including the three driveway culverts.
6. Additional pipe cover appears to be needed over the proposed driveway culverts.

Stormwater Management:

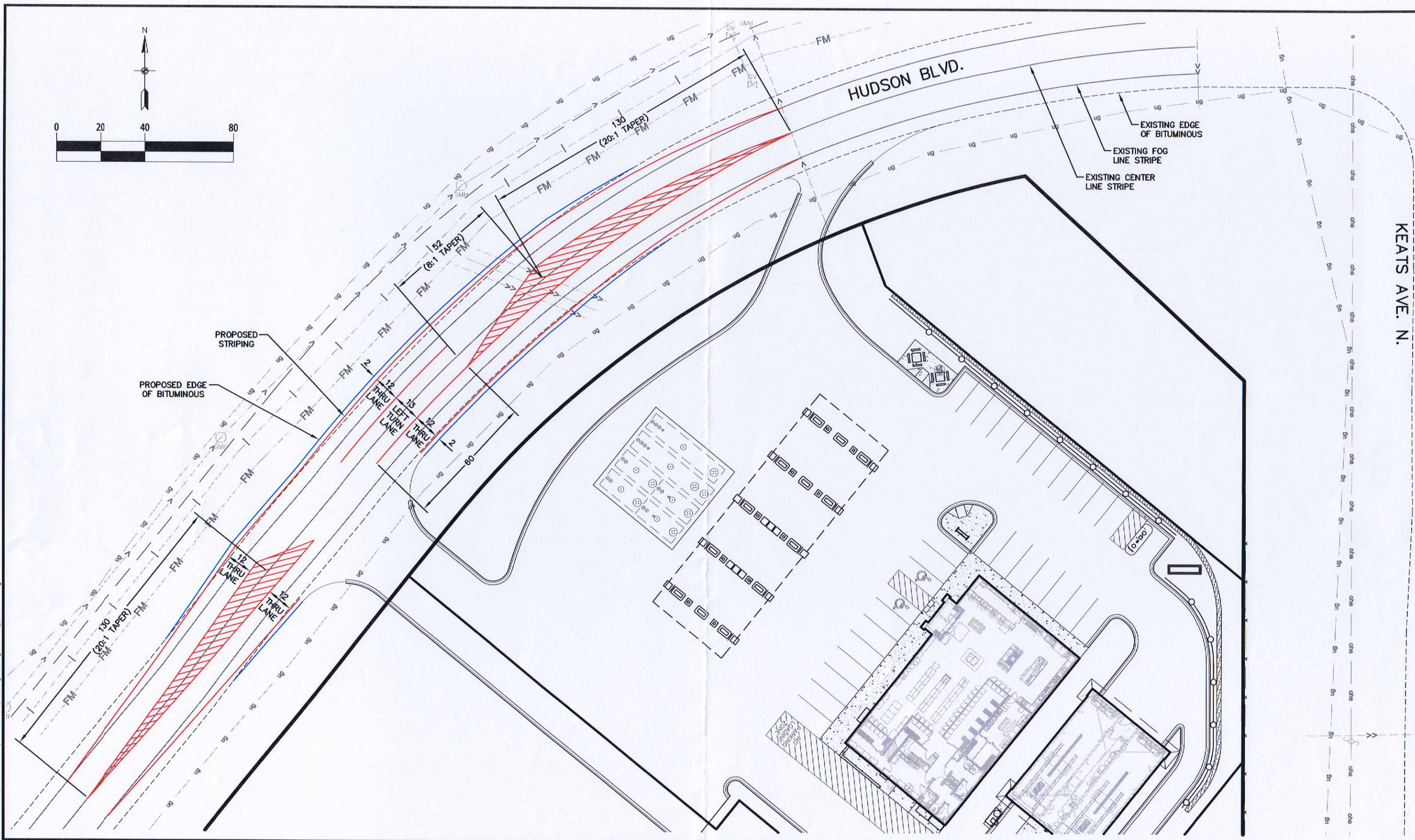
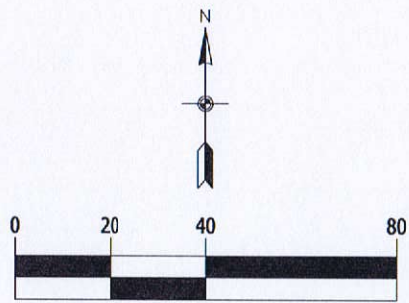
1. The Storm Water Management Plan must meet VBWD rules and requirements. The applicant must provide the City written approval from the VBWD.
2. The proposed storm water facilities will receive storm water from a privately owned and maintained storm sewer system that will not be constructed to City engineering design standards. It is therefore recommended that the storm water facilities be privately owned and maintained. A maintenance agreement in a form acceptable to the City should be executed and recorded with the County for all permanent storm water facilities to be located on private property. The agreement shall provide a maintenance plan defining the maintenance responsibilities for the private owner, the type of maintenance and the maintenance intervals.

- To provide the necessary width for the proposed left-turn lane, the through lanes in each direction on Hudson Boulevard are proposed to be shifted outward using 1:20 tapers. Given that Hudson Boulevard has a posted speed limit of 50 mph, this taper is too abrupt to shift through traffic. A taper rate of 1:50 is more appropriate and satisfies MnDOT State Aid Standards. This will increase the taper distance from 120 feet to 325 feet. There appears to be sufficient space available on each side of the proposed west access to accommodate this larger taper rate. A painted median on Hudson Boulevard would also help to reinforce the right-turn-only exit from the east access.
- Ideally, the combined turn lane length and taper length from a through lane provides for deceleration from the through lane's travel speed and sufficient space for the expected vehicle stacking. If this guideline were followed in this case, the posted speed limit of 50 mph would translate into a distance of 440 feet and a minimum of two cars for stacking would translate into a minimum of 60 feet. The minimum turn lane and taper length would then be 500 feet. However, other circumstances are frequently taken into account, such as accounting for deceleration in the through lane before the turn lane and consideration of the land use or expected traffic operations associated with the turn lane. Based on the characteristics of this site and anticipated traffic operations, a minimum total distance of 280 feet is recommended to provide for a comfortable transition from the through lane (1:10 taper rate or 130 feet) and stacking for several cars and a truck (150 feet).
- As an alternative to the tapers on Hudson Boulevard described in the two bullets above, back-to-back left-turn lanes between the west access and the intersection with Keats Avenue could be considered. The exhibit does not show improvements to the Hudson Boulevard intersection with Keats Avenue, and no documentation was provided that suggests poor traffic operations are expected. However, if the City anticipates issues, this would be an opportunity to improve traffic operations.
- The proposed width of the shifted through lanes matches the existing lane widths on Hudson Boulevard. The proposed left-turn lane on Hudson Boulevard would provide acceptable width for the traffic operations. If the overall width of the road is a concern, each lane width could be reduced to 11 feet. Although not typically desired, the reduced-width lanes would provide a measure of traffic calming and still allow for sufficient traffic operations.
- Hudson Road is posted as a bike route, and bicyclists are generally be expected to use the existing 6-foot-wide shoulder when traveling on this road. While bicycle travel in a vehicle lane of traffic is legal, maintaining a shoulder for bicyclists is a safer option. A minimum 4-foot-wide shoulder would maintain the safety of the roadway shoulders for bicyclists. The roadway shoulders could also be used by pedestrians.

Two sketches are attached to this memorandum that show potential redesign of Hudson Boulevard. One shows the recommended turn lane length and appropriate tapers. The other shows the potential back-to-back left-turn lanes on Hudson Road, assuming an improvement at its intersection with Keats Avenue. It should be noted that any widening of Hudson Road should sawcut and replace some or all of the existing shoulder. Without sufficient width, the newly constructed roadway could fail to properly bond with the existing roadway and lead to future maintenance issues.

If you have any questions or comments regarding the information presented in this memorandum, please contact me at 651.726.7944 or bryant.ficek@tkda.com.





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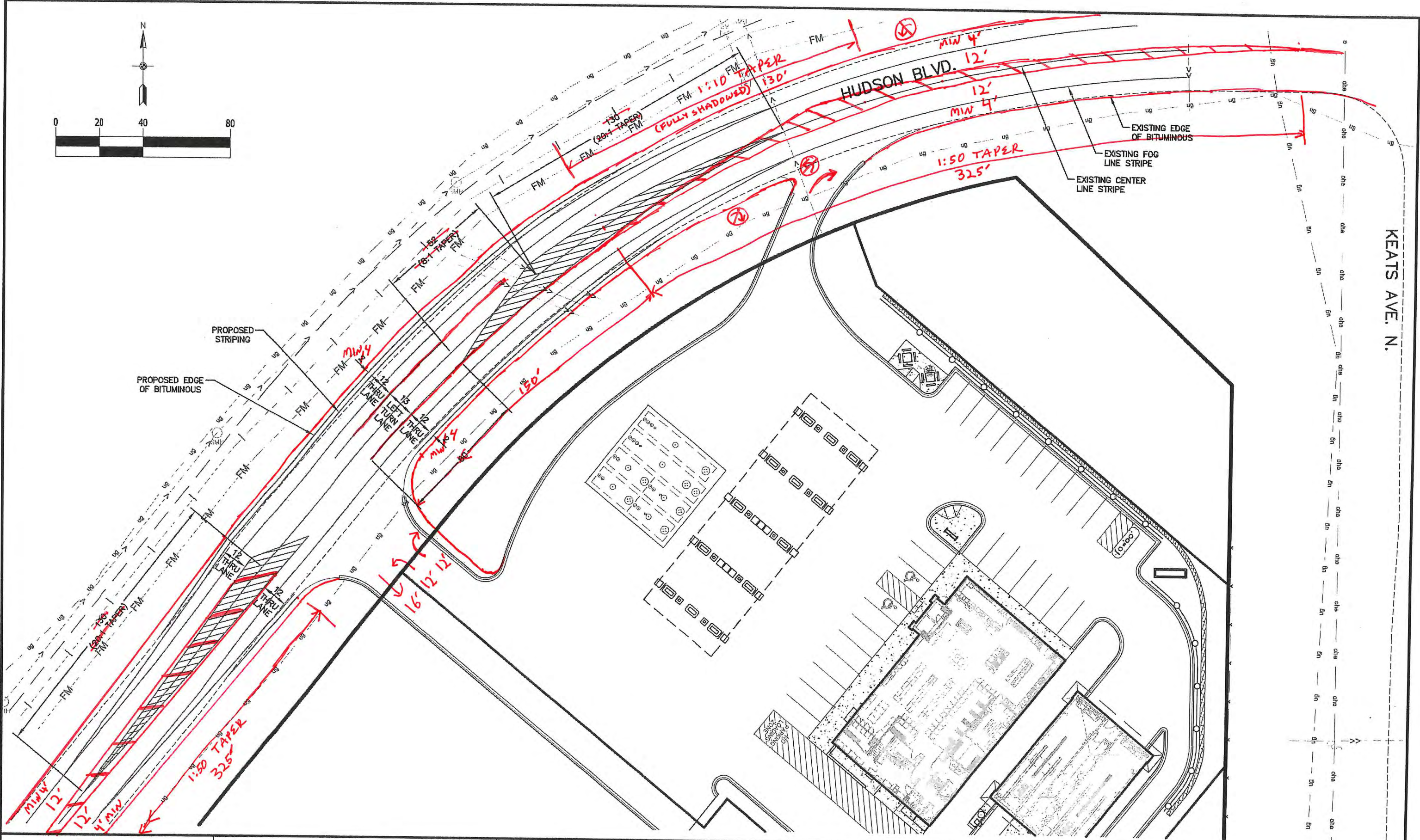
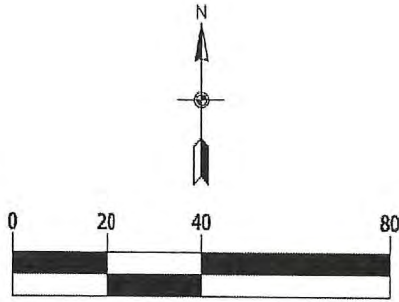


Notes:

CM PROPERTIES 94
 3470 Washington Drive, Suite 102
 Eagan, MN 55122

LAKWOOD CROSSING
 Lake Elmo, Minnesota

TURN LANE EXHIBIT



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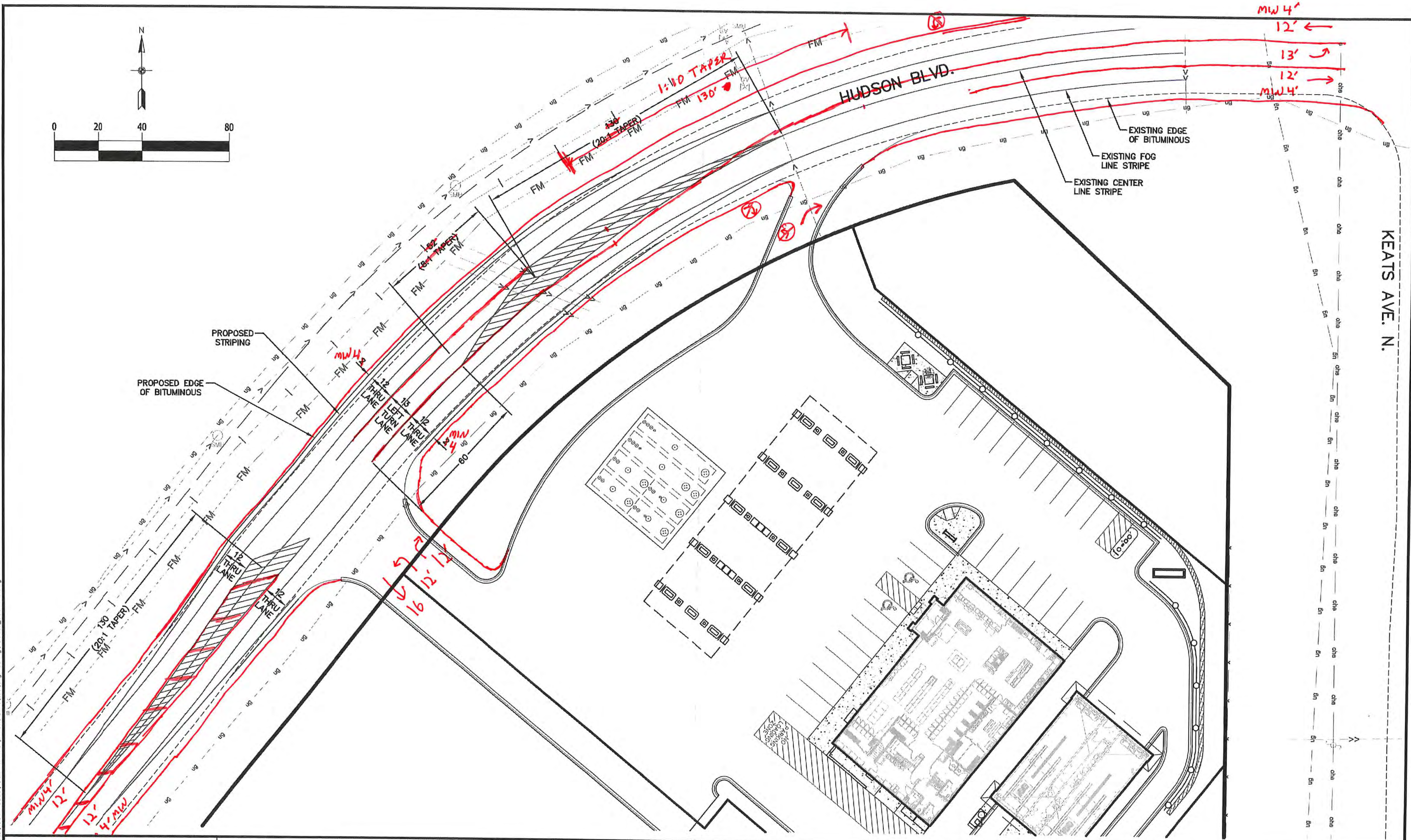
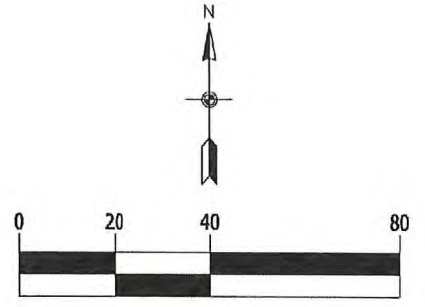


Notes:

CM PROPERTIES 94
 3470 Washington Drive, Suite 102
 Eagan, MN 55122

LAKWOOD CROSSING
 Lake Elmo, Minnesota

TURN LANE EXHIBIT



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Notes:

CM PROPERTIES 94
 3470 Washington Drive, Suite 102
 Eagan, MN 55122

LAKWOOD CROSSING
 Lake Elmo, Minnesota

TURN LANE EXHIBIT



Public Works Department

Donald J. Theisen, P.E.
Director

Wayne H. Sandberg, P.E.
Deputy Director/County Engineer

July 9, 2014,

Nick Johnson
City Planner
City of Lake Elmo
3600 Laverne Avenue North
Lake Elmo, MN 55042

RE: Washington County comments on Kwik Trip, City of Lake Elmo

Dear Nick,

Thank you for providing Washington County with the plans for Kwik Trip adjacent to County State Aid Highway (CSAH 19) Keats Avenue in Section 34 in the City of Lake Elmo. The project consists of a 5,570 square foot convenience/ gas station store, gas pumping islands, car wash and support parking. Based on the plan provided, we have the following comments:

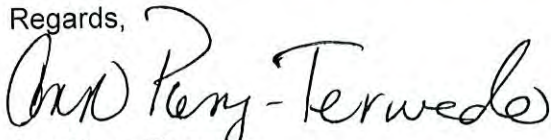
- We reviewed the public right-of-way adjacent the County State Aid (CSAH)19/Keats Avenue and determined that MNDOT has jurisdiction/ownership of a section of property between CSAH 19 and the subject property. This is referenced as Minnesota Department of Transportation Right-of-Way Plat No. 82-43
- Since there is approximately 265 feet of County and Mn Dot public right-of-way along CSAH 19/Keats Avenue no additional right-of way is required.
- The city may wish to consider a modification to the geometrics of the east access drive to Hudson Boulevard. We have provided the attached drawing for reference.
- As development increases along the I-94 Corridor in this section of Lake Elmo, traffic volumes will increase at Hudson Boulevard and CSAH 19 which will cause more increases in delays and congestion. Since the east access drive is less than the required 660 feet (1/8 mile) from CSAH 19/Keats Avenue, those vehicles exiting the site will have difficulty finding a queue in traffic in order to enter the Hudson Boulevard roadway or there may be impacts to the mobility of the roadway. The city should continue to evaluate the situation as development occurs with the possibility that drive be removed in the future.
- All utility connections that require and work in County right-of-way for the development will require Washington County Right of Way permits. Typically, these are the responsibility of the utility companies.
- Please be aware that the D2 alignment of the Gateway Corridor Transitway project runs parallel to Hudson Road in the vicinity of the proposed Kwik Trip. While the exact details of the alignment are to be determined, the County will continue to

July 9, 2014
Kwik Trip

work with the City of Lake Elmo to ensure that the transitway alignments along Gateway Corridor will be coordinated with existing land uses and future land use changes.

Thank you for the opportunity to comment on this project. If you have any questions, please contact me at 651-430-4362 or ann.pung-terwedo@co.washington.mn.us. For any permit applications, please contact Carol Hanson at Carol.hanson@co.washington.mn.us.

Regards,



Ann Pung-Terwedo
Senior Planner

C: Carol Hanson, Office Specialist

R/Plat Reviews/City of Lake Elmo/ Kwik Trip

Nick Johnson

From: Sherman, Tod (DOT) <Tod.Sherman@state.mn.us>
Sent: Thursday, July 10, 2014 8:26 AM
To: Nick Johnson
Subject: RE: Lakewood Crossing - Kwik Trip

Nick:

Sorry I can't get a complete review letter out to you by today. Our comments so far are:

Ponding will not be allowed on MnDOT right-of-way. Any use of or work within or affecting MnDOT right of way requires a permit. Permit forms are available from MnDOT's utility website at <http://www.dot.state.mn.us/utility/> Please include one 11 x 17 plan set and one full size plan set with each permit application. Please direct any questions regarding permit requirements to Buck Craig (651-234-7911) of MnDOT's Metro Permits Section.

A determination as to whether a MnDOT drainage permit is needed will be included in our full letter which we will send you early next week.

Please let me know if you have any questions.

Tod Sherman, Planning Supervisor
Mn/DOT Metro District
1500 W. County Road B-2
Roseville, MN 55113
(651) 234-7794
tod.sherman@state.mn.us

-----Original Message-----

From: Nick Johnson [mailto:NJohnson@lakeelmo.org]
Sent: Tuesday, July 08, 2014 10:32 AM
To: 'Ann Pung-Terwedo'; Sherman, Tod (DOT)
Subject: RE: Lakewood Crossing - Kwik Trip

Tod,

I should have sent this one along earlier. My apologies.

Thanks,

Nick M. Johnson | City Planner
City of Lake Elmo, Minnesota
njohnson@lakeelmo.org
(w) 651-747-3912 | (f) 651-747-3901
www.lakeelmo.org

3 Commercial Development

Applicable Zoning Districts:

- Commercial (C)
- Convenience Commercial (CC)

The future commercial areas within the I-94 Corridor and Old Village will include a variety of service, office, retail and other uses that will serve existing and future Lake Elmo residents and beyond. While the differences in character and geography of these two growth areas may attract different types of commercial uses, it is important to establish standards that will ensure quality development outcomes regardless of location and use classification. In the commercial districts, quality development outcomes consist of buildings of high architectural quality and sites that function well for all users, both drivers and pedestrians.

A. Site Design

Building Placement

Goal: To ensure access and circulation for all users in a manner that minimizes traffic disruption and safety concerns, as well as maintains good sightlines from the public street.

- Buildings must be setback at least 30 feet from the public right of way. Buildings are encouraged to be located as close to the public street as possible while still meeting the setback requirement.
- The orientation of multiple buildings on one site must be clearly coordinated.
- Buildings should be oriented parallel or perpendicular to the street they front, promoting continuity of design.
- Buildings should be arranged to provide convenient access to entrances and efficient on-site circulation for vehicles and pedestrians.
- Shared access points from the public ROW are encouraged. Vehicular access points should be limited to minimize traffic disruption.
- For master planned development, the provision of landscaped open or gathering spaces is encouraged within commercial developments.

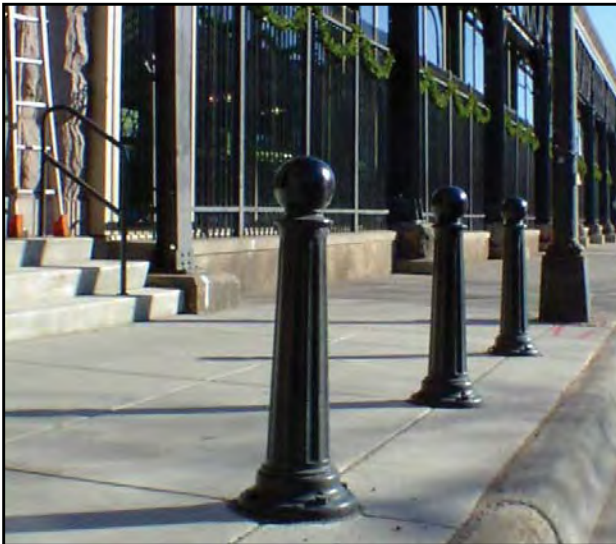


Landscaped gathering spaces provide a distinctive and welcoming space for visitors.

COMMERCIAL DEVELOPMENT



Fencing around outdoor dining areas helps delineate pedestrian walkways and patio space.



Bollards are an effective tool in delineating the pedestrian space. This style of bollard is drawn from the Lake Elmo Branding and Theming Study.

Streetscape

Goal: To create safe, pleasant and functional pedestrian spaces within commercial development, improving access and district identity.



Streetscapes should include plant beds with trees to provide shade and add aesthetic value along sidewalks.

- a. Sidewalks are required along primary street frontages, unless a suitable alternative that promotes pedestrian access safety is approved. In addition, pedestrian access to the building from the public street shall be provided.
- b. Street trees shall be installed at regular intervals along the public right of way.
- c. Ornamental or bollard lighting is encouraged to increase safety, as well as add visual interest.
- d. Fencing shall be installed around outdoor dining areas that are adjacent to pedestrian areas or streets.
- e. Site furnishings such as decorative fencing, trash receptacles, planters, bicycle racks, and benches are recommended. Applicants are encouraged to utilize design elements and site amenities from the Lake Elmo Branding & Theming Study.

Landscaping

Goal: To ensure development of a high aesthetic quality, and to reduce the amount of impervious surface at commercial sites.

- a. Parking, public and streetscape areas should utilize trees, plant beds, and potted plants to add visual interest and break up continuous hardscape.
- b. Parking, service, storage and utility areas should be buffered by plantings. Near areas of pedestrian circulation, these plantings shall not exceed 4 feet in height for safety purposes.
- c. Hardy and native plant materials that are resistant to the climate, disease and salt are encouraged.
- d. Making use of similar plant materials as adjacent properties and public spaces is encouraged to create continuity.
- e. Mature trees located on building sites should be retained whenever possible.
- f. Bare soils should be planted or mulched with bark, stone or other suitable material to avoid unnecessary runoff.

Parking

Goal: To provide parking facilities that adequately serve the needs of commercial properties, while ensuring pedestrian safety and maintaining a positive visual aesthetic from the public right of way.



Sidewalks within larger parking lots improve pedestrian circulation and safety.



Landscaping strips along public streets add a visual separation between parking areas and the public right-of-way.

- a. The linear measurement of surface parking areas parallel to the public street are encouraged not to exceed more than 60% of primary street frontages. Sites or projects that are unable to meet this guideline are encouraged to install berms and/or additional landscaping along areas of surface parking adjacent to the primary street frontage.
- b. The entrance to parking facilities should be located on secondary streets when possible.
- c. Shared parking facilities between adjacent uses or businesses are encouraged when possible to avoid excessive amounts of parking.
- d. Structure parking is encouraged, and should be located behind or beneath primary buildings when possible.
- e. Structure parking or parking areas located beneath the primary structure should be screened with architectural elements that match the primary building.

- f. Parking areas should be screened from view of public streets by means of grading and/or landscaping.
- g. Parking areas should be screened from adjacent structures with landscaping strips not exceeding 4 feet in height in order to ensure pedestrian safety.
- h. Landscaped islands should be installed within surface parking areas to break up continuous hardscape and reduce concentration of impervious surface.
- i. Lighting must be provided in parking areas at night for safety purposes. However, direct glare, spillover or other forms of light pollution directed at adjacent properties are prohibited.
- j. Parking facilities must be ADA compliant when deemed necessary.

Delivery, Service, Storage and Utility Areas

Goal: To provide physical and visual separation between delivery, service and storage areas and areas of pedestrian and automobile circulation.

- a. Service, storage, maintenance or trash collection areas should be located out of the view from the public right of way, or significantly screened through landscaping or architectural features.
- b. Service, storage and trash collection areas are not allowed in the setback areas.
- c. The location of delivery, storage and service areas should be clearly marked with signage and should not interfere with other automobile or pedestrian circulation.
- d. Storage and delivery areas should be hard surface, minimizing the dispersal of dust.

B. Building Design

Form and Facade

Goal: To ensure structures of high architectural quality that promote visual interest, thereby supporting district identity.

- a. Blank facades without windows and doors are discouraged. All sides of structures should have architectural treatment. Variety and creativity in building facade is encouraged through changes in building materials, fenestration height, and roof lines, especially on primary facades that face the public right of way.
- b. Window and door styles shall reflect the prevailing architectural style of the structure.
- c. Ground level retail and commercial uses should employ a significant amount of transparent glass in the form of windows and doors, particularly near pedestrian entrances.



Facade articulation and windows with architectural detail add visual interest and break up long expanses of continuous façade.

COMMERCIAL DEVELOPMENT



High quality and durable building materials add aesthetic value and create a more attractive environment.

- d. Minimizing continuous expanses of wall through facade articulation, recession and projection is encouraged.
- e. Structures that are oriented towards the public street are encouraged to provide multiple access points or entrances if the parking area is located to the rear of the structure.
- f. Architects and builders are encouraged to incorporate topographical features into the form of the structure when possible, utilizing natural grades to create unique design.

Building Materials

Goal: To promote quality development through durability and visual aesthetics, thereby supporting district identity.

- a. High-quality and durable materials should be used in street facing facades.
- b. Primary building materials for commercial structures should include brick, finished wood,

stone, cast stone, or pre-cast concrete panels with exposed aggregate, banding, texturing, or other similar decorative finish treatment.

- c. High quality synthetic materials that adequately duplicate natural materials may be acceptable if approved by the City, including, but not limited to, thin brick, hardi plank, decorative concrete masonry, and other materials.
- d. The following building materials are not allowed to be used as the primary facade for commercial development:
 - Unpainted galvanized metal
 - Unfinished “green-treated” lumber
 - Unfinished wood
 - Plain or unpainted concrete
 - Painted concrete block may be used on the rear of the building or sides not visible from the public right of way.
 - Vinyl siding
- e. Facade colors should reflect subtle earth tones. However, other primary facade colors will be considered by the Review Authority. Accent materials should complement the colors of the primary facade.

Scale and Mass

Goal: To establish standards for building with a human scale in mind.

- a. Buildings should be broken down into smaller parts to avoid monotonous or continuous design and the appearance of mass.
- b. Exterior design that provides the appearance of multiple structures is encouraged to reduce scale and minimize mass.
- c. Building mass should be broken up with mul-

tle roof and ridgelines perpendicular with one another.

- d. Structures of two-stories or higher should have articulated facades to minimize the appearance of mass, as well as multiple roof lines with corresponding gables.
- e. Scale should be reduced by utilizing “step-down” methods towards the public street. Entries and other bump out features are effective in this regard.



Parapets of varying heights add architectural interest and accentuate building entries.

Roof Design

Goal: To ensure architectural consideration and consistency in roof design in relation to the structure, and to reduce the visual impact of rooftop equipment.

- a. The design of the roof must be consistent with the overall architecture or design of the structure.
- b. Parapets of varying heights are required for large commercial buildings with flat roofs.
- c. Rooftop equipment, particularly on flat roofs, must be screened by the parapet or other architectural features.

Entries

Goal: To provide identifiable entryways that emphasize access, pedestrian safety, architectural quality and a human scale.

- a. Entryways to commercial structures should be accessible for pedestrians from the public right of way. Large retail sites in particular should consider installing a dedicated pedestrian way.
- b. Architectural features should be incorporated into entryways, such as facade detailing or prominent windows.
- c. The use of canopies, awnings and other sheltering features are encouraged.
- d. Pedestrian amenities such as trash receptacles, benches, or lighted bollards are encouraged near entryways to commercial buildings.

Lighting

Goal: To ensure safety of patrons, employees, pedestrians and automobiles, as well as providing visual interest and aesthetic value to a site, while limiting light pollution of the night sky to the best extent possible.

- a. Lighting must be provided in entryways, parking areas, pedestrian ways, storage and service areas, and other locations that require additional safety lighting.
- b. Lighting height shall be consistent with the City's exterior lighting standards.
- c. Lighting styles should be complementary to the architectural style of the building.
- d. Lighting of architectural features should be used to provide accent and interest, as well as identify the building entryway. Architectural lighting must be downcast and shielded to prevent light pollution.
- e. Bollard lighting is encouraged for pedestrian areas.
- f. Overhead lighting must be shielded to prevent light trespass and spill-over onto adjacent properties.
- g. Commercial uses near residential zones must utilize lighting that minimizes light trespass.
- h. Bare bulb and exposed neon lighting are not allowed.

Signage

Goal: To provide signage that clearly identifies businesses within the Commercial district, while promoting quality and consistency in terms of design and materials.

- a. Building signage should be complementary to the architecture of the structure, as well as consistent with the style of the surrounding buildings or district.
- b. Sign elements that will be evaluated for consistency include scale, color, lighting and materials.
- c. Signs must be constructed of high-quality, durable materials.
- d. Directional signage to delivery, service and storage areas is required.
- e. Two and three-dimensional signs are encouraged to promote creativity and district identity.
- f. All buildings are encouraged to incorporate elements of community theming in appropriate signage, supporting district and city identity.



Signage should complement the architectural style of the building.

Proposed Kwik Trip Location: Site Visit, 7/9/14



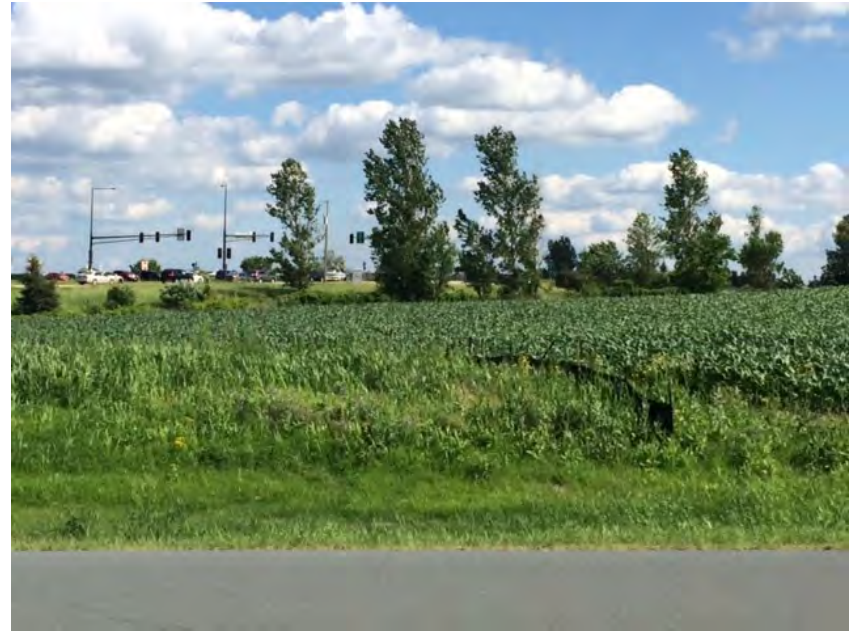
Looking west down Hudson Blvd. towards Keats Ave.



Looking east down Hudson Blvd. towards the subject property



The subject property looking southwest from Hudson Blvd.



The subject property looking south towards I-94 and Keats Ave. N.



PLANNING COMMISSION
DATE: 7/14/14
AGENDA ITEM: 4B – PUBLIC HEARING
CASE # 2014-39

ITEM: RAD-ALT Comprehensive Plan Amendment
SUBMITTED BY: Kyle Klatt, Community Development Director
REVIEWED BY: Nick Johnson, City Planner

SUMMARY AND ACTION REQUESTED:

The Planning Commission has, at multiple times over the past year or so, discuss the City’s rural development areas and made some specific recommendations to the City Council concerning potential revisions to the these rural development areas as described in the Comprehensive Plan. One of the previous recommendations from the Planning Commission was that the City Council adopt a moratorium on any development on properties that are guided for Rural Area Development – Alternative Density (RAD-ALT) in the Comprehensive Plan until the City’s revised 2040 Met Council forecast numbers were finalized. The City Council did not support a moratorium, and instead directed Staff to revisit this matter once the work on the forecast numbers has been completed.

With the Met Council’s adoption of its “Thrive 2040” report, which includes the 2040 population and household forecasts for all metro communities, Staff is asking the Planning Commission to again consider the status of the RAD-ALT future land use category, and to make a recommendation to the City Council concerning this aspect of the Comprehensive Plan. Based on Lake Elmo’s decreased forecast numbers, Staff is recommending that the Planning Commission recommend elimination of the RAD-ALT land use category from the Comprehensive Plan and that all parcels currently guided for RAD-ALT development be changed to RAD.

GENERAL INFORMATION

Applicant: City initiated action
Property Owners: 3M Corporation, Irvin Friedrich, Emerson Properties LP
Location: PID Numbers 16.029.21.24.0002; 15.029.21.31.0001; 25.029.21.44.0001 (commonly referred to as the “3M Property, Irvin Friedrich Property, and Emerson Property”)
Request: Comprehensive Plan Amendment to change the future land use designation of the subject properties from RAD-ALT to RAD
Existing Land Use: Single Family Residential/Agricultural/Agricultural Outbuildings
Existing Zoning: RR – Rural Residential
Surrounding Land Uses: Single Family Residential, Agricultural, Park

Surrounding Zoning: RS – Rural Single Family; RR – Rural Residential; A – Agriculture, OP – Open Space Preservation

Comprehensive Plan: RAD-ALT (Rural Agricultural Density Alternate Density); 2.0 units per acre

Proposed Comp Plan: RAD (Rural Agricultural Density); 0.45 units per acre

History: The City revised its Comprehensive Plan for rural areas in the early-mid 1990's to allow for open space developments. The amendments from this time period limited the use of the Residential Estates as a future land use and instead encouraged any future development of land to be consistent with the City's open space regulations. The RAD-2 category was added to the Plan in 2005 in response to Met Council growth directives. The City changed the future land use designation of the Friedrich property to RAD-2 in 2010. As part of the City's general 2010 Comprehensive Plan updates, the terminology for RAD-2 changed to RAD-ALT (with the same density of 2 units per acre)

Deadline for Action: None

Applicable Regulations: Comprehensive Plan – Chapter III: Land Use Plan
Zoning Ordinance – Article 9: Rural District Standards
Section 154.067 – OP2 Zoning Regulations

REQUEST DETAILS

The Planning Commission has spent a fair amount of time over the past year discussing issues associated with rural development areas. Given the extensive amount of information shared with the Commission related to these previous reviews, Staff will not be providing any new information at this time, and has instead attached these previous reports comprised of the following:

| Report Date | Subject |
|--------------------|--|
| 5/29/13 | General overview and discussion of RAD and RAD-ALT land uses. No recommendation from the Planning Commission. |
| 10/28/13 | Detailed analysis of rural development areas and population projections for Lake Elmo and review of key discussion topics. Planning Commission recommended further study of RAD-ALT and Residential Estates land uses. |
| 11/13/13 | Further analysis of RAD-ALT and RE (Residential Estates) land use categories, with specific options for consideration. Planning Commission recommended moratorium on RAD-ALT development; City Council rejected recommendation. |
| 1/13/14 | Consideration of specific Comprehensive Plan amendment to change the future land use designation of the Friedrich property (9434 Stillwater Boulevard North) from RAD-ALT to RAD. Planning Commission recommended approval of the amendment to change the land use; City Council vote to approve amendment failed. |

During the course of these reviews, it was noted that a key factor in determining the appropriate course of action for any land use changes in the City would be the 2040 Met Council population and household forecasts for the region. In particular, the Planning Commission's original rationale for recommending a moratorium on RAD-ALT development in November of 2013 was to allow time for the forecast numbers to be finalized. The Commission specifically noted during its review that it wanted to make a formal recommendation concerning the RAD-ALT properties once the forecast was finalized. Now that the forecast has been finalized, Staff is bringing this matter back for further review by the Commission.

Staff has scheduled a public hearing on this matter, and provided written notice to the three affected property owners.

BACKGROUND/PLANNING AND ZONING ISSUES

The complete set of 2040 Thrive forecast numbers are attached to this report, along with a summary of the forecast process and results. For the City of Lake Elmo, the previous and revised numbers are as follows:

| Forecast | Population | Households |
|------------|------------|------------|
| 2030 (OLD) | 24,000 | 8,727 |
| 2040 (NEW) | 20,500 | 8,000 |

The 2040 forecast represents a decrease of 3,500 persons and 727 households from the numbers that are current included in the City's Comprehensive Plan. In practice, these revised forecast will be used as a basis for Lake Elmo's next decennial Comprehensive Plan update that must be submitted to the Met Council in 2018. In addition to the revised 2040 forecast, the City and Met Council have also recently negotiated the termination of the Memorandum of Understanding between the two parties, which eliminates any mandates for growth that were previously in place. What this means for Lake Elmo is that the population and household numbers in the current Comprehensive Plan represent a forecast rather than a requirement for growth, and provides the City with some additional flexibility for implanting its plan.

Please note that any future Comprehensive Plan amendments must still be consistent with the City's Systems Statement, which is a document prepared by the Met Council that includes the relevant information that must be addressed in the City decennial Comprehensive Plan updates. The Lake Elmo System Statement from 2008 is still in effect and must still be used for long range planning purposes. Additionally, any future amendments to the City's Land Use Plan are subject to review by the Met Council, and the 2040 forecast numbers are still part of the City's Comprehensive Plan.

Because Lake Elmo has historically been in an unusual situation with the Met Council regarding the City's Comprehensive Plan (compared to other metro communities), the revised forecast will give the City some flexibility in the short term. Any substantial changes to the Comprehensive Plan will be subject to a more extensive review; however, and Staff is not recommending such changes in advance of the 2018 decennial update. Staff anticipates having further discussions with the Planning Commission on minor updates (including additional rural area development options) later this year.

REVIEW AND ANALYSIS

For the reasons noted in greater detail in the attached Staff reports, and based on previous Planning Commission discussions, Staff is recommending that the Planning Commission recommend amending the Comprehensive Plan to eliminate the RAD-ALT land use category. The two key factors in this recommendation include:

- The finalization of the 2040 community forecast for Lake Elmo, which reduces the total population by the City by 3,500 persons.
- The elimination of the MOU between the Met Council and Lake Elmo, which removes any specific population mandates and the related penalties for failure to achieve growth requirements.

With these changes, the pressure to allow additional growth in the City's rural development areas is greatly diminished, and the RAD-ALT areas are no longer necessary to achieve the previous household and populations totals. The City Council has also previously responded to the draft forecast numbers with a specific proposal for recommended growth in the Cities rural and sewer development areas. Although the final forecast numbers are higher than requested by the Council, they still will allow for reductions in the rural areas as proposed by the Council.

There are three parcels affected by the proposed land use change, and approximately 157 acres are currently guided for the RAD-ALT land use, which corresponds to roughly 314 units of housing. The three areas that have been assigned this designation include the following parcels:

| PIN | Owner | Area (acres) |
|-------------------|-----------------|--------------|
| 16.029.21.24.0002 | 3M Company | 96 |
| 15.029.21.31.0001 | Irvin Friedrich | 24 |
| 25.029.21.44.0001 | Terry Emerson | 37 |

In developing more specific findings to support its recommendation, Staff is proposing draft findings in the next section that have been brought forward (with minor modifications) from previous Planning Commission meetings:

DRAFT FINDINGS

- 1) That the Planning Commission has reviewed the Comprehensive Plan Amendment in accordance with the procedures as established by the Lake Elmo Planning Department and Lake Elmo Planning Commission.
- 2) That the Planning Commission conducted a public hearing on July 14, 2014 consistent with these procedures.
- 3) That the proposed amendment is to eliminate the RAD-ALT (Rural Area Development Alternate Density) future land use category from the Future Land Use map and as referenced in other portions of the plan. The amendment will change all parcels that are currently guided as RAD-ALT (2.0 units per acre) to RAD – Rural Agricultural Density (0.45 units per acre).

- 4) That the revised Met Council “Thrive 2040” population and household forecast for the City of Lake Elmo and the elimination of the 2005 MOU between the City and Met Council has reduced the pressure for additional growth and development within the City’s rural development (unsewered) areas.
- 5) That higher density residential development is encouraged in areas that will be served by public sanitary sewer where the provision of these services is more cost-effective and where the City has either invested or is planning to invest in significant infrastructure improvements.
- 6) That the Housing Chapter of the Comprehensive Plan specifically states that any future higher density housing in Lake Elmo, including senior-specific housing, will be best accommodated within the Old Village Area or I-94 corridor due to proximity to goods, services, and public facilities.
- 7) That the three existing parcels guided for RAD-ALT development do not demonstrate any characteristics that are substantially different from other areas guided for RAD development in the City of Lake Elmo or that would indicate that higher density development is more appropriate in this area than any other site within the City.
- 8) That the City is has recently adopted major Comprehensive Plan amendment related to development in the Old Village Area and the I-94 corridor. Given the current market conditions, the City encourages higher density development in areas that would help off-set the significant infrastructure costs required to serve these areas.
- 9) That higher density housing is not consistent with the City’s stated goals to preserve and enhance its rural character, especially when planned in areas that are guided for Rural Agricultural Density.
- 10) That build-out of existing empty lots in platted and developed OP developments is encouraged over the creation of new development and service areas in the community
- 11) That new access that would be needed to support development on the existing RAD-ALT parcels does not conform to the City’s Transportation Plan that encourages limited access to major collector roads and is inconsistent with the City’s access spacing guidelines.
- 12) That the 2010 Comprehensive Plan Amendment for property at 9434 Stillwater Boulevard North was designed to accommodate a specific development proposal which no longer exists.
- 13) That no development proposals have come forward since 2005 for the other two sites that area guided for RAD-ALT development.
- 14) That recent Met Council projections of population and household growth indicate less overall population growth than was expected in 2010.

- 15) That the support of local neighbors for the 2010 development proposal at 9434 Stillwater Boulevard North was based on a misunderstanding of the details of the proposal. The 2010 Planning Commission recommendation was significantly driven by the support of neighbors, which support no longer exists.
- 16) That the Planning Commission and City Council have become more educated and experienced in considering higher density development. Such development should not be considered for land not guided for sewer before 2030.
- 17) That the 2005 Comprehensive Plan that added two sites for RAD-ALT development and the subsequent 2010 action to add another property to this future land use designation could be considered spot zoning based on later information and training received by the Planning Commission. Specifically, the sites guided for RAD-ALT development are isolated and not connected to any other parcel or area that has a similar land use guidance.

RECCOMENDATION:

Staff is recommending that the Planning Commission recommend approval of a Comprehensive Plan amendment to eliminate the RAD-ALT (Rural Area Development Alternate Density) future land use category from the Future Land Use map and as referenced in other portions of the Plan and to change all parcels that are currently guided as RAD-ALT (2.0 units per acre) to RAD – Rural Agricultural Density (0.45 units per acre).

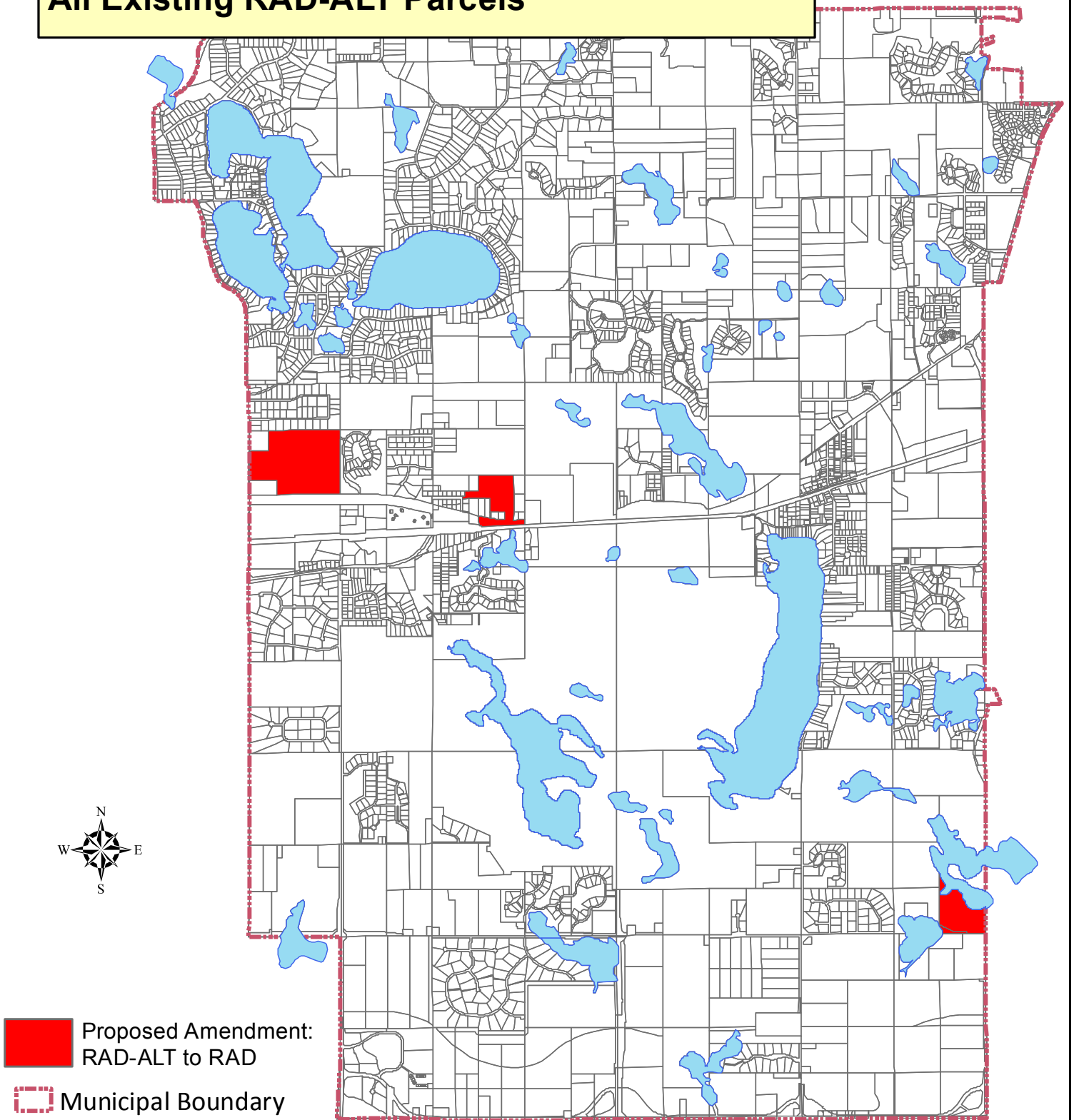
ATTACHMENTS:

- 1. Proposed Map Amendment
- 2. Thrive MSP 2040 Forecasts
- 3. MetroStats Policy Document - Thrive 2040
- 4. Previous Rural Development Review Staff Reports:
 - a. January 13, 2014
 - b. November 13, 2013
 - c. October 28, 2013
 - d. May 29, 2013

ORDER OF BUSINESS:

- IntroductionCommunity Development Director
- Report by StaffCommunity Development Director
- Questions from the Commission Chair & Commission Members
- Public Hearing CommentsChair
- Discussion by the Commission Chair & Commission Members
- Action by the Commission Chair & Commission Members

**Proposed Future Land Use Map Amendments:
All Existing RAD-ALT Parcels**



Sources: Washington County & Metro GIS
12-30-2013

Planned Land Use

Lake Elmo Comprehensive Plan 2030



Thrive MSP 2040 Forecasts
Adopted May 28, 2014



◇ = Rogers annexed Hassan Township in 2012; forecasts have been combined.
† = Laketown Township will be fully annexed before 2030; forecast has been reassigned to neighboring cities.
(pt) denotes part of a city; remainder of city is in neighboring county.

| | POPULATION | | | HOUSEHOLDS | | | EMPLOYMENT | | |
|----------------------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|
| | 2000 | 2010 | 2040 | 2000 | 2010 | 2040 | 2000 | 2010 | 2040 |
| ANOKA COUNTY | | | | | | | | | |
| Andover | 26,588 | 30,598 | 40,700 | 8,107 | 9,811 | 15,400 | 3,583 | 4,669 | 6,200 |
| Anoka | 18,076 | 17,142 | 20,100 | 7,262 | 7,060 | 8,900 | 13,489 | 12,840 | 14,600 |
| Bethel | 443 | 466 | 580 | 149 | 174 | 250 | 229 | 86 | 530 |
| Blaine (pt) | 45,014 | 57,186 | 86,000 | 15,926 | 21,077 | 33,000 | 16,757 | 19,668 | 26,600 |
| Centerville | 3,202 | 3,792 | 4,200 | 1,077 | 1,315 | 1,700 | 363 | 409 | 500 |
| Circle Pines | 4,663 | 4,918 | 5,300 | 1,697 | 2,006 | 2,300 | 2,150 | 790 | 1,450 |
| Columbia Heights | 18,520 | 19,496 | 21,700 | 8,033 | 7,926 | 9,300 | 6,397 | 3,484 | 5,300 |
| Columbus | 3,957 | 3,914 | 5,300 | 1,328 | 1,416 | 2,200 | 507 | 1,172 | 1,850 |
| Coon Rapids | 61,607 | 61,476 | 72,500 | 22,578 | 23,532 | 29,300 | 21,682 | 23,260 | 35,700 |
| East Bethel | 10,941 | 11,626 | 18,200 | 3,607 | 4,060 | 7,400 | 1,374 | 1,123 | 2,200 |
| Fridley | 27,449 | 27,208 | 29,400 | 11,328 | 11,110 | 12,800 | 26,257 | 21,333 | 29,800 |
| Ham Lake | 12,710 | 15,296 | 17,300 | 4,139 | 5,171 | 7,100 | 3,194 | 2,931 | 4,480 |
| Hilltop | 766 | 744 | 1,100 | 400 | 380 | 550 | 257 | 314 | 360 |
| Lexington | 2,142 | 2,049 | 2,300 | 819 | 787 | 1,000 | 634 | 467 | 700 |
| Lino Lakes | 16,791 | 20,216 | 29,000 | 4,857 | 6,174 | 10,600 | 2,671 | 3,313 | 6,000 |
| Linwood Township | 4,668 | 5,123 | 4,700 | 1,578 | 1,884 | 2,000 | 154 | 219 | 430 |
| Nowthen | 3,557 | 4,443 | 5,400 | 1,123 | 1,450 | 2,100 | 337 | 318 | 720 |
| Oak Grove | 6,903 | 8,031 | 10,200 | 2,200 | 2,744 | 4,100 | 359 | 741 | 1,010 |
| Ramsey | 18,510 | 23,668 | 32,800 | 5,906 | 8,033 | 13,000 | 4,008 | 4,779 | 7,600 |
| St. Francis | 4,910 | 7,218 | 12,500 | 1,638 | 2,520 | 5,100 | 1,247 | 1,537 | 3,000 |
| Spring Lake Park (pt) | 6,667 | 6,234 | 6,800 | 2,676 | 2,597 | 3,100 | 4,401 | 2,934 | 3,670 |
| Anoka County Total | 298,084 | 330,844 | 426,080 | 106,428 | 121,227 | 171,200 | 110,050 | 106,387 | 152,700 |
| CARVER COUNTY | | | | | | | | | |
| Benton Township | 939 | 786 | 740 | 307 | 297 | 300 | 282 | 274 | 350 |
| Camden Township | 955 | 922 | 830 | 316 | 329 | 330 | 15 | 56 | 60 |
| Carver | 1,266 | 3,724 | 15,000 | 458 | 1,182 | 5,600 | 176 | 187 | 1,700 |
| Chanhassen (pt) | 20,321 | 22,952 | 36,200 | 6,914 | 8,352 | 14,000 | 8,366 | 9,746 | 16,240 |
| Chaska | 17,603 | 23,770 | 34,900 | 6,169 | 8,816 | 14,200 | 10,955 | 11,123 | 16,800 |
| Cologne | 1,012 | 1,519 | 4,600 | 385 | 539 | 1,900 | 294 | 270 | 470 |
| Dahlgren Township | 1,453 | 1,331 | 720 | 479 | 494 | 300 | 203 | 202 | 200 |
| Hamburg | 538 | 513 | 600 | 206 | 201 | 250 | 117 | 109 | 150 |
| Hancock Township | 367 | 345 | 410 | 121 | 127 | 170 | 35 | 10 | 10 |
| Hollywood Township | 1,102 | 1,041 | 1,200 | 371 | 387 | 500 | 100 | 90 | 150 |
| Laketown Township † | 2,331 | 2,243 | - | 637 | 660 | - | 355 | 116 | - |
| Mayer | 554 | 1,749 | 3,000 | 199 | 589 | 1,200 | 92 | 151 | 200 |
| New Germany | 346 | 372 | 1,400 | 143 | 146 | 600 | 50 | 46 | 90 |
| Norwood Young America | 3,108 | 3,549 | 8,800 | 1,171 | 1,389 | 3,900 | 1,559 | 1,165 | 2,300 |
| San Francisco Township | 888 | 832 | 960 | 293 | 307 | 400 | 61 | 46 | 100 |
| Victoria | 4,025 | 7,345 | 15,000 | 1,367 | 2,435 | 5,700 | 932 | 1,502 | 2,270 |
| Waconia | 6,814 | 10,697 | 22,100 | 2,568 | 3,909 | 8,900 | 4,082 | 5,578 | 10,200 |
| Waconia Township | 1,284 | 1,228 | 1,500 | 429 | 434 | 600 | 72 | 98 | 400 |
| Watertown | 3,029 | 4,205 | 6,700 | 1,078 | 1,564 | 2,900 | 682 | 556 | 1,220 |
| Watertown Township | 1,432 | 1,204 | 1,100 | 478 | 468 | 500 | 207 | 392 | 400 |
| Young America Township | 838 | 715 | 760 | 267 | 266 | 300 | 105 | 119 | 120 |
| Carver County Total | 70,205 | 91,042 | 156,520 | 24,356 | 32,891 | 62,550 | 28,740 | 31,836 | 53,430 |

| | POPULATION | | | HOUSEHOLDS | | | EMPLOYMENT | | |
|----------------------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|
| | 2000 | 2010 | 2040 | 2000 | 2010 | 2040 | 2000 | 2010 | 2040 |
| DAKOTA COUNTY | | | | | | | | | |
| Apple Valley | 45,527 | 49,084 | 65,600 | 16,344 | 18,875 | 26,500 | 12,106 | 14,279 | 19,500 |
| Burnsville | 60,220 | 60,306 | 66,700 | 23,687 | 24,283 | 27,700 | 31,765 | 31,593 | 44,100 |
| Castle Rock Township | 1,495 | 1,342 | 1,300 | 514 | 504 | 520 | 344 | 356 | 360 |
| Coates | 163 | 161 | 150 | 64 | 66 | 70 | 252 | 109 | 110 |
| Douglas Township | 760 | 716 | 790 | 235 | 259 | 320 | 96 | 92 | 100 |
| Eagan | 63,557 | 64,206 | 79,000 | 23,773 | 25,249 | 31,500 | 42,750 | 49,526 | 70,200 |
| Empire Township | 1,638 | 2,444 | 5,300 | 515 | 792 | 2,000 | 217 | 255 | 300 |
| Eureka Township | 1,490 | 1,426 | 1,700 | 496 | 518 | 700 | 196 | 460 | 460 |
| Farmington | 12,365 | 21,086 | 31,500 | 4,169 | 7,066 | 12,000 | 3,986 | 4,438 | 7,200 |
| Greenvale Township | 684 | 803 | 890 | 227 | 275 | 350 | 68 | 49 | 630 |
| Hampton | 434 | 689 | 780 | 156 | 245 | 300 | 178 | 127 | 160 |
| Hampton Township | 986 | 903 | 1,100 | 320 | 329 | 450 | 186 | 85 | 90 |
| Hastings (pt) | 18,201 | 22,172 | 30,100 | 6,640 | 8,735 | 12,900 | 8,872 | 8,532 | 11,300 |
| Inver Grove Heights | 29,751 | 33,880 | 47,600 | 11,257 | 13,476 | 19,900 | 8,168 | 9,442 | 14,000 |
| Lakeville | 43,128 | 55,954 | 82,900 | 13,609 | 18,683 | 30,500 | 10,966 | 13,862 | 23,400 |
| Lilydale | 552 | 623 | 1,000 | 338 | 375 | 600 | 354 | 355 | 420 |
| Marshan Township | 1,263 | 1,106 | 1,300 | 404 | 403 | 520 | 220 | 117 | 370 |
| Mendota | 197 | 198 | 320 | 80 | 78 | 130 | 266 | 270 | 300 |
| Mendota Heights | 11,434 | 11,071 | 13,400 | 4,178 | 4,378 | 5,300 | 8,549 | 11,550 | 14,400 |
| Miesville | 135 | 125 | 130 | 52 | 52 | 60 | 97 | 116 | 140 |
| New Trier | 116 | 112 | 150 | 31 | 41 | 50 | 30 | 35 | 60 |
| Nininger Township | 865 | 950 | 950 | 280 | 372 | 400 | 165 | 149 | 340 |
| Northfield (pt) | 557 | 1,147 | 2,100 | 216 | 414 | 900 | 79 | 470 | 500 |
| Randolph | 318 | 436 | 440 | 117 | 168 | 180 | 123 | 122 | 130 |
| Randolph Township | 536 | 659 | 650 | 192 | 246 | 280 | 130 | 113 | 120 |
| Ravenna Township | 2,355 | 2,336 | 2,500 | 734 | 780 | 1,000 | 115 | 38 | 60 |
| Rosemount | 14,619 | 21,874 | 36,300 | 4,742 | 7,587 | 14,000 | 6,356 | 6,721 | 13,900 |
| Sciota Township | 285 | 414 | 500 | 92 | 140 | 190 | 21 | 33 | 500 |
| South St. Paul | 20,167 | 20,160 | 22,500 | 8,123 | 8,186 | 9,600 | 7,697 | 8,557 | 11,000 |
| Sunfish Lake | 504 | 521 | 520 | 173 | 183 | 210 | 23 | 8 | 10 |
| Vermillion | 437 | 419 | 410 | 160 | 156 | 170 | 221 | 93 | 210 |
| Vermillion Township | 1,243 | 1,192 | 1,700 | 395 | 424 | 660 | 280 | 90 | 90 |
| Waterford Township | 517 | 497 | 560 | 193 | 193 | 240 | 461 | 679 | 820 |
| West St. Paul | 19,405 | 19,540 | 23,900 | 8,645 | 8,529 | 10,500 | 8,905 | 7,471 | 10,600 |
| Dakota County Total | 355,904 | 398,552 | 524,740 | 131,151 | 152,060 | 210,700 | 154,242 | 170,192 | 245,880 |
| HENNEPIN COUNTY | | | | | | | | | |
| Bloomington | 85,172 | 82,893 | 93,600 | 36,400 | 35,905 | 41,000 | 104,548 | 86,530 | 111,000 |
| Brooklyn Center | 29,172 | 30,104 | 34,700 | 11,430 | 10,756 | 13,600 | 16,698 | 11,001 | 15,400 |
| Brooklyn Park | 67,388 | 75,781 | 95,500 | 24,432 | 26,229 | 35,500 | 23,692 | 24,084 | 42,000 |
| Champlin | 22,193 | 23,089 | 25,500 | 7,425 | 8,328 | 10,000 | 2,734 | 4,012 | 5,600 |
| Chanhassen (pt) | - | - | - | - | - | - | 979 | 1,159 | 1,160 |
| Corcoran | 5,630 | 5,379 | 11,900 | 1,784 | 1,867 | 4,700 | 1,792 | 1,093 | 2,200 |
| Crystal | 22,698 | 22,151 | 23,300 | 9,389 | 9,183 | 10,000 | 5,638 | 3,929 | 5,500 |
| Dayton (pt) | 4,693 | 4,617 | 10,600 | 1,546 | 1,619 | 4,500 | 1,057 | 921 | 3,000 |
| Deephaven | 3,853 | 3,642 | 3,900 | 1,373 | 1,337 | 1,400 | 1,021 | 688 | 820 |
| Eden Prairie | 54,901 | 60,797 | 84,800 | 20,457 | 23,930 | 34,000 | 51,006 | 48,775 | 70,000 |
| Edina | 47,425 | 47,941 | 53,300 | 20,996 | 20,672 | 23,000 | 52,991 | 47,457 | 56,100 |
| Excelsior | 2,393 | 2,188 | 2,600 | 1,199 | 1,115 | 1,300 | 1,823 | 2,220 | 2,200 |
| Fort Snelling (unorg.) | 442 | 149 | 350 | - | 135 | 200 | 35,526 | 23,215 | 26,000 |
| Golden Valley | 20,281 | 20,371 | 24,300 | 8,449 | 8,816 | 10,300 | 30,142 | 33,194 | 41,500 |
| Greenfield | 2,544 | 2,777 | 4,100 | 817 | 936 | 1,600 | 337 | 613 | 750 |
| Greenwood | 729 | 688 | 810 | 285 | 290 | 300 | 161 | 82 | 350 |
| Hanover (pt) | 332 | 609 | 520 | 113 | 196 | 200 | 86 | 36 | 50 |
| Hopkins | 17,367 | 17,591 | 21,600 | 8,359 | 8,366 | 10,000 | 11,979 | 11,009 | 14,000 |

| | POPULATION | | | HOUSEHOLDS | | | EMPLOYMENT | | |
|------------------------------|------------------|------------------|------------------|----------------|----------------|----------------|----------------|----------------|------------------|
| | 2000 | 2010 | 2040 | 2000 | 2010 | 2040 | 2000 | 2010 | 2040 |
| Independence | 3,236 | 3,504 | 5,400 | 1,088 | 1,241 | 2,200 | 169 | 587 | 770 |
| Long Lake | 1,842 | 1,768 | 2,100 | 756 | 732 | 1,000 | 2,510 | 1,093 | 1,930 |
| Loretto | 570 | 650 | 670 | 225 | 269 | 300 | 661 | 366 | 370 |
| Maple Grove | 50,365 | 61,567 | 84,800 | 17,532 | 22,867 | 33,000 | 18,309 | 29,877 | 49,500 |
| Maple Plain | 2,088 | 1,768 | 2,300 | 770 | 723 | 1,000 | 1,792 | 1,579 | 1,750 |
| Medicine Lake | 368 | 371 | 400 | 159 | 160 | 170 | 10 | 15 | 100 |
| Medina | 4,005 | 4,892 | 9,000 | 1,309 | 1,702 | 3,500 | 3,254 | 3,351 | 4,580 |
| Minneapolis | 382,747 | 382,578 | 466,400 | 162,352 | 163,540 | 202,700 | 308,127 | 281,732 | 356,000 |
| Minnetonka | 51,102 | 49,734 | 63,000 | 21,267 | 21,901 | 27,500 | 51,276 | 44,228 | 63,200 |
| Minnetonka Beach | 614 | 539 | 610 | 215 | 201 | 220 | 201 | 174 | 250 |
| Minnetrista | 4,358 | 6,384 | 13,000 | 1,505 | 2,176 | 5,000 | 379 | 665 | 740 |
| Mound | 9,435 | 9,052 | 10,500 | 3,982 | 3,974 | 4,800 | 1,811 | 1,165 | 1,900 |
| New Hope | 20,873 | 20,339 | 22,800 | 8,665 | 8,427 | 9,800 | 13,565 | 11,080 | 15,300 |
| Orono | 7,538 | 7,437 | 9,400 | 2,766 | 2,826 | 3,900 | 1,110 | 1,562 | 1,780 |
| Osseo | 2,434 | 2,430 | 3,100 | 1,035 | 1,128 | 1,500 | 2,312 | 1,749 | 2,530 |
| Plymouth | 65,894 | 70,576 | 87,800 | 24,820 | 28,663 | 35,500 | 53,491 | 46,227 | 66,500 |
| Richfield | 34,310 | 35,228 | 39,900 | 15,073 | 14,818 | 17,500 | 11,762 | 15,604 | 18,400 |
| Robbinsdale | 14,123 | 13,953 | 15,300 | 6,097 | 6,032 | 6,800 | 7,109 | 6,858 | 7,600 |
| Rockford (pt) | 144 | 426 | 800 | 57 | 184 | 400 | 384 | 94 | 550 |
| Rogers ◇ | 6,051 | 11,197 | 21,300 | 1,973 | 3,748 | 8,200 | 5,414 | 7,907 | 14,800 |
| St. Anthony (pt) | 5,664 | 5,156 | 6,300 | 2,402 | 2,210 | 3,000 | 1,992 | 1,626 | 2,090 |
| St. Bonifacius | 1,873 | 2,283 | 2,200 | 681 | 863 | 900 | 436 | 478 | 500 |
| St. Louis Park | 44,102 | 45,250 | 54,500 | 20,773 | 21,743 | 25,500 | 40,696 | 40,485 | 49,100 |
| Shorewood | 7,400 | 7,307 | 7,400 | 2,529 | 2,658 | 3,000 | 782 | 1,113 | 1,200 |
| Spring Park | 1,717 | 1,669 | 2,200 | 930 | 897 | 1,100 | 1,028 | 583 | 700 |
| Tonka Bay | 1,547 | 1,475 | 1,500 | 614 | 586 | 680 | 266 | 298 | 570 |
| Wayzata | 4,113 | 3,688 | 4,900 | 1,929 | 1,795 | 2,300 | 6,268 | 4,567 | 5,900 |
| Woodland | 480 | 437 | 540 | 173 | 169 | 180 | 22 | 8 | 20 |
| Hennepin County Total | 1,116,206 | 1,152,425 | 1,429,500 | 456,131 | 475,913 | 603,250 | 877,346 | 805,089 | 1,066,260 |
| RAMSEY COUNTY | | | | | | | | | |
| Arden Hills | 9,652 | 9,552 | 13,500 | 2,959 | 2,957 | 4,600 | 12,326 | 12,402 | 18,400 |
| Blaine (pt) | - | - | - | - | - | - | 677 | 893 | 1,000 |
| Falcon Heights | 5,572 | 5,321 | 5,300 | 2,103 | 2,131 | 2,200 | 4,190 | 5,298 | 6,800 |
| Gem Lake | 419 | 393 | 590 | 139 | 155 | 250 | 586 | 526 | 640 |
| Lauderdale | 2,364 | 2,379 | 2,400 | 1,150 | 1,130 | 1,200 | 360 | 718 | 1,000 |
| Little Canada | 9,771 | 9,773 | 11,100 | 4,375 | 4,393 | 4,900 | 5,960 | 5,467 | 8,700 |
| Maplewood | 35,258 | 38,018 | 47,900 | 13,758 | 14,882 | 19,700 | 29,259 | 27,635 | 36,600 |
| Mounds View | 12,738 | 12,155 | 13,100 | 5,018 | 4,954 | 5,500 | 4,170 | 6,386 | 8,200 |
| New Brighton | 22,206 | 21,456 | 26,000 | 9,013 | 8,915 | 11,200 | 11,007 | 9,213 | 13,500 |
| North Oaks | 3,883 | 4,469 | 4,900 | 1,300 | 1,746 | 2,100 | 1,091 | 1,260 | 1,300 |
| North St. Paul | 11,929 | 11,460 | 13,100 | 4,703 | 4,615 | 5,700 | 3,499 | 2,942 | 3,610 |
| Roseville | 33,690 | 33,660 | 38,700 | 14,598 | 14,623 | 17,000 | 39,211 | 35,104 | 44,100 |
| St. Anthony (pt) | 2,348 | 3,070 | 4,300 | 1,295 | 1,638 | 2,000 | 1,390 | 1,357 | 2,050 |
| St. Paul | 286,840 | 285,068 | 334,700 | 112,109 | 111,001 | 137,600 | 188,124 | 175,933 | 218,000 |
| Shoreview | 25,924 | 25,043 | 27,500 | 10,125 | 10,402 | 11,300 | 9,938 | 11,665 | 15,500 |
| Spring Lake Park (pt) | 105 | 178 | 220 | 48 | 75 | 100 | 202 | 66 | 100 |
| Vadnais Heights | 13,069 | 12,302 | 14,500 | 5,064 | 5,066 | 6,300 | 7,164 | 6,678 | 12,600 |
| White Bear Township | 11,293 | 10,949 | 12,000 | 4,010 | 4,261 | 4,900 | 2,131 | 2,309 | 4,780 |
| White Bear Lake (pt) | 23,974 | 23,394 | 27,500 | 9,469 | 9,747 | 12,000 | 12,020 | 11,085 | 11,800 |
| Ramsey County Total | 511,035 | 508,640 | 597,310 | 201,236 | 202,691 | 248,550 | 333,305 | 316,937 | 408,680 |

| | POPULATION | | | HOUSEHOLDS | | | EMPLOYMENT | | |
|--------------------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|------------------|
| | 2000 | 2010 | 2040 | 2000 | 2010 | 2040 | 2000 | 2010 | 2040 |
| SCOTT COUNTY | | | | | | | | | |
| Belle Plaine | 3,789 | 6,661 | 13,300 | 1,396 | 2,362 | 5,300 | 1,428 | 1,847 | 3,300 |
| Belle Plaine Township | 806 | 878 | 820 | 266 | 310 | 320 | 77 | 69 | 70 |
| Blakeley Township | 496 | 418 | 370 | 166 | 165 | 170 | 70 | 69 | 100 |
| Cedar Lake Township | 2,197 | 2,779 | 3,600 | 719 | 939 | 1,400 | 91 | 82 | 340 |
| Credit River Township | 3,895 | 5,096 | 5,000 | 1,242 | 1,662 | 1,900 | 265 | 397 | 420 |
| Elko New Market | 804 | 4,110 | 12,200 | 286 | 1,259 | 4,500 | 248 | 317 | 840 |
| Helena Township | 1,440 | 1,648 | 1,700 | 450 | 548 | 700 | 473 | 147 | 150 |
| Jackson Township | 1,361 | 1,464 | 1,300 | 461 | 486 | 500 | 92 | 168 | 530 |
| Jordan | 3,833 | 5,470 | 10,700 | 1,349 | 1,871 | 4,300 | 1,321 | 1,587 | 2,900 |
| Louisville Township | 1,359 | 1,266 | 1,200 | 410 | 425 | 430 | 476 | 298 | 300 |
| New Market Township | 3,057 | 3,440 | 3,300 | 956 | 1,146 | 1,200 | 262 | 325 | 500 |
| New Prague (pt) | 3,157 | 4,280 | 7,200 | 1,160 | 1,618 | 3,100 | 2,282 | 2,142 | 3,270 |
| Prior Lake | 15,917 | 22,796 | 39,300 | 5,645 | 8,447 | 15,700 | 7,972 | 7,766 | 12,900 |
| St. Lawrence Township | 472 | 483 | 810 | 144 | 161 | 320 | 145 | 48 | 50 |
| Sand Creek Township | 1,551 | 1,521 | 1,400 | 478 | 554 | 560 | 249 | 298 | 460 |
| Savage | 21,115 | 26,911 | 38,200 | 6,807 | 9,116 | 14,300 | 5,366 | 6,753 | 9,500 |
| Shakopee | 20,568 | 37,076 | 57,400 | 7,540 | 12,772 | 21,500 | 13,938 | 18,831 | 31,900 |
| Spring Lake Township | 3,681 | 3,631 | 4,100 | 1,217 | 1,267 | 1,600 | 176 | 390 | 400 |
| Scott County Total | 89,498 | 129,928 | 201,900 | 30,692 | 45,108 | 77,800 | 34,931 | 41,534 | 67,930 |
| WASHINGTON COUNTY | | | | | | | | | |
| Afton | 2,839 | 2,886 | 3,100 | 996 | 1,081 | 1,300 | 351 | 411 | 490 |
| Bayport | 3,162 | 3,471 | 4,400 | 763 | 855 | 1,300 | 4,900 | 3,790 | 5,100 |
| Baytown Township | 1,533 | 1,617 | 2,000 | 492 | 573 | 760 | 154 | 69 | 260 |
| Birchwood Village | 968 | 870 | 840 | 357 | 351 | 360 | 20 | 25 | 30 |
| Cottage Grove | 30,582 | 34,589 | 49,300 | 9,932 | 11,719 | 18,600 | 6,263 | 6,484 | 9,600 |
| Dellwood | 1,033 | 1,065 | 1,100 | 353 | 373 | 450 | 282 | 277 | 300 |
| Denmark Township | 1,348 | 1,737 | 2,500 | 481 | 615 | 1,000 | 386 | 629 | 650 |
| Forest Lake | 14,440 | 18,377 | 28,300 | 5,433 | 7,015 | 12,000 | 6,636 | 6,449 | 9,700 |
| Grant | 4,026 | 4,094 | 4,300 | 1,374 | 1,463 | 1,700 | 750 | 449 | 840 |
| Grey Cloud Island Township | 307 | 295 | 280 | 117 | 117 | 120 | 50 | 10 | 40 |
| Hastings (pt) | 3 | - | - | 2 | - | - | 224 | 64 | 100 |
| Hugo | 6,363 | 13,332 | 32,500 | 2,125 | 4,990 | 13,600 | 1,917 | 1,973 | 4,000 |
| Lake Elmo | 6,863 | 8,061 | 20,500 | 2,347 | 2,776 | 8,000 | 1,682 | 1,941 | 3,160 |
| Lakeland | 1,917 | 1,796 | 1,500 | 691 | 681 | 710 | 374 | 302 | 470 |
| Lakeland Shores | 355 | 311 | 360 | 116 | 117 | 160 | 20 | 26 | 40 |
| Lake St. Croix Beach | 1,140 | 1,053 | 1,000 | 462 | 460 | 500 | 50 | 129 | 130 |
| Landfall | 700 | 663 | 770 | 292 | 257 | 300 | 50 | 25 | 30 |
| Mahtomedi | 7,563 | 7,676 | 7,700 | 2,503 | 2,827 | 3,100 | 1,252 | 2,090 | 2,660 |
| Marine on St. Croix | 602 | 689 | 1,000 | 254 | 302 | 450 | 235 | 124 | 160 |
| May Township | 2,928 | 2,776 | 3,800 | 1,007 | 1,083 | 1,600 | 40 | 66 | 180 |
| Newport | 3,715 | 3,435 | 4,600 | 1,418 | 1,354 | 2,100 | 2,480 | 1,605 | 2,000 |
| Oakdale | 26,653 | 27,401 | 31,000 | 10,243 | 10,956 | 13,000 | 7,812 | 8,651 | 15,000 |
| Oak Park Heights | 3,777 | 4,445 | 5,800 | 1,528 | 1,911 | 2,600 | 2,713 | 4,358 | 7,500 |
| Pine Springs | 421 | 408 | 370 | 140 | 144 | 150 | 10 | 72 | 80 |
| St. Marys Point | 344 | 366 | 330 | 132 | 147 | 150 | 10 | 15 | 20 |
| St. Paul Park | 5,070 | 5,273 | 7,900 | 1,829 | 1,967 | 3,300 | 1,399 | 1,515 | 2,520 |
| Scandia | 3,692 | 3,934 | 5,000 | 1,294 | 1,498 | 2,100 | 272 | 519 | 730 |
| Stillwater | 15,323 | 18,227 | 22,500 | 5,797 | 7,076 | 9,500 | 10,719 | 9,628 | 11,700 |
| Stillwater Township | 2,553 | 2,364 | 2,700 | 833 | 855 | 1,100 | 120 | 165 | 250 |
| West Lakeland Township | 3,547 | 4,054 | 4,000 | 1,101 | 1,286 | 1,500 | 313 | 232 | 370 |
| White Bear Lake (pt) | 351 | 403 | 680 | 149 | 198 | 300 | 131 | 184 | 200 |
| Willernie | 549 | 507 | 480 | 225 | 218 | 230 | 135 | 182 | 200 |
| Woodbury | 46,463 | 61,961 | 87,200 | 16,676 | 22,594 | 33,100 | 15,899 | 19,438 | 28,700 |
| Washington County Total | 201,130 | 238,136 | 337,810 | 71,462 | 87,859 | 135,140 | 67,649 | 71,897 | 107,210 |
| METRO AREA | 2,642,062 | 2,849,567 | 3,673,860 | 1,021,456 | 1,117,749 | 1,509,190 | 1,606,263 | 1,543,872 | 2,102,090 |

The Metropolitan Council forecasts population, households and employment, with a 30-year horizon, for the seven-county Minneapolis-St. Paul region. The Council assesses the Twin Cities region's relative economic competitiveness and projects future population and employment using a regional economic model, REMI PI.

The Council locates regional forecasts to specific cities and townships through additional modeling. The Council's land use model projects the likely geographic pattern of future growth, given real estate and location choice dynamics, regional policies and local land use controls.

The regional forecast, together with local forecasts, will be incorporated into the *Thrive MSP 2040* regional plan, scheduled for Council adoption in May 2014.

Consistent with Minnesota Statutes 473.146 and 473.859, the regional and local forecasts provide a shared foundation for coordinated, comprehensive planning by the Council and local governments.

See *About the Forecasts* on p. 5.

For more information, contact:
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651-602-1322

The Metropolitan Council's forecast anticipates continued growth and increased diversity for the seven-county Minneapolis-St Paul region. The region's population is projected to grow by 824,000 in coming decades. By 2040, people of color will comprise 40 percent of the region; senior citizens, 21 percent.

Metropolitan Council Regional Forecast to 2040

| | 2010 | 2020 | 2030 | 2040 |
|------------|-----------|-----------|-----------|-----------|
| Population | 2,850,000 | 3,102,000 | 3,381,000 | 3,674,000 |
| Households | 1,118,000 | 1,257,000 | 1,388,000 | 1,509,000 |
| Employment | 1,548,000 | 1,819,000 | 1,953,000 | 2,097,000 |

The seven-county Minneapolis-St. Paul region is projected to gain 824,000 people in coming decades, reaching 3,674,000 residents in 2040, up from 2,850,000 in 2010. Projected growth rates, 9 percent per decade, are below peak growth rates seen in the 1980s and the 1990s – but above-average compared to national projections for the same period.

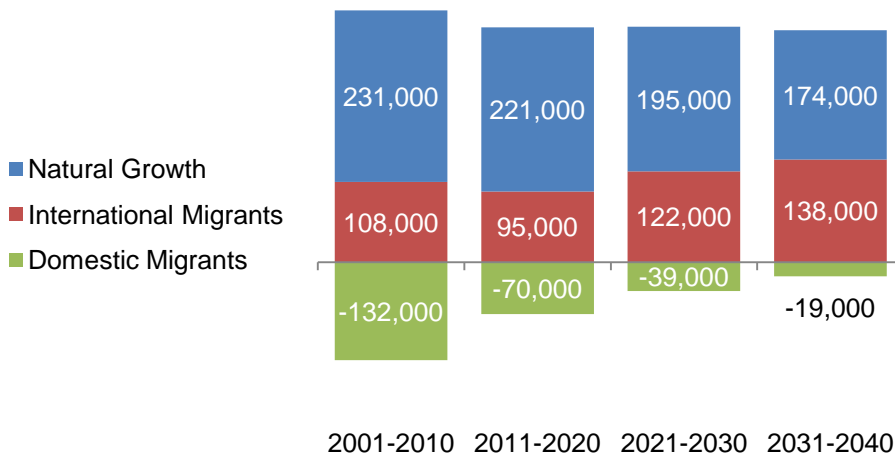
Natural population growth, or births outpacing deaths, will add 590,000 residents. Natural population growth will account for 72 percent of the total population growth from 2010 to 2040. Birth rates are higher among families of color than white families, contributing steadily and gradually to the racial and ethnic diversity of the region.

More than one-quarter (28 percent) of the region's population gain will be driven by migration. The Twin Cities region is likely to gain 355,000 new residents through international immigration while losing 128,000 residents to domestic out-migration, for a net migration gain of 227,000 during the 30-years forecast period.

The Twin Cities region will continue to be an immigration gateway throughout the 30-year period, and immigration will substantially accelerate the region's diversity trend. Of the expected international immigrants, 85 percent will be people of color, from all continents; 15 percent will be white, non-Latino, mainly from Europe and Canada.

A Growing and Changing Twin Cities Region: Regional Forecast to 2040

Components of Population Growth, 2000-2040



Net domestic migration – the flow of movers between the Twin Cities region and the rest of the nation – amounts to a net loss of 128,000 residents during 2010-40. This is not a new trend. US Census data shows emigrants leaving the region have outnumbered new domestic arrivals throughout the past decade.

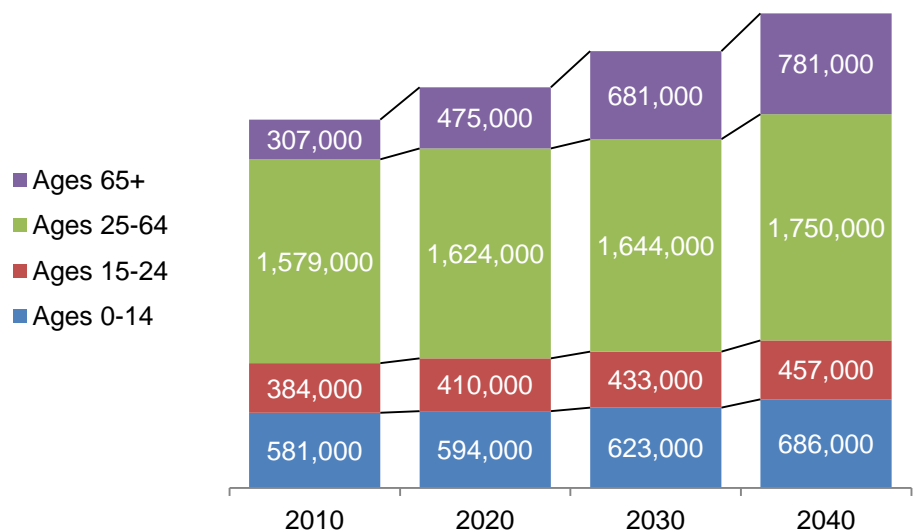
This net loss is directly related to economic conditions: The regional economy did not gain employment during the decade ending in 2010. An improving economy will steer this trend over time. Domestic migration will respond to new workforce demand.

Still, geographic situation and perception of the Twin Cities region are challenges to attracting new residents. While employment and business opportunities draw in workers and students, the region loses people who have priorities beyond work and school. For example, many long-time residents have family or sentimental connections with Greater Minnesota; much of the Twin Cities’ domestic migration deficit is due to returns or relocations to the rest of the state.

Immigration and natural population growth together will replenish the Twin Cities region’s school enrollments and workforce. The region’s under-25 population will grow 18 percent, from 965,000 in 2010 to 1,143,000 in 2040. This will partly offset the protracted retirement boom that is expected to last for the next 20 years.

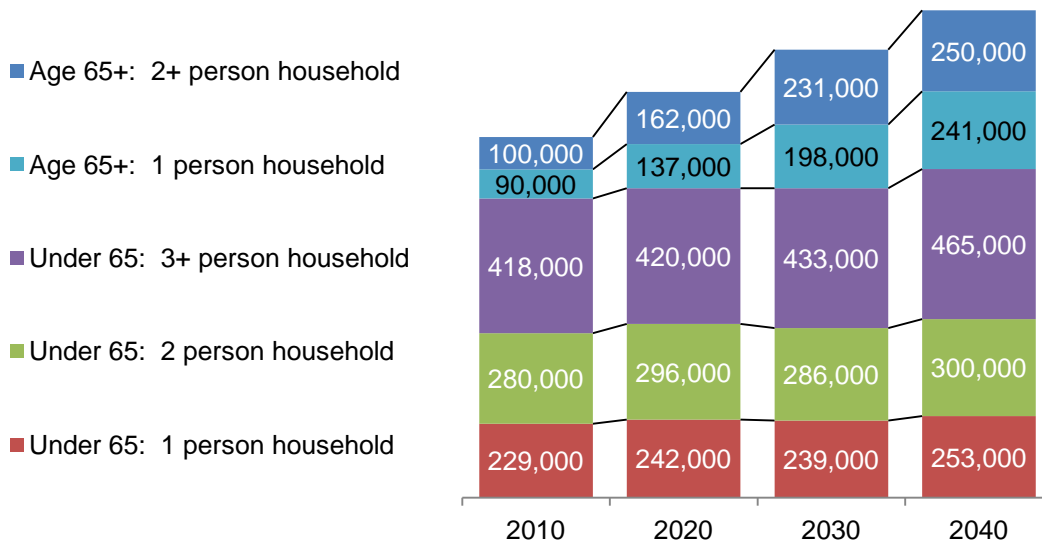
While every age cohort will grow, none will grow as fast as the senior citizens population. Twin Cities region’s senior population will double between 2010 and 2030, and will continue to grow throughout the projections period, from 307,000 seniors in 2010 to 781,000 in 2040. Between the aging of baby boomers and longer life expectancies, senior citizens will become a substantial market segment. In 2010, senior citizens were 11 percent of the region’s population; in 2040, seniors will be 21 percent of the population.

Population by Age, 2010-2040



A Growing and Changing Twin Cities Region: Regional Forecast to 2040

Households by Type, 2010-2040



As the region’s age profile changes, its households mix also changes: The senior citizens population will more than double by 2030; so too will the number of senior-headed households.

With seniors making decisions for 32 percent of region’s households in 2040, their needs and preferences will become a predominant driver of the housing market. Twin Cities region’s seniors are mostly long-time residents, and mostly have lived in single-family

detached housing. Some may choose to “age in place.” But almost half of senior-headed households are people living alone. Many seniors living alone, and senior couples, will choose to move, opting for attached housing, apartments, and age-restricted housing options. As they move, the turnover of single-family detached houses will balloon, offsetting the need for greater supply of single-family detached housing.

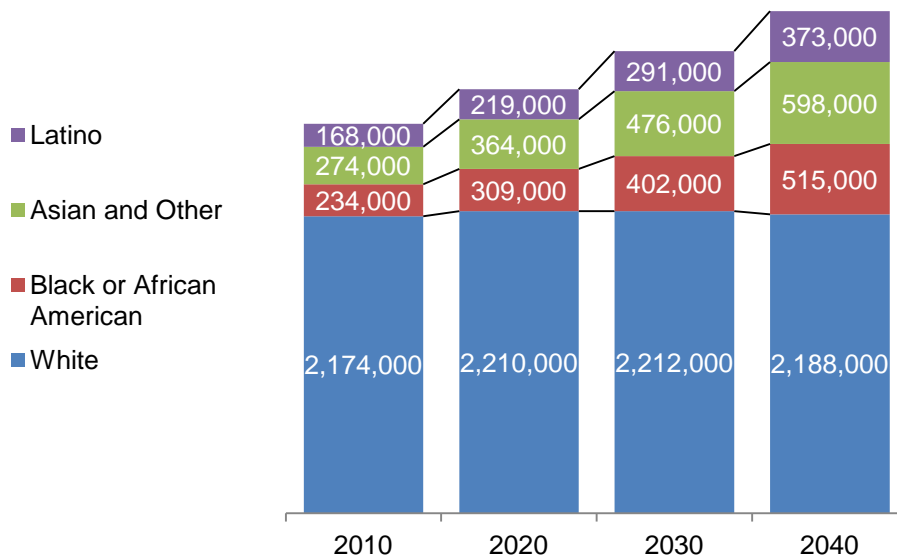
The Council forecasts 1,509,000 households in 2040, up 35 percent from the 1,118,000 households counted by Census 2010. The overall gains, and the net additional housing implied, are in line with historical trends. But the balance of market demand will be very different from recent history. The number of senior-headed households will grow by 200,000, driven upward by the aging and longevity of baby boomers. As baby boomers leave the ranks of family-age or working-age households, those market segments will grow at much reduced rates. The number of family-age or working-age households with 2 or more people will grow by only 67,000 – just 17 percent of expected households growth.

Between the churn of migration and higher birth rates among Latino, black, and Asian populations, the Twin Cities region will become more racially and ethnically diverse. In 2010, people of color comprised 24 percent of the regional population. By 2040, people of color will be 40 percent of the region. And the workforce of 2040 will reflect the diversity seen today in the region’s elementary schools.

The population of color will more than double during the forecast period, from 676,000 in 2010 to 1,486,000 in 2040, while the white population will peak and level off at its current level. Among people of color, each racial or ethnic group is forecast to more than double. The Latino population will increase from 168,000 in 2010 to 373,000 in 2040. The black population will increase from 234,000 in 2010 to 515,000 in 2040. And the population of Asians and other people of color will increase from 274,000 in 2010 to 598,000 in 2040.

A Growing and Changing Twin Cities Region: Regional Forecast to 2040

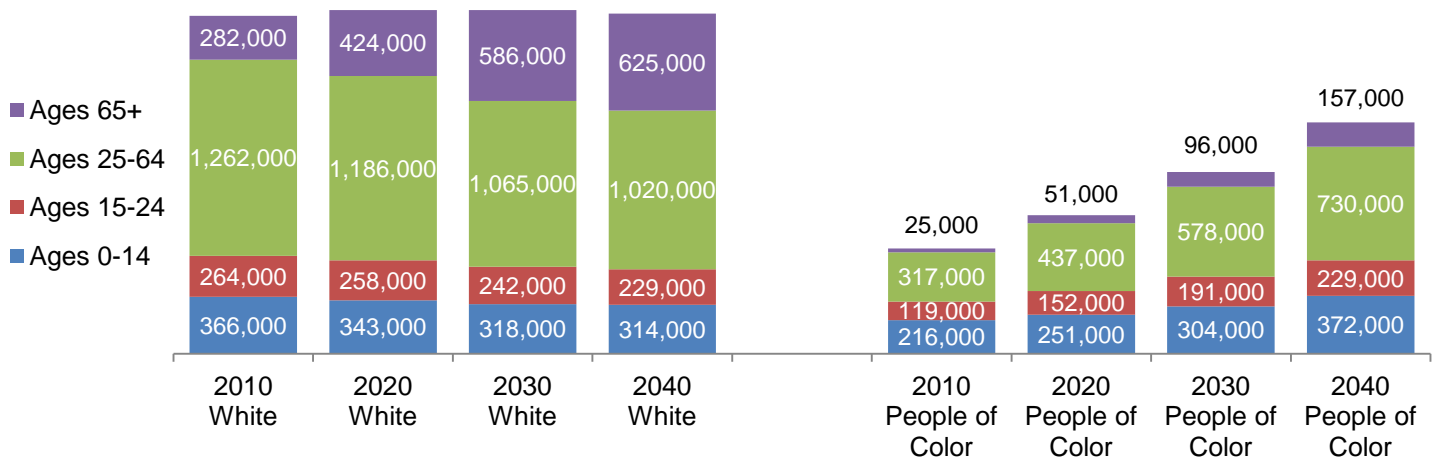
Population by Race and Ethnicity, 2010-2040



The Council’s population forecasts reveal contrasting trends in the age distributions of white residents and people of color. This will have significant implications for the future workforce of the region.

The number of white residents, ages 25-64, will shrink by 19 percent, from 1,262,000 in 2010 to 1,020,000 in 2040. Concurrently, the working-age population of color will more than double, from 317,000 in 2010 to 730,000 in 2040. Within the workforce, diversity will grow. By 2040, people of color will comprise 42 percent of working-age residents.

Population by Race / Ethnicity and Age



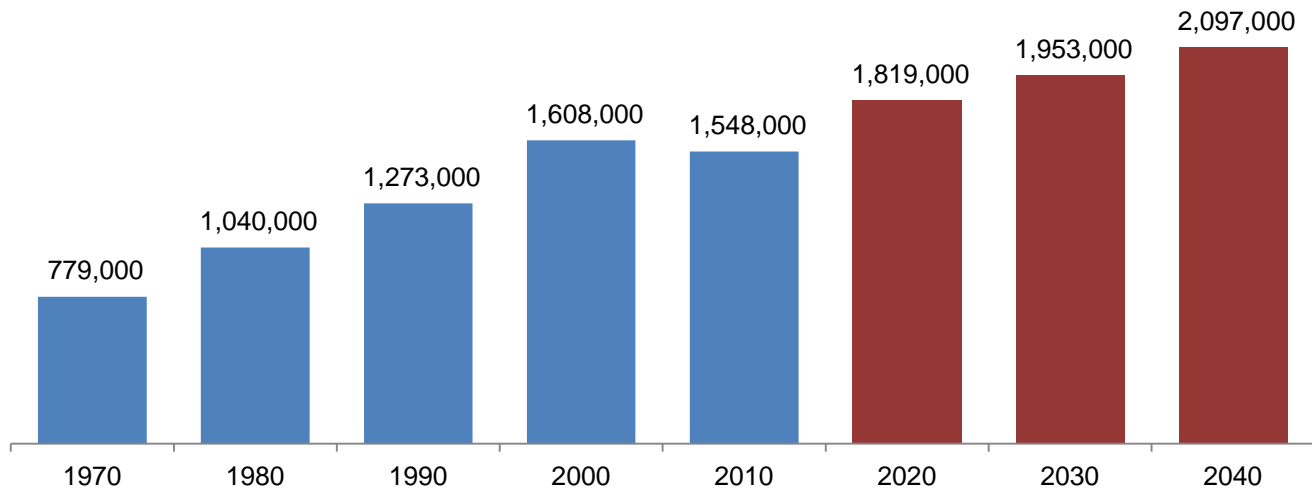
Similarly, the Council projects an increasingly diverse student body in the region. The population of color under age 25 will grow by 180 percent, from 335,000 in 2010 to 601,000 in 2040. In contrast, the number of white residents under age 25 will fall from 630,000 in 2010 to 543,000 in 2040, pulling down the share of whites among school-age children and young adults.

Migration is the major factor driving this demographic transition. People moving from the Twin Cities region to other parts of Minnesota or the nation are mostly white and older (retirees). In contrast, the region’s gain of international immigrants is predominantly people of color, mostly people in their 20s, and often immigrating with children.

A Growing and Changing Twin Cities Region: Regional Forecast to 2040

The Council forecasts employment growth of 549,000 jobs, up from 1,548,000 in 2010 to 2,097,000 in 2040. Employment will grow rapidly in the short-term (18 percent growth in the 2010s) and at a slower pace (7 percent growth) in the 2020s and 2030s. This growth compares to a net employment loss during the 2000s, and previous gains of 34 percent in the 1970s, 22 percent in the 1980s, and 26 percent in the 1990s.

Historic and Forecasted Employment



The region's Gross Metro Product, the sum of value added by all industry sectors, will rise to \$409 billion in 2040 – equivalent to 1.5 percent of the US Gross Domestic Product. For context, the Twin Cities region has less than 1.0 percent of the nation's population.

Employment opportunities in the Twin Cities region attract not only new migrants, but also commuting workers living in Greater Minnesota or Wisconsin. In 2010, the Council estimates that 6.8 percent of earnings at Twin Cities workplaces are earned by external commuters. (This is offset by 1.3 percent of residents' earnings coming from work in Greater Minnesota, Wisconsin, or elsewhere.) This balance of long-distance commuters, and earnings returning with to their places of residence, will persist long-term, as the Twin Cities region remains the predominant economic center for Minnesota and western Wisconsin.

About the Council's Forecasts

To prepare its long-range forecast, the Metropolitan Council uses REMI PI, a regional economic model. The REMI PI model utilizes computable general equilibrium and new economic geography techniques to project forward time-series of economic and demographic outcomes. The REMI PI projections are informed by data on the region's industry mix, costs and productivity, and analysis of regional competitiveness within the national economy. Employment, migration, and population outcomes directly flow from projected economic performance.

To obtain household counts, the REMI PI population projection is parsed into household types using race-specific, age-specific household formation rates from analysis of Census data.



PLANNING COMMISSION
DATE: 1/13/14
AGENDA ITEM: 5A – PUBLIC HEARING
CASE # 2014-01

ITEM: Friedrich Property Comprehensive Plan Amendment
SUBMITTED BY: Kyle Klatt, Community Development Director
REVIEWED BY: Nick Johnson, City Planner

SUMMARY AND ACTION REQUESTED:

The Planning Commission has been directed by the City Council to conduct a public hearing to consider a Comprehensive Plan Amendment that would revise the future land use map for property at 9434 Stillwater Boulevard North. The Council has specifically asked that the Planning Commission consider changing the future land use designation of this property from RAD-ALT (Rural Area Development Alternate Density) to RAD (Rural Area Development). The primary difference between these two land use categories is that the RAD-ALT designation would allow residential densities on the site of up to 2.0 units per acre while the RAD designation would limit this number to 0.45 units per acre.

Because the Planning Commission has spent a significant amount of time over the last two months reviewing and discussing the City's rural development areas in a fair amount of detail, Staff will be providing a more basic summary of the City's past action on this property within the present report and will ask that the Commission refer to the previous meeting packets for additional information concerning the history and current issues associated with the rural development areas in the community.

GENERAL INFORMATION

Applicant: City Council initiated action
Property Owners: Irvin Friedrich, 9434 Stillwater Boulevard North
Location: 9434 Stillwater Boulevard North (part of Section 15 Township 029 Range 021).
PID Number 15.029.21.31.0001
Request: Comprehensive Plan Amendment to change the future land use designation of the subject property from RAD-ALT to RAD
Existing Land Use: Single Family Residential/Agricultural/Agricultural Outbuildings
Existing Zoning: RR – Rural Residential
Surrounding Land Use: Single Family Residential, Agricultural, Park
Surrounding Zoning: RS – Rural Single Family; RR – Rural Residential; A – Agriculture
Comprehensive Plan: RAD-ALT (Rural Agricultural Density Alternate Density); 2.0 units per acre

Proposed Comp Plan: RAD (Rural Agricultural Density); 0.45 units per acre

History: The subject property has been used as an agricultural farmstead for decades. The future land use designation for the property was changed in 2010 in response to a request for a senior living/farm school development proposal that has since been rendered void due to the passing of City submittal deadlines for further review. There are no active development proposals pending on the property. Upon receipt of a Planning Commission recommendation to establish a moratorium on all RAD-ALT properties, the City Council did not adopt such a moratorium and instead directed the Commission to consider a land use change to the subject property.

Deadline for Action: None

Applicable Regulations: Comprehensive Plan – Chapter III: Land Use Plan
Zoning Ordinance – Article 9: Rural District Standards
Section 154.067 – OP2 Zoning Regulations

REQUEST DETAILS

The Planning Commission has devoted time at two of its recent meetings to discuss the City's rural development areas, and has asked that these conversations continue as part of the Commission's work plan for 2014. The Commission will specifically be looking at potential changes to the Comprehensive Plan that will study the following issues:

- Potential population and household forecast reductions associated with the Met Council's revised 2040 regional forecasts.
- Elimination of the RAD-ALT future land use category.
- Amendments to the rural development areas that will allow for additional development opportunities on parcels less than 40 acres in size.
- Consideration of the benefits and drawbacks of allowing additional development in rural areas in terms of public service costs, impacts on rural character, and other factors.

In anticipation of potential changes to the City's Comprehensive Plan related to the above discussions, the Planning Commission recommended that the City Council adopt a moratorium on all RAD-ALT development for a period of nine months. This time period was chosen to allow time for the 2040 Regional Forecast (and related projections for cities within the metro area) to be finalized before the City made any long-term decisions concerning the population and household numbers used in the land use plan. The City Council ultimately did not support the recommended moratorium concerning RAD-ALT development, but did ask the Planning Commission to consider a more immediate change on one of the City's properties that is guided in this manner.

The specific request from the City Council was that the Commission consider a Comprehensive Plan Amendment to change the future land use designation of the property at 9434 Stillwater Boulevard North from RAD-ALT to RAD. The Council's rationale for choosing only this parcel for a change includes the following:

- The City created the RAD-ALT (which was initially called RAD-2) land use category as part of the 2005 Comprehensive Plan amendment after an extensive study and review of the City's obligations under the Memorandum of Understanding between the City and Met Council.

- Based on an analysis of these obligations, it was decided that the City would need to increase densities in the rural development areas in order to achieve the household and population requirements by 2030.
- The sites chosen for the increased density (RAD-2 land use classification) were selected for specific reasons, including proximately to sewer development, isolation from other rural parcels, and locations along municipal boundaries or adjacent sewer communities.
- The land use designation for the subject parcel at 9434 Stillwater Boulevard was changed based on a specific development proposal that has not materialized and is no longer valid.

Under state statutes, the City Council cannot take action to amend the Comprehensive Plan without a recommendation from the Planning Commission and until after a public hearing has been conducted as part of the Commission's review. The Commission is therefore being asked to conduct this public hearing at its next meeting to gather additional feedback on this matter and to make a formal recommendation to the City Council concerning the appropriate land use designation for the subject property.

BACKGROUND/PLANNING AND ZONING ISSUES

The City received a land use application from Tammy Malmquist, 8549 Ironwood Trail in the spring of 2010 to change the future land use designation of the Friedrich property at 9434 Stillwater Boulevard North from RAD to RAD-2 (which is the same as the current RAD-ALT land use category). This application was made in order to allow a concurrent request for a senior living, townhouse, and farm-based preschool Planned Unit Development project to move forward. The applicant further requested an amendment to the City's zoning regulation in order to create new OP-2 Open Space Preservation Zoning regulations that was also drafted in order support the proposed development.

In the period of time since the Comprehensive Plan amendment for the subject property was approved, the project applicant has not submitted a preliminary development plan and the timeframe for submitting such a plan has expired. There is therefore no pending application moving forward on this property, and any previous or new request for development would need to go back through a concept plan review (with a new application, hearing, etc.).

In order to aid the Planning Commission's deliberation on the current Comprehensive Plan Amendment under review, Staff has attached the previous Staff Report that was prepared for the City Council when the 2010 request was initially reviewed by the City. Although over three years has passed since this review was conducted by the City, the information specific to the Comprehensive Plan and the applicant's site is still relevant and should be useful for the Planning Commission to consider with the present request. Please note the following important points from this report:

- The City split up its review of the Comprehensive Plan Amendment and project development (PUD) applications and dealt with these matters at separate meetings.
- Planning Staff recommended denial of the Comprehensive Plan Amendment for reasons that are spelled out in greater detail in the report.
- The Planning Commission unanimously (all nine members at the time) recommended approval of the request to the City Council.
- The City Council, after several meetings and a workshop session, voted 4-1 to approve the Comprehensive Plan amendment.

Since 2010, the City has adopted two major amendments to the Comprehensive Plan and adopted a new future land use map for the entire community. There were no changes made, however, to any of the land uses outside of the Village Planning Area and I-94 Corridor Planning Area with the exception of minor corrections and adjustments to fit the new map.

REVIEW AND ANALYSIS

Because this matter is being directed to the Planning Commission from the City Council, Staff will not be presenting a formal recommendation to the Commission other than to review the findings that have been previously proposed or adopted. The Commission should consider its recent discussions concerning rural area development as part of its review, and Staff would be happy to make any of these past reports available to individual Commissioners (these reports are also available on the City's website).

Staff would like to suggest the following parameters as the Commission deliberates on the present request:

1. The Planning Commission has been asked to consider a specific Comprehensive Plan Amendment for a specific property in the community.
2. A public hearing notice has been mailed to all properties within 350 feet of the subject parcel and this hearing has been scheduled for January 13, 2014.
3. Comments and discussion concerning a specific development are not appropriate and are irrelevant to the Council's specific request.
4. Discussion and comments concerning general land uses are appropriate, especially those that relate to uses and densities allowed under the RAD-ALT land use designation.
5. The zoning that correlates to the RAD-ALT category (OP-2 zoning regulations) allows senior living multi-family buildings, townhouses, and single family residential structures at a density of up to 2 dwelling units per acre. The RAD category and corresponding OP zoning allow single family homes and a very limited number of townhouses at a density of 0.45 units per acre (18 houses per 40 acres).
6. The Council is seeking public feedback on the proposed change and will be considering this feedback along with the Planning Commission's recommendation prior to taking action on the proposed amendment.

Using the previous staff recommendation and City Council action as a guide, the Planning Commission should consider the following potential findings as the basis for a recommendation to the City Council. All of these findings are based on the information that was presented or drafted during the 2010 City reviews:

Findings that Support No Change to the Comprehensive Plan (Leave Subject Property as RAD):

- 1) The current use of this site as a working farm is unique compared to other properties designated for RAD development in the Comprehensive Plan.
- 2) The proposed amendment will help provide opportunities for senior housing within the community.
- 3) Recent subdivisions in areas guided for RAD development have been approved at a density below the unit levels anticipated in the Comprehensive Plan. The proposed density increase will be offset by reductions that have previously been approved or acknowledged by the City.

- 4) The subject site is located in close proximity to public transportation along State Highway 5, and specifically, a bus route that could provide alternate transportation options for a residential development.
- 5) The applicant's site is located immediately adjacent to existing R-1 Single Family Residential zoning districts along its southern, eastern, and western boundaries. Other areas guided for RAD development are primarily surrounded by rural residential, agricultural, or public open space uses.

Findings that Support a Comprehensive Plan Amendment (Change Subject Property from RAD-ALT to RAD):

- 1) There have been no changes in circumstances since the Land Use Section of the Comprehensive Plan was updated in 2006 that warrant revisions to increase or transfer density to the subject site.
- 2) Higher density residential development is encouraged in areas that will be served by public sanitary sewer where the provision of these services is more cost-effective and where the City will receive credit towards the REC unit counts mandated under its Memorandum of Understanding with the Metropolitan Council.
- 3) The Housing Chapter of the Comprehensive Plan specifically states that any future senior-specific housing in Lake Elmo will be best accommodated within the Old Village Area due to proximity to goods, services, and public facilities.
- 4) The subject site does not demonstrate any characteristics that are substantially different from other areas guided for RAD development in the City of Lake Elmo or that would indicate that higher density development is more appropriate in this area than any other site within the City.
- 5) The City has recently adopted major Comprehensive Plan amendment related to development in the Old Village Area and the I-94 corridor. Given the current market conditions, the City encourages higher density development in areas that would help off-set the significant infrastructure costs required to serve these areas.
- 6) Higher density housing is not consistent with the City's stated goals to preserve and enhance its rural character, especially when planned in areas that are guided for Rural Agricultural Density.
- 7) Build-out of existing empty lots in platted and developed OP developments is encouraged over the creation of new development and service areas in the community
- 8) New access that would be needed to support development on the subject site does not conform to the City's Transportation Plan that encourages limited access to major collector roads and is inconsistent with the City's access spacing guidelines.

DRAFT FINDINGS

Please refer to the comments in the previous section.

RECCOMENDATION:

Staff is recommending that the Planning Commission review the materials present above, attached to this report, and previously considered by the Commission during its deliberations regarding the City’s rural development areas and make a recommendation to the City Council to either:

- a) approve an amendment to the Lake Elmo Comprehensive Plan to change the future land use designation of property at 9434 Stillwater Boulevard North from RAD-ALT to RAD based on the “Findings that Support a Comprehensive Plan Amendment” as presented above or as otherwise modified by the Planning Commission; or
- b) make no changes to the Lake Elmo Comprehensive Plan based on the “Findings that Support No Change to the Comprehensive Plan” as presented above or as otherwise modified by the Planning Commission.

ATTACHMENTS:

- 1. Staff Report – 5/4/10 City Council Meeting
- 2. Location Map
- 3. Proposed Map Amendment

ORDER OF BUSINESS:

- Introduction.....Community Development Director
- Report by Staff.....Community Development Director
- Questions from the Commission..... Chair & Commission Members
- Public Hearing CommentsChair
- Discussion by the Commission Chair & Commission Members
- Action by the Commission..... Chair & Commission Members



PLANNING COMMISSION
DATE: 11/13/13
AGENDA ITEM: 5A – BUSINESS ITEM
CASE # 2013-036

ITEM: Rural Area Development Analysis and Discussion – Further Review
SUBMITTED BY: Kyle Klatt, Community Development Director
REVIEWED BY: Nick Johnson, City Planner

SUMMARY AND ACTION REQUESTED:

The Planning Commission is being asked to continue its discussion from its October 28, 2013 meeting concerning the City's rural development areas. The Commission is specifically asked to further review the RAD-ALT land use category and to conduct an analysis of the potential to expand the use of Residential Estates zoning in the future. Staff is therefore seeking a recommendation from the Commission on the following:

1. Whether or not to continue guiding areas in the RAD-ALT land use category as part of the City's Future Land Use Map; and
2. Whether or not the City should pursue Comprehensive Plan and zoning amendments that would either expand the use of the Residential Estates land use category or revise the rural development standards in some other manner in order to allow additional development opportunities on parcels less than 40 acres.

The first inquiry may be acted upon with an amendment to the City's Future Land Use Map in the Comprehensive Plan while the latter issue will require further review and discussion by the Planning Commission should the Commission want to pursue such changes.

GENERAL INFORMATION

Applicant: City-initiated action for discussion
Request: Continue previous review and discussion of land use plans and policies concerning Rural Development Areas
History: The City revised its Comprehensive Plan for rural areas in the early-mid 1990's to allow for open space developments. The amendments from this time period limited the use of the Residential Estates as a future land use and instead encouraged any future development of land to be consistent with the City's open space regulations. The RAD-2 category was added to the Plan in 2005 in response to Met Council growth directives.
Deadline for Action: None
Applicable Regulations: Comprehensive Plan – Chapter III: Land Use Plan
Zoning Ordinance – Article 9: Rural District Standards

REVIEW AND ANALYSIS

After considering the history of the City's rural development areas and reviewing recent population projections with Staff, the Planning Commission is being asked to further discuss two aspects of this discussion from its October 28, 2013 meeting as described above. Rather than repeating the information presented at the last meeting, Staff has attached the previous Staff report for further review and discussion by the Commission. The two questions that are now being carried forward for further discussion include the future application of the RAD-ALT land use category and the potential for additional development opportunities within smaller parcels in the City's rural areas.

RAD-ALT LAND USE CATEGORY

The City of Lake Elmo currently guides 157 acres in the RAD-ALT land use category, which corresponds to roughly 314 units of housing. The three areas that have been assigned this designation include the following parcels:

| PIN | Owner | Area (acres) |
|-------------------|-----------------|--------------|
| 16.029.21.24.0002 | 3M Company | 96 |
| 15.029.21.31.0001 | Irvin Friedrich | 24 |
| 25.029.21.44.0001 | Terry Emerson | 37 |

The 3M and Emerson properties were changed to the RAD-ALT category as part of the 2005 Comprehensive Plan amendment, while the Friedrich parcel was changed to this category as part of a stand-alone amendment approved by the City in 2010. There were no corresponding reductions or revisions elsewhere in the code with the 2010 amendment, and at that time, the City found that the affected area was small enough that it would not significantly alter any of City's household projections. The Council has previously discussed the idea of implementing a development rights transfer program in order to allow transfers of density between RAD and RAD-ALT development sites, but has not taken any action since 2010 to create such a program.

As the Planning Commission considers the future status of the RAD-ALT land use category, Staff would like to point out the following:

- To date, there have been no developments approved in the City's RAD-ALT areas, and only one proposal for RAD-ALT development has been brought forward at a conceptual level.
- The concept plan noted above was for a 52-unit senior living and farm-based preschool proposal that is no longer a valid development application. The time limit for the submission of a preliminary plan for the site has expired. No new development plans for this property have come forward, although the former applicant has approached Staff recently to discuss an alternative concept plan.
- The Friedrich family does not support any changes to the future land use for their property that would lower the current density of the site.
- As part of its discussions with the Met Council concerning the recently released preliminary population and household forecasts for Lake Elmo, the City will be seeking reductions in these numbers when the forecast is finalized. As presented in Staff's previous report to the City Council, the proposed reductions would allow the City to lower the number of

households that are expected in rural development areas. Should the Met Council accept these numbers, the RAD-ALT category would not be needed to meet the old forecasts. The City may decide to keep this land use classification for other reasons (i.e. to encourage a wider mix of uses in rural development areas).

During previous discussions concerning the RAD-ALT district and based on the City's review of previous open space projects, the following questions and concerns have been raised

- The RAD-ALT category allows for 2 units of housing per acre, which is a density that is much closer to an urban or suburban form of development than a rural type of development. For comparison purposes, the single family areas within the future Lennar development are slightly under 3 units per acre on a net density basis, while the Carriage Station subdivision was platted to meet a density of 2 units per acre.
- The use of well and septic systems in RAD-ALT areas presents challenges for developers because there is so little area to work with once the homes, roads, and other infrastructure is taken into consideration. While more homes mean a more reasonable allocation of costs for shared systems, more users also require a much larger and more costly system as well.
- The Planning Commission has previously discussed the potential to serve RAD-ALT areas with public sanitary sewer. Given the relatively small differences in density between RAD-ALT and the low density urban areas, it might be better to simply guide these sites for sewer residential densities if the City wants to support more development outside of the existing sewer service area boundaries.
- Some of the areas that are guided for RAD-ALT development are located adjacent to general RAD areas or other existing lower density residential neighborhoods. The compatibility between these areas has been a concern for the neighbors surrounding the Friedrich property.
- There is also the question of whether or not it is truly feasible to develop a project under the RAD-ALT ordinance given the current requirement to preserve half of a site as open space. Acknowledging the downturn in the economy that has affected the housing market since the late 2000's, there have been no RAD-ALT projects approved by the City (and only one that even made it to a concept stage) in the last eight years.

Staff is recommending that the Planning Commission further review and discuss the City's RAD-ALT land use classification at its next meeting and provide direction to Staff should any the Comprehensive Plan changes be deemed necessary by the Commission. The options that should be considered by the City include the following:

- 1) Eliminate the RAD-ALT land use category from the Comprehensive Plan and change all areas presently guided in this manner to RAD.
- 2) Eliminate some of the RAD-ALT areas and leave only those areas as RAD-ALT that the Planning Commission believes should be guided in this manner.
- 3) Wait to make any changes to the RAD and RAD-ALT land use classifications until after the Met Council has finalized the City's 2014 forecast. This action is expected to occur sometime in the spring of 2014.

- 4) Do not take any action at this time to make changes to the Comprehensive Plan.
- 5) Expand the areas that are guided for RAD-ALT in the Comprehensive Plan or move the current areas guided for such density to other parcels.
- 6) Consider revisions to the RAD-ALT land use in order to address concerns regarding compatibility between uses.
- 7) Consider other revisions to the RAD-ALT land use to either expand the uses allowed within these areas or to further revise the regulations to promote certain types of activities (i.e. to allow certain types of housing or only low traffic-generating activities).

Of these options, Staff is recommending that the Planning Commission follow number (3) and take this issue up as part of a broader Comprehensive Plan discussion once the 2014 forecast is finalized. In the meantime, the Commission should be aware that taking this course of action would allow developments to come forward under the present guidelines, but any such projects would be subject to new public hearings and would require review by the Planning Commission. Staff does not recommend a moratorium on RAD-ALT development because the MOU between the City and Met Council is still in effect. The City should continue to work towards implementation of the current plan as long as the MOU remains in effect.

GENERAL RURAL DEVELOPMENT REGULATIONS

One of the Commission's discussion items from the last meeting also included the City's rural development areas in general, and in particular, how to best plan for the future use of parcels that are under 40 acres in size. The City's current open space ordinance allows for OP developments on parcels that are 40 acres or more in size, but would only allow such development on smaller parcels through an exception process. In practice there have only been a few OP developments that have been created on properties with less than 40 acres. Under current zoning regulations, parcels that are less than 40 acres and zoned RR – Rural Residential could be split into lots no smaller than 10 acres, while parcels zoned A – Agriculture could not be further subdivided.

The Commission may also want to further discuss the RED (Residential Estates) land use category to assess whether or not this land use could be expanded into new areas in order to provide alternative development options on smaller parcels. At present, the City's Comprehensive Plan does not identify any new areas for RED development outside of existing developments or areas that were planned for such land use prior to the 2005 land use plan. The Staff comments below concerning residential development on smaller rural parcels take into account an expansion of the RED classification.

In order to provide the Planning Commission with a better perspective on the remaining undeveloped land in the City's rural development areas, Staff will be bringing a report with a summary of the lot sizes in these areas for discussion at the meeting.

Some facts that should be considered by the Commission as it discusses this item include the following:

- There have been around 20 OP developments approved and constructed over the past 20 years in Lake Elmo. Some of these developments have been recognized nationally for best practices in conservation-based subdivisions.

- There have been no new OP developments approved by the City within since 2007. This is due partly to the downturn in the economy.
- At present, there are roughly 40-50 vacant lots available within OP developments. This number continues to drop by 20-30 lots each year, meaning the current supply of OP lots will last no more than 2 years without additional subdivisions coming forward.
- The City has seen several large lot subdivision created in the last several years (10 acre lots) that have removed land from potential development under OP regulations.
- Staff has observed a fairly healthy market for lots within RS – Rural Single Family areas, and periodically older, existing homes are razed to make way for new, larger structures within these areas. The significant number of lake-frontage lots in the Tri-Lakes area will continue to be a factor in the demand for redevelopment of existing lots.
- The City has made recent agreements to extend public sewer service into a small rural single family area on the west side of Lake Olson and has agreed to extend sewer into at least one open space development outside of the Village. Staff expects pressure to provide sewer service to the Tri-Lakes area and to open space developments that are located close to the urban service areas will be one of the more important land use decisions that should be addressed in the next major Comprehensive Plan update.
- The City has rejected proposals in the past to split land in RAD areas into parcels less than 10 acres. Staff has found that it is very difficult for potential applicants to meet all of the City's variance criterion for these types of and use applications.

Should the Planning Commission and City Council decide to pursue changes to the minimum lot sizes allowed in rural development areas or to expand the use of the Residential Estates land use to new developments, Staff would like to offer the following as general comments:

- Maintaining an adequate amount of road frontage for every platted lot will be very problematic for most parcels that are less than 40 acres in size. The City does allow one parcel to be split without road frontage in rural development areas, but this often leads to situations in which a driveway is either shared by two parties or a driveway easement crosses someone else's land. This type of situation may be acceptable when there are over 20 acres to work with, but could become problematic on smaller lots.
- The cost of servicing developments with lots that are larger than $\frac{1}{4}$ to $\frac{1}{2}$ of an acre in size is much higher than in developments with smaller and/or clustered lots. Even in situations in which sewer and water are installed on an each individual lot, the City must still provide roads, storm water improvements, fire protection, and other services that are now spread across a greater area.
- As lots become smaller, it is more difficult to find suitable area for adequate on-site septic systems. Smaller lots also provide less land that could be used to address failing systems.
- The platting of lots less than 10 acres in size would eliminate large areas of open space that are protected by the current minimum lot area requirements. One of the foremost goals in the

City Comprehensive Plan is the preservation and open space and rural character. The platting of lots of less than ten acres in size may not help the City achieve these objectives.

- Further subdivision of lots in rural areas into parcels of 2 to 5 acres in size would create an environment in these areas that is much more suburban than rural in character. With additional homes the City can expect to see additional traffic, more buildings, fewer agricultural parcels, and less vegetation than presently exists in these areas.

Because the Planning Commission has only recently completed its work on major Comprehensive Plan amendments for the City's future sewer service areas, the Commission may want to consider looking at options for updating the Comprehensive Plan and ordinances concerning rural development areas. Staff would recommend that any such work, if the Planning Commission finds that the City should study this issue further, be considered as part of the work plan for 2014.

To help the Planning Commission with its discussion on this topic, Staff has developed the following options that could be considered for further study:

- 1) Revise the Zoning Ordinance to allow OP developments on parcels of less than 40 acres in size. At one time the minimum lot size for an OP project was 20 acres; however, this provision was changed in order to encourage the preservation of larger open space areas throughout the City. The previous Staff analysis that was shared with the Planning Commission noted that this course of action would be needed in order to meet the City's 2030 growth forecasts. A revised 2040 forecast would greatly reduce the need to change the current OP ordinance minimum lot area requirement.
- 2) Change the minimum lot areas requirements in the City's A and RR zoning districts to allow smaller parcels to be created in these areas. For example, the City could reduce the minimum lot area in RR zones to 5 acres and A zones to 20 acres. A change in the minimum lot area may require the City to reconsider how it manages road frontage and lot ratio requirements in these zoning districts.
- 3) Expand the use of the Residential Estates classification to areas that are not currently guided for this type of density. Consistent with the Staff comments above, the City's RED developments have a much different look and feel than the City's OP developments, even though the OP developments allow for more homes. The Planning Commission should take this into consideration if it would like to pursue this type of land use change.
- 4) Create a new land use category that would allow for limited development of parcels less than 40 acres in size while still adhering to the basic principles for an open space development. A new land use category could potentially allow for clustering of development on smaller lots provided the undeveloped portions of a site are either protected or retained under common ownership. Staff suggests that a new category should only be created if it can meet certain expectations, for instance, allowing for efficient delivery of public services, preserving open spaces, maintaining the City's rural character, providing environmental protection, reducing storm water impacts, etc. Staff is planning on doing some additional research into how a new land use category could be created prior to the Planning Commission meeting and will share some additional information with the Commission on this concept at the meeting.
- 5) Other options or alternatives as recommended by the Planning Commission.

Because any of the options noted above will require a fair amount of time and effort to implement, Staff is recommending that the Commission conduct a general review of these options at the meeting and give Staff some general direction as to one or more specific options that are chosen for further study and analysis. At this time, Staff does not have a specific recommendation for action on any of these alternatives.

RECCOMENDATION:

Staff is recommending that the Planning Commission review the RAD-ALT options as listed in the above report, but that the Commission not take any action to amend the City’s Comprehensive Plan for the rural development areas (including RAD and RAD-ALT land use classifications) until after the Met Council 2014 regional forecast is finalizes.

Staff further recommends that the Commission provide Staff with direction on which, if any, of the general rural development options should be pursued in the future.

ATTACHMENTS:

1. Staff Report – 10/28/13 Planning Commission Meeting
2. Rural Zoning District Standards
3. OP Zoning Regulations
4. OP-2 Zoning Regulations
5. Lake Elmo Future Land Use Map (Map 3-3)

ORDER OF BUSINESS:

- IntroductionCommunity Development Director
- Report by StaffCommunity Development Director
- Questions from the Commission Chair & Commission Members
- Public CommentsChair
- Discussion by the Commission Chair & Commission Members
- Action by the Commission Chair & Commission Members



PLANNING COMMISSION
DATE: 10/28/13
AGENDA ITEM: 5B – BUSINESS ITEM
CASE # 2013-036

ITEM: Rural Area Development Analysis and Discussion
SUBMITTED BY: Kyle Klatt, Community Development Director
REVIEWED BY: Nick Johnson, City Planner

SUMMARY AND ACTION REQUESTED:

The Planning Commission is being asked to conduct a review of the City's rural development areas as defined in the Comprehensive Plan. This matter is being brought before the Planning Commission at the request of the City Council, which generally discussed the City's rural development areas as part of its retreat earlier this year. The Commission has also requested a broader discussion on this topic in response to its own recent conversations concerning growth and development in these areas.

GENERAL INFORMATION

Applicant: City-initiated action for discussion
Request: Review land use plans and policies concerning Rural Development Areas
History: The City revised its Comprehensive Plan for rural areas in the early-mid 1990's to allow for open space developments. The amendments from this time period limited the use of the Residential Estates as a future land use and instead encouraged any future development of land to be consistent with the City's open space regulations. The RAD-2 category was added to the Plan in 2005 in response to Met Council growth directives.

Deadline for Action: None

Applicable Regulations: Comprehensive Plan – Chapter III: Land Use Plan
Zoning Ordinance – Article 9: Rural District Standards

REVIEW DETAILS

For the purposes of this review, Staff is suggesting that the Planning Commission's review of the City's rural development areas focus on the following issues:

- The current plan for rural development areas and the history behind the development of this plan.
- The most recent projections for growth and development for the rural portions of the community.
- The application of the RAD and RAD-ALT (formerly RAD-2) land use designation, and whether or not the City should make any changes to these categories.

Earlier this year the City Council adopted two significant updates to the City's Comprehensive Plan that incorporated new land use plans for the Village and I-94 Corridor planning areas into the document. While the focus of these updates was on the future sewer service areas, all of the existing land use categories for the rural development areas were carried forward into the new Plan. A review of the rural development areas is therefore a worthwhile exercise at this time since these the future land use guidance for these areas has been left relatively unchanged since the last major Comprehensive Plan amendment in 2005.

The City Council has also recently drafted a response to the preliminary 2040 Met Council forecast for Lake Elmo that, if accepted, would reduce the City's projections for population and household numbers in 2040. As part of the proposed revisions requested by the City Council, the City's 2040 population target would go from 24,000 to 18,000. This reduction would allow the City to plan for a more moderate amount of growth and development within the rural portions of the community, while keeping a moderate level of growth and development within the urban service areas that will be needed to help pay for the required infrastructure improvements.

At this time, Staff does not recommend any changes to the Comprehensive Plan that would lower the overall household and population projections until a final forecast for Lake Elmo is adopted by the Metropolitan Council. This forecast is expected to be finalized sometime in the spring of 2014. The Planning Commission may want to recommend any adjustments to the plan for rural development areas based on the recent updates that focused the land use within the urban service areas.

BACKGROUND

The City of Lake Elmo's land use plan has undergone significant changes since the City was incorporated into its present form over 30 years ago. At that time, the City's Comprehensive Plan was essentially divided into two primary areas: the areas that had been developed and subdivided prior to 1980 (i.e. the Tri-Lakes and Village areas) and the City's agricultural and rural residential development areas. As far back as the late 1970's the City has maintained zoning regulations that have established a minimum lot size of 10 acres in rural residential zones and 40 acres in agricultural zones. Any developments with parcels smaller than 10 acres were mostly platted prior to 1979 under either township regulations or a City ordinance that closely followed township requirements.

Beginning with the City's adoption of the 1979 Zoning Ordinance, areas that contained existing platted lots, with a few exceptions, were grouped into a single family zoning district with a minimum lot size requirement of 1.5 acres (the former R-1 district). This minimum was based partially on the need to provide adequate room for septic systems on these lots, even though many of the parcels in these single family areas did not meet the lot size requirement. Parcels with less than the minimum required area were, and are still, considered legal non-conforming lots. The expansion of the single family R-1 district has been discouraged by the City's more recent comprehensive plans.

Since the adoption of the 1979 zoning regulations, the City has adopted three significant land use changes to regulate growth and development within the rural development areas. These major policy updates have included the following:

- As part of the 1990 Comprehensive Plan update, the City adopted a goal to establish a new zoning category for residential development with a minimum lot size of 2.5 acres per unit and a maximum net density of 3 units per 10 acres. This led to the creation of the RE – Residential Estates zoning district, which is still in place today. With subsequent comprehensive plan amendments, the City has moved away from the designation of any new land for RE development.

- In 1996 the City adopted a Comprehensive Plan amendment to guide most of the remaining rural and agricultural portions of the community for open space development. This led to the creation of the Open Space regulations that have been in place since this time. When first adopted, the open space requirements allowed up to 16 homes per 40 acres, but this was subsequently revised to allow densities of 18 homes per 40 acres (or 0.45 units per acre). With later plan amendments, the City categorized the areas guided for open space development as “Rural Agricultural Density” or RAD. Within the RAD category, working farms, alternative agricultural uses, and low density residential developments (10+ acre lots) are allowed, with a specific notation that new development is allowed consistent with the City’s open space regulations. This language has been in place since the 2005 Comprehensive Plan update.
- Without going into a detailed history of the City’s dispute with the Metropolitan Council, the City agreed to accept new sewered development as part of a negotiated agreement with the Met Council. Lake Elmo therefore undertook a major comprehensive plan revision in 2005 that identified two new growth areas within the community that will be serviced by public sanitary sewer. One such area was located along the I-94 Corridor while the other included existing and proposed development within the Village Planning area. The City did not substantially alter the RAD land use category at this time, and left any land not previously developed and located outside of the new sewered growth areas in the RAD category. Given the new household and population forecasts that were adopted by the City in 2005, it was determined that there would not be enough development in the RAD areas to accommodate this growth; hence the RAD-2 land use category was created to allow additional development in limited locations at a density of two units per acre.

For the most part, the City’s development standards within rural development areas (classified as RAD current Comprehensive Plan) have not been substantially revised since the open space zoning approach was adopted. Some areas were removed from this category to provide room for sewered development, and a few select parcels have been moved into the RAD-2 category. The City now has 18 open space developments located in all portions of the City, and there is room to accommodate additional open space developments in the future.

The RAD-2 category is somewhat unique because it was not part of the original amendment that created opportunities for open space development. The creation of the RAD-2 land use category was done after a review by the City indicated that there was not enough developable land in the RAD development areas to accommodate the number of rural households required to achieve the population forecasts that came out of the City’s 2004 Memorandum of Understanding (MOU) with the Met Council. After performing an analysis of the rural areas, it was decided that an additional 250-300 units of housing above and beyond the number that would be achieved under the existing RAD densities would be needed to meet the requirements of the MOU. Two sites with a total acreage of 142 acres were included in the RAD-2 category as part of the 2005 Comprehensive Plan update in order to accommodate 284 additional units of housing within the rural development areas.

Since the adoption of the 2005 Comprehensive Plan update, the City has approved the required decennial updates to the Plan in 2009 and further updated the land use and housing chapters to accommodate refinements to the sewered growth areas. The City did not take any action to adopt zoning regulations specific to the RAD-2 areas until it was presented with land use request to change the RAD designation of a 24-acre parcel along Stillwater Boulevard from RAD to RAD-2. This

request, and the subsequent action taken by the City, are reviewed in greater detail a little later in this memorandum.

GROWTH PROJECTIONS/DENSTY ALAYSIS

In preparation for discussions concerning proposed updates to the Comprehensive Plan in 2010, the Planning Department conducted an analysis of the City's rural development areas in order to better understand the need to accommodate additional housing in these areas. The resulting report that was presented to the City Council is attached for review by the Planning Commission. Please note that since this information was assembled, the City has received its household and population numbers from the 2010 Census, which shows a modest increase from the household numbers that Staff was using at the time. Because these numbers are very close to one another and Staff was using the 2009 figure (instead of the 2010 census amount) the information cited below has not been updated. Additionally, please keep in mind that the City Council ultimately adopted new population and household growth forecasts as part of the recent land use plan amendments. The amendments do not create the need to revise any of Staff analysis concerning rural areas from 2010 since the rural growth projections were not impacted by the sewer land use changes.

To briefly summary the 2010 Staff analysis of rural development areas, please consider the following:

- In 2009 there were estimated to be 2,814 households in Lake Elmo.
- By removing those existing households that will be connected to sewer in the future, Staff estimated that the household count in the City's RAD development areas was 2,120.
- The household projections included in the 2005 and 2013 Comp Plan updated show that 3,527 households will be located in the rural development areas in 2030.
- By subtracting existing housing units from the 2030 projection for rural areas, Staff determined that 1,407 units would need to be built in rural development areas between 2010 and 2030 to achieve the population and household forecasts.
- A review of platter lots within the rural development areas in 2010 identified 148 vacant lots. By removing these lots from the needed household count, Staff determined that the City would need to allow for 1,259 additional units of housing in rural areas in order to achieve the growth projections. Since no new rural development lots have been platted since 2010, this number still represents an accurate accounting of needed future growth.

As part of this exercise, Staff researched the amount of land that is still available to accommodate the 1,259 housing units and studied three alternatives to accommodate this growth by either:

- 1) Assuming all development would follow current zoning requirements for rural development areas, and specifically, the densities allowed under OP and RED zoning. For this scenario, Staff only considered parcels larger than 40 acres as available for OP developments since this is the current minimum area requirement. This was described a conservative approach;
- 2) Keeping the above assumptions, but calculating potential development potential using a 20-acre minimum for OP developments. This would open up additional potential for OP

developments that could otherwise not be constructed under a 40-acre minimum requirement. Staff referred to this scenario as a moderate growth strategy;

- 3) Taking a more aggressive approach with the assumptions from item (1) and projecting that all parcels over 10 acres in size would be developed as part of an open space development.

The result of this exercise showed that the City could expect rural population growth of 1,070 units using the conservative assumptions from above, 1,422 with a more moderate approach, or 1,578 or more units with a very aggressive zoning approach. These results indicate that the conservative (or status quo) approach would not allow the City to achieve its growth targets for 2030. Please note that Staff did consider the areas currently guided for RAD-2 and RED in these calculations, and the units projected for these areas are included in the numbers for each growth strategy. Expanding the areas guided for RAD-2 would alter the above estimates by increasing the household totals proportionate to the amount of land added to the RAD-2 category.

Since Staff completed this analysis, the City did proceed with updates to the Comprehensive Plan, but did not make any changes to the areas outside of the urban services boundary. The City has also recently received an updated Met Council forecast for population and household growth that is substantially lower than the 24,000 currently referenced in the City's planning documents and as specified in the 2005 MOU. In order to respond in a timely manner to the preliminary Met Council forecast, the City Council has developed a response that requests the following:

- 2040 population of 18,000 (reduced by 6,000)
- 2040 household count of 6,545 (reduced by 2,182)
- 2040 REC unit count of 5,000 (reduced by 1,600)

The Staff report to the City Council outlining a response to the Met Council forecasts is attached to this document. Should these revised numbers be accepted by the Met Council, the City would be in a position to reduce the growth projections within rural development areas to a more moderate level. In this case, the Council has recommended reducing the portion of new households within rural areas to 720 units, well below the current number of 1,407.

RAD AND RAD-ALT REVIEW AND ANALYSIS

Prior to 2010, the City had only identified two potential sites for the RAD-2 land use category. In early 2010, the City received an application to amend the Comprehensive Plan to add a new 24-acre parcel into this future land use category in order to facilitate the a proposed development at 9442 Stillwater Boulevard North. The developer for this project proposed to construct a senior living complex, townhouses, and a farm-based preschool on the site, but was not able to move forward without a land use plan amendment. In addition, the City had not yet created zoning regulations that corresponded to the RAD-2 designation; however, the applicant's request included an amendment to create a new OP-2 zoning district.

When this matter was brought before the Planning Commission for review, Staff did not recommend approval of the proposed amendments for a number of reasons, citing 1) the lack of build out within existing open space developments, 2) the lack of any substantial changes since the Comprehensive Plan was adopted that would warrant the change, 3) language in the Comprehensive Plan that encourages multi-family and senior development within sewer service areas, and 4) the incompatibility of the propose land uses with the uses allowed in the City's rural development areas.

Based partly on positive feedback from residents in the community and a desire to move forward with a unique project, the Planning Commission recommended approval of the land use and zoning amendments, which were ultimately adopted by the City Council. The City now has guided three distinct areas for RAD-2 development (which was renamed RAD-ALT as part of the recent amendments) including the property at 9442 Stillwater Boulevard North. These areas are currently depicted on the City's Future land use map (see Map 3-3 attached) and include:

- Approximately 36 acres located immediately north of 10th Street at the intersection of Manning Avenue and 10th Street.
- Approximately 173 acres of land located northeast of the intersection of State Highway 5 and Ideal Avenue close to the City's western boundary with Oakdale.
- The 24 site at 9442 Stillwater Boulevard North

One of the key questions for the City as the City moves forward with implementation of the Comprehensive Plan is whether or not there should be an alternate land use category that allows for increased development and land uses other than single family homes within the City's rural development areas. Based on the analysis presented above, the areas that have been guided for RAD-ALT development will help the City achieve its growth targets; however, it does appear very likely that the City will be receiving some form of relief from the 2005 population targets. Should the revised projections be implemented, the original pressure that led to the creation of the RAD-2 category would likely go away.

In addition, there are other ways that the City can accommodate growth within rural areas, for instance, by either increasing the allowed density in RAD areas (i.e. from 18 homes per 40 acres to 20 homes per 40 acres) or by allowing OP developments on parcels less than 40 acres. Another option to add units within rural development areas would be to reduce the minimum lot size requirements for A – Agriculture and RR – Rural Residential zoning districts, which are presently set at 40 acres and 10 acres respectively. The other relevant question that should be considered by the Planning Commission is whether or not the City should be considering an expansion of the allowed uses within rural development areas to accommodate things like senior housing and the provision of goods and services for the community. The City Council has also asked that the Planning Commission consider reducing the minimum lot size requirement within rural areas, and to specifically consider allowing new residential construction on parcels of less than 10 acres.

Any of these, or similar, actions will have implications for how the City's rural area will grow and develop over the next 30 years and should be considered within the context of the City's overall goals and objectives. With the above comments in mind, please note the City's overarching mission statement for land use planning as adopted by the Council which reads:

“To thoughtfully adopt a City-wide Comprehensive Plan that maintains the open space character of the community while balancing attractive, sustainable growth opportunities that meet the requirements of the Metropolitan Council.”

PLANNING COMMISSION ACTION (QUESTIONS)

Since this matter is being brought before the Planning Commission without a specific request or recommendation, Staff would like to suggest that the Commission consider the following questions as it considers the City's land use planning in rural development areas:

- Should the minimum lot sizes in A and RR zoning districts be lowered in order to accommodate increased opportunities for the use of larger parcels? Is 10 acres the optimal number for preserving open space character?
- Does the City need to retain the RAD-ALT land use category, and should this land use be eliminated if the proposed population forecasts are accepted by the Met Council?
- Would an increase in the types of allowed uses within rural development areas (i.e. townhouses, senior housing, congregate housing, schools, community services, neighborhood commercial) be consistent with the City's stated goal of preserving open space character?
- Are the current uses allowed within rural areas appropriate? Is agriculture something that should be more heavily encouraged by the City?
- Is the density allowed within OP developments acceptable or is it too high or too low? Have the existing OP developments contributed towards or detracted from the City's open space character?
- The City's land use plan does not allow for the expansion of RS – Rural Single Family or RE – Residential Estates areas; these zoning districts have been restricted to existing development only. Should new developments be allowed that are zoned in this manner?

In order to facilitate the Planning Commission discussion on these matters, or any others that might be raised by the public of the Commission, Staff will spend time at the meeting reviewing the reports, projections and land use plans that are referenced in this report and will present a more detailed summary of the questions that are raised above. At this point, Staff is seeking general guidance from the Commission, and will be presenting any specific direction from the meeting back to the City Council.

RECCOMENDATION:

Staff is recommending that the Planning Commission consider the list of questions that have been raised in the Staff report within the context of the information present in and attached this report. Any specific recommendations should be made in the form of a motion.

ATTACHMENTS:

1. Met Council Forecast Discussion – 10/15/13 Council Report
2. Rural Development Analysis – 7/6/10 Council Report
3. OP-2 Zoning Regulations
4. Lake Elmo Future Land Use Map (Map 3-3)

ORDER OF BUSINESS:

- IntroductionCommunity Development Director
- Report by StaffCommunity Development Director
- Questions from the Commission Chair & Commission Members
- Public ComentsChair

- Discussion by the Commission Chair & Commission Members
- Action by the Commission Chair & Commission Members



Planning Commission
Date: 5/29/13
Business Item
Item: 6b

ITEM: Discussion of Rural Agricultural Density (RAD and RAD-ALT) Guidance in the Comprehensive Plan

REQUESTED BY: Planning Department

SUBMITTED BY: Kyle Klatt, Planning Director

REVIEWED BY: Nick Johnson, City Planner

SUMMARY AND ACTION REQUESTED:

The Planning Commission is being asked to review and discuss the portions of the recently adopted Comprehensive Plan pertaining to areas in the City that are guided for Rural Agricultural Density (RAD). In particular, the City Council has directed the Commission to review the areas that are guided for alternative RAD densities (formerly RAD2 designations in the previous Comprehensive Plan) in light of the recent expiration of concept plans for a proposed development on the site of the Friedrich Farm at 9434 Stillwater Boulevard North.

At this time, Staff is not recommending that the Planning Commission make any changes to the Comprehensive Plan, but has put together the brief report below in order to help guide discussion on this topic. Should the Commission wish to make any changes to the City's Plan, Staff has also suggested a range of actions/direction that the Planning Commission should consider.

BACKGROUND/ADDITIONAL INFORMATION:

For roughly two decades, the City of Lake Elmo has guided land outside any existing platted lots for a land use classified as Rural Agricultural Development (RAD). This future land use designation allows the continued use of land for agricultural and rural residential purposes, but also allows for the development of land that is consistent with the City's Open Space Preservation Ordinance. The allowed residential densities in these areas have changed slightly over these two decades, but at present, the RAD designation allows up to 0.45 units per acre (or 18 residential lots per 40 acres).

As part of the City's agreement with the Metropolitan Council to accommodate its share of the regional population growth forecasts, Lake Elmo has agreed to designate portions of the City for urban development at higher densities and to serve these areas with regional sewer services. As part of this compromise, the City was able to keep undeveloped areas outside of the future sewer areas (the Village and I-94 Planning Areas) under the previous land use guidance of RAD. Furthermore, all areas that were not previously platted or that are outside of the urban service areas will continue to be guided for rural development in accordance with the general RAD classification.

One of the issues that arose from the agreement with the Met Council; however, is that the City was given an overall population and household target for the year 2030, and was granted some discretion on how to allocate these units across the entire City. So although a large portion of these future units were sent into the sewer areas, there were still a significant number of residential units that were allocated to the rural development areas. After reviewing the maximum

development potential within the City's rural areas (using the 0.40 units per acre maximum in place at the time), the City determined that it would either need to allocate more units into the sewer development areas or find an alternate way to accommodate more growth within the rural areas. The resulting action by the City to address this problem was to increase the RAD density to 0.45 units per acre and to create a RAD-2 classification that allows up to two units per acre in areas guided accordingly. The City then identified two sites with 142 acres that, along with the bump in the previous RAD density, allowed the household and population targets to be met.

When the City updated its Comprehensive Plan for the I-94 Corridor, the entire land use plan (outside of the Village Planning Area) was updated as well. The current plan kept all of the previous land use categories within the rural areas, but provided a somewhat modified description for each and renamed the RAD-2 classification to RAD – Alternative Densities in order to better reflect the purpose for this land use category. The RAD categories are now described as follows:

RURAL AREA DEVELOPMENT – This category represents the large areas of rural residential development within the City. Common uses found in these areas include working farms, alternative agricultural uses as defined by City Code, and rural single family detached residences. Development in these areas requires 10+ acres, or a conditional use permit to authorize a cluster development meeting the City's Open Space Preservation regulations. Densities are allowed up to 0.45 dwelling units per buildable acre when planned as part of an Open Space Preservation development. No new areas of rural area development are being established by the official land use plan. [Corresponding Zoning District(s): A, RR, OP]

RURAL AREA DEVELOPMENT – ALTERNATE DENSITY – This land use category represents a subset of land guided for Rural Area Development and provides for an increase in the densities allowed through an Open Space Preservation development of up to 2.0 dwelling units per buildable acre. Further increases in the base density may be allowed through a Planned Unit Development through incentives for density bonuses that are permitted as part of a PUD and that maintain the open space character of a development. In addition to single-family residences and townhouses, multi-family housing for seniors is permitted in this district. [Corresponding Zoning District(s): A, RR, OP-2]

All other rural development categories (including Residential Estates and Rural Single Family areas) were carried forward from the previous version of the Comprehensive Plan.

CURRENT ISSUES:

Prior to adoption of the revised Comprehensive Plan, the City received a request to change the future land use designation of 9434 Stillwater Boulevard North from RAD to RAD-2 in conjunction with a proposed Senior Living/Farm School project on the site of the Friedrich family farmstead. This request was approved by the City Council and added a third area to the RAD-2 future land use category. As part of this approval, the Planning Commission and Council adopted findings noting that recently approved subdivisions in the City's rural areas were approved at a density below the unit levels anticipated in the Comprehensive Plan and that the density increase would be offset by reductions previously approved and acknowledged by the City.

Upon approval of the Comprehensive Plan amendment for this site, the City approved a second OP – Open Space Preservation zoning district in order to accommodate development within the RAD-2 areas and also approved a concept plan for a 49-unit senior living project for the property at 9434 Stillwater Boulevard North. The City's zoning ordinance specifies that an applicant must submit preliminary plans for a development within a year of concept plan approval in order for these plans to remain valid; however, the applicant, after two extension requests were granted by the City, did not submit preliminary plans in accordance with this deadline. The project is no longer valid as previously approved by the City, and future development on the site will need to restart at the concept plan stage.

Please note that the Council resolution approving the Comprehensive Plan amendment did note that the approval was contingent upon Council approval of final plans for the site. Under State law, this contingency does not mean that the property will automatically revert back to the previous land use category. Any change to the Comprehensive Plan must go through the formal review process (including a public hearing with the Planning Commission and review by the Met Council), and cannot proceed without the authorization of the Council. Given the cost, time and effort associated with such a plan amendment, Staff will not proceed with any further modifications of the Comprehensive Plan without direction from the City Council.

Staff would also like to note the following about the Friedrich RAD-2 site:

- The RAD-2 designation (now called RAD-ALT) was kept for the property as part of the 2013 future land use updates.
- The property owner has expressed their preference to keep the current land use designation and does not want to see any changes made to their property.
- Any new development proposal for the site would need to start at the concept plan stage and comply with all associated application and review requirements.

As part of its review of the Friedrich development plans, the Council did discuss the potential to allow density transfers within RAD areas as one way to accommodate additional density on some sites while retaining the overall housing and population targets for the City. Staff has not done any additional research into a potential density transfer program since the initial Council discussion, and has instead been focused on other Comprehensive Plan and Zoning Ordinance amendment projects to date.

Staff has previously done some analysis as recently as 2010 to examine development patterns throughout the City and to gauge whether or not the City is on track with its household and population targets. At that time, Staff projected that the City would need an additional 1,259 rural households to meet its targets, and that there were 3,386 developable acres of land guided RAD to accommodate this growth. Although a rough calculation would indicate that there is adequate acreage to accommodate the 1,259 units in these RAD areas, Staff pointed out that much of the land that is guided RAD is in parcels of 20 acres or less, and therefore not eligible for open space development (which requires 40 acres as a minimum). Staff looked at three growth strategies from conservative to aggressive for these areas that would allow OP development on parcels down to 10 acres in size and found that a moderate approach would be in the best interest of the City. This means that the City will need to find ways to accommodate additional growth within rural areas either by increasing densities across the board, applying the RAD-ALT designation to additional properties, or by reducing the minimum OP development size requirement down to 20 acres.

STAFF REPORT/OPTIONS:

Because the Council has only given general direction to the Planning Commission, Staff will be discussing City's RAD areas with Commission at its meeting, and will be seeking any direction from the Commission as a result of this discussion. Given the complexity of the terms, issues and history associated with the RAD and RAD-ALT use designations, Staff will spend time at the next Planning Commission meeting reviewing this memorandum and taking questions from the Commission.

At this point, given the numerous planning and zoning issues in front of the Planning Commission, Staff is not recommending any changes to the Comprehensive Plan to re-designate any RAD or RAD-ALT areas to a different land use category. This recommendation is also partially based on the fact that the City will be receiving a revised regional forecasts from the Met Council fairly soon. The Met Council has agreed to continue working with the City at refining the population, household, and sewer unit mandates that are part of its agreement with the City, and furthermore, this work will incorporate changes to the regional forecasts and could lead to revised population targets and a revised time frame for compliance with those targets. Any

revisions will likely have impacts to the City's rural development areas which will need additional review and discussion in the near future.

Should the Commission wish to address the City's RAD and RAD-ALT areas, Staff would recommend the following as potential direction from the Commission:

- No change – see Staff recommendation above.
- Amend the Comprehensive Plan to change the Friedrich RAD-ALT site back to the RAD classification.
- Amend the Comprehensive Plan to change all RAD-ALT sites back to RAD with or without a corresponding increase in density somewhere else in the Comprehensive Plan.
- Change all RAD densities to 0.5 units per acre (20 homes per 40 acres) and either eliminate or keep the RAD-ALT densities of 2.0 units per acre.
- Allow OP development on parcels smaller than 40 acres consistent with the moderate growth strategy previously offered by Staff.
- Direct all RAD-ALT densities to urban sewered districts and revert these areas back to the RAD designation.
- Any other direction as deemed prudent by the Planning Commission.

Staff will be able to further discuss any of the above options or alternative actions with the Commission at the meeting.

RECOMMENDATION:

Staff recommends that the Planning Commission discuss the City's RAD and RAD-ALT development areas and to provide any direction concerning these areas for consideration by the City Council.

ATTACHMENTS (1):

1. Lake Elmo Existing and Proposed RAD-2 Area (from Previous Comprehensive Plan Amendment)



PLANNING COMMISSION
DATE: 7/14/14
AGENDA ITEM: 4C – PUBLIC HEARING
CASE # 2013-38

ITEM: Zoning Map Amendments – Perfecting Amendments

SUBMITTED BY: Kyle Klatt, Community Development Director

REVIEWED BY: Nick Johnson, City Planner

SUMMARY AND ACTION REQUESTED:

The Planning Commission is being asked to adopt an updated Zoning Map that addresses several land use and development projects that have recently been approved by the City. Staff is also recommending amendments that will remove the transitional zoning for several properties in the Village that are guided for rural/agricultural use as part of the Village Open Space Plan in addition to other minor corrections from previous maps.

GENERAL INFORMATION

Applicant: City-initiated action for discussion

Request: Adopt an updated Zoning Map for the City.

History: The City completed a major map revision in 2013 to incorporate the RT – Rural Transitional Zoning classification as part of the map. Other recent amendments have been made in conjunction with specific development proposals.

Deadline for Action: None

Applicable Regulations: Comprehensive Plan – Chapter III: Land Use Plan
Zoning Ordinance – Article 8: Zoning Districts and Zoning Map

REVIEW DETAILS

All of the requested changes to the Zoning Map are based on approvals given or actions already taken by the City, or are intended to bring the Map into conformance with the Comprehensive Plan. The proposed changes include the following:

| Parcel/Description | Existing Zoning | Proposed Zoning |
|---|-----------------|------------------------|
| Well Number 4 – 11210 50 th Street North | RR | PF - Public Facilities |
| 5 parcels west of Lake Elmo Avenue and north of 43 rd Street (Village Planning Open Space) | RT | A – Agriculture |
| 3 parcels west of Lake Elmo Avenue and south of 43 rd Street (Village Planning Open Space) | RT | RR – Rural Residential |

| | | |
|---|----|--------------------------------------|
| Parcel immediately north of Village Preserve Development and east of Lake Elmo Avenue (Village Planning Open Space) | RT | RR – Rural Residential |
| 3280 Klondike Avenue | RT | RS – Rural Single Family Residential |
| 3150 Klondike Avenue | RT | A – Agriculture |
| 3000 Klondike Avenue | RT | RR – Rural Residential |
| 11580 30 th Street N | RT | RS – Rural Single Family Residential |
| 3075 Lisbon Avenue N | RT | RS – Rural Single Family Residential |
| 11520 30 th Street North | RT | RS – Rural Single Family Residential |
| 3080 Lisbon Avenue N | RT | PF – Public Facility |
| 3040 Lisbon Avenue N (Townhouses) | RT | RS – Rural Single Family Residential |
| Hammes Estates | RT | LDR – Low Density Residential |
| Northwest quadrant of I-94 and Keats Avenue (includes Kwik Trip site) | RT | C – Commercial |
| Hunter’s Crossing | RT | LDR – Low Density Residential |
| Air Lake Development – northeast quadrant of I-94 and Lake Elmo Avenue | RT | BP – Business Park |

Because the proposed map amendments are intended to address developments or projects that have already been approved by the City, Staff is recommending approval of all changes as presented.

RECCOMENDATION:

Staff is recommending that the Planning Commission adopt an updated Zoning Map that incorporates the revisions noted above.

ATTACHMENTS:

1. Proposed Zoning Map
2. Existing Zoning Map

ORDER OF BUSINESS:

- IntroductionCommunity Development Director
- Report by StaffCommunity Development Director

- Questions from the Commission..... Chair & Commission Members
- Public CommentsChair
- Discussion by the Commission Chair & Commission Members
- Action by the Commission..... Chair & Commission Members



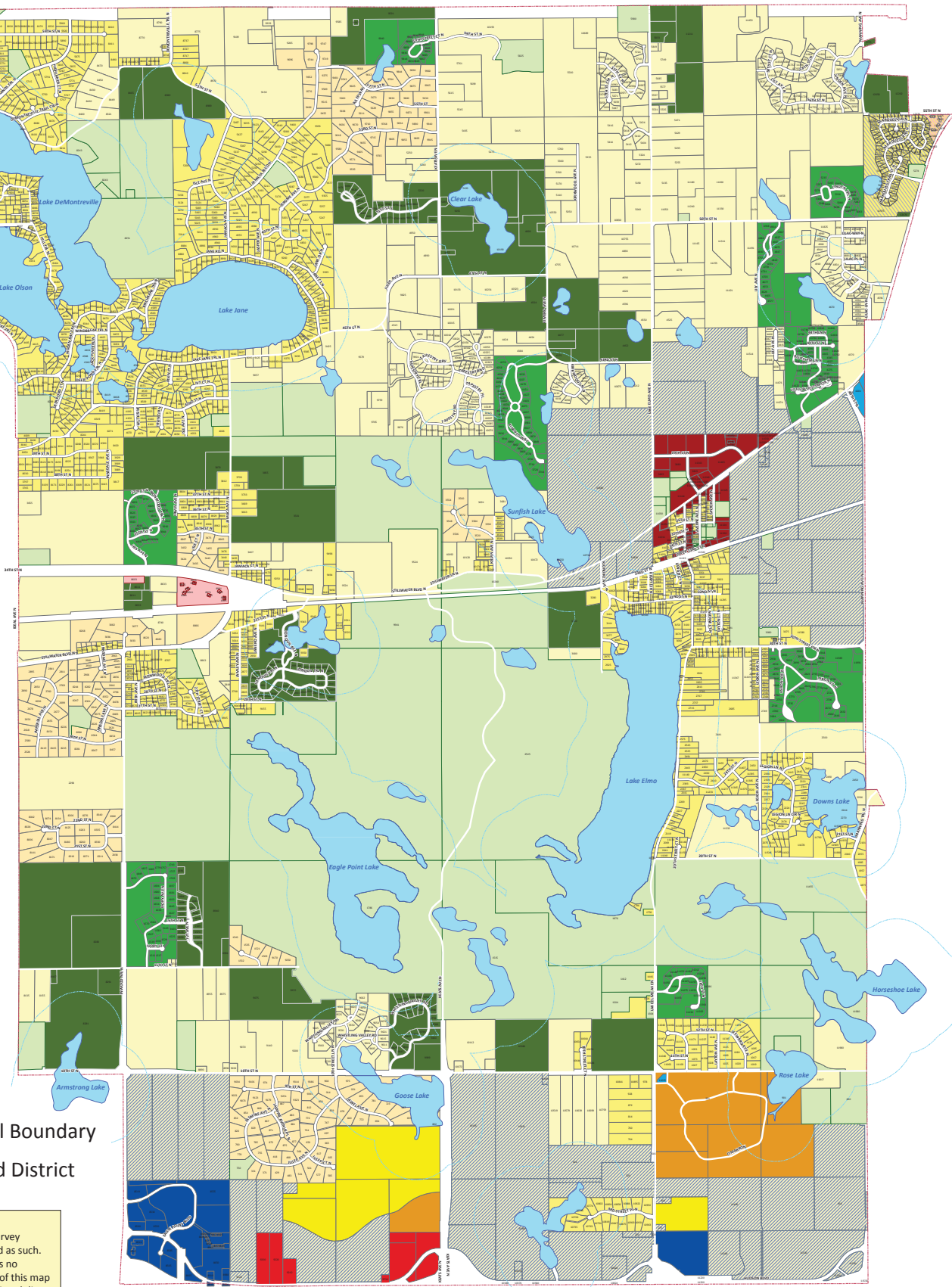
ZONING DISTRICTS

- A
- BP
- C
- CC
- GB
- LC
- LC-PUD
- LDR
- MDR
- OP
- PF
- R2-PUD
- RE
- RR
- RS
- RT

Municipal Boundary

Shoreland District

Data Disclaimer:
 This map is not a boundary survey and is not intended to be used as such. The City of Lake Elmo assumes no liability for the completeness of this map or responsibility for any associated direct, indirect, or consequential damages that may result from its use or misuse.



Sources: Washington County & Metro GIS
 7-11-2014

Official Zoning Map

Proposed

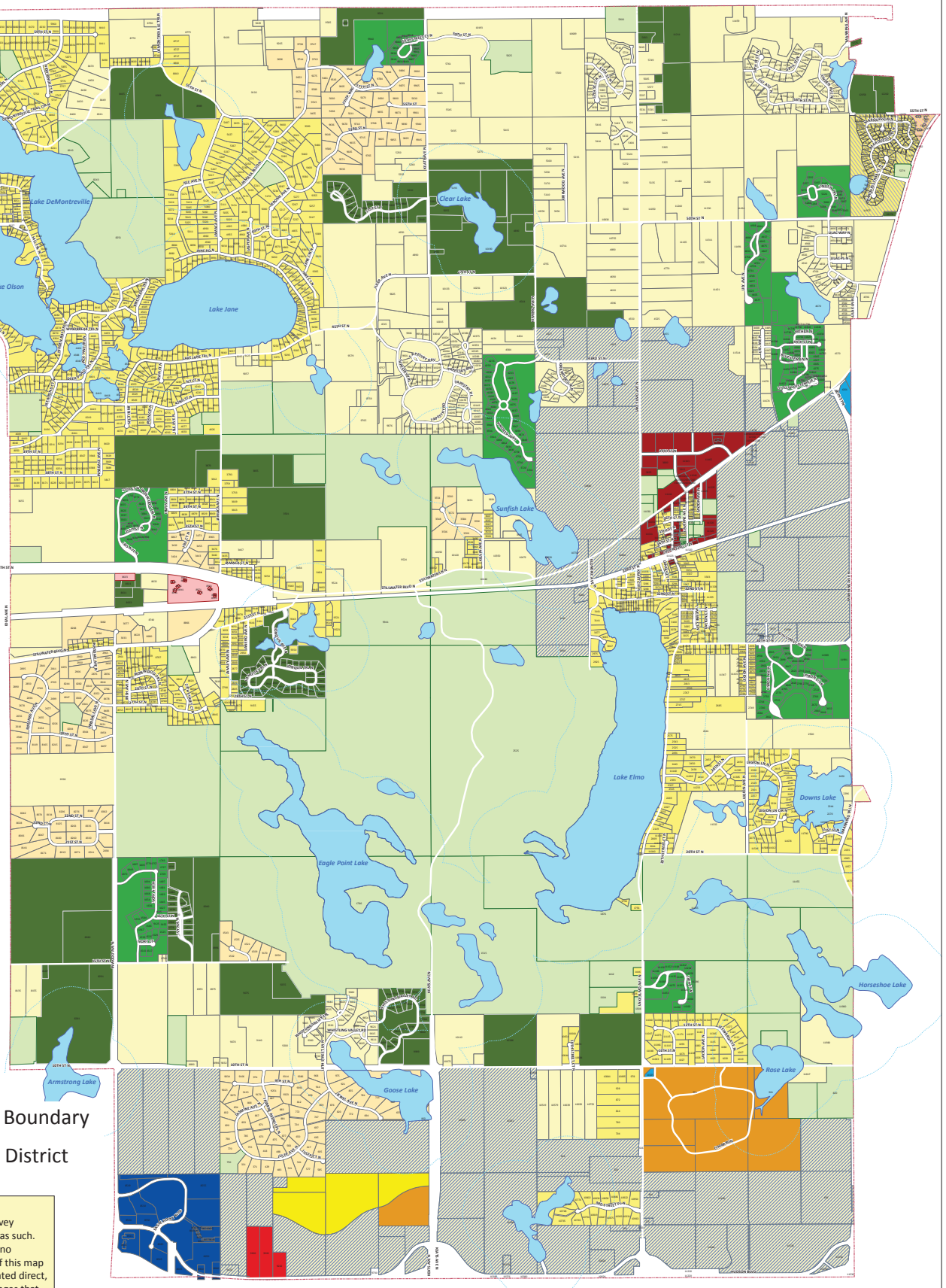




ZONING DISTRICTS

- A
- BP
- C
- CC
- GB
- LC
- LC-PUD
- LDR
- MDR
- OP
- PF
- R2-PUD
- RE
- RR
- RS
- RT
- Municipal Boundary
- Shoreland District

Data Disclaimer:
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Sources: Washington County & Metro GIS
 2-26-2014

Official Zoning Map

