

MAYOR & COUNCIL COMMUNICATION

DATE: July 1, 2014
CONSENT
ITEM # 12

AGENDA ITEM: Eagle Point Medical Preliminary and Final PUD Development Plans

SUBMITTED BY: Kyle Klatt, Community Development Director

THROUGH: Dean Zuleger, City Administrator

REVIEWED BY: Planning Commission
Nick Johnson, City Planner
Lake Elmo Development Review Committee

SUGGESTED ORDER OF BUSINESS:

- Introduction of ItemCommunity Development Director
- Report/Presentation.....Community Development Director
- Questions from Council to Staff Mayor Facilitates
- Call for Motion Mayor & City Council
- Discussion Mayor & City Council
- Action on Motion..... Mayor Facilitates

POLICY RECCOMENDER: The Planning Commission is recommending approval of a preliminary and final development plans for a new medical building to be located within the Eagle Point Business Park. As a part of the Planning Commission recommendation and findings, the Commission did review the project for conformance with the Lake Elmo Design Guidelines and Standards Manual.

FISCAL IMPACT: None – all utilities and improvements have either been installed as part of the Eagle Point Business Park or will be completed by the applicant at the time a building permit is issued for the building.

SUMMARY AND ACTION REQUESTED: The City Council is being asked to consider a request from Davis Real Estate Services, 222 South 9th Street, #3255, Minneapolis, MN for approval of preliminary and final Planned Unit Development (PUD) plans associated with a 28,500 square foot, two story, multi-tenant medical building to be located within the Eagle Point Business Park. Because the site under consideration has previously been platted as a buildable lot, Staff is recommending that the City process its review of the preliminary and final development plans simultaneously.

The Planning Commission is recommending approval of the preliminary plat with conditions of approval. *If removed from the consent agenda*, the suggested motion to adopt the Planning Commission recommendation is as follows:

“Move to adopt Resolution No. 2014-54 approving the preliminary and final PUD plans for the Eagle Point Medical Building.”

LEGISLATIVE HISTORY/PLANNING COMMISSION REPORT: The attached staff report to the Planning Commission provides an overview of the request and a detailed description of the issues associated with the request as identified by Staff. The Planning Commission considered the PUD plans at its June 23, 2014 meeting and conducted a public hearing on the applicant’s request at this time. The Commission did not receive any public comments concerning the proposed building. Representatives of the applicant addressed the Commission and provided additional information concerning the design of the building and the future tenants of the facility.

The Planning Commission reviewed the project for consistency with the Lake Elmo Design Guidelines and Standards Manual, and found that the project complied with all applicable provisions for development in the City’s BP – Business Park Zoning District.

The Planning Commission adopted a motion to recommend approval of the preliminary and final development with the findings and conditions as noted in the attached Resolution 2014-054. The motion passed unanimously.

BACKGROUND INFORMATION (SWOT):

- | | |
|----------------------|--|
| Strengths | <ul style="list-style-type: none">• The proposed building is consistent with the Lake Elmo Design Guidelines and Standards Manual• The project is consistent with the Lake Elmo Comprehensive Plan and BP – Business Park Zoning• The proposed development will occupy an existing, platted, vacant lot within the Eagle Point Business Park |
| Weaknesses | <ul style="list-style-type: none">• None |
| Opportunities | <ul style="list-style-type: none">• The proposed development will bring a very well-regarded neurological center to the community• The proposed building will fit in very well with the Eagle Point Business Park |
| Threats | <ul style="list-style-type: none">• None identified. |

RECOMMENDATION: Based upon the above background information, Staff report and Planning Commission recommendation, it is recommended that the City Council approve the preliminary and final PUD development plan for the Eagle Point Medical Center with the seven

conditions of approval as documented in the resolution of approval by taking the following action / with the following motion (*if removed from the consent agenda*):

“Move to adopt Resolution No. 2014-54 approving the preliminary and final PUD plans for the Eagle Point Medical Building.”

ATTACHMENTS:

1. Resolution No. 2014-54
2. Planning Commission Report (6/23/14)
3. Application Form
4. Project Narrative Letter
5. Site Development Summary
6. Legal Description
7. Storm Water Narrative
8. Eagle Point Business Park Development Standards
9. Lake Elmo Design Guidelines and Manual – Business Park Development
10. Review Comments
 - a. City Engineer
 - b. Washington County
11. PUD Development Plans
 - a. Cover Sheet
 - b. Landscape Plan
 - c. Existing Conditions Survey
 - d. Site Plan
 - e. Grading Drainage and Erosion Control Plan
 - f. Utility Plan
 - g. Details
 - h. Photometric (Lighting) Plan
 - i. Easement Sketch
 - j. Architectural Drawings (3)
12. Color Building Rendering
13. Eagle Point Business Park 7th Addition Final Plat

**CITY OF LAKE ELMO
WASHINGTON COUNTY, MINNESOTA**

RESOLUTION NO. 2014-54

*A RESOLUTION APPROVING PRELIMINARY AND FINAL DEVELOPMENT PLANS FOR
THE EAGLE POINT MEDICAL BUILDING*

WHEREAS, the City of Lake Elmo is a municipal corporation organized and existing under the laws of the State of Minnesota; and

WHEREAS, Davis Real Estate Services, 222 South 9th Street #3255, Minneapolis, MN has submitted an application to the City of Lake Elmo (City) for preliminary and final plans for Eagle Point Medical Building to be located on Lot 1, Block 1 of the Eagle Point Business Park 7th Addition, a copy of which is on file in the City of Lake Elmo Planning and Zoning Department; and

WHEREAS, the City has agreed to conduct a simultaneous review of the preliminary and final PUD plans for the Eagle Point Medical Building because the site has previously been platted and improved as a buildable lot within the Eagle Point Business Park and is appropriate because of the limited scale of the proposal; and

WHEREAS, the Lake Elmo Planning Commission held a public hearing on June 23, 2014 to consider the preliminary and final development plans for the Eagle Point Medical Building; and

WHEREAS, on June 23, 2014 the Lake Elmo Planning Commission adopted a motion to recommend that the City Council approve the preliminary and final plan for the Eagle Point Medical Building with conditions; and

WHEREAS, the Lake Elmo Planning Commission has submitted its report and recommendation concerning the preliminary and final development plans as part of a memorandum to the City Council from Community Development Director Kyle Klatt for the July 1, 2014 Council Meeting; and

WHEREAS, the City Council reviewed the preliminary and final development plans for the Eagle Point Medical Building at its meeting held on July 1, 2014.

NOW, THEREFORE, based upon the testimony elicited and information received, the City Council makes the following:

FINDINGS

- 1) That the procedure for obtaining approval of said Preliminary and Final PUD plans is found in the Lake Elmo City Code, Section 154.800.

- 2) That all the requirements of said City Code Section 154.800 related to the Preliminary and Final PUD plans have been met by the Applicant.
- 3) That the proposed Preliminary and Final PUD Plans for the Eagle Point Medical Building consists of a 28,500 square foot, two story, multi-tenant medical building with 166 parking stalls in accordance with plans and specifications submitted to the City and will be located on property legal described as follows: Lot 1, Block 1 of Eagle Point Business Park 7th Addition.
- 4) That the proposed PUD will allow a more flexible, creative, and efficient approach to the use of the land.
- 5) That the proposed PUD is in conformance with the Comprehensive Guide Plan for the City.
- 6) That the proposed PUD complies with the Lake Elmo Zoning District regulations for the BP – Business Park Zoning District.
- 7) That the proposed PUD complies with the development and design standards for the Eagle Point Business Park.
- 8) That the proposed PUD is consistent with the Lake Elmo Design Guidelines and Standards Manual, and specifically, the guidelines for business park development.
- 9) That the proposed PUD meets the identified objectives associated with a Planned Unit Development project as listed in Section 154.801 of the Lake Elmo Zoning Ordinance.

NOW, THEREFORE, BE IT RESOLVED THAT the City Council does hereby approve the Preliminary and Final PUD Plans for the Eagle Point Medical Building subject to the following conditions:

- 1) The landscape plan shall be reviewed and approved by the City's consulting landscape architect. Any recommended revisions shall be incorporated into the plan prior to the issuance of the building permit for the medical building.
- 2) All required modifications to the plans as requested by the City Engineer in a review letter dated June 18, 2014 shall be incorporated into the plans prior to approval of a building permit for the medical building.
- 3) The applicant shall address all review comments from Washington County as documented in a review letter dated June 17, 2014 prior to the issuance of a building permit for the medical building.
- 4) The applicant shall follow all of the rules and regulations spelled out in the Wetland Conservation Act, and shall acquire the needed permits from the South Washington

Watershed District prior to the commencement of any grading or development activity on the site.

- 5) The applicant shall enter into a maintenance agreement with the City concerning the storm water infiltration areas prior to the issuance of a building permit for the medical building.
- 6) The applicant shall pay a fee in lieu of park land dedication as determined by the City prior to the final plat being released for recording.
- 7) The applicant shall secure a sign permit for all signage associated with the proposed medical building. The Community Development director shall review all such signs for conformance with the Eagle Point Business Park Design and Development Standards.

Passed and duly adopted this 1st day of July 2014 by the City Council of the City of Lake Elmo, Minnesota.

Mike Pearson, Mayor

ATTEST:

Adam Bell, City Clerk

Date Received: _____
Received By: _____
Permit #: _____



651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

LAND USE APPLICATION

- ☐ Comprehensive Plan ☐ Zoning District Amend ☐ Zoning Text Amend ☐ Variance*(see below) ☐ Zoning Appeal
- ☐ Conditional Use Permit (C.U.P.) ☐ Flood Plain C.U.P. ☐ Interim Use Permit (I.U.P.) ☐ Excavating/Grading
- ☐ Lot Line Adjustment ☐ Minor Subdivision ☐ Residential Subdivision Sketch/Concept Plan
- ☐ PUD Concept Plan ☒ PUD Preliminary Plan ☒ PUD Final Plan

Applicant: DAVIS Real Estate Services
Address: 222 South 9th Street, #3255, Minneapolis, MN 55402
Phone #: 612-341-3242
Email Address: mdavis@davisrealestatemn.com

Fee Owner: United Land, LLC
Address: 3600 American Blvd. West, #750, Bloomington, MN 55431
Phone #: 952-837-8525
Email Address: bill.katter@uproperties.com

Property Location (Address and Complete (long) Legal Description): See attached.

Detailed Reason for Request: New medical building development.

*Variance Requests: As outlined in Section 301.060 C. of the Lake Elmo Municipal Code, the applicant must demonstrate practical difficulties before a variance can be granted. The practical difficulties related to this application are as follows:

N/A

In signing this application, I hereby acknowledge that I have read and fully understand the applicable provisions of the Zoning ordinance and current administrative procedures. I further acknowledge the fee explanation as outlined in the application procedures and hereby agree to pay all statements received from the City pertaining to additional application expense.

Signature of applicant: [Signature] Date: 5-22-14

Signature of fee owner: [Signature] Date: 5/22/14

Bill Katter
United Properties



7001 France Avenue South
Suite 200
Edina, Minnesota 55435

p 952.893.9020
f 952.893.9299

June 12, 2014

Kyle Klatt

The City of Lake Elmo
3800 Laverne Ave. N.
Lake Elmo, MN 55042

Mr Klatt,

The following is a description of the Eagle Point Medical building being proposed for the southeast corner of Inwood Avenue N. and Eagle Point Blvd. in Lake Elmo, Minnesota

Description:

The proposed Eagle Point Medical Building is a 28,500 square foot two story multi-tenant medical building. The site is 3.77 acres and includes a surface parking lot with 166 parking spaces. The parking lot is accessed at the south end of the site with parking aisles oriented perpendicular to the building to minimize patient - vehicular conflicts. An entrance canopy offers a covered drop off and pick up point at the building entrance for patients. Rain gardens provide infiltration and storm water maintenance. The exterior of the building includes a brick and stone veneer exterior wall with large window areas at the entrance lobby and building corners and metal accent banding.

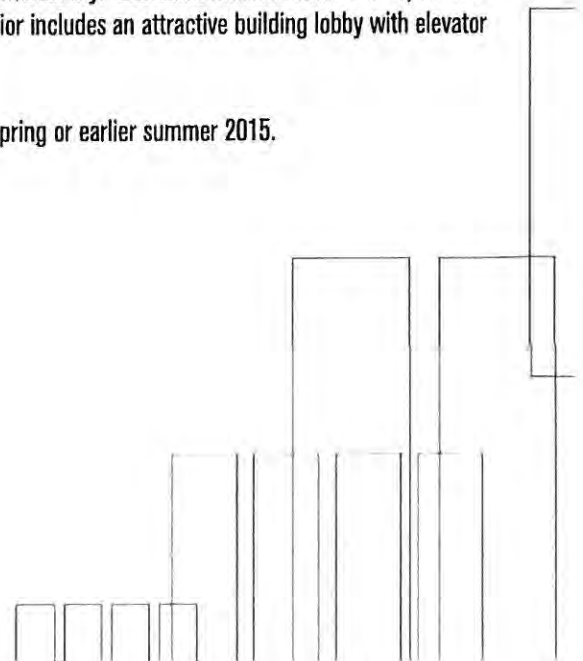
The primary building tenant is Noran Neurological Clinic occupying the entire first floor of the facility. The clinic includes exam rooms, an MRI suite for diagnostic imaging and an infusion bay for cancer treatments. The building interior includes an attractive building lobby with elevator and open staircase to the second floor.

The project is scheduled to begin construction in August of 2014 with completion in late spring or earlier summer 2015.

Sincerely,

A handwritten signature in black ink, appearing to read 'Patrick J. Giordana'.

Patrick J. Giordana AIA
VP Architecture



Site Development Summary

- ZONING: PUD EAGLE POINT BUSINESS PARK
- PROPERTY AREA: 164,347 SF (3.77 AC)
- EXISTING SITE
 - PERVIOUS SURFACE: 154,435 SF (94.0%)
 - IMPERVIOUS SURFACE: 9,912 SF (6.0%)
- PROPOSED SITE
 - PERVIOUS SURFACE: 71,220 SF (43.3%)
 - IMPERVIOUS SURFACE: 93,127 SF (56.7%)
- BUILDING GROSS SIZE: 28,500 SF
- BUILDING SETBACK PER CODE: 50'=FRONT
10'=SIDE
30'=REAR
- PARKING SETBACK PER CODE: 20'=FRONT
10'=REAR
20'=SIDE WITH INTERIOR ROAD
- PARKING SPACE/DRIVE AISLE: 9' WIDE X 18' LONG, 22' AISLE
- PARKING RATIO REQUIREMENT:
 - MEDICAL: 1 SPACES / 200 SF OF BLDG
 - TOTAL SPACES REQUIRED: $(28,500 \text{ SF}) / (1 \text{ SPACE}/200 \text{ SF}) = 143$
- PARKING PROVIDED: 166

DESCRIPTION OF PROPERTY SURVEYED

Lot 1, Block 1, EAGLE POINT BUSINESS PARK 7TH ADDITION, according to the recorded plat thereof, Washington County, Minnesota.

PROPOSED UTILITY EASEMENT

A 40.00 foot easement over, under and across part of Lot 1, Block 1, EAGLE POINT BUSINESS PARK 7TH ADDITION, according to the recorded plat thereof, Washington County, Minnesota. The centerline of said easement is described as beginning at the intersection of the west line of said Lot 1 with the north line of the south 20.00 feet of said Lot 1; thence South 88 degrees 46 minutes 25 seconds East, assumed bearing, along said north line of the south 20.00 feet of Lot 1 a distance of 298.98 feet; thence North 27 degrees 12 minutes 26 seconds East a distance of 167.63 feet to the northeasterly line of said Lot 1 and said centerline there terminating. Except that part of said easement that lies within the drainage and utility easements as dedicated in EAGLE POINT BUSINESS PART 4TH ADDITION and said EAGLE POINT BUSINESS PARK 7TH ADDITION, according to the recorded plats thereof, Washington County, Minnesota.

MEMORANDUM

Westwood Professional Services

7699 Anagram Drive
Eden Prairie, MN 55346

Voice 952-937-5150
Fax 952-937-5822
Toll Free 1-888-937-5150
Email wps@westwoodps.com
www.westwoodps.com



Date: May 15, 2014

Re: **Eagle Point Medical Center**
Inwood Ave. N. / Eagle Point Blvd.
Stormwater Narrative
File 0003667.00

To: City of Lake Elmo

From: PJ Disch, PE

Project Description

The project site is located on the SE corner of the intersection between Inwood Avenue North and Eagle Point Boulevard in Lake Elmo, MN. The proposed project scope is to construct an 18,800 SF, two-story, medical center with an adjacent parking lot. The existing property is 3.77 acres.

Existing and Proposed Drainage Areas

This is the breakdown of the existing and proposed areas.

Existing

Pervious:	154,435 SF
Impervious:	9,912 SF
Total:	164,347 SF

Proposed

Pervious:	71,220 SF
Impervious:	93,127 SF
Total:	164,347 SF

Existing Drainage Conditions

The existing property consists of three drainage areas (see attached drainage map). Stormwater that falls on the north side of the property drains into Eagle Point Boulevard as well as the ditch running along Inwood Avenue North. Stormwater that falls on the remaining middle and south portions of the property drain to the existing road to the east. The existing property has minimal impervious surfaces. The property consists of mainly soils from Hydraulic Soil Group B which will allow for adequate infiltration.



Proposed Drainage Conditions

The proposed property will be composed of four drainage areas (see attached drainage map). Area 1P will model all stormwater that drains offsite during a rain event. Areas 2P-4P will model runoff that drains to each of three proposed rain gardens on site.

Runoff Storage Volume

The City of Lake Elmo requires that all stormwater that falls over impervious areas from the 1" storm event be infiltrated on site. To comply, three rain gardens will be constructed along the east side of the property. The rain gardens will have drain tile with a gate valve installed to help mitigate ponding, if the soils prove to be inadequate for infiltration. The following are the calculations for our volume requirement.

Volume Required for New Impervious Area

$$(93,127 \text{ SF}) \times (1.0 \text{ in}) \times (1 \text{ ft} / 12 \text{ in}) = 7,761 \text{ CF}$$

If the soils do not allow for infiltration on site, the volume requirement will increase by 30%.

$$7,761 \text{ CF} \times 1.3 = 10,089 \text{ CF}$$

Proposed Volume in Rain Garden #1

Area of 1000.5' Contour	2,027 SF
Volume from 997.0' to 1000.5'	$(2,027 \text{ SF}) \times (3.5 \text{ ft}) = 7,095 \text{ CF}$
Open Volume from 999.0' to 1000.5'	2,250 CF (from HydroCAD)
Volume of Planting Medium	$(7,095 \text{ CF}) - (2,250 \text{ CF}) = 4,845 \text{ CF}$
Void Volume of Planting Medium	$(4,845 \text{ CF}) \times (0.40) = 1,938 \text{ CF}$
Total Volume (Storage)	Void Volume of Planting Medium + Open Volume $(1,938 \text{ CF}) + (2,250 \text{ CF}) = 4,188 \text{ CF}$

Proposed Volume in Rain Garden #2

Area of 996.5' Contour	2,387 SF
Volume from 993.0' to 996.5'	$(2,387 \text{ SF}) \times (3.5 \text{ ft}) = 8,355 \text{ CF}$
Open Volume from 995.0' to 996.5'	2,721 CF (from HydroCAD)
Volume of Planting Medium	$(8,355 \text{ CF}) - (2,721 \text{ CF}) = 5,634 \text{ CF}$



Void Volume of Planting Medium	$(5,634 \text{ CF}) \times (0.40) = 2,254 \text{ CF}$
Total Volume (Storage)	Void Volume of Planting Medium + Open Volume $(2,254 \text{ CF}) + (2,721 \text{ CF}) = 4,975 \text{ CF}$

Proposed Volume in Rain Garden #3

Area of 994.5' Contour	2,450 SF
Volume from 991.0' to 994.5'	$(2,450 \text{ SF}) \times (3.5 \text{ ft}) = 8,575 \text{ CF}$
Open Volume from 993.0' to 994.5'	2,900 CF (from HydroCAD)
Volume of Planting Medium	$(8,575 \text{ CF}) - (2,900 \text{ CF}) = 5,675 \text{ CF}$
Void Volume of Planting Medium	$(5,675 \text{ CF}) \times (0.40) = 2,270 \text{ CF}$
Total Volume (Storage)	Void Volume of Planting Medium + Open Volume $(2,270 \text{ CF}) + (2,900 \text{ CF}) = 5,170 \text{ CF}$

Total Volume Provided = 4,188 CF + 4,975 CF + 5,170 CF = 14,333 CF

Volume provided is greater than volume required, therefore no additional volume is necessary.

Stormwater Runoff Summary

The following table summarizes the existing conditions and post-development peak runoff rates from the site for the 2, 10, and 100-year storm events.

Rate Control

Drainage Area	Storm Event		
	2-Year (2.80") (cfs)	10-Year (4.17") (cfs)	100-Year (7.35") (cfs)
Existing	2.73	7.05	19.34
Proposed	0.82	4.65	18.42

Attachments

- A Existing and Proposed Drainage Area Maps
- B Existing and Proposed HydroCAD Model



Lake Elmo City Hall
651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

Development Standards for Eagle Point Business Park

300.12 subd. (1)(B) and 300.12(2). No Business Park structure shall exceed 60' in height. Parapet walls shall not exceed more than 4' above the height permitted of the building.

300.13 subd (6)(A)(3) Parking spaces shall be 9' X 18' at the developments north of Hudson Blvd and 10' X 20' South of Hudson Blvd, with the exception of the hotel and restaurant which needs to be 9' X 18' spacing. Parking spaces for the handicapped shall be minimum of 12' X 20'. 20% of the spaces in an office development can be compact car size 8' X 16". The size of parking space may be altered upon approval of Zoning Administrator.

300.13 subd. (6)(B)(6)(D) The primary landscaping materials shall be shade trees with shrubs, hedges, etc., used only to complement trees, not as the sole means of landscaping. Landscaping and irrigation will be done on a project by project basis. If landscaping within the NSP easement is disturbed, it needs to be replaced by NSP, or the property owner. Berming and landscaping must be approved within one year of City Council approval.

Permitted Uses: Banks, medical clinics, offices, schools (business, professional and private trade), office showrooms.

Conditional Uses: Business services, conference centers, health clubs, hotels and motels, day care centers, limited retail, medical, dental and research labs, recording studios, restaurants and cafeterias, theaters, teleconferencing transmission facilities, veterinary clinics, and hotel.

Minimum Lot Area: 2.0 acres. Lots less than 2.0 acres might be approved on a project by project basis through a conditional use permit.

Minimum Lot Frontage: 200' with the exception of 50' pm a cul-de-sac.

Lot Width/Depth Ratio: 1/3.5

Minimum Building Setbacks: 50' for the front and street frontage, and 10' for side and rear frontage. When abutting residential uses, the current ordinance requirement will apply.

Minimum Parking Setbacks: Front: 20', Side: 10', Side(street): 20', Rear: 10'

Minimum Building Foundation Size: 10,000 s.f.

Parking Ratio: One space for each 250 s.f. of office building area or one space per two employees, whichever is greater. One space per 2,000 s.f of storage, warehouse and 1 space per 1,000 s.f. of showroom.

Pathways: 8' wide pathways as part of the City's trail system and the City will maintain.

Storm Water Control: Storm water management requirements should be averaged over the PUD area as a whole. The tributary setback will be 30' from the tributary setback, and the parking setback will be 10' from the tributary setback.

Lighting Height: 30' maximum

Sign Height: Businesses can have signage on the building and a monument sign at the property's entrance, and that a pylon sign must be approved on a case by case basis by the City.

Pond Maintenance: Recommend the creation of a District to take care of pond maintenance.

4 Business Park Development

Applicable Zoning Districts:

- Business Park/Light Manufacturing (BP)

The intent of this zoning district is to provide opportunities for high quality business park development for office, light manufacturing, and other non-retail uses. In terms of design, this manual establishes consistent architectural standards between various users in the BP district. These architectural standards are intended to promote a coordinated identity and avoid mismatched design. In addition, the manual ensures the installation of open-space character within business park development through effective site design and landscaping.

A. Site Design

Building Placement

Goal: To establish standards for building location that ensure effective automobile and pedestrian circulation and promote coordination of buildings between adjacent sites and structures.

- Buildings must be setback at least 50 feet from the public right of way.
- Buildings should be located in a manner that allows for effective automobile and pedestrian circulation.
- Shared access points from the public ROW are encouraged. Vehicular access points should be limited to minimize traffic disruption.
- The orientation of buildings should be compat-



Utilizing natural topography within business park development adds aesthetic value and reinforces Lake Elmo's open space character.

- ible with adjacent structures and sites.
- Utilizing the natural topography or features of the site is encouraged to create unique landscapes and add visual interest and value to the design.
- The provision of common and open spaces to the rear of the site is encouraged for the use of employees and visitors, reinforcing Lake Elmo's open-space character.



Street trees provide an attractive streetscape, as well as help screen and provide shade within parking areas.

Streetscape

Goal: To provide high quality landscaping in areas visible from the public view, as well as promote pedestrian connections in the BP district.

- Street trees shall be installed at regular intervals along the public right of way. Species of street trees should be selected according to root zone and salt tolerance.
- Additional landscaping along public streets is encouraged. Landscape materials should be low-maintenance and native to ensure heartiness.
- Sidewalks along the public right of way are encouraged to extend pedestrian connections throughout the BP district.
- Utilizing site amenities as guided by Lake Elmo Theming Study is encouraged.

BUSINESS PARK DEVELOPMENT

Landscaping

Goal: To reduce continuous hardscape and impervious surface, as well as ensure development of a high visual quality.

- a. Trees, plant beds, and potted plants should be installed in parking, sidewalk, and other hard surfaced areas to add visual interest and break up continuous impervious surface.
- b. Parking, service, storage and utility areas should be buffered by plantings, particularly when in view of public streets. Near areas of pedestrian circulation, these plantings shall not exceed 4 feet in height for safety purposes.
- c. Hardy and native plant materials that are resistant to the climate, disease and salt are encouraged.
- d. Making use of similar plant materials as adjacent properties and public spaces is encouraged to create continuity.
- e. Mature trees located on building sites should be retained whenever possible.
- f. Bare soils should be planted or mulched with bark, stone or other suitable material to avoid unnecessary runoff.



Landscape islands greatly improve that character of surface parking lots.

Parking

Goal: To adequately serve the parking needs of businesses in the BP district, while ensuring pedestrian safety, reduced impervious surface, and a high quality visual aesthetic and appearance.

- a. The linear measurement of surface parking areas parallel to the public street are encouraged not to exceed more than 75% of primary street frontages. Sites or projects that are unable to meet this guideline are encouraged to install berms and/or additional landscaping to buffer areas of surface parking adjacent to the primary street frontage.
- b. The entrance to parking facilities should be located on secondary streets when possible.
- c. Shared parking facilities between adjacent uses or businesses are strongly encouraged when possible to avoid excessive amounts of parking.

- d. Structure parking is encouraged, and should be located behind or beneath primary buildings when possible.
- e. Structure parking or parking areas located beneath the primary structure should be screened with architectural elements that match the primary building.
- f. Parking areas should be screened from view of public streets by means of grading and/or landscaping.
- g. Parking areas should be screened from adjacent structures with landscaping strips not exceeding 4 feet in height in order to ensure pedestrian safety.
- h. Landscaped islands should be installed within surface parking areas to break up continuous hardscape and reduce concentration of impervious surface.
- i. Lighting must be provided in parking areas at night for safety purposes. However, direct glare, spillover or other forms of light pollution directed at adjacent properties are prohibited.
- j. Parking facilities must be ADA compliant when deemed necessary.

Delivery, Service, Storage and Utility Areas

Goal: To provide physical and visual separation of delivery, service, storage and utility areas from the public right of way and areas of automobile and pedestrian circulation.

- a. Delivery, service, storage, maintenance or trash collection areas should be located out of the view from the public right of way, or significantly

BUSINESS PARK DEVELOPMENT

screened through landscaping or architectural features that match the primary structure.

- b. Service, storage and trash collection areas are not allowed in the setback areas.
- c. The location of delivery, storage and service areas should be clearly marked with signage and should not interfere with other automobile or pedestrian circulation.
- d. Storage and delivery areas should be hard surfaced, minimizing the dispersal of dust.

B. Building Design

Form and Facade

Goal: To promote buildings of high architectural quality and creativity in design.

- a. Blank facades without windows and doors are discouraged. All sides of the structures shall have architectural treatments.



Garbage collection areas should be located to the rear of the site and screened using materials that match the principal structure.

- b. Window and door styles should reflect the prevailing architectural style of the structure.

c. Variety and creativity in building materials, encouraged through changes in building materials, fenestration height, and roof lines. Primary facades should not present a continuous wall without architectural details that add visual interest.

- d. Minimizing continuous expanses of wall through facade articulation, recession or projection is encouraged.

e. Architects and builders are encouraged to incorporate topographical features into the form of the structure when possible, utilizing natural grades to create unique design.

Building Materials

Goal: To promote quality development through durability and visual aesthetics, thereby supporting district identity.



Corner treatments to larger structures add visual interest and break up monotonous design.

- a. High-quality and durable materials should be used in street facing facades.
- b. Primary building materials for structures in the BP district should include brick, stone, cast stone, quality metals, glass, Exterior Insulation Finish Systems (EIFS), or pre-cast concrete panels with exposed aggregate, banding, texturing or other similar decorative finish treatment.
- c. High quality synthetic materials that adequately duplicate natural materials may be acceptable if approved by the City, including, but not limited to, thin brick, hardi plank, decorative concrete masonry, and other materials.
- d. The following building materials are not allowed to be used as primary finished facade material for business park development:
 - Unpainted galvanized metal
 - Red "green-treated" lumber
 - Unfinished wood
 - Plain or unpainted concrete



Long expanses of wall can be broken up using windows and other treatments.

BUSINESS PARK DEVELOPMENT



High quality building materials are required for street-facing facades.

- Painted concrete block may be used on the rear of the building or sides not visible from the public right of way.
- g. Facade colors should reflect subtle earth tones. However, other primary facade colors will be considered by the Review Authority. Accent materials shall complement the colors of the primary facade.

Scale and Mass

Goal: To reduce the appearance of mass in the BP district.

- a. Scale should be reduced by utilizing “step-down” methods, particularly near areas of pedestrian circulation. Entries and other bump out features are effective in this regard.
- b. Structures of two-stories or higher should utilize facade treatments, such as multiple building materials or additional windows, to minimize the appearance of mass.



Parapets of varying height provide additional architectural detail that add aesthetic value.

Roof Design

Goal: To ensure architectural consideration and consistency in roof design in relation to the architecture of the building, and to reduce the visual impact of rooftop equipment.

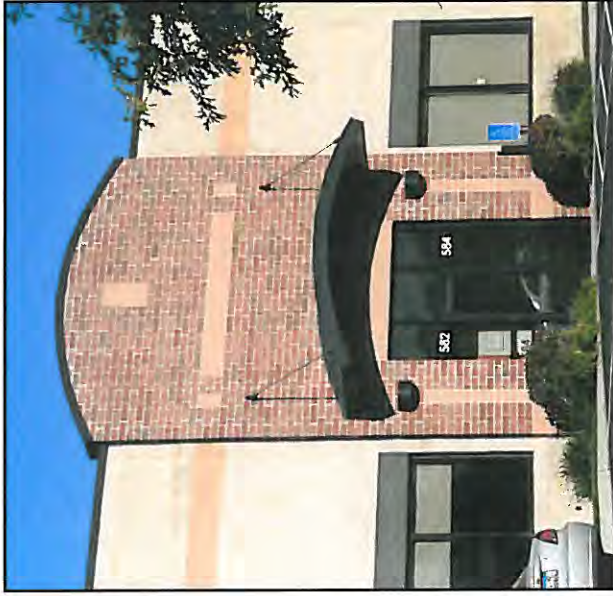
- a. The design of the roof must be consistent with the overall architecture or design of the structure.
- b. Parapets of varying heights are required for buildings in the BP district with flat roofs.
- c. Rooftop equipment, particularly on flat roofs, must be screened by the parapet or other architectural features.

Entries

Goal: To provide identifiable entryways that emphasize access, pedestrian safety, architectural quality and a human scale.

- a. Entryways to buildings in the BP district should be accessible for pedestrians from the public right of way.
- b. Architectural features should be incorporated into entryways, such as facade detailing or prominent windows.
- c. The use of canopies, awnings and other sheltering features are encouraged.
- d. Pedestrian amenities such as ornamental

BUSINESS PARK DEVELOPMENT



Canopies and changes in building materials help accentuate entryways.

trash receptacles, benches or lighted bollards are encouraged near entryways to buildings in the BP district.

Lighting

Goal: To ensure safety of patrons, employees, pedestrians and automobiles, as well as providing visual interest and aesthetic value to a site, while limiting light pollution of the night sky to the best extent possible.

- Lighting must be provided in entryways, parking areas, pedestrian ways, storage and serving areas.

- Lighting height shall be consistent with City's exterior lighting standards.
- Lighting styles should be complementary to the architectural style of the building.
- Lighting of architectural features should be used to provide accent and interest, as well as identify the building entryway. Architectural lighting must be downcast and shielded to prevent light pollution.
- Bollard lighting is encouraged for pedestrian areas.
- Overhead lighting must be shielded to prevent light trespass and spill-over onto adjacent properties.
- Buildings near residential zones must utilize lighting that minimizes light trespass.
- Bare bulb and exposed neon lighting are not allowed.

Signage

Goal: To provide signage that clearly identifies businesses within the BP district, while promoting quality and consistency in terms of design and materials.

- Building signage shall be complementary to the architecture of the structure, as well as consistent with the style of the surrounding buildings or district as a whole.
- Sign elements that will be evaluated for consistency include scale, color, lighting and materials.
- Signs must be constructed of high quality, durable materials.
- Directional signage to delivery, service and storage areas is required.
- Two and three-dimensional signs are encouraged to promote creativity and district identity.
- All buildings are encouraged to incorporate elements of community theming in appropriate signage, supporting district and city identity.



Two and three-dimensional signage provides creativity and visual interest.

MEMORANDUM

FOCUS ENGINEERING, inc.

Cara Geheren, P.E. 651.300.4261
Jack Griffin, P.E. 651.300.4264
Ryan Stempski, P.E. 651.300.4267
Chad Isakson, P.E. 651.300.4285

Date: June 18, 2014

To: Kyle Klatt, Planning Director
Cc: Nick Johnson, City Planner

Re: Eagle Point Medical Center
Plan Review

From: Jack Griffin, P.E., City Engineer

An engineering review has been completed for the Eagle Point Medical Center. Site Plans were received on June 11, 2014. The submittal consisted of the following documentation:

- Site Plans (Electronic only) prepared by Westwood Professional Services, dated 06/11/2014.

Engineering review comments are as follows:

General Comments:

1. Engineering has not reviewed the proposed lighting plan, landscape plan, or building plans.
2. The City public utilities must be field located to ensure design conflicts are avoided and incorporated into the plan set before the plan set is approved for construction.

Stormwater Management:

1. The Storm Water Management Plan must meet South Washington Watershed District (SWWD) rules and requirements. The applicant must provide the City written approval from the SWWD.
2. Soil borings need to be taken at all infiltration basin locations to determine feasibility.
3. The proposed storm water facilities will receive storm water from a privately owned and maintained storm sewer system that will not be constructed to City engineering design standards. It is therefore recommended that the storm water facilities and storm sewer system be privately owned and maintained. A maintenance agreement in a form acceptable to the City should be executed and recorded with the County for all permanent storm water facilities to be located on private property. The agreement shall provide a maintenance plan defining the maintenance responsibilities for the private owner, the type of maintenance and the maintenance intervals.

Utility Plans:

1. The proposed connection location to City water requires further review and possible plan revision. The water service connection needs to consider existing service stubs to the property, fire hydrant locations within the site, and delineation of ownership of the watermain between City and property owner.
2. The proposed connection to City sanitary sewer must be relocated to move the proposed manhole outside of the proposed drainage swale with positive drainage away from the manhole.
3. Please provide a detail for San. MH-1 and a connection detail for the proposed sanitary sewer service.



Public Works Department

Donald J. Theisen, P.E.
Director

Wayne H. Sandberg, P.E.
Deputy Director/County Engineer

June 17, 2014

Kyle Klatt
Community Development Director
City of Lake Elmo
3600 Laverne Avenue North
Lake Elmo, MN 55042

RE: Washington County comments on Eagle Point Medical Center, City of Lake Elmo

Dear Kyle:

Thank you for providing Washington County with the plans for Eagle Point Medical Center adjacent to County State Aid Highway (CSAH 13) Inwood Avenue in the City of Lake Elmo. The project consists of a 28,500 square foot medical office building with 166 parking spaces. Based on the plan provided, we have the following comments:

- Washington County commented on the plans for the Eagle Point Business Park in 2001 and worked with the city on review and approval of the Eagle Point Boulevard/ CSAH 13 intersection improvements. These streets as well as Hudson Boulevard, provide access to this existing parcel of record.
- Although this is not a subdivision plat, it is noted that there is adequate county and public right-of-way along CSAH 13 for the future.
- According to the *Trip Generation Manual, 7th Addition ITE, 2003*, this development will generate 1000 Average Vehicle Trips (AVT) per day.
- The developer or the city must submit the drainage report and calculations to our office for review of any downstream impacts to the county drainage system. Along with the drainage calculations, we will request written conclusions that the volume and rate of stormwater run-off into the county right-of-way will not increase as part of the project.
- Any grading within County right-of-way will require a Washington County Right of Way Permit.
- All utility connections for the development require Washington County Right of Way permits. Typically, these are the responsibility of the utility companies.

Thank you for the opportunity to comment on this project. If you have any questions, please contact me at 651-430-4362 or ann.pung-terwedo@co.washington.mn.us. For any permit applications, please contact Carol Hanson at Carol.hanson@co.washington.mn.us.

Regards,

A handwritten signature in black ink that reads "Ann Pung-Terwedo".

Ann Pung-Terwedo
Senior Planner

C: Carol Hanson, Office Specialist

EAGLE POINT MEDICAL CENTER

LAKE ELMO, MN

Project Information
EAGLE POINT MEDICAL CENTER

LAKE ELMO, MN



1 1/1001 - BUILDING RENDERING
T001 3/4" = 1'-0"



2 SITE LOCATION MAP
T001 3/4" = 1'-0"



CONTACT LIST	
OWNER REPRESENTATIVE	
DAVIS SERVICES REAL ESTATE GROUP 222 SOUTH 9TH STREET SUITE 200 MINNEAPOLIS, MN 55402 MARK DAVIS	EMAIL: DAVIS@DSR.COM PHONE: 612-341-3242
ARCHITECT	
BDM + YOUNG INTERIORS ARCHITECTURE 7001 FRANCE AVENUE SOUTH SUITE 200 EDINA, MN 55435 PATRICK J. GORDANA AIA	EMAIL: PGORDANA@BDHYOUNG.COM PHONE: 952-345-8306
GENERAL CONTRACTOR	
TMCO CONSTRUCTION 14700 20TH AVENUE S.E. SUITE 40 PLYMOUTH, MN 55447 TIM MCKEE	EMAIL: TMCKEE@TMCO-CONST.COM PHONE: 763-694-8251 JOB SUPERVISOR: RANDY CHRISTENSEN EMAIL: RCHRISTENSEN@TMCO-CONST.COM PHONE: 612-309-4730
CIVIL ENGINEER	
WESTWOOD 7009 ANAQUAN DR EDEN PRAIRIE, MN 55344 P.J. DESCH P.E.	EMAIL: PJDESCH@WESTWOODS.COM PHONE: 952-956-3211
STRUCTURAL ENGINEER	
KREICH, O'BRIEN, MUELLER & ASSOCIATES 6110 CABELL AVENUE RIVER GROVE HEIGHTS, MN 55076 MATT J. VAN HOOF	EMAIL: MVANHOOF@KOMAINC.COM PHONE: 651-785-4129
LANDSCAPE ARCHITECT	
BRODSKI CONSULTING 696 NORTHERIDGE COURT EAGAN, MN 55123 DEBRA BRODSKI	EMAIL: DBRODSKI@JACK.COM PHONE: 651-658-8023

SHEET INDEX	
Sheet Number	Sheet Name
TITLE	
T001	TITLE SHEET, CODE REVIEW, KEY PLANS
LANDSCAPE	
L1	LANDSCAPE PLAN, PLANTING DETAILS AND NOTES
CIVIL	
C0	BOUNDARY, LOCATION, TOPOGRAPHIC, AND UTILITY SURVEY
C1	SITE PLAN
C2	GRADING, DRAINAGE, EROSION CONTROL PLAN
C3	UTILITY PLAN
C4	DETAILS
C5	PHOTOMETRIC PLAN
C6	EASEMENT SKETCH
ARCHITECTURAL	
A200	FIRST FLOOR BUILDING SHELL FLOOR PLAN
A201	SECOND FLOOR BUILDING SHELL FLOOR PLAN
A300	EXTERIOR ELEVATIONS

Issue Record		
No.	Date	Description

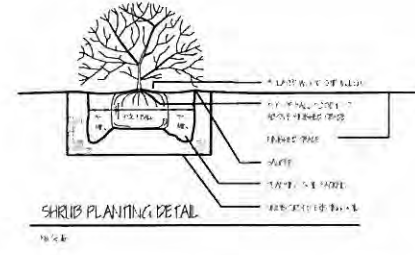
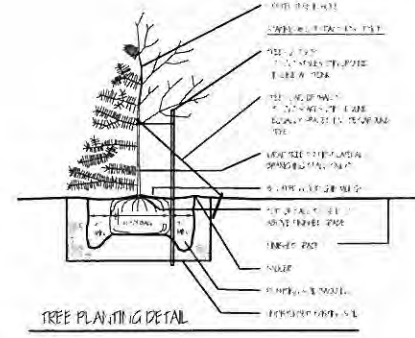
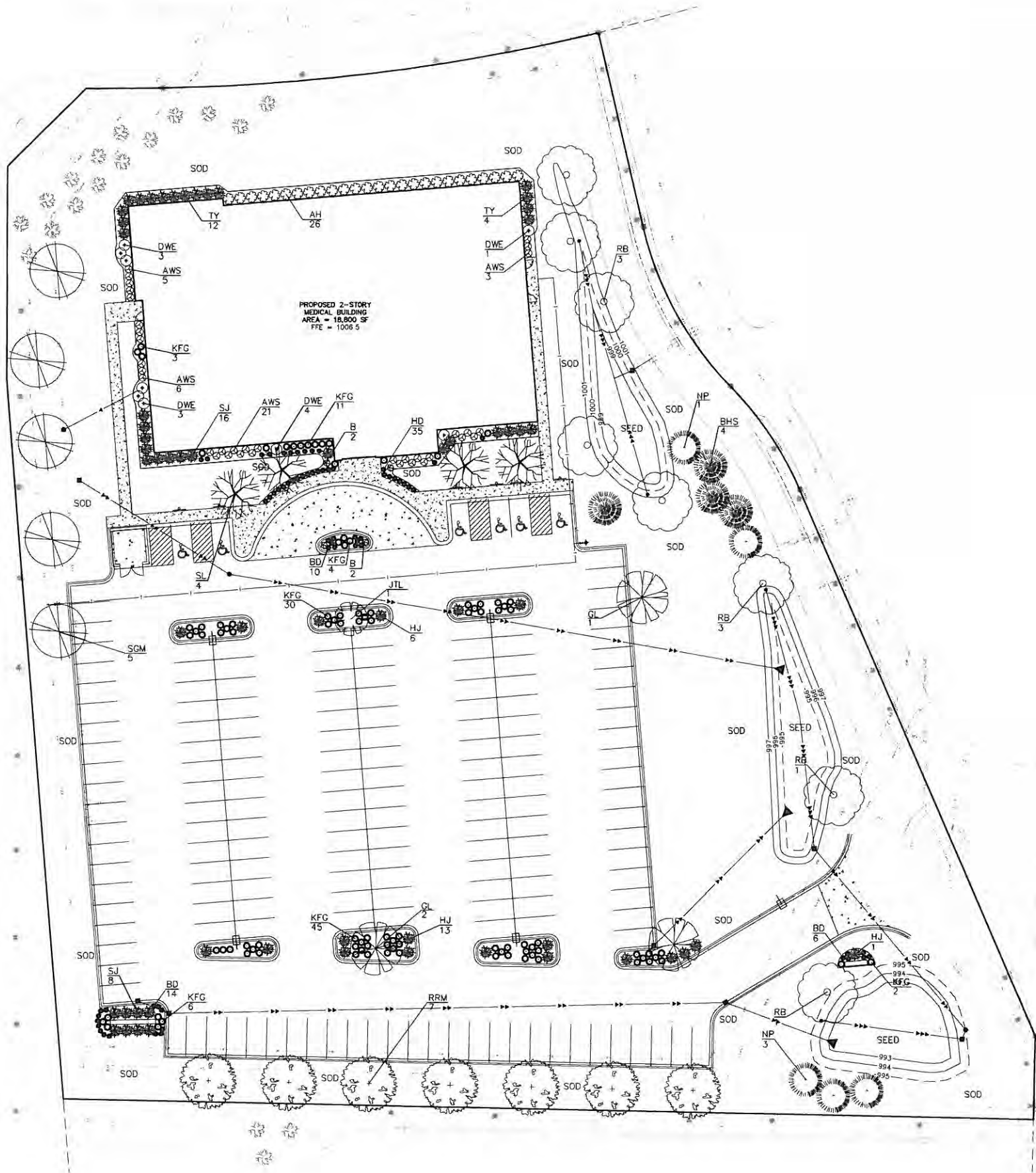
Sheet Title
TITLE SHEET, CODE REVIEW, KEY PLANS

Sheet Information

Job Code: A DAVIS/LEM
Drawn By: NCO/ML Reviewed By: PG

Date
05.21.2014

T001



PLANTING NOTES:

Contractor shall provide two year guarantee of all plant materials. The guarantee begins on the date of the Landscape Architect's written acceptance of the final planting. Replacement plant materials shall have a one year guarantee commencing upon planting.

All plants to be northern-grown and hardy. Plants to be installed on per standard AAS planting practices. Use minimum 12" loose planting soil on trees and 8" on shrubs.

Contractor shall verify locations with all utilities prior to installation of plants.

All landscaping, lawn grass, and seed and soil areas shall be irrigated with an underground drip system. Irrigation of trees and shrubs shall be installed and tested prior to planting.

Wrap all smooth-barked trees from top and bottom, remove by April 1.

Open top of plants on 80 materials; remove soil on planted plants; split and break apart plant pots.

Prune plants as necessary - per standard nursery practices.

Owner shall be responsible for maintenance after acceptance of the work by the Owner.

Plants shall be immediately planted upon arrival at site. Properly health-inspected if necessary.

All disturbed areas to be seeded unless otherwise noted. Soil shall be northern grown and hardy.

Planting holes for shrubs shall have 12" x 12" x 12" seed boxes. Seed shall be 1/2" x 1/2" x 1/2" washed River Rock mulch and 4" vertical (commercial grade) black poly edging. The edging shall be placed and sealed with mesh fabric.

Double shredded dark brown hardwood mulch 4" deep shall be provided around all new trees.

Seed infiltration basins with a seed mixture to be selected and approved by the Owner and City.

Field adjust foundation plantings if necessary to complement building architectural elements.

Noting notes are not the responsibility of the Landscape Architect. Contact the Project Engineer for details, questions, materials, and specifications for all planting work.

Construction and all items to be determined by the site Engineer.

REQUIRED PLANT MATERIALS

Based on the lot perimeter of 1175' and 27' Overstory Trees

1 Overstory tree per 50 L.F. = 23.5	27 Overstory Trees
1 Overstory tree per 15 parking stalls = 11	8 Conifer Trees
24 x 11 = 35 trees	1 Ornamental Tree
	120 Shrubs

PLANT LIST: Site Plantings

QTY	KEY BOTANICAL NAME	COMMON NAME	SIZE/ROOT
TREES			
5	SOD	Acer x Freeman 'Spartan'	2-1/2" B&B
7	RRM	Acer platanoides 'Royal Red'	2-1/2" B&B
8	RB	Betula nigra	8-10' B&B
4	SL	Chedoke triacanthos 'Sunset'	2-1/2" B&B
3	GL	Thuja occidentalis 'Greenlife'	2-1/2" B&B
4	SHS	Pinus strobus 'Spartan'	6" B&B
4	NP	Pinus resinosa	6" B&B
35	Total		
1	JTL	Syringa reticulata	1-1/2" B&B
SHRUBS			
9	DWE	Euonymus alatus 'Compactus'	5 Gal. Pot
26	AH	Hydrangea arborescens 'Annabelle'	5 Gal. Pot
35	AWS	Spiraea x bumalda 'Anthony Waterer'	5 Gal. Pot
10	HJ	Juniperus horizontalis 'Witcher'	5 Gal. Pot
24	SJ	Juniperus sibirica 'Scurdie'	5 Gal. Pot
16	TY	Taxus x media 'Toucan'	5 Gal. Pot
PERENNIALS & ORNAMENTAL GRASSES			
24	KFG	Colerogonella scutellaria 'Karl Forester'	3 Gal. Pot
30	BD	Hemerocallis sp.	1 Gal. Pot
35	HD	Hemerocallis sp.	1 Gal. Pot
4	B	Accent boulders set 1/3 into grade 28"-36" Dia. Select for size and shape.	
SOD & SEED			
Sod in all areas noted. Drought tolerant Highland sod.			
Seed infiltration basins with mulch/wetland mixture to be approved by Owner and City.			

Westwood

Westwood Professional Services, Inc.
3701 12th Street North, Suite 204
St. Cloud, MN 56303

PHONE 320-253-8495
FAX 320-253-8737
TOLL FREE 1-800-270-8495
www.westwoodps.com

Designed: ☐ **DB**
Checked: ☐ **DB**
Drawn: ☐ **DB**
Record Drawing by/date:

Revisions:

1. 05/21/14 DTM SUBMITTAL
2. 06/11/14 DTM REVISIONS

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly licensed Landscape Architect under the laws of the State of Minnesota.

Dwora Brodsky
REG. NO. 23849 DATE: 5/11/14

BRODSKY CONSULTING
Landscape Architects & Planners
588 Northwood Court
St. Cloud, MN 56303
PHONE: 320-253-8023
FAX: 320-253-8737

Prepared for:

BDH & Young

7001 France Avenue South, Suite 200
Edina, Minnesota 55435

Eagle Point Medical Center
Lake Elmo, Minnesota

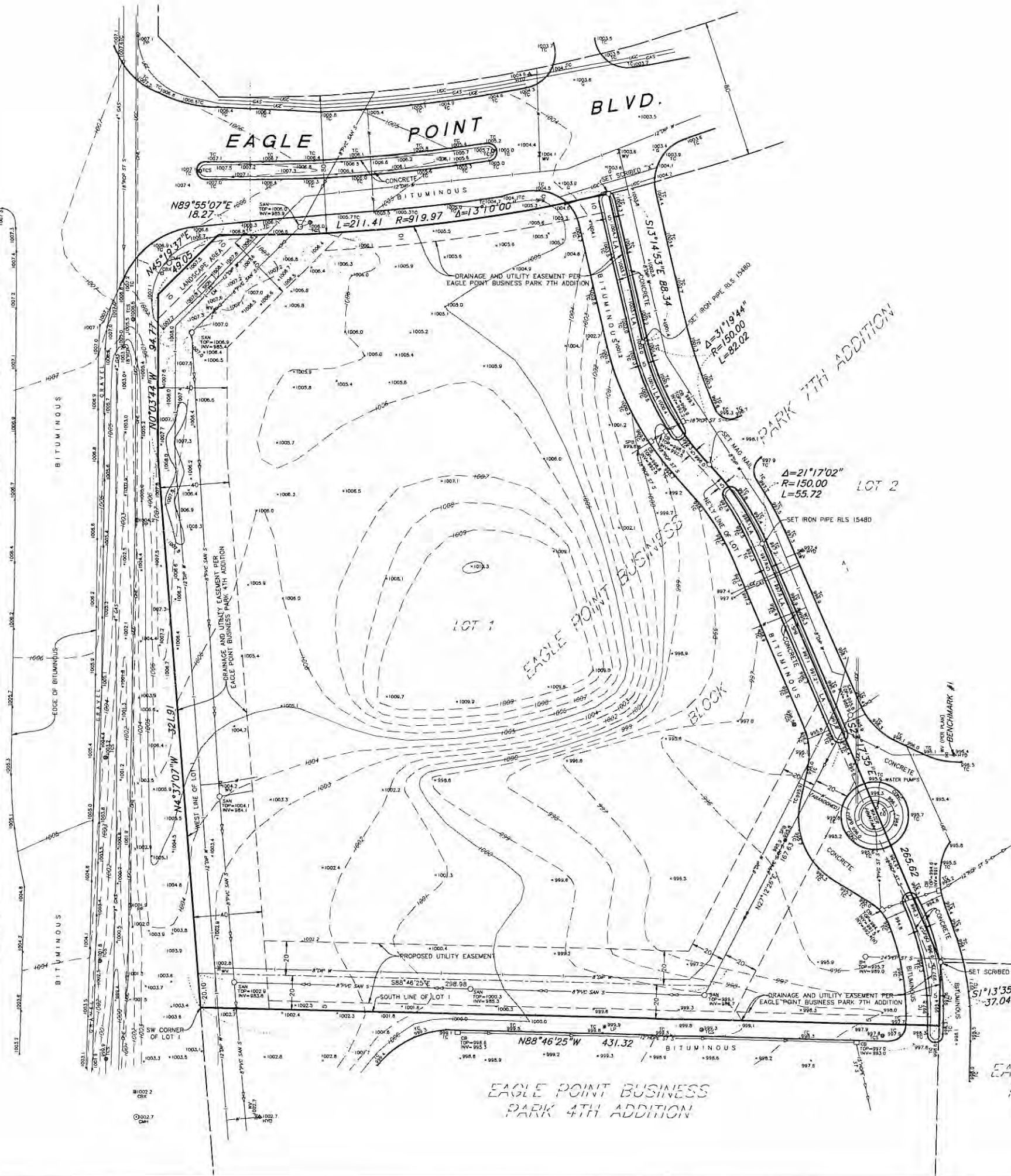
LANDSCAPE PLAN

Date: 05/21/14
Sheet: L1 OF 1
000366/7/PL01.dwg

NOT FOR CONSTRUCTION

0' 20' 40' 60'

INWOOD AVENUE N. (C.S.A.H. NO. 13)



DESCRIPTION OF PROPERTY SURVEYED

Lot 1, Block 1, EAGLE POINT BUSINESS PARK 7TH ADDITION, according to the recorded plat thereof, Washington County, Minnesota.

PROPOSED UTILITY EASEMENT

A 40.00 foot easement over, under and across part of Lot 1, Block 1, EAGLE POINT BUSINESS PARK 7TH ADDITION, according to the recorded plat thereof, Washington County, Minnesota. The centerline of said easement is described as beginning at the intersection of the west line of said Lot 1 with the north line of the south 20.00 feet of said Lot 1; thence South 88 degrees 46 minutes 25 seconds East, assumed bearing, along said north line of the south 20.00 feet of Lot 1 a distance of 298.98 feet; thence North 27 degrees 12 minutes 26 seconds East a distance of 187.63 feet to the northeasterly line of said Lot 1 and said centerline there terminating. Except that part of said easement that lies within the drainage and utility easements as dedicated in EAGLE POINT BUSINESS PARK 4TH ADDITION and said EAGLE POINT BUSINESS PARK 7TH ADDITION, according to the recorded plats thereof, Washington County, Minnesota.

NOTES

- 1.) Location and sizes of underground utilities shown hereon are approximate only and are shown based on field location of visible fixtures in combination with available data provided by various sources. Utilities shown are dependent on the completeness and accuracy of data provided. Other underground utilities of which we are unaware may exist. Verify all utilities critical to construction or design.
- 2.) Contact GOPHER STATE ONE CALL at 651-454-0002 (800-252-1166) for precise onsite location of utilities prior to any excavation.
- 3.) This survey was prepared without the benefit of current title work. Easements, appurtenances, and encumbrances may exist in addition to those shown hereon. This survey is subject to revision upon receipt of a current title insurance commitment or attorney's title opinion.
- 4.) Area = 164,347 Sq. Ft. or 3.773 Acres
- 5.) Survey coordinate and bearing basis: Washington County

LEGEND

BH	Denotes beehive catch basin
CB	Denotes catch basin
CBX	Denotes communication box
CMH	Denotes communication line
DIP	Denotes ductile iron pipe
EM	Denotes electric meter
EO	Denotes electric outlet
G	Denotes gutter
HDPE	Denotes high density plastic pipe
HHE	Denotes electric hand hole
HYD	Denotes fire hydrant
INV	Denotes structure invert
LA	Denotes landscaped area
LP	Denotes light pole
OHE	Denotes overhead electric line
PEP	Denotes polyethylene pipe
PP	Denotes power pole
PVC	Denotes plastic pipe
RCP	Denotes reinforced concrete pipe
SAN	Denotes sanitary manhole
SAN S	Denotes sanitary sewer
SPB	Denotes sprinkler box
ST S	Denotes storm sewer
TC	Denotes top of concrete curb
TCS	Denotes traffic control sign
UGC	Denotes underground communication line
UDE	Denotes underground electric line
W	Denotes water line
WV	Denotes water valve
	Denotes deciduous tree
	Denotes coniferous tree

BENCHMARKS

- 1.) Top of top nut of fire hydrant, approximately 39 feet southeast of the southwest building corner of 8530 Eagle Point Office Center. Elevation = 998.54 feet
- 2.) Top of tile at south main entrance to 8530 Eagle Point Office Center. Elevation = 997.10 feet

I hereby certify that this survey, plan, or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Dated this 23rd day of October, 2007.

SUNDE LAND SURVEYING, LLC

By: Mark S. Hanson
Mark S. Hanson, P.L.S. Minn. Lic. No. 15480

Revision	By	Date
	BMS	
Drawing Title: BOUNDARY, LOCATION, TOPOGRAPHIC and UTILITY SURVEY FOR: UNITED PROPERTIES		
SUNDE LAND SURVEYING www.sunde.com		
Main Office: 9001 East Bloomington Freeway (35W) • Suite 118 Bloomington, Minnesota 55420-3435 952-881-2455 (fax: 952-888-9526) North Office: Brooklyn Park, Minn. 763-784-9346		
Project: 95-155-M	By: Prg 800/70	Date:
Township: 29	Range: 21	Section: 33
File: 95155M001.dwg		Sheet: 1 of 1

Site Key Notes

- A B612 CONCRETE CURB & GUTTER
- B CONCRETE SIDEWALK
- C STANDARD DUTY BITUMINOUS PAVEMENT
- D HEAVY DUTY BITUMINOUS PAVEMENT
- E HANDICAP ACCESSIBLE SIGNAGE AND STRIPING
- F MATCH TO EXISTING CURB AND GUTTER
- G EXISTING CONCRETE CURB AND GUTTER TO REMAIN
- H PLANTED AREA
- I PROPOSED MONUMENT SIGN
- J TRASH ENCLOSURE
- K PARKING LOT STRIPING (SEE DETAIL)
- L CONCRETE PAVEMENT
- M FLUSH CURB CONDITION
- N 10' TRANSITION FROM B612 CURB TO FLUSH CURB
- O RAIN GARDEN

Site Legend

EXISTING	PROPOSED	
		PROPERTY LINE
		LOT LINE
		SETBACK LINE
		EASEMENT LINE
		CURB AND GUTTER
		FENCE
		CONCRETE PAVEMENT
		CONCRETE SIDEWALK
		HEAVY DUTY BITUMINOUS PAVEMENT
		NUMBER OF PARKING STALLS
		TRANSFORMER
		SITE LIGHTING
		TRAFFIC SIGN
		POWER POLE
		BOLLARD / POST

General Site Notes

- BACKGROUND INFORMATION FOR THIS PROJECT PROVIDED BY SUNDE LAND SURVEYING, BLOOMINGTON, MINNESOTA, OCTOBER 23, 2007.
- LOCATIONS AND ELEVATIONS OF EXISTING TOPOGRAPHY AND UTILITIES AS SHOWN ON THIS PLAN ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY SITE CONDITIONS AND UTILITY LOCATIONS PRIOR TO EXCAVATION/CONSTRUCTION. IF ANY DISCREPANCIES ARE FOUND, THE ENGINEER SHOULD BE NOTIFIED IMMEDIATELY.
- REFER TO BOUNDARY SURVEY FOR LOT BEARINGS, DIMENSIONS AND AREAS.
- ALL DIMENSIONS ARE TO FACE OF CURB OR EXTERIOR FACE OF BUILDING UNLESS OTHERWISE NOTED.
- REFER TO ARCHITECTURAL PLANS FOR EXACT BUILDING DIMENSIONS AND LOCATIONS OF EXITS, RAMPS, AND TRUCK DOCKS.
- ALL CURB RADI SHALL BE 3.0 FEET (TO FACE OF CURB) UNLESS OTHERWISE NOTED.
- ALL CURB AND GUTTER SHALL BE B612 UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGGERS AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. PLACEMENT OF THESE DEVICES SHALL BE APPROVED BY THE CITY AND ENGINEER PRIOR TO PLACEMENT. TRAFFIC CONTROL DEVICES SHALL CONFORM TO APPROPRIATE MNDOT STANDARDS.
- BITUMINOUS PAVEMENT AND CONCRETE SECTIONS TO BE IN ACCORDANCE WITH THE RECOMMENDATIONS OF THE GEOTECHNICAL ENGINEER.
- CONTRACTOR SHALL MAINTAIN FULL ACCESS TO ADJACENT PROPERTIES DURING CONSTRUCTION AND TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES.
- SITE LIGHTING SHOWN ON PLAN IS FOR REFERENCE ONLY. REFER TO LIGHTING PLAN PREPARED BY OTHERS FOR SITE LIGHTING DETAILS AND PHOTOMETRICS.

Site Development Summary

• ZONING:	PUD EAGLE POINT BUSINESS PARK
• PROPERTY AREA:	164,347 SF (3.77 AC)
• EXISTING SITE	
• PERVIOUS SURFACE:	154,435 SF (94.0%)
• IMPERVIOUS SURFACE:	9,912 SF (6.0%)
• PROPOSED SITE	
• PERVIOUS SURFACE:	71,220 SF (43.3%)
• IMPERVIOUS SURFACE:	93,127 SF (56.7%)
• BUILDING GROSS SIZE:	28,500 SF
• BUILDING SETBACK PER CODE:	50'-FRONT 10'-SIDE 30'-REAR
• PARKING SETBACK PER CODE:	20'-FRONT 10'-REAR 20'-SIDE WITH INTERIOR ROAD
• PARKING SPACE/DRIVE AISLE:	9' WDC X 18' LONG, 22' AISLE
• PARKING RATIO REQUIREMENT:	1 SPACES / 200 SF OF BLDG
• TOTAL SPACES REQUIRED:	(28,500 SF) / (1 SPACE/200 SF) = 143
• PARKING PROVIDED:	155



Westwood

Westwood Professional Services, Inc.
1701 12th Street North, Suite 206
St. Cloud, MN 56303
PHONE 320-253-9495
FAX 320-253-5737
TOLL FREE 1-800-270-9485
www.westwoodpl.com

Designed: JPD
Checked: JPD
Drawn: MKC
Record Drawing by/drawn:

Revisions:
1. 05/21/14 CITY REVIEW
2. 06/11/14 CITY APPROVED

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

JF Dick

Date: 06/11/14 License No. 49933

Prepared for:

BDH & Young

7001 France Avenue South, Suite 200
Edina, Minnesota 55435

**Eagle Point
Medical Center**

Lake Elmo, Minnesota

SITE PLAN

Date: 05/21/14
Sheet: C1 OF 5

0003667SP01.dwg

NOT FOR CONSTRUCTION



FORM NEW CURB AND GUTTER
AROUND EXISTING SANITARY
MANHOLE @ RE=1002.1

RADIUS SHOWN IS
TYPICAL FOR BOTH
CORNERS ADJACENT
TO ONE ANGLE INDEX

TYPICAL WITH ALL
PARKING ISLANDS

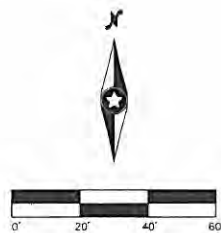
REMOVE EXISTING CONCRETE
CURB AND GUTTER FOR NEW
DRIVEWAY ENTRANCE. MATCH
PROPOSED BITUMINOUS DRIVEWAY
INTO EXISTING CONCRETE
ROADWAY PER CITY STANDARDS.

Grading Legend

EXISTING	PROPOSED	
		PROPERTY LINE
		INDEX CONTOUR
		INTERVAL CONTOUR
		CURB AND GUTTER
		SILT FENCE
		STORM SEWER
		FLARED END SECTION (WITH RIPRAP)
		WATER MAIN
		SANITARY SEWER
		DRAIN TILE
		ROCK CONSTRUCTION ENTRANCE
		SPOT ELEVATION
		INLET PROTECTION

Grading Key Notes (A)

- A SILT FENCE
B ROCK CONSTRUCTION ENTRANCE
C INLET PROTECTION



City of Lake Elmo - Grading/Erosion Control Notes

- THE CONTRACTOR SHALL CONDUCT OPERATIONS AND IMPLEMENT MINNESOTA POLLUTION CONTROL AGENCY (MPCA) BEST MANAGEMENT PRACTICES (BMP) TO CONTROL SITE SILTATION AND EROSION INTO DRAINAGE WAYS. THE CONTRACTOR SHALL COMPLY WITH ALL CONDITIONS AND COMPLETION DATES RELATIVE TO ALL PERMITS ISSUED FOR THE WORK TO BE COMPLETED. THE ENGINEER MAY ISSUE A STOP WORK ORDER FOR ALL DEVELOPMENT WORK AND BUILDING CONSTRUCTION FOR NONCOMPLIANCE WITH THESE MEASURES.
- SEQUENCING. ALL SILT FENCE AND OTHER EROSION CONTROL MEASURES SHALL BE IN PLACE AND APPROVED BY ENGINEER PRIOR TO ANY REMOVALS, EXCAVATION OR CONSTRUCTION AND SHALL BE MAINTAINED UNTIL Viable TURF OR GROUND COVER HAS BEEN ESTABLISHED AND APPROVED BY THE ENGINEER.
- SILT FENCE. THE CONTRACTOR SHALL INSTALL SILT FENCE AT THE LOCATIONS SHOWN ON THE PLANS AND IN ACCORDANCE WITH THE CITY STANDARD DETAILS. SILT FENCE DAMS AND INTERIM SUMPS SHALL BE PLACED TO INTERCEPT SILT FROM CONCENTRATED RUNOFF FROM OPEN GRADED AREAS. ADDITIONAL SILT FENCE SHALL BE REQUIRED AS DIRECTED BY THE ENGINEER.
- STOCKPILES. ALL STOCKPILE AREAS SHALL HAVE SILT FENCE OR SEDIMENT TRAPPING SYSTEMS PLACED AROUND THE ENTIRE PERIMETER.
- INLET PROTECTION. THE CONTRACTOR SHALL INSTALL INLET PROTECTION ON ALL EXISTING STORM SEWER INLETS IN ACCORDANCE WITH THE CITY STANDARD DETAILS. INLET PROTECTION SHALL ALSO BE PROVIDED ON ALL PROPOSED STORM SEWER INLETS IMMEDIATELY FOLLOWING CONSTRUCTION OF THE INLET. INLET PROTECTION MUST BE INSTALLED IN A MANNER THAT WILL NOT IMPOUND WATER FOR EXTENDED PERIODS OF TIME OR IN A MANNER THAT PRESENTS A HAZARD TO VEHICULAR OR PEDESTRIAN TRAFFIC.
- TEMPORARY SEDIMENT BASINS. THE CONTRACTOR SHALL INCORPORATE TEMPORARY SEDIMENT BASINS THROUGHOUT THE CONSTRUCTION SITE TO CAPTURE RUNOFF AND SLOW THE FLOW OF WATER AND ALLOW SEDIMENT TO SETTLE OUT. TEMPORARY SEDIMENT BASINS SHALL BE INSTALLED AS DIRECTED BY THE CITY ENGINEER.
- ROCK CONSTRUCTION ENTRANCE. A ROCK ENTRANCE SHALL BE CONSTRUCTED AND MAINTAINED AS SHOWN ON THE PLAN TO REDUCE TRACKING OF SILT AND DIRT ONTO THE PUBLIC STREETS. A GEOTEXTILE FABRIC SHALL BE PLACED UNDERNEATH THE ROCK. THE ROCK SHALL BE PERIODICALLY REPAIRED TO MAINTAIN THE INTENDED PERFORMANCE. MUD AND DEBRIS SHALL BE REMOVED OR SCRAPPED FROM TREES AND VEHICLE UNDERCARRIAGE PRIOR TO LEAVING THE SITE.
- STREET SWEEPING. ALL STREETS USED FOR ACCESS TO THE SITE AND HAUL ROUTES USED FOR CONSTRUCTION EQUIPMENT AND MATERIAL SUPPLIES SHALL BE CLEANED AT THE END OF EACH WORKING DAY. THE CITY OR ENGINEER MAY ORDER ADDITIONAL SWEEPING OF THE STREETS AS DEEMED REQUIRED AT DEVELOPER/CONTRACTOR EXPENSE.
- POSITIVE DRAINAGE AND PROTECTION. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE THROUGHOUT THE SITE AT ALL TIMES. LOW POINTS WITHIN AND ALONG ROADWAYS ARE EXPRESSLY PROHIBITED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TEMPORARY DITCHES, PIPING OR OTHER MEANS TO FACILITATE PROPER DRAINAGE DURING CONSTRUCTION. TO PROTECT PREVIOUSLY GRADED AREAS FROM EROSION, WOOD FIBER BLANKET SHALL BE PLACED IMMEDIATELY ON STEEP SLOPES (1:3 OR GREATER) AND EMBANKMENTS, PERMANENT AND TEMPORARY PONDING, AND OUTLETS AND OVERFLOWS TO PROTECT THE COMPLETED GRADE AND MINIMIZE SILT IN THE RUNOFF.
- DRAINAGE DITCHES. THE NORMAL HEATED PERIMETER OF ANY TEMPORARY OR PERMANENT DRAINAGE DITCH OR SWALE THAT DRAINS WATER FROM ANY PORTION OF THE CONSTRUCTION SITE, OR DIVERTS WATER AROUND THE SITE, MUST BE STABILIZED WITHIN 200 LINEAL FEET FROM THE PROPERTY EDGE, OR FROM THE POINT OF DISCHARGE INTO ANY SURFACE WATER. STABILIZATION OF THE LAST 200 LINEAL FEET MUST BE COMPLETED WITHIN 24 HOURS AFTER CONNECTING TO A SURFACE WATER. STABILIZATION OF THE REMAINING PORTIONS OF ANY TEMPORARY OR PERMANENT DITCHES OR SWALES MUST BE COMPLETED WITHIN 14 DAYS AFTER CONNECTING TO A SURFACE WATER AND CONSTRUCTION IN THAT PORTION OF THE DITCH HAS TEMPORARILY OR PERMANENTLY CEASED. TEMPORARY OR PERMANENT DITCHES OR SWALES THAT ARE BEING USED AS A SEDIMENT CONTAINMENT SYSTEM (WITH PROPERLY DESIGNED ROCK DITCH CHECKS, SO ROLLS, SILT DIKES, ETC.) DO NOT NEED TO BE STABILIZED. THESE AREAS MUST BE STABILIZED WITHIN 24 HOURS AFTER NO LONGER BEING USED AS A SEDIMENT CONTAINMENT SYSTEM.
- TURF ESTABLISHMENT. ALL EXPOSED SOIL AREAS MUST BE STABILIZED AS SOON AS POSSIBLE TO LIMIT SOIL EROSION. BUT IN NO CASE LATER THAN 14 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
- MAINTENANCE AND INSPECTION. EROSION CONTROL MEASURES SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION AND UNTIL SATISFACTORY ESTABLISHMENT OF PERMANENT GROUND COVER IS OBTAINED. ALL EROSION AND SEDIMENTATION CONTROL MEASURES, AND STORMWATER OUTFALLS MUST BE INSPECTED WEEKLY, AND WITHIN 24 HOURS OF THE SITE RECEIVING 0.5 INCHES OF RAIN. REPAIRS MUST BE MADE ON THE SAME DAY OR FOLLOWING DAY OF THE INSPECTION. UNSATISFACTORY CONDITIONS NOT REPAIRED OR WORN UP WITHIN 48-HOURS OF NOTIFICATION SHALL RESULT IN A STOP WORK ORDER, AND/OR SAID WORK SHALL BE COMPLETED AT CONTRACTOR'S EXPENSE.
- REMOVAL. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL TEMPORARY EROSION CONTROL MEASURES, STRUCTURES AND DEVICES ONLY AFTER RECEIVING ENGINEER APPROVAL. ALL DEBRIS, STAKES, AND SILTS ALONG SILT FENCES SHALL BE REMOVED AND DISPOSED OFF SITE. THE CONTRACTOR SHALL HAND RAKE SITED AREAS AND ONLY AFTER RECEIVING ENGINEER APPROVAL SHALL FINAL GRADE AND SHALL RESTORE THE GROUND SURFACE WITH SEED OR SOO, AS REQUIRED, TO MATCH THE FINISHED GRADE TO THE ADJACENT AREA.
- FINAL STORM SEWER SYSTEM. AT THE COMPLETION OF THE WORK AND BEFORE THE FINAL WALK THROUGH, THE CONTRACTOR SHALL REMOVE STORM SEWER INLET PROTECTION MEASURES AND THOROUGHLY FLUSH THE STORM SEWER SYSTEM. SEDIMENT AND DEBRIS SHALL BE COMPLETELY REMOVED AND CLEANED AT THE INLETS, OUTLETS, AND DOWNSTREAM OF EACH OUTLET. RIPRAP AND GEOTEXTILE FABRIC MAY REQUIRE REPLACEMENT AS DIRECTED BY THE ENGINEER TO OBTAIN A LIKE NEW INSTALLATION ACCEPTABLE TO THE CITY.
- DITCH CHECK (BIOROLL BLANKET SYSTEM). BIOROLL AND BLANKET SYSTEMS SHALL BE INSTALLED AS DITCH CHECKS ONLY IN SPECIFIED LOCATIONS AS APPROVED BY THE CITY ENGINEER. BIOROLLS ARE NOT TO BE UTILIZED IN AREAS WHERE VEHICLE AND CONSTRUCTION TRAFFIC OCCUR.
- FLOTATION SILT CURTAIN. FLOTATION SILT CURTAIN SHALL BE UTILIZED WHEN CONSTRUCTION ACTIVITIES OCCUR DIRECTLY ADJACENT TO LAKES, STREAMS OR WETLANDS IN ORDER TO CONTAIN SEDIMENTS NEAR THE BANKS OF WORKING AREAS. THE INSTALLATION OF FLOTATION SILT CURTAINS WILL BE REQUIRED AS DIRECTED BY THE CITY ENGINEER.
- CONCRETE WASHOUT ON-SITE. ALL LIQUID AND SOLID WASTES GENERATED BY CONCRETE WASHOUT OPERATIONS MUST BE CONTAINED IN A LEAK-PROOF CONTAINMENT FACILITY OR IMPERMEABLE LINER. A COMPACTED CLAY LINER THAT DOES NOT ALLOW WASHOUT LIQUIDS TO ENTER GROUND WATER IS CONSIDERED AN IMPERMEABLE LINER. THE LIQUID AND SOLID WASTES MUST NOT CONTACT THE GROUND, AND THERE MUST NOT BE RUNOFF FROM THE CONCRETE WASHOUT OPERATIONS OR AREAS. LIQUID AND SOLID WASTES MUST BE DISPOSED OF PROPERLY AND IN COMPLIANCE WITH MPCA REGULATIONS. SIGN MUST BE INSTALLED ADJACENT TO EACH WASHOUT FACILITY TO INFORM CONCRETE EQUIPMENT OPERATORS TO UTILIZE THE PROPER FACILITIES.

Grading Notes "FOR PRIVATE INFRASTRUCTURE COMPONENTS ONLY"

- LOCATIONS AND ELEVATIONS OF EXISTING TOPOGRAPHY AND UTILITIES AS SHOWN ON THIS PLAN ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY SITE CONDITIONS AND UTILITY LOCATIONS PRIOR TO EXCAVATION/CONSTRUCTION. THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY IF ANY DISCREPANCIES ARE FOUND.
- CONTRACTORS SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULE, SLOPED PAVEMENT, EXIT PORCHES, RAMPS, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS, EXACT BUILDING UTILITY ENTRANCE LOCATIONS, AND EXACT LOCATIONS AND NUMBER OF DOWNSPOUTS.
- ALL EXCAVATION SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR TRENCH EXCAVATION AND BACKFILL/SURFACE RESTORATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA.
- ALL DISTURBED UNPAVED AREAS ARE TO RECEIVE FOUR INCHES OF TOPSOIL AND SOO OR SEED. THESE AREAS SHALL BE WATERED UNTIL A HEALTHY STAND OF GRASS IS OBTAINED. SEE LANDSCAPE PLAN FOR PLANTING AND TURF ESTABLISHMENT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. PLACEMENT OF THESE DEVICES SHALL BE APPROVED BY THE ENGINEER PRIOR TO PLACEMENT. TRAFFIC CONTROL DEVICES SHALL CONFORM TO APPROPRIATE MNDOT STANDARDS.
- ALL SLOPES SHALL BE GRADED TO 3:1 OR FLATTER, UNLESS OTHERWISE INDICATED ON THIS SHEET.
- CONTRACTOR SHALL UNIFORMLY GRADE AREAS WITHIN LIMITS OF GRADING AND PROVIDE A SMOOTH FINISHED SURFACE WITH UNIFORM SLOPES BETWEEN POINTS WHERE ELEVATIONS ARE SHOWN OR BETWEEN SUCH POINTS AND EXISTING GRADES.
- SPOT ELEVATIONS SHOWN INDICATE FINISHED PAVEMENT ELEVATIONS & GUTTER FLOW LINE UNLESS OTHERWISE NOTED. PROPOSED CONTOURS ARE TO FINISHED SURFACE GRADE.
- SEE SOILS REPORT FOR PAVEMENT THICKNESSES AND HOLD DOWNS.
- CONTRACTOR SHALL DISPOSE OF ANY EXCESS SOIL MATERIAL THAT EXISTS AFTER THE SITE GRADING AND UTILITY CONSTRUCTION IS COMPLETED. CONTRACTOR SHALL DISPOSE OF ALL EXCESS SOIL MATERIAL IN A MANNER ACCEPTABLE TO THE OWNER AND THE REGULATING AGENCIES.
- ALL CONSTRUCTION SHALL CONFORM TO LOCAL, STATE AND FEDERAL RULES INCLUDING THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS.
- PRIOR TO PLACEMENT OF ANY STRUCTURE OR PAVEMENT, A PROOF ROLL, AT MINIMUM, WILL BE REQUIRED ON THE SUBGRADE. PROOF ROLLING SHALL BE ACCOMPLISHED BY MAKING MINIMUM OF 2 COMPLETE PASSES WITH FULLY-LOADED TAND-AXLE CUMPT TRUCK, OR APPROVED EQUIVALENT, IN EACH OF 2 PERPENDICULAR DIRECTIONS WHILE UNDER SUPERVISION AND DIRECTION OF THE INDEPENDENT TESTING LABORATORY. AREAS OF FAILURE SHALL BE EXCAVATED AND RECOMPACTED AS SPECIFIED HEREIN.
- EMBANKMENT MATERIAL PLACED BENEATH BUILDINGS AND STREET OR PARKING AREAS SHALL BE COMPACTED IN ACCORDANCE WITH THE SPECIFIED DENSITY METHOD AS OUTLINED IN MNDOT 2105.3F1 AND THE REQUIREMENTS OF THE GEOTECHNICAL ENGINEER.
- EMBANKMENT MATERIAL NOT PLACED IN THE BUILDING PAD, STREETS OR PARKING AREA, SHALL BE COMPACTED IN ACCORDANCE WITH REQUIREMENTS OF THE ORDINARY COMPACTION METHOD AS OUTLINED IN MNDOT 2105.3F2.
- ALL SOILS AND MATERIALS TESTING SHALL BE COMPLETED BY AN INDEPENDENT GEOTECHNICAL ENGINEER. EXCAVATION FOR THE PURPOSE OF REMOVING UNSTABLE OR UNSUITABLE SOILS SHALL BE COMPLETED AS REQUIRED BY THE GEOTECHNICAL ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOILS TESTS AND INSPECTIONS WITH THE GEOTECHNICAL ENGINEER.



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Designed: JPD
Checked: JPD
Drawn: MK
Record Drawing by/desc:

Revisions:
1. 05/21/14 CITY SUBMITTAL
2. 06/11/14 CITY APPROVED

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer. I am duly Licensed under the laws of the State of Minnesota.

Project: 49933
Date: 06/11/14
Sheet No.: 49933

Prepared for:

BDH & Young

7001 France Avenue South, Suite 200
Edina, Minnesota 55435

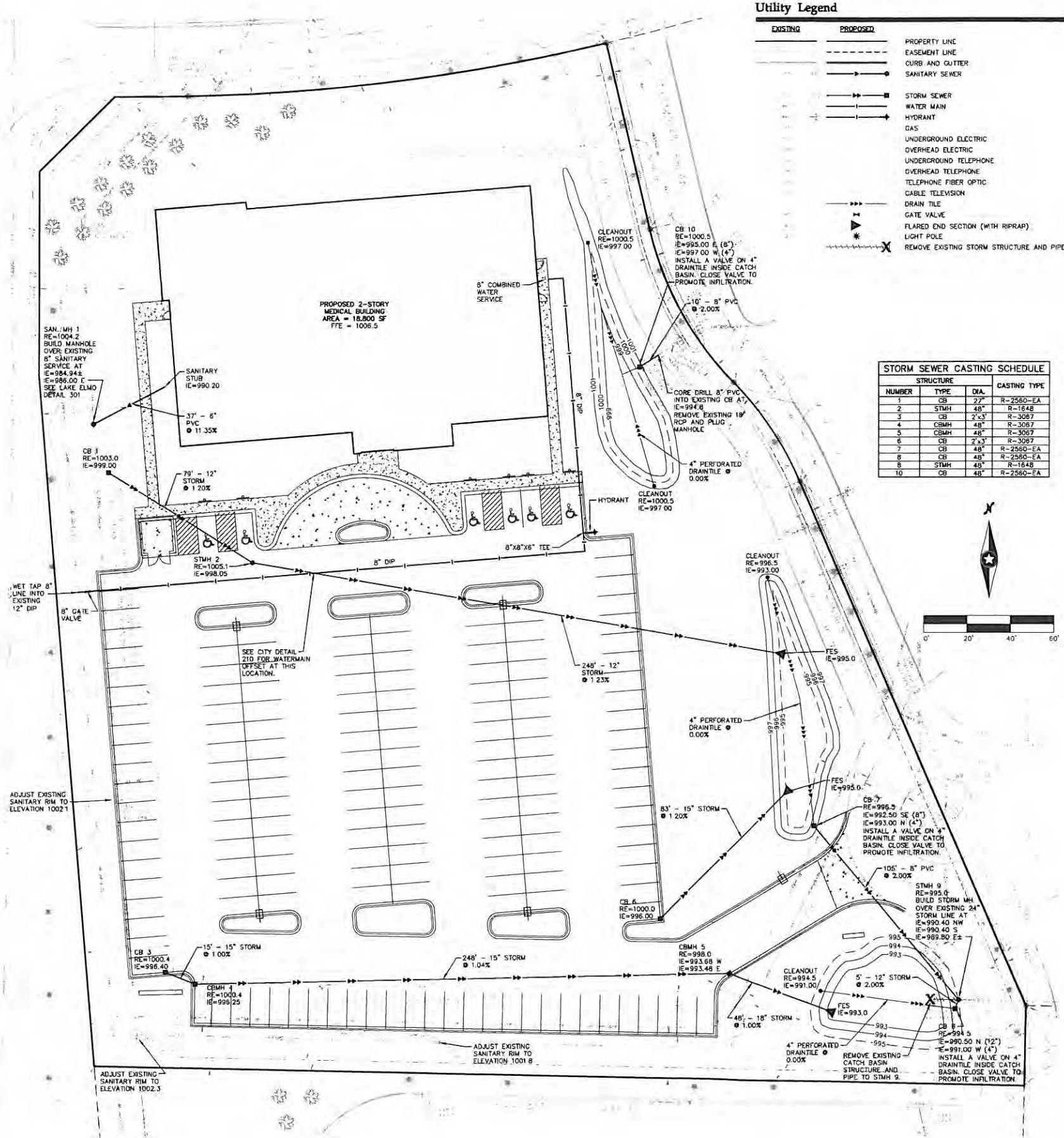
Eagle Point
Medical Center

Lake Elmo, Minnesota

GRADING, DRAINAGE
& EROSION
CONTROL PLAN

Date: 05/21/14
Sheet: C2 OF 5

NOT FOR CONSTRUCTION



City of Lake Elmo - Sanitary/Water Notes

SANITARY SEWER NOTES

1. ALL SANITARY SEWER AND ACCESSORIES MUST BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF LAKE ELMO STANDARD SPECIFICATIONS AND DETAILS.
2. ALL SANITARY SEWER PVC PIPE SHALL BE INSTALLED ACCORDING TO CITY STANDARD DRAWING 103 GRANULAR MATERIAL BEDDING METHOD (FOR PVC SANITARY SEWER PIPE).
3. UNLESS NOTED OTHERWISE, ALL SMOOTH WALLED SANITARY SEWER PVC PIPE AND FITTINGS SHALL BE SDR 35 WITH ELASTOMERIC GASKETED JOINTS.
4. ALL SANITARY SEWER SERVICES SHALL BE 4-INCH PVC, SCH. 40 OR SDR 26 PIPE.
5. SMOOTH WALLED PVC PIPE AND FITTINGS SHALL CONFORM WITH THE REQUIREMENTS OF ASTM D-3034 FOR THE SIZE, STANDARD DIMENSION RATIO (SDR), AND STRENGTH REQUIREMENTS INDICATED ON THE PLANS, SPECIFICATIONS, AND SPECIAL PROVISIONS.
6. REINFORCED CONCRETE PIPE AND FITTINGS SHALL CONFORM WITH THE REQUIREMENTS OF MANDOT SPEC 3236 (REINFORCED CONCRETE PIPE) FOR THE TYPE, SIZE, AND STRENGTH CLASS SPECIFIED HEREIN.
7. JOINTS OF MANHOLE RISER SECTIONS SHALL BE TONGUE AND GROOVE WITH RUBBER "O" RING JOINTS PROVIDED ON ALL SANITARY SEWER MANHOLES.
8. SANITARY SEWER INLET AND OUTLET PIPES SHALL BE JOINED TO THE MANHOLE WITH A GASKETED, FLEXIBLE, WATER-TIGHT CONNECTION TO ALLOW DIFFERENTIAL SETTLEMENT OF THE PIPE AND MANHOLE TO TAKE PLACE.
9. A 1'-0" TO 1'-4" MANHOLE SECTION SHALL BE INSTALLED UNDER THE CONE SECTION TO ALLOW FOR HEIGHT ADJUSTMENT WHENEVER POSSIBLE.
10. ALL SERVICE LINE STUBS MUST HAVE A 2" X 2" HARDWOOD MARKER WITH METAL SPIKE RUNNING FROM THE END OF THE PIPE TO FINISHED GRADE ELEVATION.
11. UPON MAKING A CONNECTION TO AN EXISTING SANITARY SEWER STUB OR MANHOLE, DIRT AND DEBRIS SHALL BE PREVENTED FROM ENTERING THE EXISTING SEWER BY IMMEDIATELY INSTALLING WATER-TIGHT PLUGS AS NEEDED IN THE EXISTING MANHOLE.

WATER MAIN NOTES

1. ALL WATERMAIN AND ACCESSORIES MUST BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF LAKE ELMO STANDARD SPECIFICATIONS AND DETAILS.
2. MANIPULATION OF EXISTING VALVES SHALL BE PERFORMED ONLY BY CITY PERSONNEL.
3. WATERMAIN SHALL BE DUCTILE IRON PIPE, CLASS-52.
4. ALL FITTINGS SHALL COMPLY WITH CEAM SPEC. 2611 2.1.1. ALL FITTINGS SHALL BE DUCTILE IRON PIPE WITH POLYETHYLENE ENCASEMENT. ALL CONNECTIONS SHALL BE INSTALLED UTILIZING COR-BLUE NUTS AND BOLTS.
5. USE GATE VALVES FOR ALL APPLICATIONS UP THROUGH 12 INCHES.
6. GATE VALVES SHALL BE RESILIENT WEDGE AMERICAN FLOW CONTROL SERIES 2500 OR APPROVED EQUAL. GATE VALVES MUST COMPLY WITH CEAM SPEC 2611 2. C.2.
7. USE BUTTERFLY VALVES FOR ALL APPLICATIONS GREATER THAN 12 INCHES.
8. BUTTERFLY VALVES SHALL BE MUELLER UNISEAL III, OR APPROVED EQUAL. BUTTERFLY VALVES SHALL COMPLY WITH CEAM SPEC. 2611 2. C.3.
9. BOLTS AND NUTS ON ALL VALVES AND HYDRANTS SHALL BE STAINLESS STEEL.
10. ALL HYDRANTS SHALL BE INSTALLED 5.0 FEET BACK OF CURB.
11. HYDRANTS SHALL BE WATERLOUS "PACER," MODEL WB-67 OR APPROVED EQUAL, FITTED WITH FH 800 SERIES FLEX STAKE AND PAINTED RED.
12. HYDRANTS SHALL HAVE TWO OUTLET NOZZLES FOR 2-1/2 (I.D.) HOSE CONNECTIONS AND ONE 4" STORZ NOZZLE.
13. THE CURB STOP SERVICE ASSEMBLY SHALL HAVE A MINIMUM 1-FT ADJUSTMENT RANGE AND SHALL EXTEND 6 INCHES ABOVE FINISHED GRADE FULLY EXTENDED.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING WATER TO HOMES AND BUSINESSES WHOSE WATER SUPPLY IS INTERRUPTED DURING THE COURSE OF THE PROJECT.
15. ALL WATER SERVICES SHALL BE 1" TYPE K COPPER UNLESS OTHERWISE NOTED.

General Utility Notes "FOR PRIVATE INFRASTRUCTURE COMPONENTS ONLY"

1. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS ARE BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND LIMITED MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION SHALL NOT BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR SHALL VERIFY EXISTING CONDITIONS PRIOR TO CONSTRUCTION AND NOTIFY THE OWNER OR ENGINEER OF DISCREPANCIES.
2. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN THE NECESSARY FEDERAL, STATE AND LOCAL PERMITS FOR THE PROPOSED WORK OR VERIFY WITH THE OWNER OR ENGINEER THAT PERMITS HAVE BEEN OBTAINED. PERMIT FEES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR UNLESS OTHERWISE ARRANGED WITH THE OWNER.
3. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATION AND DIMENSIONS OF DOORWAYS, RAMPS, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY CONNECTION LOCATIONS.
4. ALL PRIVATE UTILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE SPECIFICATIONS OF THE APPROPRIATE UTILITY COMPANY. THE CONTRACTOR SHALL COORDINATE THE SERVICE LINE CONNECTION WITH THE UTILITY COMPANIES.
5. CONTRACTOR SHALL OBTAIN ALL NECESSARY CITY PERMITS FOR UTILITY CONNECTIONS, AND UTILITIES SHALL BE INSPECTED AND APPROVED BY THE CITY. THE CITY SHALL BE NOTIFIED 48-HOURS PRIOR TO COMMENCING WITH THE UTILITY CONSTRUCTION OR ANY REQUIRED TESTING. CONTRACTOR SHALL NOT OPERATE, INTERFERE WITH, CONNECT ANY PIPE OR HOSE TO, OR TAP ANY WATER MAIN BELONGING TO THE CITY UNLESS DULY AUTHORIZED TO DO SO BY THE CITY. ANY ADVERSE CONSEQUENCES OF SCHEDULED OR UNSCHEDULED DISRUPTIONS OF SERVICE TO THE PUBLIC ARE TO BE THE RESPONSIBILITY OF THE CONTRACTOR.
6. WATER MAIN LENGTHS AS SHOWN ARE APPROXIMATE HORIZONTAL LENGTHS. ALLOW FOR ADDITIONAL PIPE WHEN INSTALLING ON SLOPES OR WHEN DEFLECTIONS ARE REQUIRED. THE JOINT DEFLECTIONS SHALL NOT EXCEED THE MAXIMUM RECOMMENDED BY THE PIPE MANUFACTURER OR BY LOCAL GOVERNING SPECIFICATIONS. FITTINGS REQUIRED TO CONSTRUCT WATER MAIN SHALL BE INCLUDED IN WATER MAIN CONSTRUCTION.
7. PROVIDE WATER MAIN THRUST RESTRAINTS PER CITY STANDARD REQUIREMENTS.
8. A MINIMUM VERTICAL SEPARATION OF 18 INCHES IS REQUIRED AT ALL WATER MAIN CROSSINGS WITH SANITARY SEWER OR STORM SEWER.
9. UTILITY SERVICES TYPICALLY TERMINATE 5' OUTSIDE BUILDING WALL UNLESS OTHERWISE SHOWN OR NOTED.
10. ALL MATERIALS SHALL COMPLY WITH THE REQUIREMENTS OF THE CITY.
11. ALL WATER LINES SHALL HAVE A 7.5' MINIMUM COVER. PROVIDE MINIMUM SEPARATION OF 18" FROM SANITARY SEWER & STORM SEWER. INSULATE WATER MAIN IF LESS THAN 7.5' OF COVER.
12. INSULATION SHALL BE DOW STYROFOAM HI BRAND 35 OR EQUIVALENT, WITH 4 INCHES OF THICKNESS.
13. SANITARY SEWER PIPE OUTSIDE THE BUILDING ENVELOPE SHALL BE POLYVINYL CHLORIDE (PVC) SDR 35 OR 26 SDR 26 IS REQUIRED FOR DEPTHS GREATER THAN 15 FEET. SANITARY SEWER PIPE WITHIN 5 FEET OF BUILDING AND UNDER FOOTINGS, PIPE SHALL BE PVC SCHEDULE 40.
14. STORM SEWER PIPE SHALL BE REINFORCED CONCRETE PIPE (CLASS 5 FOR PIPE DIAMETERS 18" AND SMALLER, CLASS 3 FOR PIPE DIAMETERS 21" AND LARGER UNLESS OTHERWISE NOTED) WITH R-4 GASKETS, OR HDPE STORM SEWER PIPE IF ALLOWED BY THE CITY. HDPE STORM PIPE SHALL MEET REQUIREMENTS OF ASTM F2648. PIPE SHALL BE WATER TIGHT ACCORDING TO ASTM D3212 REQUIREMENTS. SEE PLAN FOR LOCATIONS WHERE RCP IS REQUIRED. PVC STORM SEWER PIPE SHALL BE SCHEDULE 40 PIPE. FLARED END SECTIONS SHALL BE RCP WITH TRASH GUARDS & RIP-RAP.
15. POST INDICATOR VALVES SHALL BE CLOW F-5750 (OR EQUIVALENT) MEETING AWWA STANDARD C909 AND CITY STANDARDS. VALVE TO BE MECHANICAL JOINT RESILIENT WEDGE GATE VALVE POST TO BE ADJUSTABLE FOR 8 FEET WATER MAIN DEPTH. THE ELECTRICAL ALARM SWITCH SHALL BE PART NO. PCV52 (OR EQUIVALENT).
16. AFTER CONSTRUCTION IS COMPLETED, THE CONTRACTOR SHALL PROVIDE THE OWNER WITH AN AS-BUILT RECORD OF UTILITY CONSTRUCTION. THE AS-BUILT SHALL INCLUDE LOCATION AND LENGTH DEVIATIONS OR CHANGES TO THE PLAN. CONTRACTOR TO VERIFY WITH OWNER OR ENGINEER WHETHER A PLAN WITH POST-CONSTRUCTION ELEVATIONS IS REQUIRED.



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Designed: JTD
Checked: JTD
Drawn: MKR
Record Drawing by/dwg:

Revisions:
1. 05/21/14 CITY REQUEST
2. 06/11/14 CITY REQUESTED

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Project:

Date: 06/11/14 License No.: 49933

Prepared for:

BDH & Young

7001 France Avenue South, Suite 200
Edina, Minnesota 55435

Eagle Point
Medical Center

Lake Elmo, Minnesota

UTILITY PLAN

Date: 05/21/14

Sheet: C3 OF 5

NOT FOR CONSTRUCTION

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Designed: TYD
Checked: TYD
Drawn: MX
Record Drawing by/date:

Revisions:
1. 06/27/14 GUY SUBMITTAL, 06/27/14 CITY/STATE/LOCAL
2.
3.
4.
5.
6.
7.
8.
9.
10.

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Licensed PROFESSIONAL ENGINEER under the laws of the State of Minnesota.

PJ Check

Date: 06/11/14 License No. 49953

Prepared for:

BDH & Young

7001 France Avenue South, Suite 200
Edina, Minnesota 55435

**Eagle Point
Medical Center**

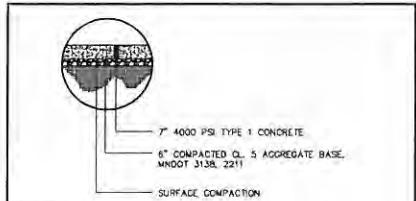
Lake Elmo, Minnesota

DETAIL SHEET

Date: 05/21/14

Sheet: C4 OF 5

00036870TP01.dwg



NOTES:

CONCRETE SHALL BE REINFORCED WITH WELDED WIRE FABRIC OR REINFORCING BARS TO ASSIST IN CONTROLLING CRACKING FROM DRYING SHRINKAGE AND THERMAL CHANGES.

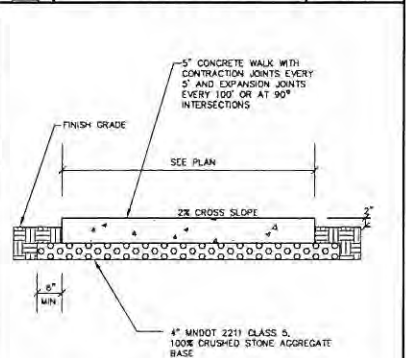
SAVED OR FORMED CONTROL JOINTS SHOULD BE INCLUDED FOR EACH 225 SQUARE FEET OF AREA OR LESS (15 FEET BY 15 FEET). CONTRACTOR SHALL PREPARE JOINTING PATTERN AND SUBMIT TO ENGINEER PRIOR TO CONSTRUCTION.

SAW CUTS SHOULD NOT CUT THROUGH THE WELDED WIRE FABRIC OR REINFORCING STEEL, AND DOMES SHOULD BE UTILIZED AT FORMED AND/OR COLD JOINTS.

DOMES SHALL BE UTILIZED AT FORMED AND/OR COLD JOINTS. DOMES SHALL BE 30\"/>

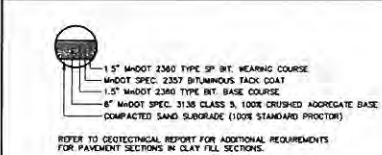
FOR PRIVATE INFRASTRUCTURE COMPONENTS ONLY

HEAVY DUTY CONCRETE
SECTION LOADING DOCK
LAST REVISED: 03/20/08
S121

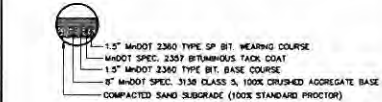


FOR PRIVATE INFRASTRUCTURE COMPONENTS ONLY

PRIVATE CONCRETE SIDEWALK
LAST REVISED: 03/20/08
S108



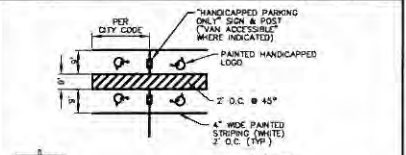
STANDARD DUTY PAVEMENT SECTION



HEAVY DUTY PAVEMENT SECTION

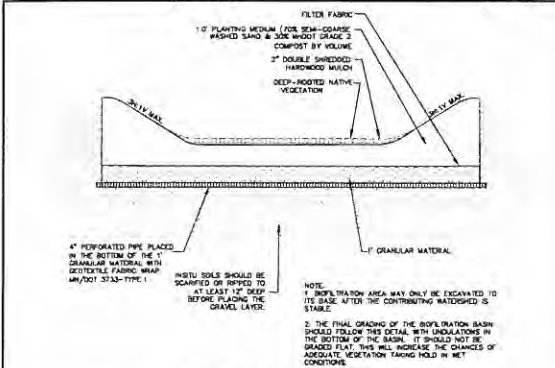
FOR PRIVATE INFRASTRUCTURE COMPONENTS ONLY

PAVEMENT SECTIONS
LAST REVISED: 03/20/08
S119



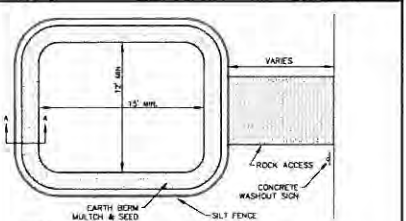
HANDICAP SIGN
NOTE: PROVIDE DESIGNATED WHEEL CHAIR SYMBOL @ EACH HANDICAPPED PARKING STALL.
FOR PRIVATE INFRASTRUCTURE COMPONENTS ONLY

HANDICAP ACCESSIBLE SIGNAGE AND STRIPING
LAST REVISED: 03/20/08
S115



FOR PRIVATE INFRASTRUCTURE COMPONENTS ONLY

RAIN GARDEN
LAST REVISED: 03/20/08
S120



NOTE: CONCRETE WASHOUT AREAS SHALL HAVE AN IMPERMEABLE LINER TO PREVENT CONCRETE WASHOUT WATER FROM INFILTRATING/CONTACTING WITH SOIL. IMPERMEABLE LINER INCLUDES TO 10\"/>



FOR PRIVATE INFRASTRUCTURE COMPONENTS ONLY

CONCRETE WASHOUT AREA
LAST REVISED: 03/20/08
C008



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Designed: JPD
Checked: JPD
Drawn: MKK
Record Drawing by/drawn

Revisions:
1. CORRECT CITY, SOUTHERN
2. 06/11/14 CITY/MSH/MSH

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

JF Dick

Date: 06/11/14 License No. 49953

Prepared for:

BDH & Young

7001 France Avenue South, Suite 200
Edina, Minnesota 55435

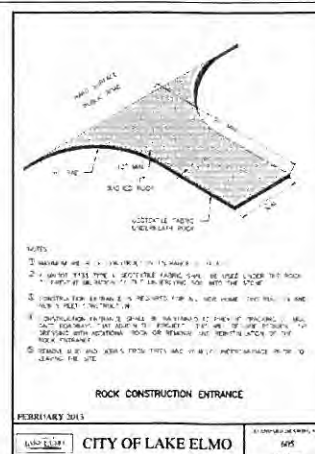
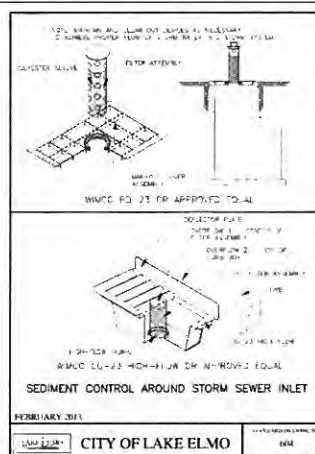
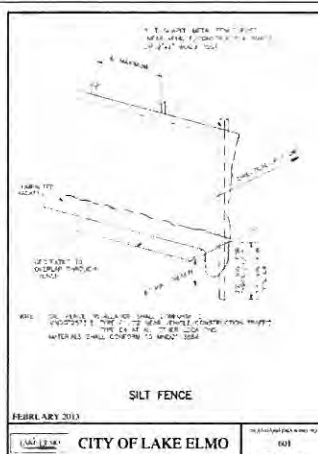
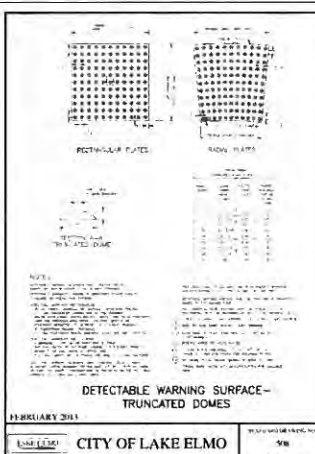
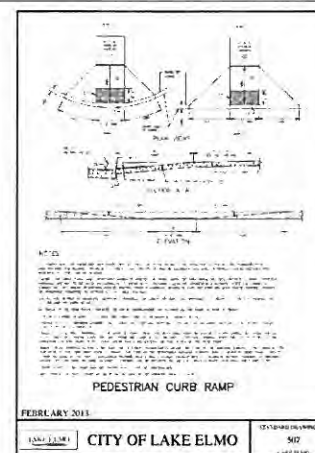
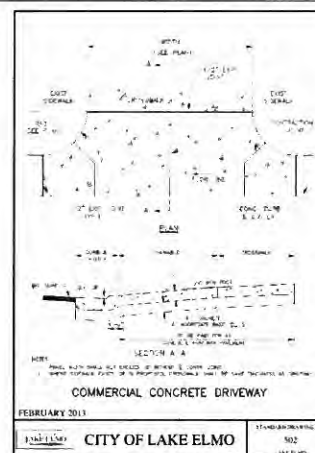
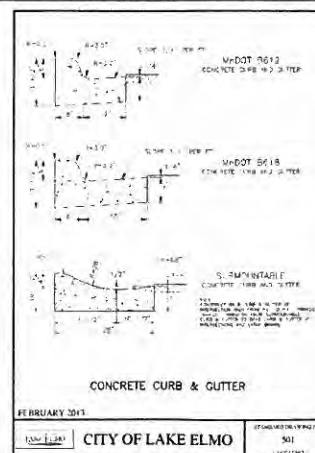
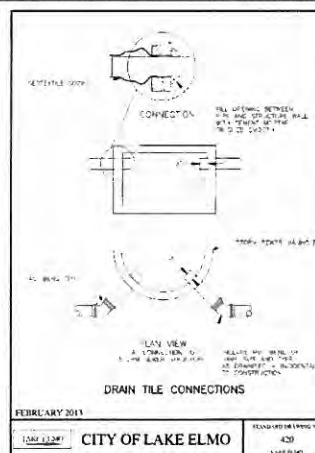
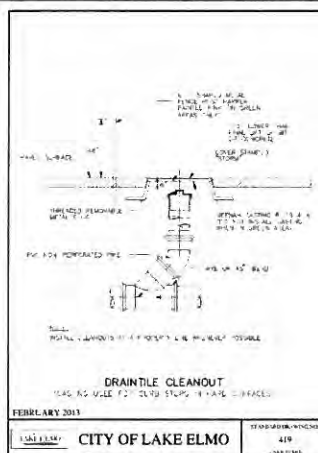
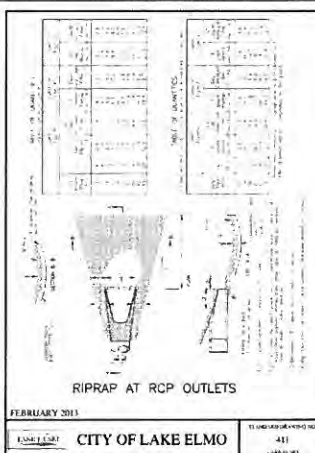
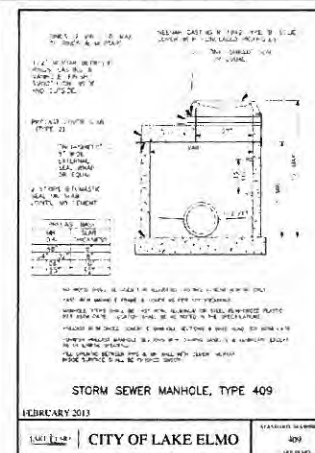
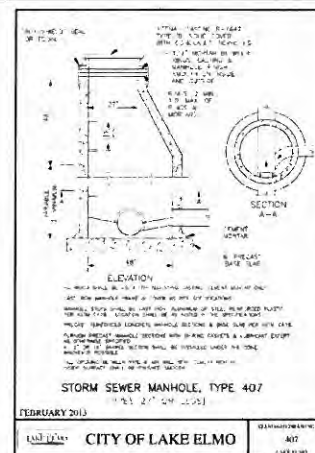
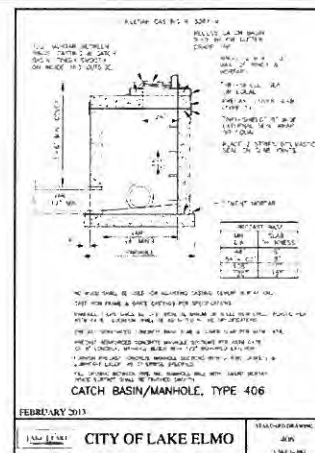
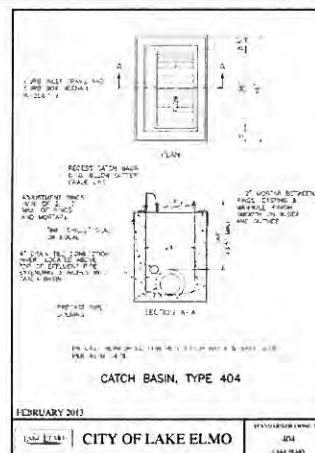
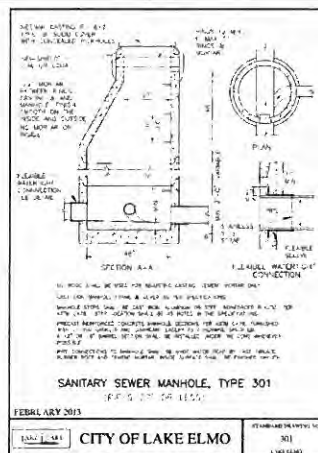
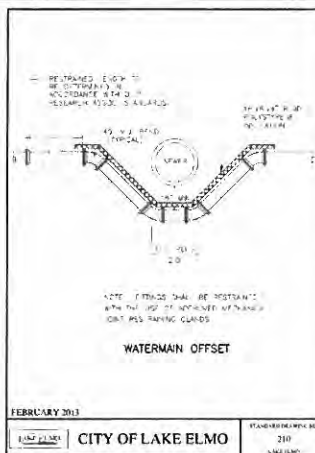
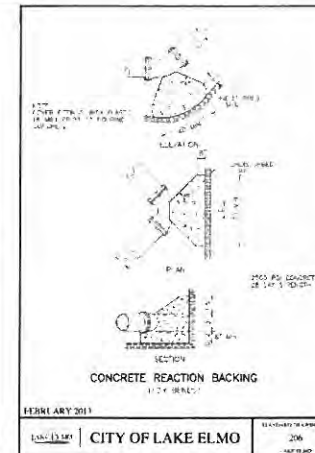
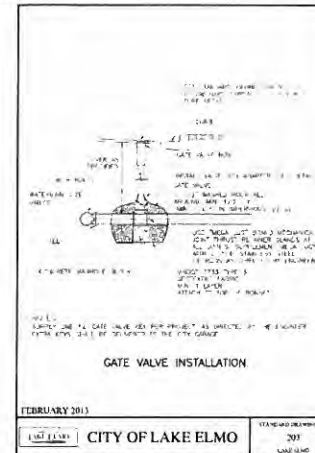
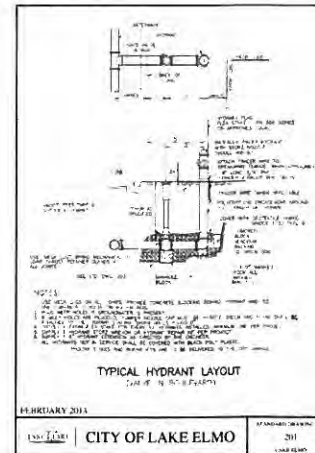
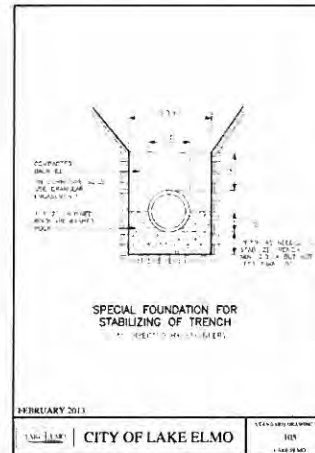
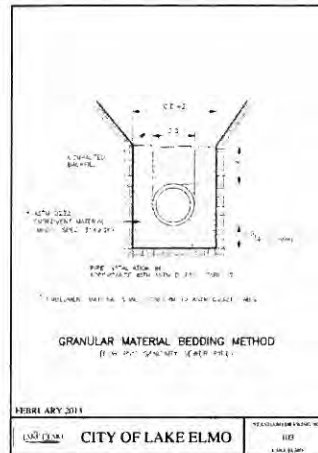
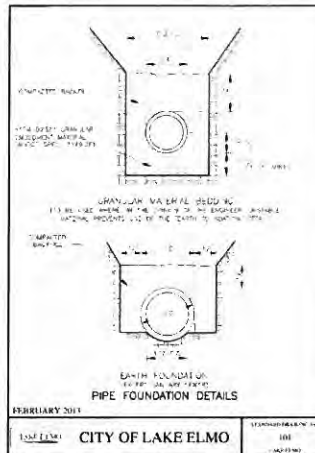
**Eagle Point
Medical Center**

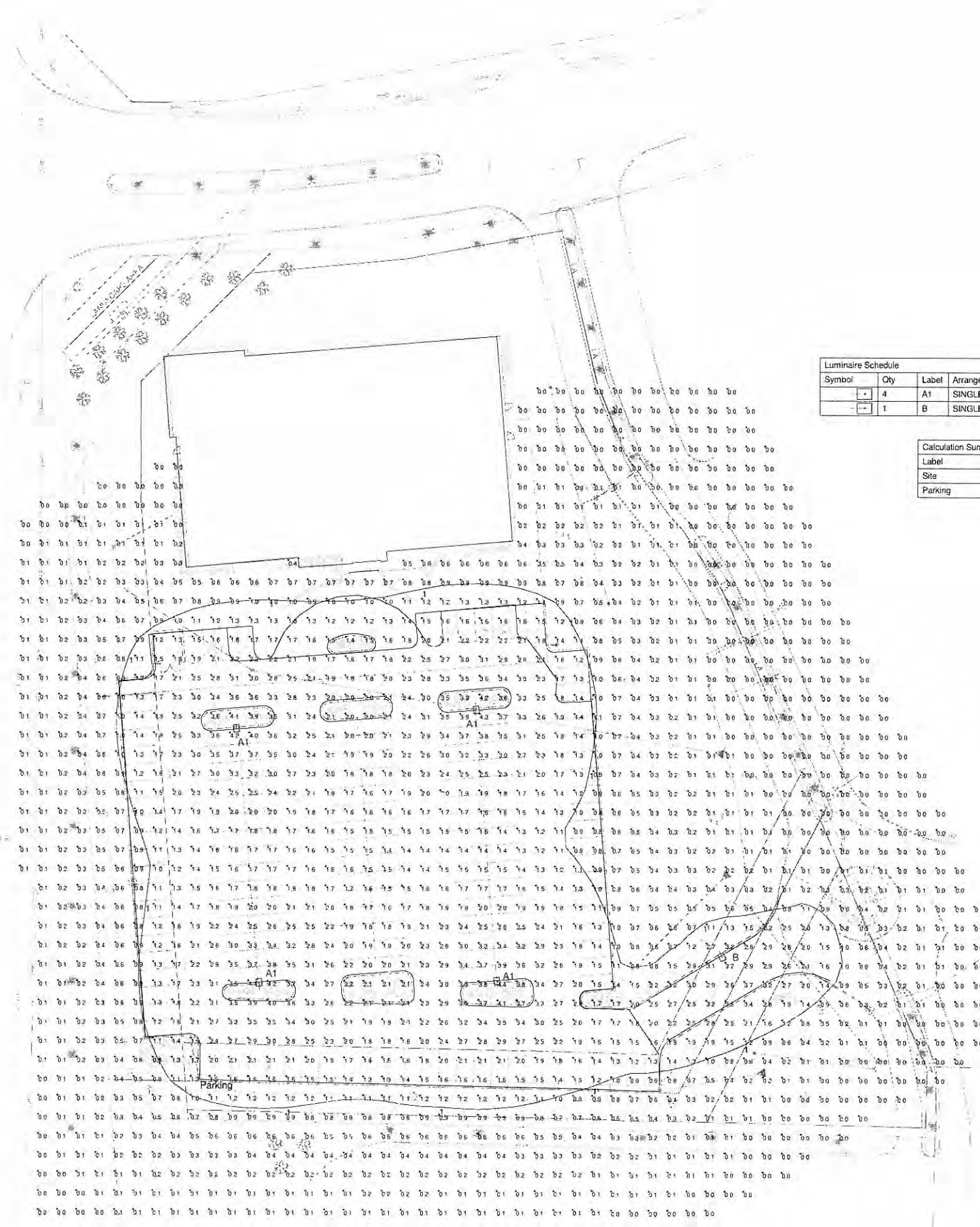
Lake Elmo, Minnesota

CITY DETAIL SHEET

Date: 05/21/14
Sheet: C5 OF 5

000366701P02.dwg





Luminaire Schedule						
Symbol	Qty	Label	Arrangement	Total Lamp Lumens	LLF	Description
+	4	A1	SINGLE	N.A.	0.900	McGraw Galleon GLEON-AE-08-LED-E1-5WQ 8-sq LED Type V single 25ft
-	1	B	SINGLE	N.A.	0.900	McGraw Galleon GLEON-AE-04-LED-E1-T3 4-sq LED type IV wide 25ft

Calculation Summary							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
Site	Illuminance	Fc	0.97	4.4	0.0	N.A.	N.A.
Parking	Illuminance	Fc	2.14	4.4	0.8	2.68	5.50

Luminaire Location Summary					
LumNo	Label	X	Y	Z	Orient
1	A1	474157	175771	25	94.619
2	A1	474144	175912	25	275.332
3	A1	474269	175922	25	275.332
4	A1	474281	175772	25	94.619
6	B	474397.5	175792.5	25	295.925

Lake Elmo Building



never ordinary

6130 Blue Circle Dr. Minneapolis, MN 55438
Ph: 952.217.6438 Fax: 952.346-1632
www.pulseproducts.com

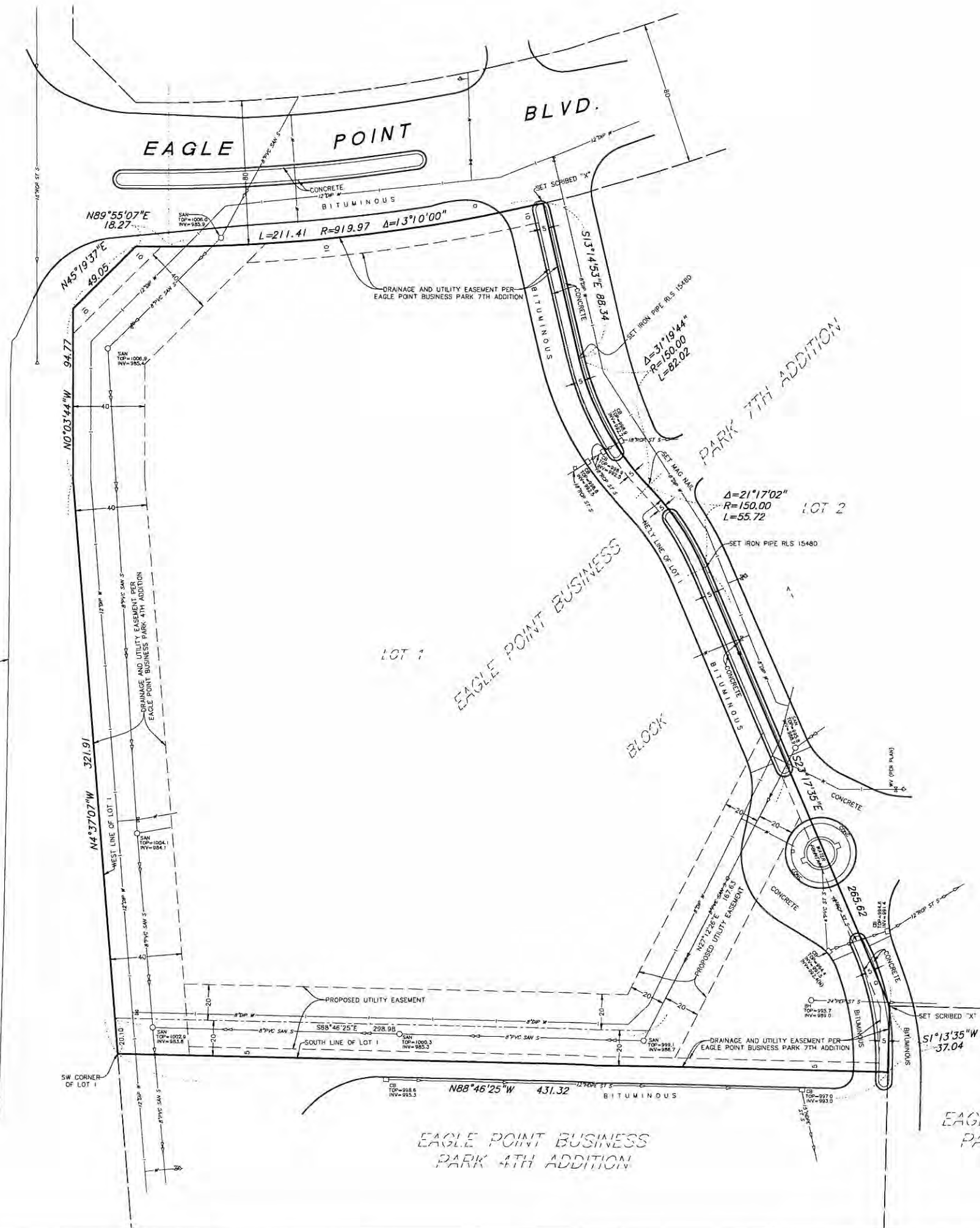
Contact: Trent Shore
Date: May 10, 2014
Revised: May 21, 2014

Arch D-Size
1" = 30'-0"

INWOOD AVENUE N. (C.S.A.H. NO. 13)

BITUMINOUS

BITUMINOUS

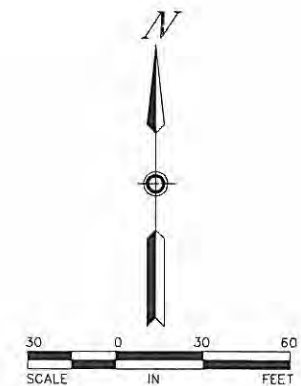


DESCRIPTION OF PROPERTY SURVEYED

Lot 1, Block 1, EAGLE POINT BUSINESS PARK 7TH ADDITION, according to the recorded plat thereof, Washington County, Minnesota

PROPOSED UTILITY EASEMENT

A 40.00 foot easement over, under and across part of Lot 1, Block 1, EAGLE POINT BUSINESS PARK 7TH ADDITION, according to the recorded plat thereof, Washington County, Minnesota. The centerline of said easement is described as beginning at the intersection of the west line of said Lot 1 with the north line of the south 20.00 feet of said Lot 1; thence South 88 degrees 46 minutes 25 seconds East, assumed bearing, along said north line of the south 20.00 feet of Lot 1 a distance of 298.98 feet; thence North 27 degrees 12 minutes 26 seconds East a distance of 167.63 feet to the northeasterly line of said Lot 1 and said centerline there terminating. Except that part of said easement that lies within the drainage and utility easements as dedicated in EAGLE POINT BUSINESS PARK 4TH ADDITION and said EAGLE POINT BUSINESS PARK 7TH ADDITION, according to the recorded plats thereof, Washington County, Minnesota.



I hereby certify that this sketch, plan, or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Dated this 4th day of October, 2007.

SUNDE LAND SURVEYING, LLC.

By: *Mark S. Hanson*
Mark S. Hanson, P.L.S. Minn. Lic. No. 15480

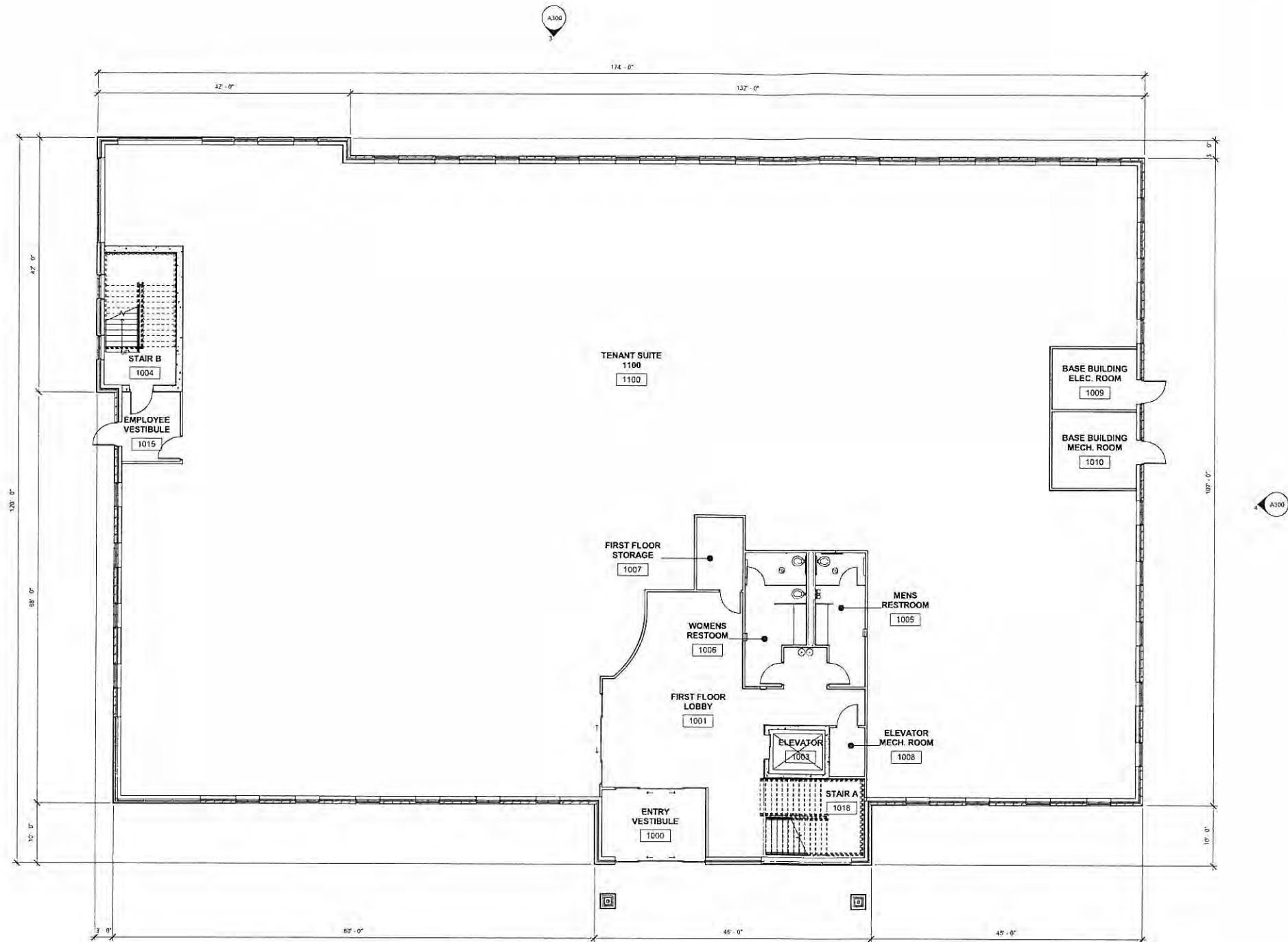
Revision	By	Date
	BMS	

Drawing Title:

EASEMENT SKETCH FOR:
UNITED PROPERTIES

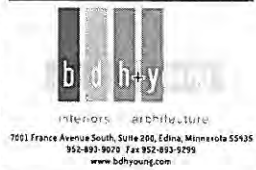
SUNDE LAND SURVEYING www.sunde.com	Main Office: 9001 East Bloomington Freeway (35W) • Suite 118 Bloomington, Minnesota 55420-3435 952-881-2455 (fax: 952-888-9526)
	North Office: Brooklyn Park, Minn. 763-784-9346

Project: 95-155-M	By/Ty: B00/70	Date:
Township: 29	Range: 21	Section: 33
File: 95155M002.dwg		Sheet: 1 of 1



1 FIRST FLOOR FLOOR PLAN
A200 1/8" = 1'-0"

ROOM SCHEDULE	
1000	ENTRY VESTIBULE
1001	FIRST FLOOR LOBBY
1002	STAIR A
1003	ELEVATOR
1004	STAIR B
1005	MENS RESTROOM
1006	WOMENS RESTROOM
1007	FIRST FLOOR STORAGE
1008	ELEVATOR MECH. ROOM
1009	BASE BUILDING ELEC. ROOM
1010	BASE BUILDING MECH. ROOM
1014	Room
1015	EMPLOYEE VESTIBULE
1018	STAIR A
1100	TENANT SUITE 1100
2000	BUILDING CONFERENCE ROOM
2001	SECOND FLOOR LOBBY
2002	STAIR A
2003	ELEVATOR
2004	STAIR B
2004	MECHANICAL SHAFT
2005	MENS RESTROOM
2006	WOMENS RESTROOM
2007	CORRIDOR
2008	CORRIDOR
2009	SECOND FLOOR ELEC. ROOM
2100	TENANT SUITE 2100
2200	TENANT SUITE 2200
2300	TENANT SUITE 2300



Project Information
EAGLE POINT MEDICAL CENTER

LAKE ELMO, MN

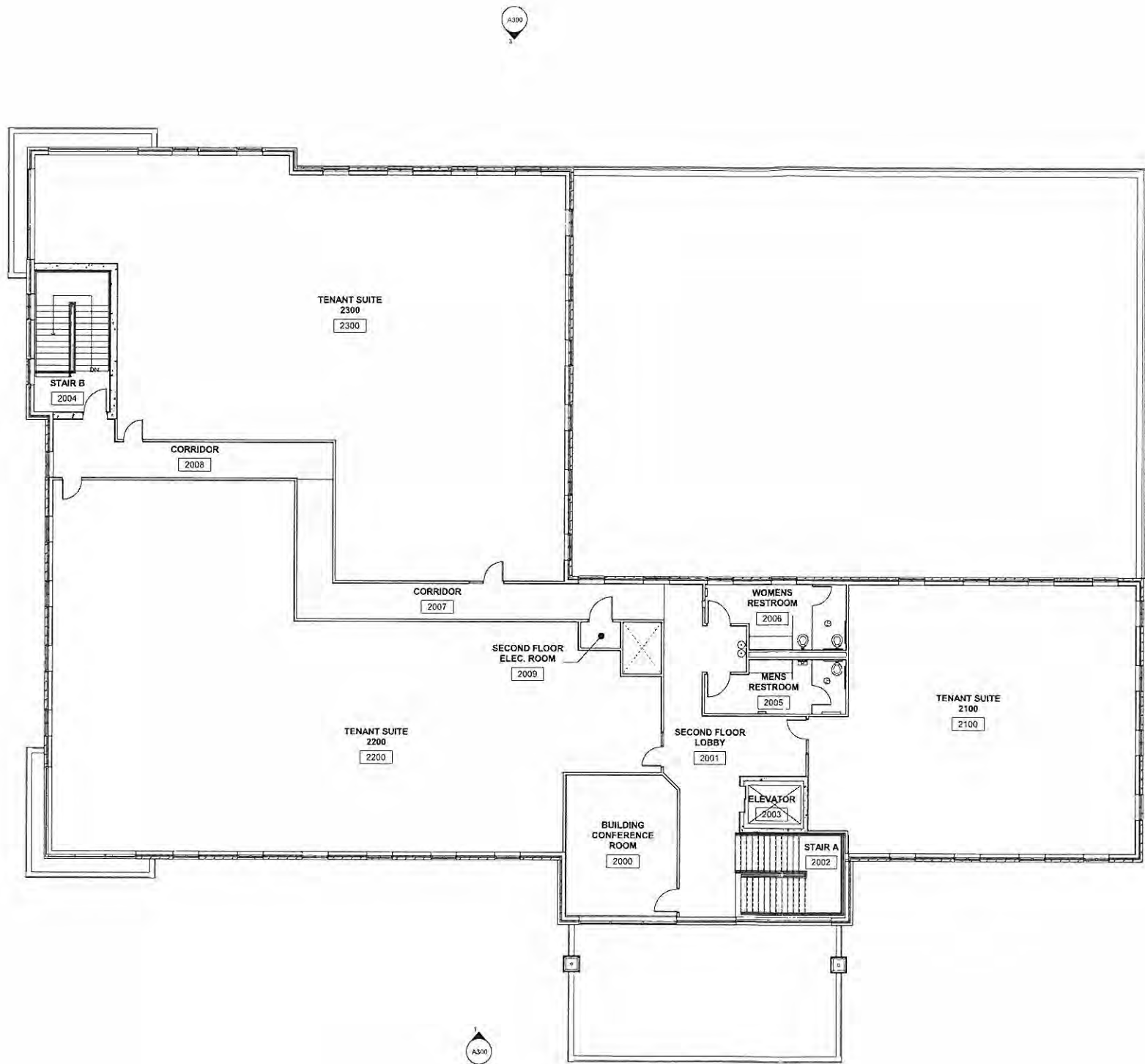
Issue Record		
No	Date	Description

Sheet Title
FIRST FLOOR BUILDING SHELL
FLOOR PLAN

Sheet Information

Job Code A DAVIS/CEAT
Drawn By AQJ/ML Reviewed By TCG

Date 05/21/2014
A200



1 SECOND FLOOR FLOOR PLAN
A301 1/8" = 1'-0"

ROOM SCHEDULE	
1000	ENTRY VESTIBULE
1001	FIRST FLOOR LOBBY
1002	STAIR A
1003	ELEVATOR
1004	STAIR B
1005	MENS RESTROOM
1006	WOMENS RESTROOM
1007	FIRST FLOOR STORAGE
1008	ELEVATOR MECH. ROOM
1009	BASE BUILDING ELEC. ROOM
1010	BASE BUILDING MECH. ROOM
1014	Room
1015	EMPLOYEE VESTIBULE
1018	STAIR A
1100	TENANT SUITE 1100
2000	BUILDING CONFERENCE ROOM
2001	SECOND FLOOR LOBBY
2002	STAIR A
2003	ELEVATOR
2004	STAIR B
2004	MECHANICAL SHAFT
2005	MENS RESTROOM
2006	WOMENS RESTROOM
2007	CORRIDOR
2008	CORRIDOR
2009	SECOND FLOOR ELEC. ROOM
2100	TENANT SUITE 2100
2200	TENANT SUITE 2200
2300	TENANT SUITE 2300



Project Information
EAGLE POINT MEDICAL CENTER

LAKE ELMO MN

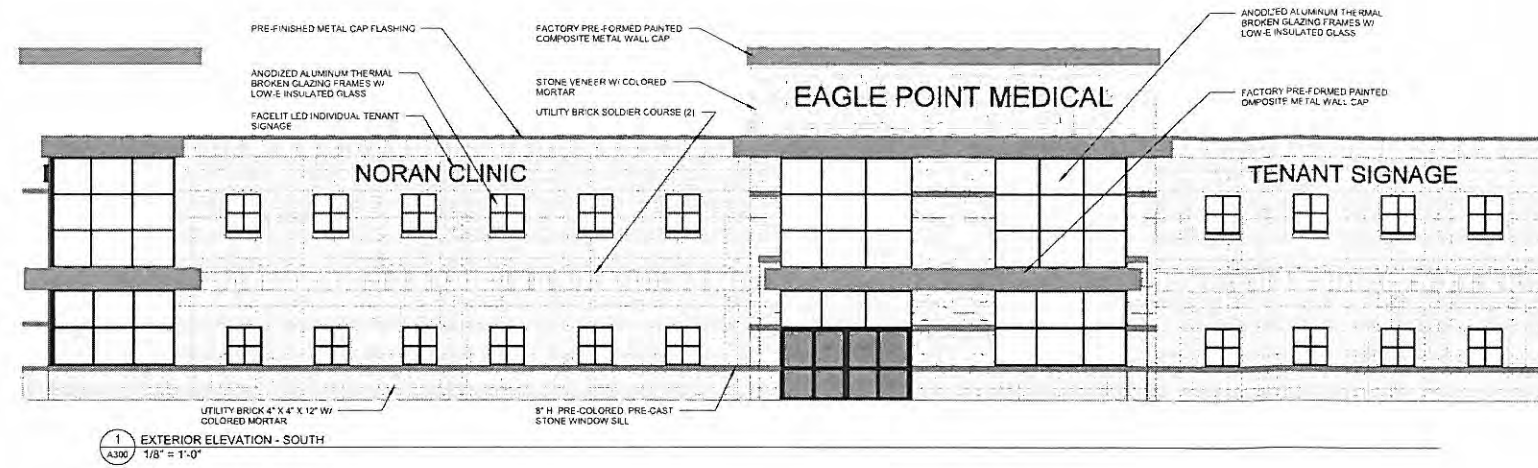
Issue Record		
No.	Date	Description

Sheet Title
SECOND FLOOR BUILDING SHELL
FLOOR PLAN

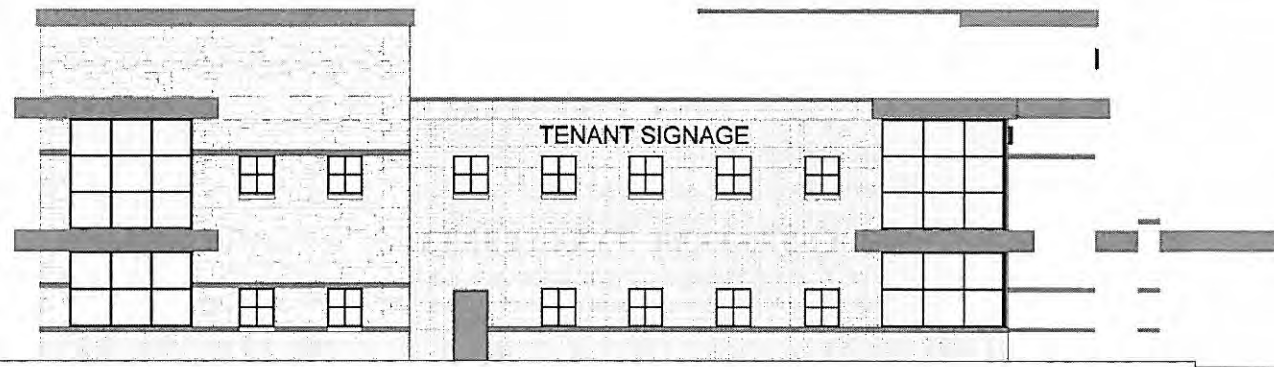
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Job Code: A-DAVIS/LEAF
Drawn By: MCG/ML Reviewed By: PG
Date: 05/21/2014

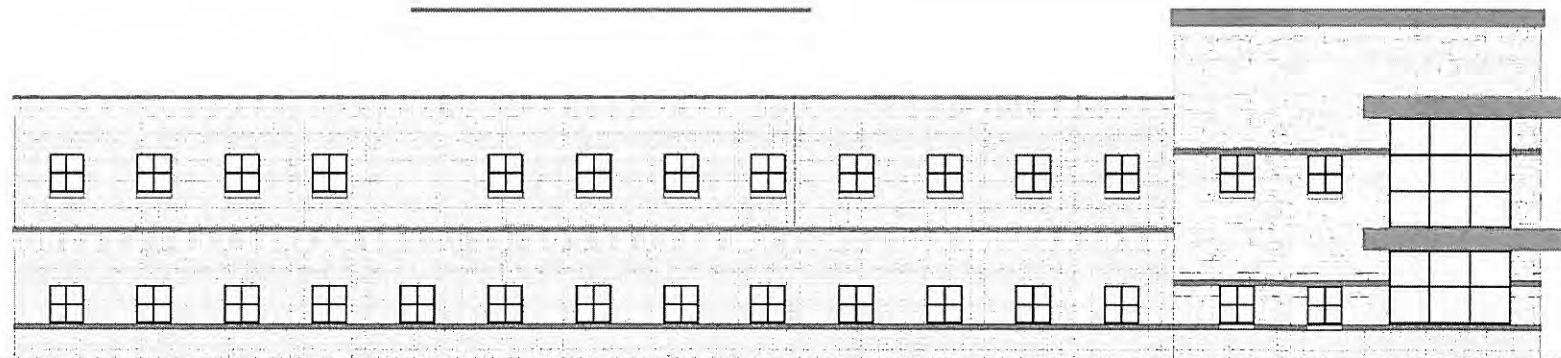
A201



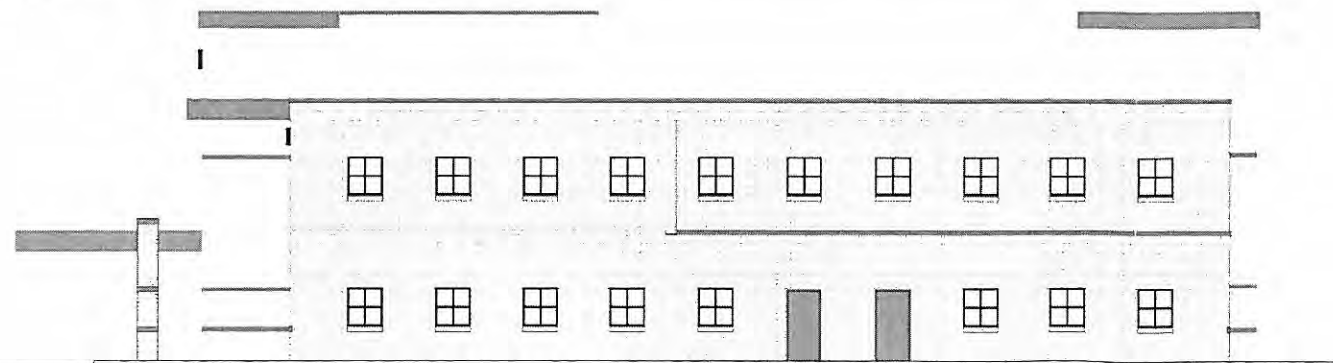
1 EXTERIOR ELEVATION - SOUTH
A300 1/8" = 1'-0"



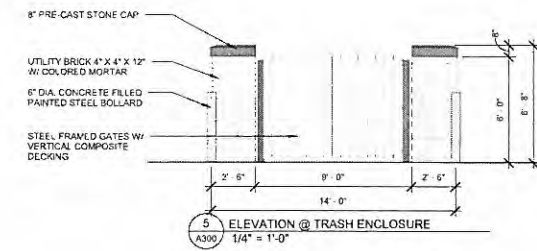
2 EXTERIOR ELEVATION - WEST
A300 1/8" = 1'-0"



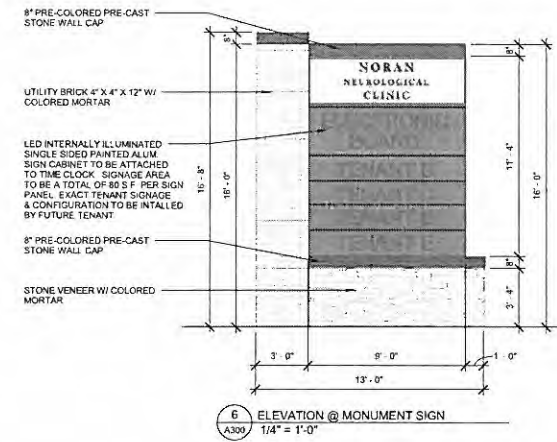
3 EXTERIOR ELEVATION - NORTH
A300 1/8" = 1'-0"



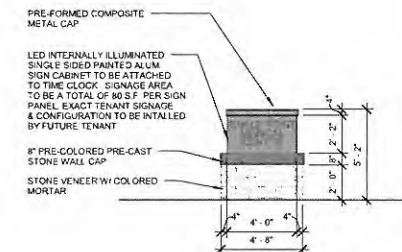
4 EXTERIOR ELEVATION - EAST
A300 1/8" = 1'-0"



5 ELEVATION @ TRASH ENCLOSURE
A300 1/4" = 1'-0"



6 ELEVATION @ MONUMENT SIGN
A300 1/4" = 1'-0"



7 ELEVATION @ DIRECTIONAL SIGN
A300 1/4" = 1'-0"

Issue Record

No	Date	Description

Sheet Title
EXTERIOR ELEVATIONS

Sheet Information

Job Code A-DAVIS/LEAM
Drawn By MKV/ML **Reviewed By** PG
Date 03.21.2014
A300



EAGLE POINT BUSINESS PARK 7TH ADDITION

KNOW ALL BY THESE PRESENTS: That UNITED LAND LLC, a Minnesota limited liability company, owner and proprietor of the following described property, situated in the County of Washington, State of Minnesota to wit:

Outlot A, EAGLE POINT BUSINESS PARK 5TH ADDITION, according to the plat thereof filed of record in the office of the Registrar of Titles in and for Washington County, Minnesota.

As evidenced by Certificate of Title No. _____

Has caused the same to be surveyed and platted as EAGLE POINT BUSINESS PARK 7TH ADDITION, and does hereby donate and dedicate to the public the easements created by this plat for drainage and utility purposes only.

In witness whereof said UNITED LAND LLC, a Minnesota limited liability company, has caused these presents to be signed by its proper officers this _____ day of _____, 200__.

UNITED LAND LLC

BY: _____ its _____ and _____ its _____

STATE OF _____
COUNTY OF _____

The foregoing instrument was acknowledged before me this _____ day of _____, 200__ by _____ its _____ and _____ of said UNITED LAND LLC, a Minnesota limited liability company, on behalf of said company.

Notary Public, _____ County, _____
My Commission Expires _____

I, Mark S. Hanson, hereby certify that I have surveyed and platted the property described on this plat as EAGLE POINT BUSINESS PARK 7TH ADDITION; that this plat is a correct representation of the survey; that all distances are correctly shown on the plat in feet and hundredths of a foot; that all monuments have been correctly placed in the ground as shown; that the outside boundary lines are correctly designated on the plat; and that there are no wet lands as defined in MS 505.02, Subd. 1 or public highways to be designated other than as shown.

Mark S. Hanson, Land Surveyor
Minnesota License No. 15480

STATE OF MINNESOTA
COUNTY OF _____

The foregoing Surveyor's Certificate was acknowledged before me this _____ day of _____, 200__ by Mark S. Hanson, Minnesota License No. 15480.

Notary Public, _____ County, Minnesota
My Commission Expires _____

This plat of EAGLE POINT BUSINESS PARK 7TH ADDITION was approved by the City Council of the City of Lake Elmo, Minnesota, this _____ day of _____, 200__, and hereby certifies compliance with all requirements as set forth in Minnesota Statutes, Section 505.03, Subd. 2.

SIGNED: _____
Mayor

SIGNED: _____
City Administrator

Pursuant to Chapter 820, Laws of Minnesota, 1971, this plat has been approved this _____ day of _____, 200__.

BY: _____
Washington County Surveyor

BY: _____
Deputy

There are no delinquent taxes, the current taxes due and payable for the year 200__ have been paid, and transfer has been entered this _____ day of _____, 200__.

BY: _____
Washington County Auditor/Treasurer

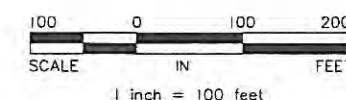
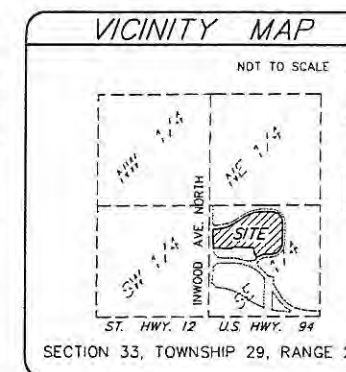
BY: _____
Deputy

Document Number _____

I hereby certify that this instrument was recorded in the Office of the Registrar of Titles for record on this _____ day of _____, 200__, at _____ o'clock _____ M., and was duly recorded in Washington County Records.

BY: _____
Washington County Registrar of Titles

BY: _____
Deputy



Orientation of this bearing system is based on the Washington County Coordinate system. (NAD83)

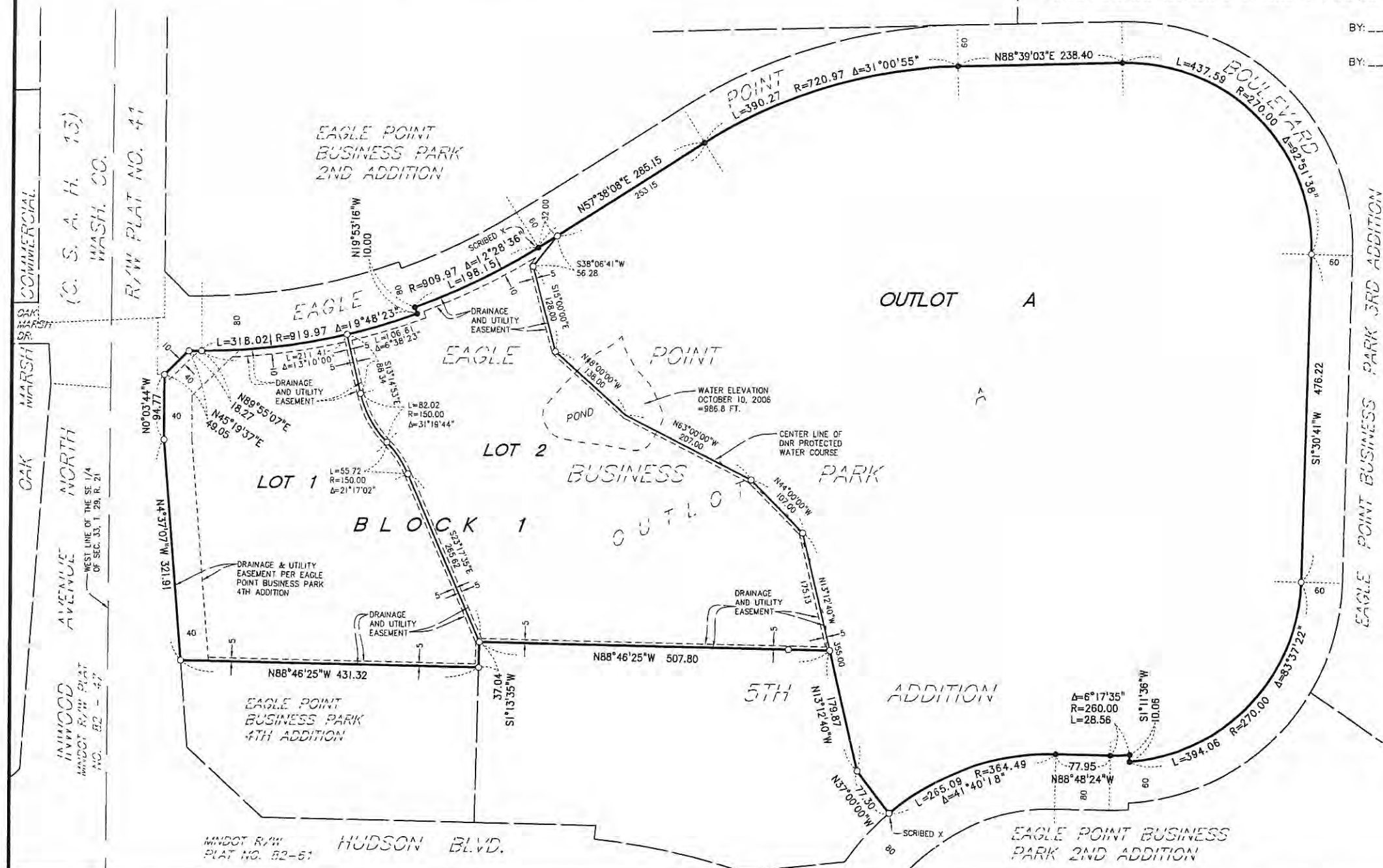
○ Denotes, 1/2 inch by 16 inch iron pipe with plastic cap inscribed RLS 15480, unless otherwise noted.

● Denotes found, 1/2 inch by 16 inch iron pipe with plastic cap inscribed RLS 16456, unless otherwise noted.

BENCH MARK:

Railroad spike in south face of 3rd set of double power poles east of Inwood Avenue North. Elevation = 1012.68 feet (N.G.V.D. 1929)

Sunde Land Surveying, LLC.





MAYOR & COUNCIL COMMUNICATION

DATE: July 1, 2014

CONSENT

ITEM # 13

AGENDA ITEM: Hammes Estates Preliminary Plat

SUBMITTED BY: Nick M. Johnson, City Planner

THROUGH: Dean Zuleger, City Administrator

REVIEWED BY: Kyle Klatt, Community Development Director
Jack Griffin, City Engineer
Greg Malmquist, Fire Chief
Stephen Mastey, City's Landscape Consultant
Ann Pung-Terwedo, Washington County Public Works
Molly Shodeen, DNR
John Hanson, Valley Branch Watershed District

SUGGESTED ORDER OF BUSINESS:

- Introduction of ItemCommunity Development Director
- Report/Presentation.....Community Development Director
- Questions from Council to Staff Mayor Facilitates
- Call for Motion Mayor & City Council
- Discussion Mayor & City Council
- Action on Motion..... Mayor Facilitates

POLICY RECCOMENDER: The Planning Commission has previously reviewed a Sketch Plan for the proposed Hammes Estates single family residential subdivision. The City's Subdivision Ordinance requires that the City Council review and approve a Preliminary Plat before an applicant may proceed with the preparation and recording of a Final Plat.

FISCAL IMPACT: TBD – The City will require that the applicant enter into a developer's agreement with the City to specify the financial responsibilities for various aspects of the subdivision and related public improvements.

SUMMARY AND ACTION REQUESTED: The City Council is being asked to consider a preliminary plat request from Hammes West, LLC for a 163-unit single family residential subdivision to be located on approximately 78 acres within Stage 1 of the City's I-94 Corridor Planning Area. The proposed plat would be located on property owned by Ellie Hammes and

Dorothy Lyons and located immediately west of Keats Ave. N. (CSAH 19) and approximately 1,300 feet south of 10th Street N. (CSAH 10). The preliminary plat has been prepared in response to the City's adopted Comprehensive Plan for the corridor, which guides the subject property as Urban Low Density Residential (LDR).

Staff and the Planning Commission are recommending that the City Council approve the preliminary plat with 17 conditions of approval with the following motion:

"Move to adopt Resolution No. 2014-55 approving the Hammes Estates Preliminary Plat subject to 17 conditions of approval."

BACKGROUND INFORMATION:

Attached are the original detailed Staff reports that were provided to the Planning Commission regarding the applicant's request for a Preliminary Plat, which includes general information about the application, details concerning the request, a summary of the relevant planning and zoning issues, a thorough review and analysis of the Preliminary Plat (including a draft list of recommended conditions of approval), draft findings, and the Staff recommendation to the Planning Commission. Because the Planning Commission postponed consideration of the Preliminary Plat during its initial review meeting, Staff prepared a table as part of the Staff Report dated 6/23/14 to document how the resubmitted plans addressed many of the outstanding issues identified as part of the original review on 5/12/14.

It should be noted that the applicants applied for a variance to allow for reduced riparian dedication and structure setbacks to the southern man-made channel of Goose Lake. The Planning Commission held a public hearing on the request on 6/23/14 and recommended denial of the variance. Instead of recommending approval of the variance, the Planning Commission encouraged the applicant to close off the southern channel of Goose Lake by following the DNR permitting process. By completing this process, the applicant should be able to proceed with the residential subdivision as planned. However, the Planning Commission and Staff are recommending that the lots impacted by the required 150-foot riparian dedication (Lots 1-6, Block 10 and Lots 1-5 and 11-12, Block 9) may not be final platted until the DNR permitting process to cut off the southern channel is complete. This condition allows the plat to proceed as planned, but addresses the DNR's concern about closer encroachment to Goose Lake.

PLANNING COMMISSION REPORT:

The Planning Commission initially reviewed the Preliminary Plat application at its May 12, 2014 meeting and conducted a public hearing on the request at this time. During the public hearing, the Planning Commission received the following testimony:

- Todd Ptacek, 812 Julep Avenue North, shared his experiences living in the Stonegate subdivision, noting that the trails that were included in the development were never finished. He noted his concern that the connection to the Goose Lake property was cut off due to the man-made southern channel of Goose Lake. He stated that he did not agree

with the acceptance of the entire buffer area as parkland dedication, even with a trail improvement. Finally, he stated that the density should be stepped back from Stonegate and other existing subdivisions in the I-94 Corridor.

- Wayne Prowse, 697 Julep Avenue North, expressed concern that the proposed subdivision was not providing enough parkland. He requested larger lots and reduced next to Stonegate. He encouraged the Planning Commission to require more parkland.
- Chairman William read a letter submitted by Walt Kreuger, 694 Jewel Avenue North, expressing his desire to have the buffer trail located further to the south in the buffer area.
- Chairman Williams also entered a brief email from Molly Shodeen, DNR, into the public record, stating that there should be some riparian buffering around the southern channel of Goose Lake.

The Commission closed the public hearing, but elected to postpone taking action on the application until Preliminary Plans were resubmitted to address the numerous outstanding issues identified in the Staff Report dated 5/12/14.

Because the Commission received comments from several individuals during the hearing, and also heard testimony from the Park Commission chair and additional individuals at the July 29th meeting, Staff has attached a copy of the Commission's approved June 22nd meeting minutes along with a copy of the draft minutes from the June 29th meeting. These minutes also include a detailed description of the Planning Commission's discussion and action on the preliminary plat.

In addition to Condition #16, requiring the southern channel of Goose Lake to be closed off before Lots 1-6, Block 10 and Lots 1-5 and 11-12, Block 9 may be final platted, the Planning Commission also recommended that the buffer trail along the northern portion of the property be moved to the south whenever feasible. Moving the trail further to the south, as requested by the Stonegate landowners, has been difficult due to the fact that impervious surfaces are not allowed within wetland buffer areas per Valley Branch Watershed District Rules. This rule has necessitated the trail to be located to the north in many areas, as there are many wetland along the northern portion of the site. The final list of conditions is included in Resolution 2014-055.

The Planning Commission recommended approval of the preliminary plat as submitted with 17 conditions of approval. The vote to recommend approval of the Hammes Estates Preliminary Plat was unanimous (Vote: 7-0).

STRENGTHS, WEAKNESSES, OPPORTUNITIES, THREATS:

Strengths: Approval of the Preliminary Plat with the 17 conditions as recommended by the Planning Commission and Staff allows the applicant to move forward with the preparation of a Final Plat application and final construction documents for a new single family residential subdivision in Stage 1 of the I-94 Corridor Planning Area.

Weaknesses: None

Opportunities: Approval of the plat application allows the development plans for the Hammes site to proceed as planned in the Comprehensive Plan. Moving forward with

sewered single family growth should allow the City to add additional users to the City's public sanitary sewer system, helping to finance the City's investments in sanitary sewer. Finally, requiring that the southern channel of Goose Lake be closed off before Lots 1-6, Block 10 and Lots 1-5 and 11-12, Block 9 may be final platted allows the plat to move forward, but at the same time addresses the water quality concerns for Goose Lake.

Threats: None

RECOMMENDATION:

Based on the aforementioned, the Planning Commission and Staff is recommending that the City Council approve the Hammes Estates Preliminary Plat subject to 17 conditions of approval through the following motion:

“Move to adopt Resolution 2014-55, approving the Hammes Estates Preliminary Plat subject to 17 conditions of approval.”

ATTACHMENTS:

1. Resolution 2014-55
2. Staff Report to the Planning Commission, 6/16/14
3. Staff Report to the Planning Commission, 5/12/14
4. Location Map
5. Application Form and Project Narrative
6. Hammes Estates Preliminary Plat, dated 6/6/14
7. Hammes Estates Preliminary Landscape and Tree Preservation Plans, dated 5/2/14
8. Illustrative Park Plan
9. Parkland Credit Exhibit w/Area Calculation
10. City Engineer Review Memorandum, dated 6/16/14
11. City Landscape Consultant Review Memorandum, dated 5/7/14
12. Washington County Review Memorandum, dated 5/6/14
13. MN DNR Review

**CITY OF LAKE ELMO
WASHINGTON COUNTY
STATE OF MINNESOTA**

RESOLUTION NO. 2014-55

A RESOLUTION APPROVING A PRELIMINARY PLAT FOR HAMMES ESTATES

WHEREAS, the City of Lake Elmo is a municipal corporation organized and existing under the laws of the State of Minnesota; and

WHEREAS, Hammes West, LLC, 36 Moonlight Bay, Stillwater, MN has submitted an application to the City of Lake Elmo (City) for a Preliminary Plat for Hammes Estates, a copy of which is on file in the City of Lake Elmo Planning and Zoning Department; and

WHEREAS, the Lake Elmo Planning Commission held public hearing on May 12, 2014 to consider the Preliminary Plat request and continued discussion of the Preliminary Plat at its June 23, 2014 meeting ; and

WHEREAS, the Lake Elmo Planning Commission adopted a motion recommending approval of the Preliminary Plat subject to 17 conditions of approval; and

WHEREAS, the Lake Elmo Planning Commission has submitted its report and recommendation concerning the Preliminary Plat as part of a memorandum to the City Council from City Planner Nick Johnson for the July 1, 2014 Council Meeting; and

WHEREAS, the City Council reviewed the Preliminary Plat at its meeting held on July 1, 2014 and made the following findings of fact:

- 1) That the Hammes Estates Preliminary Plat is consistent with the Lake Elmo Comprehensive Plan and the Future Land Use Map for this area.
- 2) That the Hammes Estates Preliminary Plat complies with the City's LDR- Urban Low Density Residential zoning district.
- 3) That the Hammes Estates Preliminary Plat complies with the City's subdivision ordinance.

NOW, THEREFORE, BE IT RESOLVED THAT the City Council does hereby approve the Hammes Estates Preliminary Plat subject to the following conditions:

- 1) Within six months of preliminary plat approval, the applicant shall complete the following:
 - a. The applicant shall provide adequate title evidence satisfactory to the City Attorney.
 - b. The applicant shall submit a revised Preliminary Plat and plans meeting all conditions of approval. All of the above conditions shall be met prior to the City accepting an

application for Final Plat and prior to the commencement of any grading activity on the site.

- 2) The City Engineer shall review and approve all revised Preliminary Plans that are submitted to the City in advance of Final Plat to satisfy Condition #1.
- 3) The Preliminary Plat approval is conditioned upon the applicant meeting all minimum City standards and design requirements.
- 4) All required modifications to the plans as requested by the City Engineer in a review letter dated June 16, 2014 shall be incorporated into the plans prior to consideration of a Final Plat.
- 5) Prior to the acceptance of the public improvements for the Hammes Estates plat, all wetland buffers shall be delineated and identified via staking or signage that is acceptable to the City.
- 6) The landscape plan shall be updated per the recommendations of the City's landscape consultant in a review memo dated 5/7/14.
- 7) The applicant shall be responsible for the construction of all improvements within the Keats Avenue (CSAH 19) right-of-way as required by Washington County and further described in the review letter received from the County dated May 6, 2014. The required improvements shall include, but not be limited to: construction of a modified median crossing, construction of a trail/sidewalk to the south side of the median, turn lanes, and other improvements as required by the County.
- 8) The developer shall follow all of the rules and regulations spelled out in the Wetland Conservation Act, and shall acquire the needed permits from Valley Branch Watershed District prior to the commencement of any grading or development activity on the site.
- 9) Landscape islands shall be platted as part of the right-of-way and shall be maintained by the Home Owners Association. The applicant shall enter into a maintenance agreement with the City that clarifies the individuals or entities responsible for any landscaping installed in areas outside of land dedicated as public park and open space on the final plat.
- 10) With an eligible parkland dedication of 5.7 acres provided, the applicant is responsible to pay a fee in lieu of land dedication for the equal market value amount of 2.1 acres of land at the time of the Final Developers Agreement. The City will work with the developer to clarify any and all park fee payments at the time of the Final Developers Agreement.
- 11) No more than 100 units may be approved as part of a final plat until secondary access is provided to the subdivision via a connection to 5th Street through the Savona subdivision.
- 12) For trails proposed to be located in any wetland buffer, the applicant must present a suitable design or material that is acceptable to the City and Valley Branch Watershed District.
- 13) The applicant must enter into a separate grading agreement with the City prior to the commencement of any grading activity in advance of final plat and plan approval. The City Engineer shall review any grading plan that is submitted in advance of a final plat, and said plan shall document extent of any proposed grading on the site.

- 14) The applicant must incorporate a play structure into the proposed park at Goose Lake per the request of the Lake Elmo Park Commission. Furthermore, the applicant must submit an updated design of the park property that meets City approval in advance of Final Plat.
- 15) The applicant shall work with the Planning Staff to name all streets in the subdivision prior to submission of a Final Plat.
- 16) Lots 1-6, Block 10 and Lots 1-5 and 11-12, Block 9 shall not be platted until the southern channel of Goose Lake is closed off from the lake, or the shoreland issue around the channel is resolved in some other manner that is acceptable to the DNR.
- 17) The applicant shall work to relocate segments of the northern buffer trail further to the south of the Stonegate subdivision wherever it is feasible as long as the trail does not encroach on any required wetland buffers.

Passed and duly adopted this 1st day of July 2014 by the City Council of the City of Lake Elmo, Minnesota.

Mike Pearson, Mayor

ATTEST:

Adam Bell, City Clerk



PLANNING COMMISSION

DATE: 6/23/2014

AGENDA ITEM: 5A – BUSINESS ITEM

CASE # 2014-14

ITEM: Hammes Estates Residential Subdivision – Preliminary Plat cont.

SUBMITTED BY: Nick Johnson, City Planner

REVIEWED BY: Kyle Klatt, Community Development Director
Jack Griffin, City Engineer

SUMMARY AND ACTION REQUESTED:

The Planning Commission is being asked to consider a Preliminary Plat application from Hammes West, LLC for a 163-unit single family residential development to be located on 78.1 acres immediately west of Keats Avenue (CSAH 19) and within Stage 1 of the City's I-94 Corridor Planning Area. The Planning Commission previously reviewed the application and held a public hearing on 5/12/14, at which time consideration of the plat application was postponed. The applicant has since resubmitted preliminary plans to address outstanding issues identified in the review on 5/12/14. Staff is recommending approval of the request subject to compliance with 15 conditions as noted in this report.

GENERAL INFORMATION

Applicant: Hammes West, LLC, 36 Moonlight Bay, Stillwater, MN 55082.

Property Owners: Ellie Hammes, 1187 Forest Ave., Maplewood, MN 55109, and Dorothy Lyons, 10105 10th Street North, Lake Elmo, MN 55042.

Location: Part of Section 34 in Lake Elmo, immediately west of Keats Avenue (CSAH 19), approximately 1,300 feet south of 10th Street (CSAH 10), and immediately south of Goose Lake. PID Number 34.029.21.13.0001.

Request: Application for preliminary plat approval of a 163-unit single family residential subdivision to be named Hammes Estates.

Existing Land Use and Zoning: Active mining and gravel operation and other vacant land.
Current Zoning: RT – Rural Development Transitional Zoning District; Proposed Zoning: LDR - Urban Low Density Residential

Surrounding Land Use and Zoning: North –Goose Lake and Stonegate Residential Estates (RE) subdivision; west – Stonegate RE subdivision; south – Lennar Savona Urban Low Density Residential (LDR) subdivision.

Comprehensive Plan: Urban Low Density Residential (2.5 – 4 units per acre).

History: Preliminary Plat review and public hearing on 5/12/14. Sketch Plan review by Planning Commission on 6/24/13. Sketch Plan review by the Park Commission on 7/15/13 and 1/30/14.

Deadline for Action: Application Complete – 5/2/2014
60 Day Deadline – 6/30/14
Extension Letter Mailed – No
120 Day Deadline – 8/29/14

Applicable Regulations: Chapter 153 – Subdivision Regulations
Article 10 – Urban Residential Districts (LDR)
§150.270 Storm Water, Erosion, and Sediment
Article 17 – Shoreland Management Overlay District

REQUEST DETAILS

The City of Lake Elmo has received a request from Hammes West, LLC for a preliminary plat to subdivide approximately 78 acres of land located within the I-94 Corridor planning area into 163 single family lots. The proposed plat would be located on property currently owned by the Hammes family, and would be located immediately west of Keats Avenue (CSAH 19), approximately 1,300 feet south of 10th Street (CSAH 10), and approximately ½ of a mile north of the I-94 right-of-way. The Planning Commission previously reviewed the Hammes Estates Preliminary Plat on 5/12/14, at which time a public hearing was held. At the meeting, the Planning Commission postponed consideration of the plat application until revised plans were submitted to address critical outstanding issues. The applicant resubmitted Preliminary Plans (Sheets 1-18) on 6/6/14 to address the concerns identified by the Planning Commission. Staff has found that the resubmitted plans address many of the more glaring concerns of the previous plat submission. Based upon review of the plat, the resubmitted plans will meet all applicable City requirements for conditional approval, and any deficiencies or additional work that is needed is noted as part of the review record. It is staff's expectation that all the deficiencies and requested revisions will be resolved in advance of Final Plat.

REVIEW AND ANALYSIS

In order to clearly present how the applicant has addressed the issues identified by the Planning Commission, as well as the other conditions of approval identified in the Staff Report dated 5/12/14, staff has provided the below table to identify how the various issues have been resolved. In order to review the previous Staff Report, dated 5/12/14, the materials for the May 12th meeting are posted on the City's website under Planning Commission agendas. In addition to the staff explanation, the applicant has also identified how the previously recommended conditions of approval have been addressed with a response letter dated 6/11/14 (Attachment #2). The resubmitted plat has addressed the several of the conditions of approval in the following manner:

Condition (Staff Report dated 5/12/14)	Response/Result
Condition #4: The approval of the Preliminary Plat is contingent upon the City approving a revised shoreland ordinance that would allow for the lot	The City of Lake Elmo adopted a new shoreland ordinance (Ord. 08-111) on 5/20/14, allowing for use of base zoning district standards with riparian dedication. However, the DNR has submitted a letter

sizes proposed in the portion of the Hammes Estates subdivision located within the shoreland district.	requesting riparian dedication around the southern man-made channel of Goose Lake. The applicant has applied for a variance to allow reduced riparian dedication around the southern channel. If the City grants the variance, this issue will be resolved. If the City does not grant the variance, the plat can be recommended for approval conditioned upon the southern channel being closed off from Goose Lake via a berm as recommended by the DNR.
Condition #5: The applicant shall submit evidence that a Phase I Environmental Review if the site has been completed and that further environmental review is not necessary.	The applicant has provided a City with a certified copy of the Phase I Environmental Review for the Hammes site. The report was produced by Element Materials Technology on March 10, 2014. The conclusions of the environmental assessment identify no recognized, controlled or historical environmental conditions of the property. The Assessment concludes that no additional investigation or evaluation is warranted based on the findings of the Phase I Environmental Review.
Condition #7: The applicant shall provide for a minimum green belt/buffer of 100 feet around all of the adjacent Stonegate subdivision, and must either revise the preliminary plat in the vicinity of Lot 16, Block 1 to properly account for this buffer or provide evidence to the City of any acquired open space easements from the adjacent property in the Stonegate subdivision.	The applicant has revised the preliminary plat in the southwest corner of the plat to provide a continuous 100-foot green belt, as specified in the City's Comprehensive Plan.
Condition #9: The utility plan shall be updated to relocate/add fire hydrants to meet spacing requirements and ensure ideal operational effectiveness per the direction of the Lake Elmo Fire Chief.	The utility plans have been revised to add and relocate fire hydrants per the specifications of the Fire Chief.
Condition #13: The developer shall be required to submit an updated parkland dedication calculation in advance of Final Plat.	The applicant has submitted an updated parkland dedication calculation (Attachment #4), resulting in an eligible dedication of approximately 5.7 acres. With this level of eligible dedication, the developer would be responsible to pay fees in lieu of land dedication in an amount equal to the fair market value of the remaining required dedication of 2.1 acres (7.8 acres (required dedication) – 5.7 acres (provided) = 2.1 acres remaining)
Condition #14: Any land under which trails are located will be accepted as park land provided the trail is located within a	This condition is no longer necessary as the applicant has submitted an updated parkland dedication calculation documenting the areas that will be

dedicated outlot and the developer constructs the said trails as part of the public improvements for the subdivision.	accepted as parkland dedication. For trails that are constructed by the developer but not located within dedicated outlots, staff would recommend the City consider some parkland credit for the construction and dedication of these trails to the City's public trail system.
Condition 16a: For trails in wetland buffers, the applicant must present a suitable design that is acceptable to the City and Valley Branch Watershed District.	The applicants are currently showing the limited trail segment in wetland buffer as a mowed or woodchip trail, which would meet VBWD rules. The City would recommend that this design is finalized in advance of Final Plat (Condition #12).
Condition 16b: The trail segment aligned to the north of Wetland A must be revised to be aligned to the south of the wetland due to concerns of the High Water Level.	The applicant has revised this segment of the trail to be located to the south of Wetland A.
Condition 16c: The trail segment between Lots 14 and 15, Block 2 shall be located on a 30-foot outlot dedicated to the City.	The applicant has provided the requested outlot (Outlot E) for the trail in between Lot 14, Block 2 and Lot 11, Block 3 at the end of Street 9.
Condition 18a: The Preliminary Plat must be revised to meet City standard requirements for utility easements.	The applicant has revised the Preliminary Plat to provide the requested utility easements.
Condition 18c: The Preliminary Plat must be revised to remove the Street 1 right-of-way from the minimum 25-foot Wetland Buffer of Wetland B. The entire street right-of-way must be relocated outside of the minimum wetland buffer.	The applicant has revised the Preliminary Plat to shift Street 1 to the north to maintain a 25-foot minimum wetland buffer outside of the Street 1 right-of-way.
Condition 18e: The Grading and Storm Water Management Plan must be revised so that the subgrade of Street 1 is above the HWL for infiltration basin 5A on Outlot D.	The applicant states in his narrative that they will work with the City Engineer on a suitable design to effectively protect the road subgrade. Staff is comfortable making this a conditional approval through addressing the review comments found in the City Engineer's review memo dated 6/16/14 in advance of Final Plat.
Condition 18f: Maintenance access roads must be relocated or improved to meet City standard requirements.	The applicants has revised the Preliminary Plat to provide maintenance access roads that meet City standards.
Condition 18g: The Preliminary Plat must be revised to provide additional details for the proposed improvements to the City park property at Goose Lake.	The applicants have provided an illustrative park plan to show their desired vision for the park property. The proposed park still does not contain a play structure. Staff would recommend that a play structure is

	somehow incorporated into the design in advance of Final Plat to be consistent with the recommendation of the Park Commission. In addition, the Engineer has requested additional information for the park property related to grading and 100-year flood elevations. Staff is recommending that an agreed upon park design be approved in advance of Final Plat.
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The conditions and their status outlined in the above table highlight the key revisions to the plat that were included as part of the resubmitted Preliminary Plans. In Staff's judgment, the applicants have met the City's request for updated Preliminary Plans. However, it is important to note that some of the original conditions that were recommended as part of the Staff Report dated 5/12/14 still apply, while others may need to be slightly modified. In order to further explain some key issues and review items that inform the recommended conditions of approval, staff would offer the following review comments:

- Goose Lake Southern Channel - Shoreland Variance.*** In the previous review of the Preliminary Plat, staff recommended that approval of the preliminary plat be conditioned on the City adopting a new shoreland ordinance. Since that time, the City adopted new shoreland regulations (Ord. 08-111) that include provisions requiring riparian dedication around lakes within close proximity to urban planning areas in Lake Elmo. By dedicating more than 150' of riparian area on the southern shore of Goose Lake, staff determined that the riparian dedication requirement had been met. However, the City received a review letter from the DNR stating that the shoreland provisions would apply to the southern man-made channel of Goose Lake in addition to the southern shoreline. According to the applicant, previous discussion with the DNR led them to believe that the Wetland Conservation Act and Valley Branch Watershed District would have jurisdiction over the southern channel, necessitating the rules of the WCA and VBWD. However, the DNR is recommending that the shoreland provisions apply to the southern channel. In order to address this request, the applicants have submitted a variance application citing practical difficulties and unique circumstances. Staff is recommending approval of the variance. If the City grants a variance to the applicant to allow reduced riparian dedication around the southern channel, then the shoreland issue will be resolved. However, if the City recommends denial of the variance, then an alternative design or mitigation strategy will be required. In the DNR's review letters (Attachment #7), they recommend as an alternative to the variance that a berm be used to close off the channel from Goose Lake. This action would be considered a restoration and would need permitting from the DNR. If the variance application related to the shoreland provisions around the southern channel is recommended for denial, Staff would recommend to the Planning Commission to add a 16th condition requiring the applicant to restore original shoreline of Goose Lake per the recommendation of the DNR. This path should allow the preliminary plat to move forward as it is currently designed with the condition of completing the restoration of Goose Lake.
- Landscaping and Tree Preservation.*** As part of the effort to resubmit Preliminary Plans, the applicant did not resubmit new Landscaping and Tree Preservation Plans. In reviewing these plans as part of the previous Preliminary Plat submittal that was reviewed on 5/12/14, the City's landscape consultant noted that the provided landscape material did not meet the requirements of the City's ordinance (landscape consultant's memo is Attachment #6). At

the meeting on 5/12/14, staff recommended a condition of approval that the Landscape Plan shall be updated per the recommendations of the City's landscape consultant. As the landscape plans have not been resubmitted, it should be noted that staff is recommending that this condition (#6) remain in place per the review of the City's landscape consultant.

- **City Engineer Review.** The City Engineer has provided the Planning Department with a detailed review memorandum (Attachment #5), dated 6/16/14, as a summary of his review of the Preliminary Plans. The vast majority of the issues identified are technical in nature, mostly relating to storm sewer and storm water management. Staff is confident that these issues can be resolved in advance of Final Plat. To resolve these issues in advance of Final Plat, Staff has included a general condition (Condition #4) that all issues identified in the City Engineer's memo dated 6/16/14 must be addressed by the applicant prior to approval of a final plat for any portion of the Hammes Estates subdivision. In addition, in regards to storm water management, the applicants still are required to meet the rules and regulations of the Wetland Conservation Act and Valley Branch Watershed District (Condition #8)
- **Improvements to Keats Ave. N. (CSAH 19).** Washington County previously submitted a memo, dated May 6, 2014, detailing the required improvements to Keats Ave. at the access location for the Hammes Estates neighborhood. Staff is recommending that the Preliminary Plat approval is conditioned upon the applicant completing the requested improvements to Keats Ave. (Condition #7)

Based on the above Staff report and analysis, Staff is recommending approval of the preliminary plat with 15 conditions intended to address the outstanding issues noted above and to further clarify the City's expectations in order for the developer to move forward with a Final Plat. The recommended conditions are as follows:

Recommended Conditions of Approval:

- 1) Within six months of preliminary plat approval, the applicant shall complete the following:
 - a. The applicant shall provide adequate title evidence satisfactory to the City Attorney.
 - b. The applicant shall submit a revised Preliminary Plat and plans meeting all conditions of approval. All of the above conditions shall be met prior to the City accepting an application for Final Plat and prior to the commencement of any grading activity on the site.
- 2) The City Engineer shall review and approve all revised Preliminary Plans that are submitted to the City in advance of Final Plat to satisfy Condition #1.
- 3) The Preliminary Plat approval is conditioned upon the applicant meeting all minimum City standards and design requirements.
- 4) All required modifications to the plans as requested by the City Engineer in a review letter dated June 16, 2014 shall be incorporated into the plans prior to consideration of a Final Plat.
- 5) Prior to the acceptance of the public improvements for the Hammes Estates plat, all wetland buffers shall be delineated and identified via staking or signage that is acceptable to the City.

- 6) The landscape plan shall be updated per the recommendations of the City's landscape consultant in a review memo dated 5/7/14.
- 7) The applicant shall be responsible for the construction of all improvements within the Keats Avenue (CSAH 19) right-of-way as required by Washington County and further described in the review letter received from the County dated May 6, 2014. The required improvements shall include, but not be limited to: construction of a modified median crossing, construction of a trail/sidewalk to the south side of the median, turn lanes, and other improvements as required by the County.
- 8) The developer shall follow all of the rules and regulations spelled out in the Wetland Conservation Act, and shall acquire the needed permits from Valley Branch Watershed District prior to the commencement of any grading or development activity on the site.
- 9) Landscape islands shall be platted as part of the right-of-way and shall be maintained by the Home Owners Association. The applicant shall enter into a maintenance agreement with the City that clarifies the individuals or entities responsible for any landscaping installed in areas outside of land dedicated as public park and open space on the final plat.
- 10) With an eligible parkland dedication of 5.7 acres provided, the applicant is responsible to pay a fee in lieu of land dedication for the equal market value amount of 2.1 acres of land at the time of the Final Developers Agreement. The City will work with the developer to clarify any and all park fee payments at the time of the Final Developers Agreement.
- 11) No more than 100 units may be approved as part of a final plat until secondary access is provided to the subdivision via a connection to 5th Street through the Savona subdivision.
- 12) For trails proposed to be located in any wetland buffer, the applicant must present a suitable design or material that is acceptable to the City and Valley Branch Watershed District.
- 13) The applicant must enter into a separate grading agreement with the City prior to the commencement of any grading activity in advance of final plat and plan approval. The City Engineer shall review any grading plan that is submitted in advance of a final plat, and said plan shall document extent of any proposed grading on the site.
- 14) The applicant must incorporate a play structure into the proposed park at Goose Lake per the request of the Lake Elmo Park Commission. Furthermore, the applicant must submit an updated design of the park property that meets City approval in advance of Final Plat.
- 15) The applicant shall work with the Planning Staff to name all streets in the subdivision prior to submission of a Final Plat.

DRAFT FINDINGS

Staff is recommending that the Planning Commission consider the following findings with regards to the proposed Hammes Estates preliminary plat:

- That the Hammes Estates Preliminary Plat is consistent with the Lake Elmo Comprehensive Plan and the Future Land Use Map for this area.

- That the Hammes Estates Preliminary Plat complies with the City’s LDR- Urban Low Density Residential zoning district.
- That the Hammes Estates Preliminary Plat complies with the City’s subdivision ordinance.

RECCOMENDATION:

Staff recommends that the Planning Commission recommend approval of the Hammes Estates Preliminary Plat with the 15 conditions recommended in the Staff Report. Suggested motion:

“Move to recommend approval of the Hammes Estates Preliminary Plat with the 15 conditions of approval as drafted by Staff based on the findings of fact listed in the Staff Report.”

ATTACHMENTS:

1. Westwood Response Letter, dated 6/11/14
2. Updated Preliminary Plat and Plans (18 sheets)
3. Illustrative Park Plan
4. Land Credit Exhibit w/Area Calculation
5. City Engineer Review Memorandum, dated 6/16/14
6. Landscape Consultant Memorandum, dated 5/7/14
7. DNR Review Letters

ORDER OF BUSINESS:

- Introduction.....Planning Staff
- Report by StaffPlanning Staff
- Questions from the Commission..... Chair & Commission Members
- Discussion by the Commission Chair & Commission Members
- Action by the Commission..... Chair & Commission Members



PLANNING COMMISSION
DATE: 5/12/2014
AGENDA ITEM: 4A – PUBLIC HEARING
CASE # 2014-14

ITEM: Hammes Estates Residential Subdivision – Preliminary Plat

SUBMITTED BY: Nick Johnson, City Planner

REVIEWED BY: Kyle Klatt, Community Development Director
Jack Griffin, City Engineer
Ann Pung-Terwedo, Washington County
John Hanson, Valley Branch Watershed District
Greg Malmquist, Fire Chief
Stephen Mastey, Landscape Architecture, Inc.

SUMMARY AND ACTION REQUESTED:

The Planning Commission is being asked to consider a Preliminary Plat request from Hammes West, LLC for a 164-unit single family residential development to be located on 78.1 acres immediately west of Keats Avenue (CSAH 19) and within Stage 1 of the City's I-94 Corridor Planning Area. Staff is recommending approval of the request subject to compliance with 19 conditions as noted in this report.

GENERAL INFORMATION

Applicant: Hammes West, LLC, 36 Moonlight Bay, Stillwater, MN 55082.

Property Owners: Ellie Hammes, 1187 Forest Ave., Maplewood, MN 55109, and Dorothy Lyons, 10105 10th Street North, Lake Elmo, MN 55042.

Location: Part of Section 34 in Lake Elmo, immediately west of Keats Avenue (CSAH 19), approximately 1,300 feet south of 10th Street (CSAH 10), and immediately south of Goose Lake. PID Number 34.029.21.13.0001.

Request: Application for preliminary plat approval of a 164-unit single family residential subdivision to be named Hammes Estates.

Existing Land Use and Zoning: Active mining and gravel operation and other vacant land.
Current Zoning: RT – Rural Development Transitional Zoning District; Proposed Zoning: LDR - Urban Low Density Residential

Surrounding Land Use and Zoning: North –Goose Lake and Stonegate Residential Estates (RE) subdivision; west – Stonegate RE subdivision; south – Lennar Savona Urban Low Density Residential (LDR) subdivision.

Comprehensive Plan: Urban Low Density Residential (2.5 – 4 units per acre).

History: Sketch Plan review by Planning Commission on 6/24/13. Sketch Plan review by the Park Commission on 7/15/13 and 1/30/14.

Deadline for Action: Application Complete – 5/2/2014
60 Day Deadline – 6/30/14
Extension Letter Mailed – No
120 Day Deadline – 8/29/14

Applicable Regulations: Chapter 153 – Subdivision Regulations
Article 10 – Urban Residential Districts (LDR)
§150.270 Storm Water, Erosion, and Sediment

REQUEST DETAILS

The City of Lake Elmo has received a request from Hammes West, LLC for a preliminary plat to subdivide approximately 78 acres of land located within the I-94 Corridor planning area into 164 single family lots. The proposed plat would be located on property currently owned by the Hammes family, and would be located immediately west of Keats Avenue (CSAH 19), approximately 1,300 feet south of 10th Street (CSAH 10), and approximately ½ of a mile north of the I-94 right-of-way. The 78 acre parcel has historically been used as a gravel mining operation. Activity has recently ceased and the landowner has proceeded with reclamation work related to rebalancing and grading the site. This work is permitted under the active mining permit for the Hammes properties.

The preliminary plat has been developed in response to the City's Comprehensive Plan, which identifies the applicant's property for urban low density residential development. The plat incorporates 164 single family lots, most of which are designed with widths of 75 or 81 feet.

In terms of access, the preliminary plat shows a connection to Keats Avenue in the northeastern portion of the plat. In addition to the Keats Ave. connection, the proposed plat on the Hammes site will be connected to the approved Savona single family subdivision via two north-south local roads (Streets 1 and 5), which ultimately connect to the 5th Street minor collector road.

This subdivision is the City's second subdivision that will receive public sanitary sewer service, which has been made available to the site via the nearly completed Section 34 429 Public Utility Project. The improvements constructed as part of the Section 34 Utility Project include a new sewer lift station that is sited on the property adjacent to Goose Lake just north of the eastern portion of the Hammes property. The utility project also includes the construction of a sewer main that will connect to the regional sewer interceptor within the Eagle Point Business Park, and the extension of water lines that will provide a connection to the City's water system. At present, the City's water for this area is provided by the City of Oakdale. However, Lake Elmo's long-range water plans call for an eventual connection to the City's water system via a new trunk line along Inwood Avenue. There is enough capacity in the Oakdale system to provide water to the Hammes development and a significant portion of the Stage 1 portion of the I-94 Corridor Planning Area until Lake Elmo can make the needed connections to its system. Sewer for the Hammes site is accessible near the location of the lift station in the northeastern portion of the property, and water main is accessible along the Keats Ave right-of-way in the southeastern portion of the site.

One of the other major features of the proposed subdivision is a series of outlots that will provide for open space, trails, and storm water management throughout the development area. Based on multiple reviews of the proposed parkland dedication with the Lake Elmo Park Commission, the applicant is proposing an extensive system of trails within the development, with the main trail being sited within

the 100-foot buffer/greenway area along the border of the Stonegate subdivision in the western and northern portions of the property. This buffer/greenway is consistent with the land use guidance of the Comprehensive Plan. In addition, the applicant is proposing park related improvements at the City owned parcel on Goose Lake, including a shelter and a fishing pier. All outlots that are planned for park land or storm water use will be deeded to the City, while the future home owner's association will retain ownership of the remaining outlots.

Due to the former use of the site as a gravel mining operation, the applicants have stated that a Phase 1 environmental review of the site has been completed. The purpose of such review is to test the site and soils for any hazardous or harmful materials related to the previous use of the site as a sand and gravel mine. In order to ensure that the site complies with all pertinent environmental requirements and regulations of the Minnesota Pollution Control Agency and Washington County Health Department, staff would recommend that the applicants submit evidence that a Phase 1 Environmental Review has been completed and that further environmental review is not necessary. Evidence of environmental review must be provided in advance of Final Plat.

The applicant is proposing to bring forward a final plat for the Hammes Estates development in stages, starting in the northeast corner of the site. The applicants have identified the area in the southeast portion of the plat as a future phase project due to necessary settling of the soils that must occur in that location.

In addition, the applicant has noted that the proposed builders of the residential homes in the subdivision will consist of a mix of one national builder, Ryland Homes, and other custom or local builders. In the narrative, the applicants have noted that the custom and local builders will build on lots closer to the Stonegate subdivision. The intent of this strategy is to offer greater variety and transition from the existing rural subdivision of Stonegate to the urban low density site to the south being developed by Lennar Homes.

PLANNING AND ZONING ISSUES

The Hammes site is guided for urban low density development in the City's Comprehensive Plan, and the applicant will be required to zone the site LDR – Low Density Residential at some point in the future (prior to Final Plat approval). The overall subdivision plan has therefore been prepared in order to comply with the district standards for the LDR zoning district in terms of lot size, lot widths, building setbacks, and other design criteria.

The arrangement of lots and blocks loosely follows two primary streets (Street 1 and Street 2), both of which follow curvilinear alignments. The proposed plat also includes three cul-de-sacs (Streets 4, 8 and 9), all of which meet the City's maximum length of 600 feet for lots less than 2.5 acres in size. It should be noted that two of the three cul-de-sacs within the plat are connected to the greater trail/sidewalk system through trail connections at the end of the streets. All other streets have been designed to comply with the City's current street standards. In terms of the overall design of the proposed subdivision, the location of the streets and lots are strongly guided by the 7 existing wetland on the site. Per Valley Branch Watershed District rules, the applicant is required to provide buffering of various sizes around the wetlands depending on wetland size, and type. The location of the wetlands and the required buffers have a large impact on the design of the subdivision.

Sidewalks and trails are planned throughout the subdivision. The proposed plans provide for sidewalks on one side of all streets, which is consistent with the Staff recommendation for sewered single family residential subdivisions. In terms of proposed trails, all are designed to be eight feet in

width and constructed of bituminous asphalt, which is consistent with City standard. In addition to the buffer/greenway trail, the proposed subdivision includes trails connecting cul-de-sacs on Streets 9 and 4, as well as a trail along the Keats Ave. corridor that is consistent with the same planned improvement as part of the Savona subdivision to the south. Finally, there is also a linkage trail provided between Lots 12 and 13, Block 11 to provide internal access to the subdivision from the Keats Ave. trail.

A typical lot building plan (detail) is included as part of the attached subdivision packet, and each lot as depicted in the plans includes a description of the lot size, dimensions, and all required setbacks. There are three general lot sizes proposed for the single family area with average dimensions of 75 by 140 feet, 81 by 140 feet and 85 feet by 140 feet. All of the lots meet the City's minimum area requirement of 8,000 for single family lots in a LDR district, with the smallest lot (Lot 15, Block 1) proposed at 9,836 square feet. The site plans further illustrate that throughout the single family area the lots will average 12,285 square feet, which exceeds the minimum requirements by a fairly wide margin. The median lot size within the development is 11,736 sq. ft. and the largest lot in the development (Lot 13, Block 11) is 21,941 sq. ft.

The following is a general summary of the subdivision design elements that have proposed as part of the Hammes preliminary plat and plans:

Zoning and Site Information:

- Existing Zoning: RT – Rural Development Transitional District
- Proposed Zoning: LDR - Urban Low Density Residential
- Total Site Area: 78.1 acres
- Total Residential Units: 164
- Proposed Density (Net): 2.60 units/acre

Proposed Lot Dimensional Standards:

- Min. Lot Width: 75 ft.
- Lot Depth: 140 ft. typical
- Lot Area: 8,000 sq. ft. (9,836 min.)
- Front Yard Setback: 25 ft.
- Side Yard Setback: 7.5 ft. (average)
- Rear Yard Setback: 20 ft.

Proposed Street Standards:

- ROW Width – Local 60 ft. (per Subdivision Ordinance)
- Street Widths – Local: 28 ft.(per City standard)

The standards listed above are all in compliance with the applicable requirements from the City's zoning and subdivision regulations. Based on Staff's review of the preliminary plat, the applicant has demonstrated compliance with all applicable code requirements at the level of detail that is required for a preliminary plat.

As with any new subdivision the City Code requires that a portion of the plat be set aside for public park use. In this case, the applicant has indicated that certain outlot areas will be dedicated to the City for this purpose, including portions of Outlots A and B. Since a large portion these areas represent green belt or trail corridors and are not suitable for other types of active recreation usage, it

is Staff's recommendation that the City only accept the trail corridors as part of the park land dedication requirements if the developer constructs the planned trail over these areas in conjunction with other required infrastructure improvements. As a general policy, Staff is recommending that the City consider accepting smaller land dedications in exchange for a more robust and connected trail system that will provide access to the City's numerous parks (including the regional park preserve). This recommendation is consistent with the general policy direction of the Park Commission and results of the 2013 City Park Survey.

The Subdivision Ordinance requires 10% of the land in urban residential districts to be set aside as open space, which totals 7.8 acres. On the plans (sheet 3), the applicant has noted that 8.15 acres of land associated with the greenbelt/buffer have been dedicated as park. If this calculation is accurate, then the required land dedication has been satisfied. However, sufficient detail regarding the calculation for public land dedication has not been provided. Staff would recommend that the applicant must submit a detailed and updated parkland dedication calculation in advance of Final Plat. Upon review of the updated calculation, if any gap exists between the eligible land dedication provided and the required land dedication amount, the applicant will be required to submit a fee in lieu of land dedication to satisfy the total land dedication requirement (10%) per the Subdivision Ordinance.

It is also important to note that a large portion of the Hammes property is currently in the shoreland district due to its proximity to Goose Lake. As the City's existing shoreland ordinance does not include standards for subdivision of properties in shoreland districts to be served by sanitary sewer, the existing ordinance does not provide solid guidance in this case. In an effort to update the City's shoreland standards, staff presented a draft shoreland ordinance to the Planning Commission on April 28, 2014. One of the key features of the draft ordinance was the provision of 150' riparian buffers for surface waters, including Goose Lake, located within the City's urban planning areas (I-94 Corridor and Village). Seeking additional guidance and feedback on the draft ordinance, City staff met with Molly Shodeen, the Area Hydrologist for the DNR, on April 29, 2014. Ms. Shodeen was familiar with the provision of riparian buffers in urban areas, a strategy that has been used successfully in Woodbury and other cities. In addition to reviewing the draft ordinance, staff also shared the Hammes Estates preliminary plat with the DNR. When reviewing the Hammes plat, it is clear that a 150-foot riparian buffer would be maintained with the current configuration of lots and outlots. City staff requested review comments for the plat from the DNR and have not received any review or feedback at this time. Moving forward, staff intend to hold a public hearing on the draft shoreland ordinance on May 28th, 2014. In order to ensure conformance to the City's shoreland rules, staff would recommend that the approval of the Preliminary Plat is contingent upon the City approving a revised shoreland ordinance to allow for the lot sizes proposed in the portion of the Hammes Estates subdivision that is in the shoreland district.

REVIEW AND ANALYSIS

City Staff has reviewed the proposed preliminary plat, which has gone through several versions in advance of the formal application being accepted as complete by the City. During the course of these reviews, several of the issues and concerns that were pointed out by Staff have been addressed by the applicant with updated submission documents. However, there are other elements of the plat that remain in conflict with City or Valley Branch Watershed District standards, which must still be addressed or corrected by the applicant. In general, the proposed plat will meet all applicable City requirements for conditional approval, and any deficiencies or additional work that is needed is noted

as part of the review record. Staff recognizes that this plat requires a number of revisions that will need to be resolved in advance of Final Plat.

The City has received a detailed list of comments from the City Engineer and the Washington County concerning the proposed subdivision, in addition to general comments from the Valley Branch Watershed District, Fire Chief, and the City's landscape consultant, Stephen Mastey, all of which are attached for consideration by the Commission.

In addition to the general comments that have been provided in the preceding sections of this report, Staff would like the Planning Commission to consider the issues and comments related to the following discussion areas as well:

- **Comprehensive Plan.** The proposed subdivision is consistent with the Lake Elmo Comprehensive Plan for this area and with the densities that were approved as part of this plan. The net densities for the development fall within the range allowed for the urban low density land use category. Other aspects of the Comprehensive Plan relate to the Hammes Estates subdivision as follows:
 - **Density Calculation.** The subject property is guided Urban Low Density Residential in the Comprehensive Plan, which allows for a density range of 2.5-4.0 units/acre (net). The applicants have completed the density calculation using the methodology consistent with the Metropolitan Council's practice. The resulting net density calculation resulted in a net density of 2.60 units/acre (164 units/63.03 net developable acres). Therefore, the proposed subdivision is consistent with the guidance of the Comprehensive Plan.
 - **Parks.** The City's park plan identifies proposed location for neighborhood parks based on the anticipated population that should be served by each park. The plan calls for one additional parks in the vicinity of the Hammes Estates Plat. As noted earlier by Staff, the Park Commission did review the Hammes Estates Sketch Plan on two occasions and unanimously approved the proposed park facilities to be included in the plat contingent upon construction of a shelter, fishing pier, various trail connections, and playground equipment on the site adjacent to Goose Lake.
 - **Water.** Water will be provided to this area via existing watermain along Inwood Avenue. The Hammes Estates subdivision will be able to be served under the City's current agreement with the City of Oakdale until the Inwood extension is completed.
 - **Sanitary Sewer.** The Hammes Estates subdivision will be served by sanitary sewer that will connect to the lift station constructed as part of the Section 34 utility project. All of the wastewater will flow to the lift station in the northeast boundary of the site via gravity sewer main.
 - **Phasing.** The Hammes Estates subdivision is located within the Stage 1 phasing area for the I-94 Corridor and therefore the proposed development is acceptable as the second development within the planned urban residential areas.
- **Zoning.** The proposed zoning for the Hammes Estates site will be LDR – Low Density Residential. The submitted development plans demonstrate compliance with the City's urban residential zoning requirements. Single family detached housing is a permitted use within the LDR zoning district.

- ***Subdivision Requirements.*** The City's Subdivision Ordinance includes a fairly lengthy list of standards that must be met by all new subdivisions, and include requirements for blocks, lots, easements, erosion and sediment control, drainage systems, monuments, sanitary sewer and water facilities, streets, and other aspects of the plans. For instance, the Ordinance includes limitations on the maximum length of a block (1,800 feet), and based on the spacing of the proposed streets, the Hammes Estates plat complies with this requirement. The majority of these requirements have been addressed as part of the City Engineer's comments (which are summarized below) or have been reviewed as part of Staff's ongoing communications with the applicant regarding the project.
- ***Infrastructure.*** The developer will be required to construct all streets, sewer, water, storm water ponds, and other infrastructure necessary to serve the development.
- ***Wetlands and Wetland Buffers.*** Due to the existence of 7 wetlands (A-G) on the site, the applicant is required to provide substantial buffering around these wetlands to comply with watershed district rules concerning wetland buffering. In order to demonstrate that adequate buffering has been achieved, the applicant has submitted a preliminary wetland buffer plan (sheets 11-12). This plan documents the proposed buffers intended to meet the VBWD rules. In addition, the applicant is proposing to include minimal portions of wetland buffers on a small number of private lots. As a broad policy, City staff are opposed to allowing substantial buffering on private lots. However, if done on a very limited amount of lots and restrained to the typical drainage and utility easement areas, staff has been willing to accept minimal wetland buffering on a limited number of properties. To illustrate where the proposed buffers encroach on private lots, the applicants have submitted a Wetland Buffer Encroachment Sketch (Attachment #4). This exhibit documents that wetland buffers encroach only within the normal drainage and utility easements of 16 total lots (Lot 3, Block 6, Lots 2 and 3, Block 5, Lots 17 and 16, Block 2, Lots 8-11, Block 6 and Lot 1-7, Block 7). In order to protect these areas within private lots, staff required these areas to be located within the normal drainage and utility easements. In addition, staff would recommend that prior to the acceptance of the public improvements for the Hammes Estates plat, all wetland buffers shall be delineated and identified via some form of staking or signage that is acceptable to the City. In addition, it should be noted that the minimum 25' wetland buffer for Wetland B encroached on the City's public right-of-way of Street 1. The encroachment of the minimum buffer does not comply with City standards, and staff is recommending that Street 1 be shifted north/revised. Further details on this specific encroachment are found the review comments of the City Engineer.
- ***Trails.*** The applicants are proposing an extensive system of trails throughout the development. Staff reviewed the proposed trails and has the following comments:
 - The proposed buffer trail that aligns to the north of Wetland A appears to encroach into the High Water Level (HWL) of Wetland A. Staff is recommending that the trail be aligned to the south of Wetland A within an outlot dedicated to the City.
 - In several instances, trail are proposed to encroach on wetland buffer areas. Per Valley Branch Watershed District rules, no impervious surface is allowed within required wetland buffers. In order to allow for trails in any wetland buffer, the applicant must present a suitable design or material for proposed multi-purpose trails in buffer areas that is acceptable to the City and VBWD. In addition, staff

recommends that the applicant limit the encroachment of trails into buffer areas to the greatest extent possible.

- The proposed trail that connects the cul-de-sac on Street 9 to Street 1 is currently shown is being located within dedicated easements to the City. In meeting with the applicant, staff agreed to allow the trail to be located on dedicated easements between Lots 6-9 on Block 5. However, City staff did request that the trail segment between Lots 14 and 15, Block 2 be located on a dedicated 30-foot outlot. Staff would recommend that this revision be completed to ensure maximum amount of trail are in dedicated outlots to the City as per staff recommendation.
- In terms of the buffer/greenway trail, staff recognizes that the City has received requests in the past from Stonegate residents to keep the trail located in the southern portion of the greenway/buffer. However, in the instance of the trail north of Wetland F, the trail has been located on the City-owned park property in the northern portion of the greenway due to the required wetland buffers surrounding Wetlands F and G. If the trails were to be located in the southern portion of the greenway in this area, the trails would not be allowed to be constructed of bituminous or any other type of impervious surface. In other words, some type of boardwalk trail or other design would be required. From a maintenance perspective, this design is not ideal for the City, as maintenance activity is also restricted within wetland buffers. For these reasons, staff is recommending that the greenway trail be located on the City owned property.
- ***Landscaping and Tree Preservation.*** The landscape and tree preservation plans have been reviewed by the City's consulting landscape architect, Stephen Mastey. Mr. Mastey's review memorandum related to the landscape and tree preservation plans is found in Attachment #6e. The most critical component of Mr. Mastey's review relates to the fact that the total amount of trees/plant material proposed for the subdivision does not currently meet the requirements of the City's Landscape and Tree Preservation Ordinances. More specifically, the landscape plan as proposed is 149 trees short of the City's requirements. In the narrative, the applicants request some flexibility from total number of plantings due to the City's ordinances being amended since the last subdivision (Savona) was approved. It is accurate that the City has adopted new landscaping standards and tree preservation requirements since the Savona development was approved. However, in the professional judgment of Mr. Mastey, the City's requirements are fair and reasonable compared to other metro communities. For this reason, staff is recommending that the applicant revise the landscape plan through one of the following methods outlined in Mr. Mastey's review memo to make up the difference for the required trees/plant materials. In addition, any updates to the landscape plan should identify location of species of various plant material, landscape irrigation plans, as well as seed or planting plans around storm water facilities, all of these recommendations per Mr. Mastey. The applicants' consultant is advised to contact Mr. Mastey with any questions and/or recommendations.
- ***Green Belt/Buffer.*** The Comprehensive Plan identifies an area north and west of the Hammes Estates plat as a greenway/buffer space with a minimum width of 100 feet. The Hammes subdivision includes an area that has been design to comply with this aspect of the Comprehensive Plan. However, there is one lot (Lot 16, Block 1) where the green belt as shown does not appear to meet the intent of the Comprehensive Plan. Staff would

recommend that the plans are revised in advance of final plat to meet the requirement of the Comprehensive Plan. As an alternative to this requirement, the applicant could submit evidence of an open space easement acquired from the adjacent property owner in the Stonegate subdivision to fulfill the 100' required greenbelt dimension next to Lot 16, Block 1 of the Hammes plat.

- ***Streets.*** The proposed street system has been designed to comply with all applicable subdivision requirements and City engineering standards. It should be noted that the applicants are planning a temporary cul-de-sac or turn around on Street 2 to the north of the Future Phase boundary (see sheet 4). Staff would recommend that the design of the temporary cul-de-sac meet the approval of the City Engineer, Public Works Superintendent and Fire Chief.
- ***Secondary Access.*** In order to ensure effective traffic circulation and appropriate access for emergency services, staff recommends providing secondary access to the site at the earliest convenience/phasing possible. As part of the Savon Final Plat, a connection to Street 5 within the Hammes plat was provided. This connection will likely be the best opportunity to provide secondary access to the site, as the connection through the Savona development provides access to the 5th Street minor collector road. Staff would recommend that only a certain threshold or number of units are platted before secondary access is provided.
- ***Street Names.*** Staff is recommending that the street names for the proposed subdivision follow the Washington County street naming system. As a condition of approval, Staff is asking that the applicant continue to work with the City at developing street names for the project, and that these names be included with the final plat submission.
- ***City Engineer Review.*** The City Engineer has provided the Planning Department with a detailed comment letter (Attachment #6a) as a summary of his preliminary plat review. Staff has incorporated the more significant issues identified by the Engineer as part of the recommended conditions of approval, and has also included a general condition that all issues identified by the City Engineer must be addressed by the applicant prior to approval of a final plat for any portion of the Hammes Estates subdivision. The Engineer notes that several of the City's standards are not currently met, and the necessary revisions and corrections must be made in advance of Final Plat.
- ***Fire Department Review.*** The Fire Chief has reviewed the plat and has requested that the locations of the proposed fire hydrants be revised to comply with the City's requirements. More specifically, the Fire Chief has identified multiple hydrant locations that do not meet the criteria for required spacing or are in locations that are not ideal for operation effectiveness. The Fire Chief's review memo is Attachment #6d. Staff is recommending that the applicant revise the location of the proposed hydrants at the direction of the Fire Chief.
- ***Washington County Review.*** County Staff has reviewed the Hammes Estates plat and provided specific comments to the City in a letter dated May 6, 2014. The most significant of the County's concerns is that the applicant will need to make improvements to the County road system in order to provide the necessary access to Hammes Estates. As a condition of approval, Staff has noted that the applicant will be responsible for including all improvements to Keats Ave. (CSAH 19) as required by the County as part of the construction plans for the development, and that the developer will be responsible for constructing these

improvements, which will include the modification of the median crossing within the CSAH 19 right-of-way, the installation of turn lanes into the development, and construction of a trail/sidewalk into the south side of the median.

- ***Watershed Districts.*** The project area lies within the Valley Branch Watershed District (VBWD). John Hanson, the VBWD Engineer, has provided informal comments and emails to staff regarding the project (Attachment #6c). Per VBWD's informal comments, the proposed plat conflicts with required standards and VBWD rules. The developer must meet all VBWD rules and will need to secure permits from the VBWD in order to proceed with the development as planned.

Based on the above Staff report and analysis, Staff is recommending approval of the preliminary plat with 19 conditions intended to address the outstanding issues noted above and to further clarify the City's expectations in order for the developer to move forward with a final plat. The recommended conditions are as follows:

Recommended Conditions of Approval:

- 1) Within six months of preliminary plat approval, the applicant shall complete the following:
 - a. The applicant shall provide adequate title evidence satisfactory to the City Attorney.
 - b. The applicant shall submit a revised preliminary plat and plans meeting all conditions of approval. All of the above conditions shall be met prior to the City accepting an application for final plat and prior to the commencement of any grading activity on the site.
- 2) The City Engineer shall review and approve all revised Preliminary Plans that are submitted to the City in advance of Final Plat to satisfy Condition #1.
- 3) The Preliminary Plat approval is conditioned upon the applicant meeting all minimum City standards and design requirements.
- 4) The approval of the Preliminary Plat is contingent upon the City approving a revised shoreland ordinance that would allow for the lot sizes proposed in the portion of the Hammes Estates subdivision located within a shoreland district.
- 5) The applicants shall submit evidence that a Phase 1 Environmental Review of the site has been completed and that further environmental review is not necessary. Evidence of environmental review must be provided in advance of Final Plat.
- 6) Prior to the acceptance of the public improvements for the Hammes Estates plat, all wetland buffers shall be delineated and identified via staking or signage that is acceptable to the City.
- 7) The applicant shall provide for a minimum green belt/buffer of 100 feet around all of the adjacent Stonegate subdivision, and must either revise the preliminary plat in the vicinity of Lot 16, Block 1 to properly account for this buffer or provide evidence to the City of any acquired open space easements from the adjacent property in the Stonegate subdivision.

- 8) The landscape plan shall be updated per the recommendations of the City's Landscape consultant.
- 9) The utility plan shall be updated to relocate/add fire hydrants to meet spacing requirements and ensure ideal operational effectiveness per the direction of the Lake Elmo Fire Chief.
- 10) The applicant shall be responsible for the construction of all improvements within the Keats Avenue (CSAH 19) right-of-way as required by Washington County and further described in the review letter received from the County dated May 6, 2014. The required improvements shall include, but not be limited to: construction of a modified median crossing, construction of a trail/sidewalk to the south side of the median, turn lanes, and other improvements as required by the County.
- 11) The developer shall follow all of the rules and regulations spelled out in the Wetland Conservation Act, and shall acquire the needed permits from Valley Branch Watershed District prior to the commencement of any grading or development activity on the site.
- 12) Landscape islands shall be platted as part of the right-of-way and shall be maintained by the Home Owners Association. The applicant shall enter into a maintenance agreement with the City that clarifies the individuals or entities responsible for any landscaping installed in areas outside of land dedicated as public park and open space on the final plat.
- 13) The developer shall be required to submit an updated parkland dedication calculation in advance of Final Plat. Upon submission of the calculation, if the amount of eligible parkland that is dedicated does not equal the required total land dedication of 7.8 acres, the applicant will be required to pay a fee in lieu of park land dedication equivalent to the fair market value for the amount of land required to meet the total dedication requirements less the eligible land dedicated for park purposes. Any cash payment in lieu of land dedication shall be paid by the applicant prior to the release of the final plat for recording.
- 14) Any land under which public trails are located will be accepted as park land provided the trail is located within a dedicated outlot and the developer constructs said trails as part of the public improvements for the subdivision.
- 15) No more than 100 units may be approved as part of a final plat until secondary access is provided to the subdivision via a connection to 5th Street through the Savona subdivision.
- 16) Modifications to the proposed trails in the subdivision shall include the following:
 - a. For trails proposed to be located in any wetland buffer, the applicant must present a suitable design or material that is acceptable to the City and Valley Branch Watershed District. In addition, staff recommends that the applicant limit the encroachment of trails into buffer areas to the greatest extent possible.
 - b. The trail segment aligned to the north of Wetland A must be revised to be aligned to the south of the wetland due to concerns of the High Water Level. The trail shall be located within an outlot dedicated to the City.
 - c. The trail segment between Lots 14 and 15, Block 2 shall be located on a 30-foot outlot dedicated to the City.

- 17) The applicant must enter into a separate grading agreement with the City prior to the commencement of any grading activity in advance of final plat and plan approval. The City Engineer shall review any grading plan that is submitted in advance of a final plat, and said plan shall document extent of any proposed grading on the site.
- 18) All required modifications to the plans as requested by the City Engineer in a review letter dated May 8, 2014 shall be incorporated into the plans prior to consideration of a final plat. Specific requirements include, but are not limited to, the following:
- a. The Preliminary Plat must be revised to meet City standard requirements for utility easements.
 - b. The Preliminary Plat must be revised to incorporate the necessary improvements to Keats Ave. (CSAH 19) as required by Washington County
 - c. The Preliminary Plat must be revised remove the Street 1 right-of-way from the minimum 25-foot buffer for Wetland A. The entire street right-of-way must be relocated outside of the minimum wetland buffer.
 - d. The grading plan must be revised to meet the Valley Branch Watershed District standards for grading within wetland buffers.
 - e. The Grading and Storm Water Management Plan must be revised so that the subgrade of Street 1 is above the HWL for infiltration basin 5A on Outlot D.
 - f. Maintenance access roads must be relocated or improved to meet City standard requirements.
 - g. The Preliminary Plat must be revised to provide additional details for the proposed improvements to the City park property adjacent to Goose Lake and the City's lift station to demonstrate that the improvements can be completed as shown.
- 19) The applicant shall work with the Planning Staff to name all streets in the subdivision prior to submission of a final plat.

DRAFT FINDINGS

Staff is recommending that the Planning Commission consider the following findings with regards to the proposed Hammes Estates preliminary plat:

- That the Hammes Estates preliminary plat is consistent with the Lake Elmo Comprehensive Plan and the Future Land Use Map for this area.
 - That the Hammes Estates preliminary plat complies with the City's LDR- Urban Low Density Residential zoning district.
 - That the Hammes Estates preliminary plat complies with the City's subdivision ordinance.
-

RECCOMENDATION:

Staff recommends that the Planning Commission recommend approval of the Hammes Estates preliminary plat with the 19 conditions of approval as listed in the Staff report. Suggested motion:

“Move to recommend approval of the Hammes Estates preliminary plat with the 19 conditions of approval as drafted by Staff based on the findings of fact listed in the Staff Report.”

ATTACHMENTS:

1. Location Map
2. Application Forms and Project Narrative
3. Preliminary Plat and Plans (25 sheets)
4. Wetland Buffer Encroachment Sketch
5. Hammes Wetland Report
6. Review Comments:
 - a. City Engineer
 - b. Washington County
 - c. Valley Branch Watershed District
 - d. Fire Chief
 - e. Landscape Consultant

ORDER OF BUSINESS:

- IntroductionPlanning Staff
- Report by StaffPlanning Staff
- Questions from the Commission Chair & Commission Members
- Open the Public HearingChair
- Close the Public Hearing.....Chair
- Discussion by the Commission Chair & Commission Members
- Action by the Commission..... Chair & Commission Members



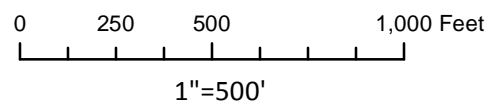
Location Map: Hammes Estates Plat (PID: 34.029.21.13.0001)



Data Source: Washington County, MN
5-9-2014



Hammes Property



Date received: _____
Received by: _____
LJH: _____

LAKELAND

651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

PRELIMINARY PLAT APPLICATION

Applicant: Hammes West LLC Brian McGoldrick
Address: 36 Moonlight BAY Stillwater, MN
Phone #: 651-387-1000
Email Address: brian.mcgoldrick@hotmail.com
Fee Owner: Ellie Hammes
Address: 1187 Forest Ave
Phone #: 651-274-6054
Email Address: Mnplewood1mnw 55109

Property Location (Address and Complete (long) Legal Description): See Attached
ALTA Survey

General information of proposed subdivision: _____

Proposed Single-Family Residential
development will create 165 lots

Conducted pre-application meeting with Staff? _____

☒ Yes

☐ No

In signing this application, I hereby acknowledge that I have read and fully understand the applicable provisions of the Zoning Ordinance and current administrative procedures. I further acknowledge the fee explanation as outlined in the application procedures and hereby agree to pay all statements received from the City pertaining to additional application expense.

Signature of applicant: _____

Date: _____

3/7/14

City Use Only

Planning/Zoning District: _____

Reviewed by: _____

Date: _____

Subject to the following conditions: _____

Engineering Reviewed by: _____

Date: _____

Subject to the following conditions: _____



Lake Elmo City Hall
651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

ACKNOWLEDGEMENT OF RESPONSIBILITY

This is to certify that I am making application for the described action by the City and that I am responsible for complying with all City requirements with regard to this request. This application should be processed in my name and I am the party whom the City should contact regarding any matter pertaining to this application.

I have read and understand the instructions supplied for processing this application. The documents and/or information I have submitted are true and correct to the best of my knowledge. I will keep myself informed of the deadlines for submission of material and of the progress of this application.

I understand that this application may be reviewed by City staff and consultants. I further understand that additional information, including, but not limited to, traffic analysis and expert testimony may be required for review of this application. I agree to pay to the City upon demand, expenses, determined by the City, that the City incurs in reviewing this application and shall provide an escrow deposit to the City in an amount to be determined by the City. Said expenses shall include, but are not limited to, staff time, engineering, legal expenses and other consultant expenses.

I agree to allow access by City personnel to the property for purposes of review of my application.

Hammes West LLC
Signature of applicant Brian J. McGoldrick Date 3/2/14
Name of applicant BRIAN MCGOLDRICK Phone 651-387-1000
(Please Print)

Name and address of Contact (if other than applicant) _____



Lake Elmo City Hall
651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

AFFIRMATION OF SUFFICIENT INTEREST

I hereby affirm that I am the **fee title owner** of the below described property or that I have written authorization from the owner to pursue the described action.

Name of applicant Hammes West LLC
(Please Print)

Street address/legal description of subject property _____

South 1/2 of the Northeast Quarter, Section 34,
Township 29, Range 21, except the east 60 feet
of the North 967 feet, and except the Parcel 3
of Washington County Highway Right of Way 49-19B,
Washington County Minnesota.

Eleanor Hammer
Signature

3/7/14
Date

If you are not the fee owner, attach another copy of this form which has been completed by the fee owner or a copy of your authorization to pursue this action.

If a corporation is fee title holder, attach a copy of the resolution of the Board of Directors authorizing this action.

If a joint venture or partnership is the fee owner, attach a copy of agreement authorizing this action on behalf of the joint venture or partnership.

AFFIRMATION OF SUFFICIENT INTEREST

I hereby affirm that I am the **fee title owner** of the below described property or that I have written authorization from the owner to pursue the described action.

Name of applicant Hammes West LLC
(Please Print)

Street address/legal description of subject property _____
South 1/2 of the Northeast Quarter, Section 34,
Township 29, Range 21, except the east 60 feet
of the North 967 feet, and except the Parcel 3
of Washington County Highway Right of Way 49-19B,
Washington County Minnesota.

Edward R Kuhlman
Signature

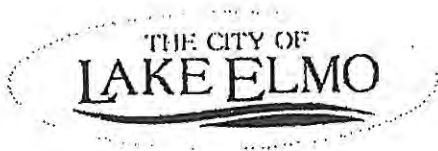
3/7/14
Date

Edward Kuhlman, Personal Representative of the estate of Earl R Hammes

If you are not the fee owner, attach another copy of this form which has been completed by the fee owner or a copy of your authorization to pursue this action.

If a corporation is fee title holder, attach a copy of the resolution of the Board of Directors authorizing this action.

If a joint venture or partnership is the fee owner, attach a copy of agreement authorizing this action on behalf of the joint venture or partnership.



Lake Elmo City Hall
651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

AFFIRMATION OF SUFFICIENT INTEREST

I hereby affirm that I am the fee title owner of the below described property or that I have written authorization from the owner to pursue the described action.

Name of applicant Hammes West LLC
(Please Print)

Street address/legal description of subject property _____

South 1/2 of the Northeast Quarter, Section 34,
Township 29, Range 21, except the east 60 feet
of the North 967 feet, and except the Parcel 3
of Washington County Highway Right of Way 49-19B,
Washington County Minnesota.

Elmer Hammer _____
Signature Date

as attorney in fact for Leo Hammes

If you are not the fee owner, attach another copy of this form which has been completed by the fee owner or a copy of your authorization to pursue this action.

If a corporation is fee title holder, attach a copy of the resolution of the Board of Directors authorizing this action.

If a joint venture or partnership is the fee owner, attach a copy of agreement authorizing this action on behalf of the joint venture or partnership.



Lake Elmo City Hall
651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

AFFIRMATION OF SUFFICIENT INTEREST

I hereby affirm that I am the fee title owner of the below described property or that I have written authorization from the owner to pursue the described action.

Name of applicant Hammes West LLC
(Please Print)

Street address/legal description of subject property _____

South ½ of the Northeast Quarter, Section 34, Township 29, Range 21, except the East 60 feet

of the North 967 feet, and except Parcel 3 of Washington County Highway Right of Way

Plat 49-19B, Washington County, Minnesota;

Dorothy H. Lyons 3/19/14
Dorothy H. Lyons Date

If you are not the fee owner, attach another copy of this form which has been completed by the fee owner or a copy of your authorization to pursue this action.

If a corporation is fee title holder, attach a copy of the resolution of the Board of Directors authorizing this action.

If a joint venture or partnership is the fee owner, attach a copy of agreement authorizing this action on behalf of the joint venture or partnership.

HAMMES ESTATES NARRATIVE

a. Contact Information:

Owner: Hammes West, LLC
c/o Brian McGoldrick
36 Moonlight Bay
Stillwater, MN 55082
651-387-1000

Owner: Eleanor Hammes
1187 Frost Avenue
Maplewood, MN 55109

With Copy to:
The Afton Law Office
3121 St. Croix Trail South
Afton, MN 55001
651-436-8656

Owner: Dorothy Lyons
10105 – 10th Street
Lake Elmo, MN 55042

With Copy to:
Brian D. Chmielewski
6043 Hudson Road, Suite 340
Woodbury, MN 55125
651-330-7191

Engineer: Westwood Professional Services
Attention: Ryan Bluhm, PE, LEED, AP
7699 Anagram Drive
Eden Prairie, MN 55344
952-906-7432

b. Site Data.

Parcel Size: The parcel size is 78 acres (3,397,680 square feet)
PID: 34.029.21.13.0001
Zoning: Rural Development Transitional District and guided
Low Density Residential in the Comprehensive Plan,
Planned Use section. *It is anticipated the zoning will be
changed to LDR, Low Density Residential in conjunction
with the preliminary plat process.*
Legal Description: South ½ of the Northeast Quarter, Section 34, Township
29,
and
Range 21, except the East 60 feet of the North 967 feet,
except Parcel 3 of Washington County Highway Right of
Way Plat 49-19B, Washington County, Minnesota.

c. General Information:

Proposed Name: Hammes Estates
Proposed Lots: 165
Open Space: 20+ acres

- d. Issues since Sketch Plan. At the sketch plan stage, Lake Elmo Planning staff requested a buffer for the Stonegate neighborhood on the north and west as well as a small playground, such as a “tot lot”, for use by residence in the existing neighborhood. The current plan has extensive trails throughout a linear park and encompasses a 20+ acre buffer between the Hammes and Stonegate developments. A small community play area with fishing pier is planned on the northeast corner of the property along the city-owned parcel.
- e. Intent of Project. The intent of the project is to convert a prior mining operation into a progressive, thriving community encompassing a broad range of home with varying values. The project is structured to utilize various lot sizes and designs together with a buffer zone to enhance the open space between the new lots and the adjoining neighborhood.
- f. Proposed Density. All of the lots will meet or exceed the minimum standards of the LDR Low Density Residential zoning district. The density of the project is 2.11 units per gross area and 2.67 units per net area. These are within the density allowed in the

Comprehensive Plan (2.5 per 4 units of net density allowed.) The average lot width for the custom lots are approximately 82 feet and the average lot width for the national builder lots is 75 feet. The minimum lot area within the LDR is 8,000 square feet; therefore, no variance or exceptions are anticipated.

g. Proposed Infrastructure Improvements and Phasing.

The project intends to use sanitary sewer and water which have been installed by the City of Lake Elmo on the east side of the 78-acre parcel.

The owners plan to begin reclamation of the mining site in mid-March, which shall encompass moving and testing of approximately 500,000 yards of fill that have been brought into the site.

The homes will be accessed by streets designed with the new standard urban street section. This will feature concrete curb and gutter sections where appropriate with bituminous surfacing and sections built to the depth and thickness appropriate for the traffic anticipated.

The storm water facilities will be designed in accordance with the requirements of the Valley Branch Watershed District.

The project shall include walking/hiking trails proposed to be bituminous surface 8 feet wide.

The first phase will include 50 custom lots and 70 national builder lots, primarily in the northern one half of the 78-acre site.

The project includes extensive efforts to save and replenish as many trees as possible under the following guidelines:

Tree Replacement requirements (based on current plans – efforts to save more trees will continue to be refined):

- 958 caliper inches of plantings need to be replaced
- Equivalent amount of tree plantings = 383 trees (at minimum size of 2-1/2")

Landscape Requirements:

- 5 trees per disturbed acre = 336 trees (minimum size of 2-1/2")

- 1 street tree every 50' of street frontage = 328 trees (minimum size of 2-1/2")
- TOTAL = 664 trees

Landscape and Tree Replacement Proposed:

- Street Trees: 348 trees (approx. 50' on center)
- Other overstory trees: 51 trees (green & open space areas)
- Future Yard trees (2 per lot, by builder): 330 trees (in either front or back yards)
- Evergreen Trees: 129 trees (mostly buffer trees)
- Ornamental Trees: 40 trees (accent trees)
- TOTAL TREES PROVIDED: 898 Trees Total

The current proposal is 149 trees short of the new requirements, however, the applicant is requesting some flexibility in meeting these guidelines due to the following factors:

The old code required 6 trees per disturbed acre and one boulevard tree for every 50 feet utilizing 1" to 1½" trees. The new code requires minimum size of 2½" trees and does not allow any of the tree replacement planting to satisfy the landscape requirements. The new tree requirements result in the applicant being forced to plant double the amount of trees than the adjoining development.

- h. **Neighboring Property Issues.** The only issue raised in response to the original proposal was the impact on the Stonegate development to the north and west. In order to address these concerns, the plan includes a transitional area along the 20 acre buffer with custom builder lots that are substantially larger than the lots in the southern portion of the development. This transition approach is designed to accentuate the buffering that was envisioned by the City to keep the newer projects from creating negative impacts on the existing homes.
- i. **Conflicts with Nearby Land Uses.** The proposed project does not conflict with any of the nearby land uses. The development to the south is a higher density development than what this development. Significant efforts have been made to minimize disturbance to the adjoining development to the north and west.

A wetland delineation has been completed, and the project will meet all requirements of the Valley Branch Watershed District to minimize disturbances to and protect the existing wetlands and natural areas.

- j. No Excess Burden. The phasing of the project and build-out of the infrastructure should not place any excessive burden on the Lake Elmo facilities and services since Lake Elmo has been mandated and anticipated the increase in residents to the community.
- k. Proposed Lakeshore Access. The proposed project includes a small park in the northeast corner which shall include a small parking area, picnic tables and kayak/canoe racks and a small dock for use by the residents of the development. Access to this park will be from a short driveway immediately entering the development in the northeast corner - (lift station in on the south side of our entrance).
- l. Proposed Parks and/or Open Space. The project includes approximately 20 acres of open space with 13.6 acres for parks, comprised of the outlot areas less any wetland and infiltration/ponding acres below the normal water level. In addition, there is other open space of 2.16 acres of ponding and infiltration areas below the normal water level.
- m. Proposed Development Schedule.
 - Preliminary Plat Submittal – March 7, 2014
 - Valley Branch Submittal – March 7, 2014
 - Planning Commission/City Council Meeting – Early to Mid-April, 2014
 - Submit Final Plat for Initial Phase – May 5, 2014
 - Complete Site Grading for the Initial Phase – June 15, 2014
 - Final Plat Review Planning Commission/City Council Meetings – mid-June, 2014
 - Start utilities/street work – June 15, 2014
 - Street work complete – August 31, 2014

Preliminary Plans

for

Plat, Site, Grading, Erosion Control,
Utilities and Landscape

for

Hammes Property
Lake Elmo, Minnesota

Prepared for:

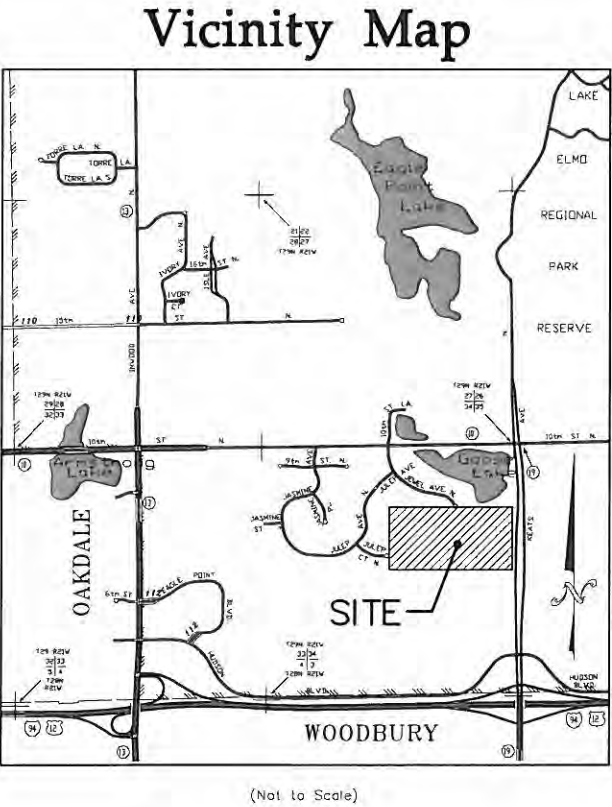
Ryland Homes
7599 Anagram Drive
Eden Prairie, Minnesota 55344
Contact: Mark Sonstegard
Phone: 952-229-6000
Fax: 952-229-6024

Prepared by:

Westwood
Project number: 0002905.00
Contact: Ryan M. Bluhm

Westwood Professional Services, Inc.
7699 Anagram Drive
Eden Prairie, MN 55344
PHONE 952-937-5150
FAX 952-937-5822
TOLL FREE 1-888-937-5150

www.westwoodps.com

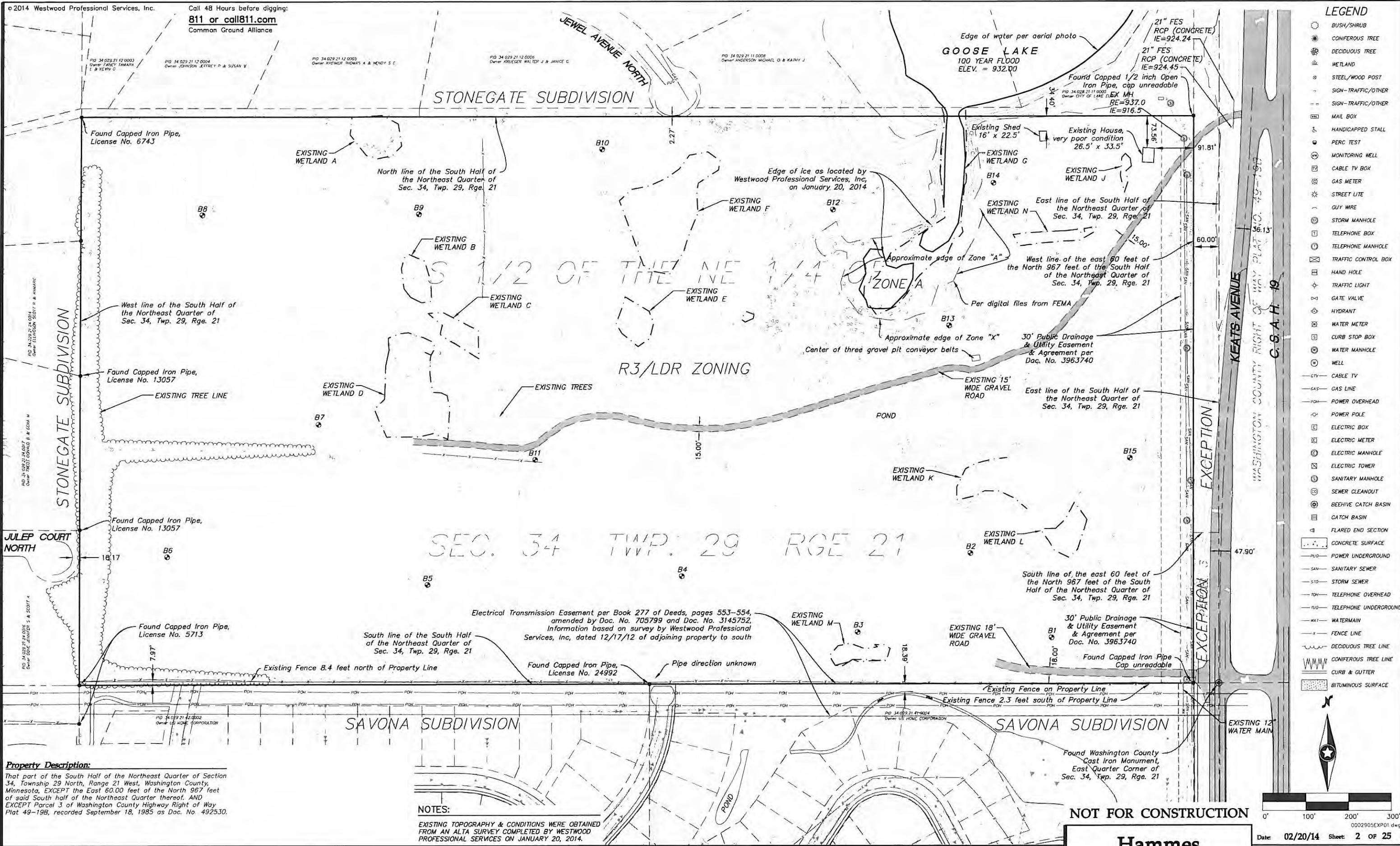


Sheet List Table	
Sheet Number	Sheet Title
1	Cover
2	Existing Conditions
3	Preliminary Plat Plan
4	Preliminary Street & Storm Sewer Plan
5	Preliminary Geometric Site Plan
6	Overall Preliminary Grading, Drainage & Erosion Control Plan
7	Preliminary Grading, Drainage & Erosion Control Plan
8	Preliminary Grading, Drainage & Erosion Control Plan
9	Preliminary Grading, Drainage & Erosion Control Plan
10	Preliminary Grading, Drainage & Erosion Control Plan
11	Preliminary Wetland Buffer Plan
12	Preliminary Wetland Buffer Plan
13	Preliminary Utility Plan
14	Details
15	Details
16	Details
17	Street Profiles
18	Street Profiles
19	Preliminary Landscape Plan
20	Preliminary Landscape Plan
21	Preliminary Tree Preservation Plan
22	Preliminary Tree Preservation Plan
23	Preliminary Tree Preservation Plan
24	Preliminary Tree Preservation Plan
25	PRELIMINARY TREE PRESERVATION PLAN

NO.	DATE	REVISION	SHEETS
1	04/08/14	CITY COMMENTS	ALL
2	05/02/14	CITY COMMENTS	ALL
3	06/06/14	CITY COMMENTS	1-18

NOT FOR CONSTRUCTION

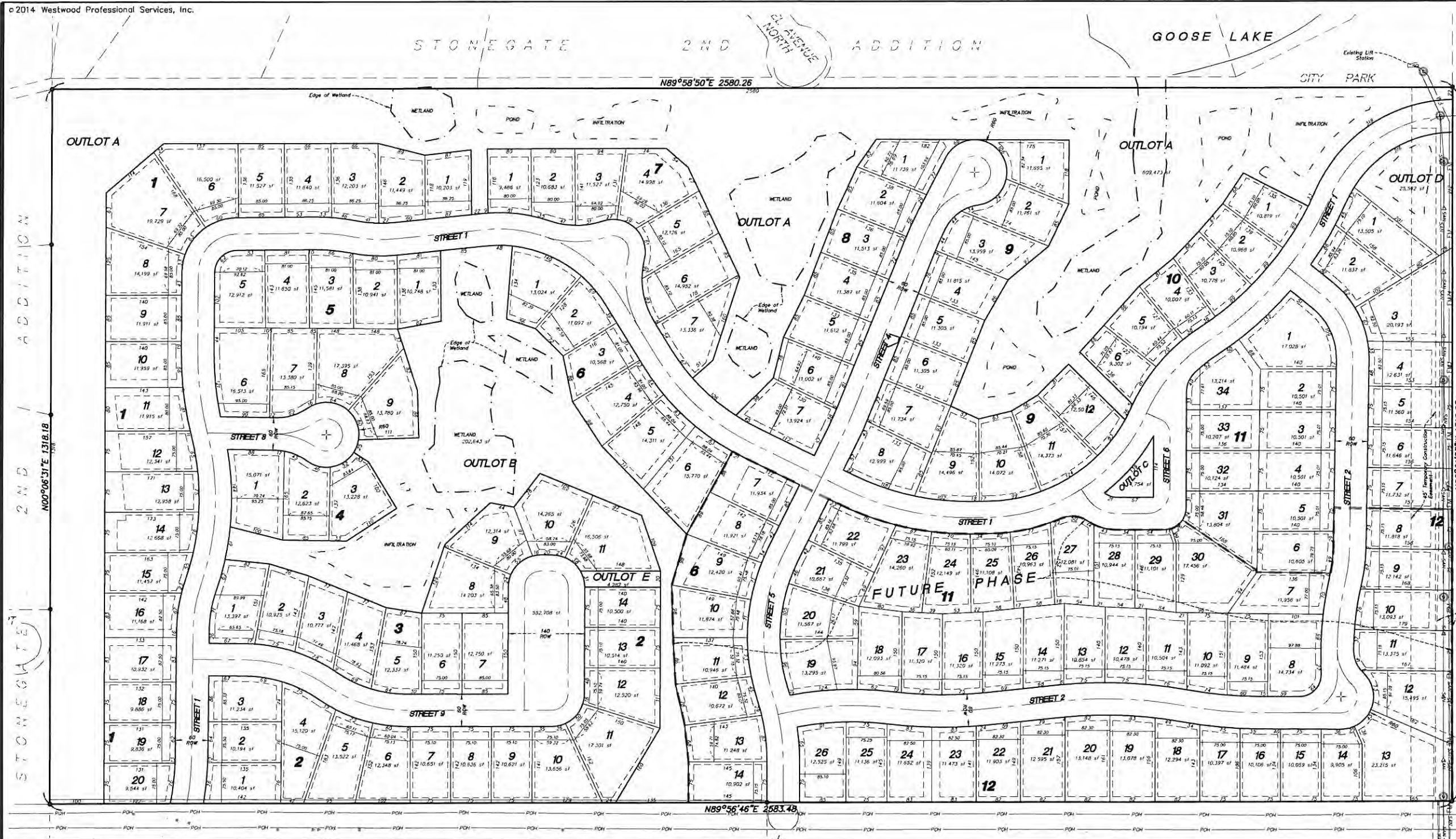
Preliminary Plans
for
Plat, Site, Grading, Erosion Control,
Utilities and Landscape
for
Hammes Property
Lake Elmo, Minnesota



Property Description:
That part of the South Half of the Northeast Quarter of Section 34, Township 29 North, Range 21 West, Washington County, Minnesota, EXCEPT the East 60.00 feet of the North 967 feet of said South half of the Northeast Quarter thereof, AND EXCEPT Parcel 3 of Washington County Highway Right of Way Plat 49-19B, recorded September 18, 1985 as Doc. No. 492530.

NOTES:
EXISTING TOPOGRAPHY & CONDITIONS WERE OBTAINED FROM AN ALTA SURVEY COMPLETED BY WESTWOOD PROFESSIONAL SERVICES ON JANUARY 20, 2014.

NOT FOR CONSTRUCTION

**Development Data:**

Gross Site Area:	78.08 ac
Existing Wetlands:	2.80 ac
Greenway Buffer Open Space:	8.15 ac
100' corridor on West & North, net existing wetlands	
Wetland Buffers:	4.10 ac
Proposed wetland buffers, within Outlot areas only & excluding 100' greenway buffer	
Net Developed Area:	63.03 ac
Park Dedication (buffer w/trail):	4.20 ac
Proposed Single Family Lots:	163 lots
75' wide x 140' deep typical:	110 lots
81' wide x 140' deep typical:	15 lots
85' wide x 140' deep typical:	37 lots
Overall Gross Density:	2.09 un/ac
(163 un/78.08 gross ac.)	
Overall Net Density:	2.59 un/ac
(163 un/63.03 net developable ac.)	

Proposed Single Family Development Standards:

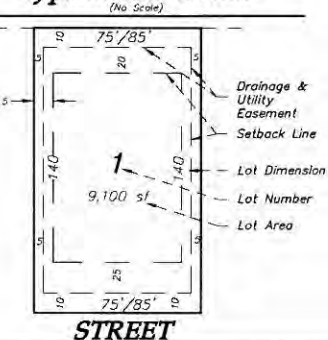
Front Setback:	25'
Side Setback: (See Note 6)	7.5'/7.5' (15' total)
Side Setback: (Corner Lots):	15'
Rear Setback:	20'
Minimum Lot Area:	9,302 sf
Average Lot Area:	12,282± sf

Outlot Table:

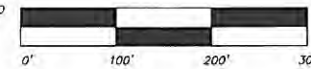
Name	Use	Ownership	Size
Outlot A	Park, Wetland, Ponding	City	13.99 ac.
Outlot B	Wetland, Ponding	City	4.65 ac.
Outlot C	Open Space	City	0.11 ac.
Outlot D	Ponding	City	0.59 ac.
Outlot E	Open Space	City	0.10 ac.

Property Description:

That part of the South Half of the Northeast Quarter of Section 34, Township 29 North, Range 21 West, Washington County, Minnesota, EXCEPT the East 60.00 feet of the North 967 feet of said South half of the Northeast Quarter thereof. AND EXCEPT Parcel 3 of Washington County Highway Right of Way Plat 49-19B, recorded September 18, 1985 as Doc. No. 492530.

Typical Lot Detail**Development Notes**

- ALL LOT DIMENSIONS ARE ROUNDED TO THE NEAREST FOOT.
- ALL AREAS ARE ROUNDED TO THE NEAREST SQUARE FOOT.
- STREET NAMES ARE SUBJECT TO APPROVAL BY THE CITY.
- DRAINAGE AND UTILITY EASEMENTS SHALL BE PROVIDED AS REQUIRED. DRAINAGE AND UTILITY EASEMENTS WILL BE PROVIDED OVER ALL PUBLIC UTILITIES, BLANKET DRAINAGE AND UTILITY EASEMENTS OVER THE ENTIRE OUTLOT TO BE PROVIDED ON FINAL PLAT FOR ALL OF OUTLOT A THROUGH OUTLOT D.
- STREET WIDTHS SHOWN ARE FROM BACK OF CURB TO BACK OF CURB.
- SIDE SETBACKS OF 7.5' & 7.5' (15' TOTAL) REPRESENTS THE AVERAGE OF CODE REQUIRED 5' & 10' (15' TOTAL) SETBACKS.



Date: 02/20/14 Sheet: 3 OF 25



Westwood Professional Services, Inc.
7699 Anagram Drive
Eden Prairie, MN 55344
PHONE 952-937-5150
FAX 952-937-5822
TOLL FREE 1-888-937-5150
www.westwoodps.com

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly licensed LAND SURVEYOR under the laws of the State of Minnesota.

Craig W. Morse, RLS
Date: 06/06/14 License No. 23021

Revisions:

04/08/14 CITY COMMENTS
05/02/14 CITY COMMENTS
06/06/14 CITY COMMENTS

Designed:

Checked:

Drawn:

Record Drawing by/date:

Prepared for:

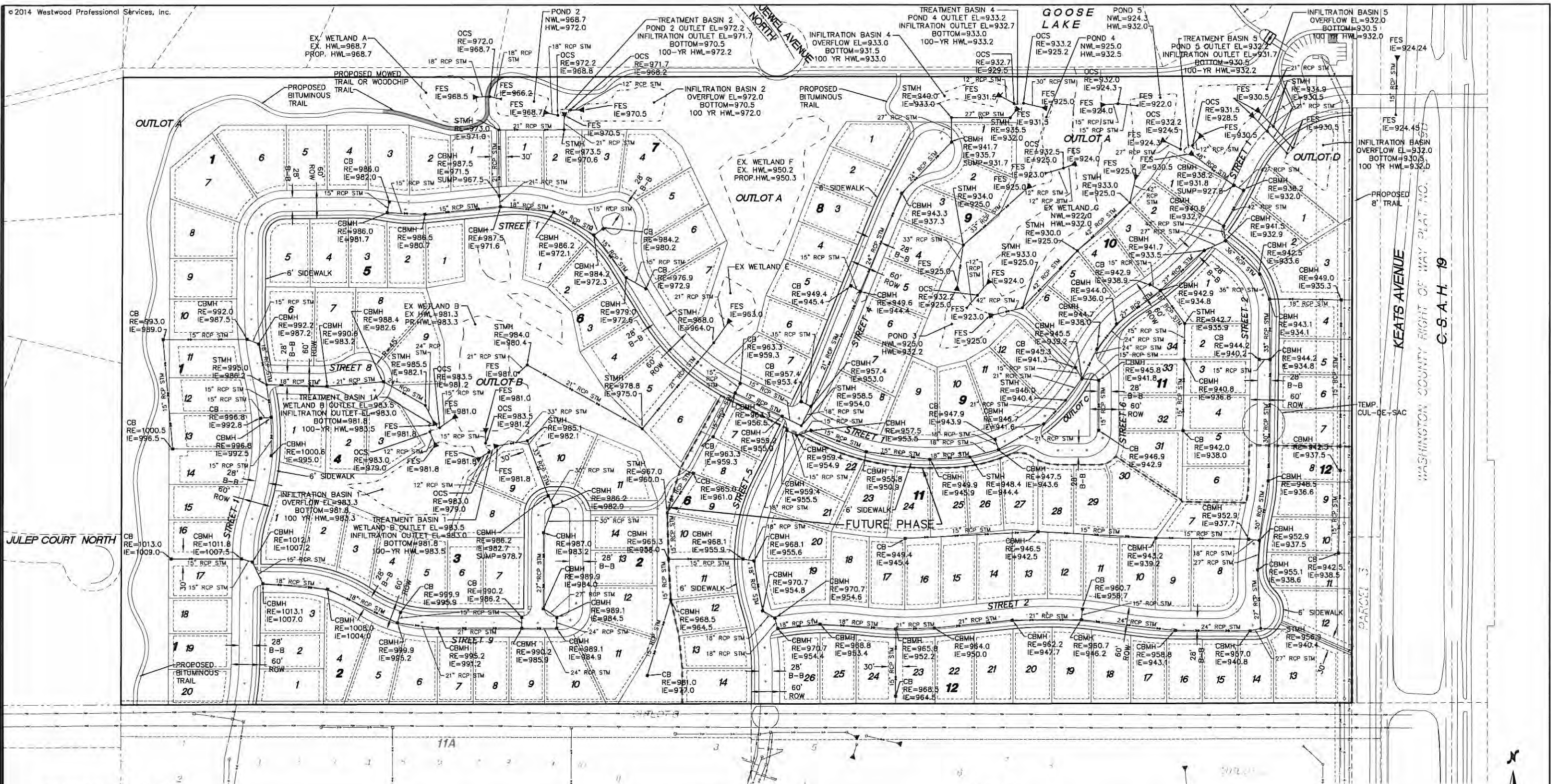
Ryland Homes

7599 Anagram Drive
Eden Prairie, Minnesota 55344

Hammes Property

Lake Elmo, Minnesota

Preliminary Plat Plan



GENERAL UTILITY NOTES:

- THE CONTRACTOR SHALL VERIFY ALL EXISTING CONDITIONS PRIOR TO CONSTRUCTION AND NOTIFY THE OWNER OF ANY DIFFERENCES.
- UNLESS OTHERWISE NOTED, ALL MATERIALS, CONST. TECHNIQUES AND TESTING SHALL CONFORM TO THE 1999 ED. OF THE "STANDARD UTILITIES SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION AND SANITARY SEWER AND STORM SEWER INSTALLATION BY THE CITY ENGINEERING ASSOCIATION OF MINN." AND TO THE "STANDARD SPECIFICATION FOR HIGHWAY CONSTRUCTION" MINN. DEPT. OF TRANS. AUGUST 31, 2005 INCLUDING THE CURRENT ADDENDUM. THE CONTRACTOR SHALL BE REQUIRED TO FOLLOW ALL PROCEDURES AS OUTLINED BY THE LOCAL AGENCY.
- THE CONTRACTOR SHALL RECEIVE THE NECESSARY PERMITS FOR ALL WORK OUTSIDE OF THE PROPERTY LIMITS.
- VERIFY EXISTING INVERT LOC. & ELEV. PRIOR TO BEGINNING CONSTRUCTION.
- THE WATER SERVICE SHALL BE INSTALLED WITH A MIN. OF 7.5 FT. OF COVER. THE CONTRACTOR SHALL VERIFY THE CITY AS-BUILT CONSTRUCTION PLANS TO CHECK LOCATION AND MATERIAL TYPE.
- ALL STORM SEWER PIPE SHALL BE CLASS 5 UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL CONTACT "GOPHER STATE ONE CALL" FOR UTILITY LOCATIONS PRIOR TO UTILITY INSTALLATION.

LEGEND

FUTURE

Sanitary Sewer
Water
Hyd. w/Valve
Storm Sewer

EXISTING

Sanitary Sewer
Water
Hyd. w/Valve
Storm Sewer

PROPOSED

Sanitary Sewer
Water
Hyd. w/Valve
Storm Sewer

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**Hammes
Property**

Lake Elmo, Minnesota

Preliminary Street &
Storm Sewer Plan

Date: 02/20/14 Sheet: 4 OF 25



Westwood Professional Services, Inc.
7599 Anagram Drive
Eden Prairie, MN 55344
PHONE: 952-937-5150
FAX: 952-937-5622
TOLL FREE: 1-888-937-5150
www.westwoodps.com

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly licensed PROFESSIONAL ENGINEER under the laws of the State of Minnesota.

Ryan M. Bluhm
Date: 06/06/14 License No. 41257

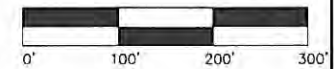
Revisions:

04/08/14 CITY COMMENTS
05/02/14 CITY COMMENTS
06/06/14 CITY COMMENTS

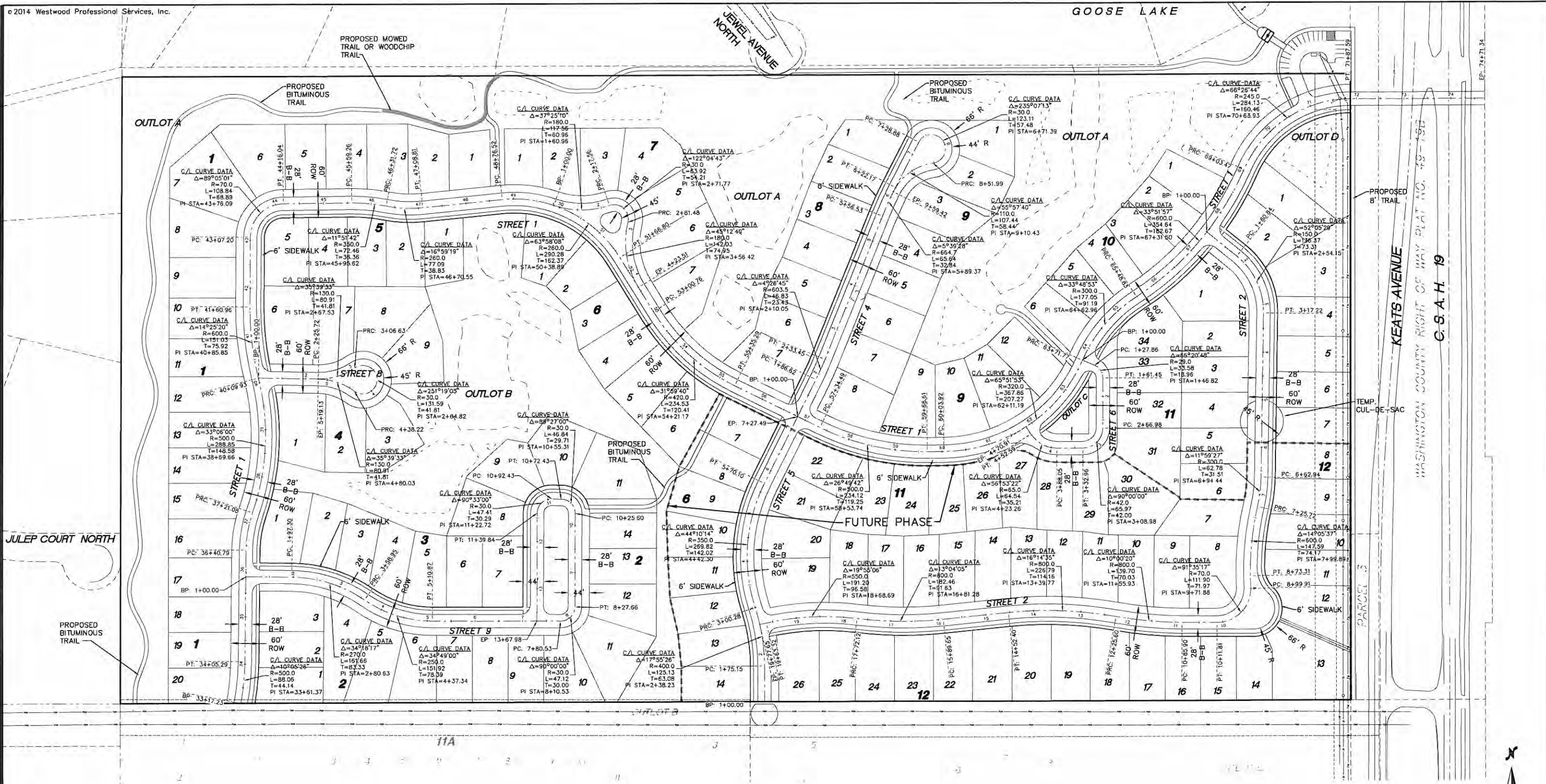
Designed: CLJ
Checked: RMB
Drawn: HW
Record Drawing by/date:

Prepared for:

Ryland Homes
7599 Anagram Drive
Eden Prairie, Minnesota 55344



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JULEP COURT NORTH

GOOSE LAKE

KEATS AVENUE
WASHINGTON COUNTY RIGHT OF WAY PLAT NO. 49-192
C.S.A.H. 19



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**Hammes
Property**
Lake Elmo, Minnesota

Preliminary Geometric
Site Plan
Date: 02/20/14 Sheet: 5 OF 25

Westwood
Westwood Professional Services, Inc.
7999 Anagram Drive
Eden Prairie, MN 55344
PHONE 952-937-6150
FAX 952-937-5622
TOLL FREE 1-888-937-5150
www.westwoodps.com

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly licensed PROFESSIONAL ENGINEER under the laws of the State of Minnesota.
Ryan M. Bluhm
Date: 06/06/14 License No. 41257

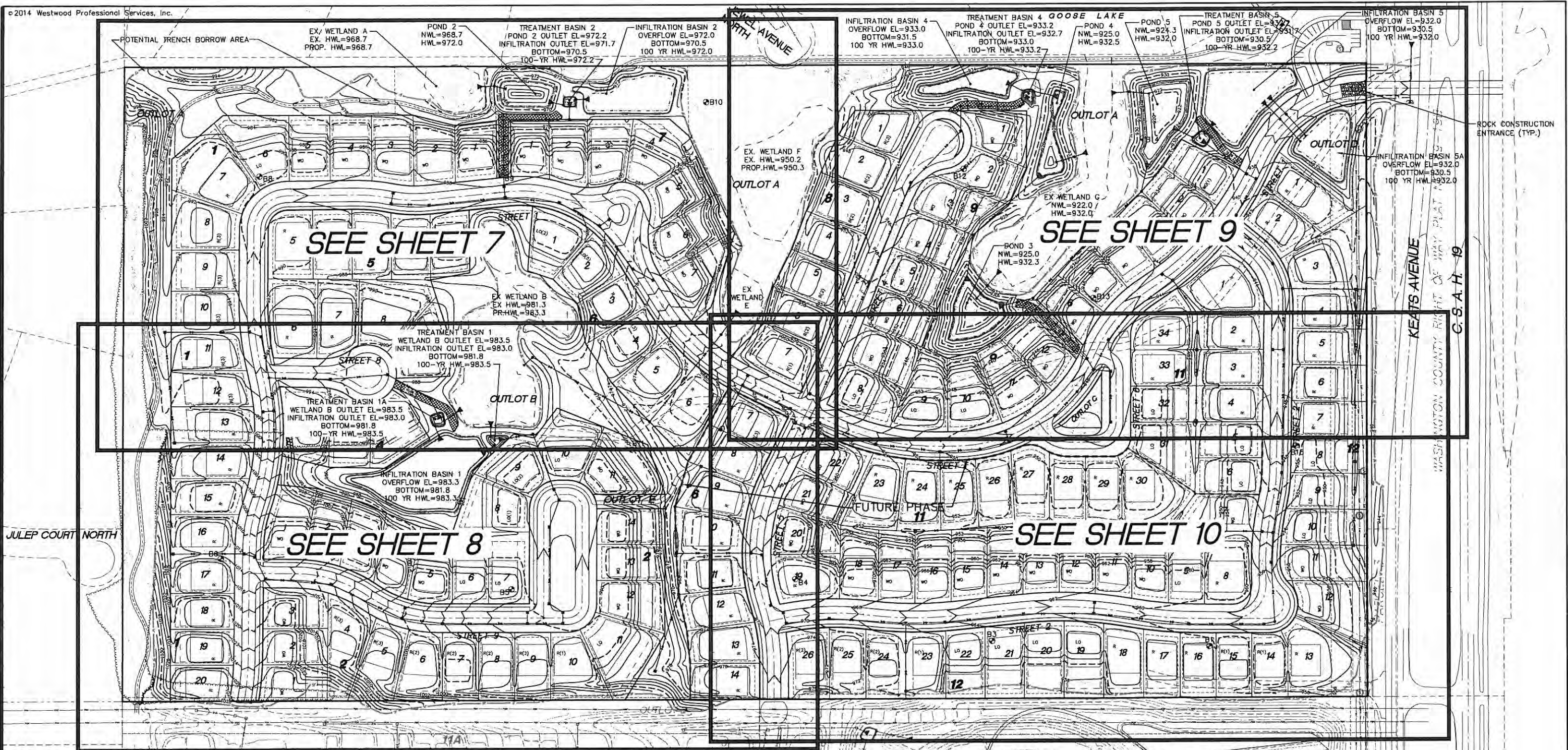
Revisions:
04/08/14 CITY COMMENTS
05/02/14 CITY COMMENTS
06/06/14 CITY COMMENTS

Designed: **CLJ**
Checked: **KMB**
Drawn: **HW**
Record Drawing by/Date:

Prepared for:

Ryland Homes
7599 Anagram Drive
Eden Prairie, Minnesota 55344

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**GENERAL GRADING & DRAINAGE NOTES:**

- ALL CONTOURS AND SPOT ELEVATIONS ARE SHOWN TO FINISHED SURFACE/GUTTER GRADES UNLESS OTHERWISE NOTED.
- REFER TO THE SITE PLAN/RECORD PLAT FOR MOST CURRENT HORIZONTAL SITE DIMENSIONS AND LAYOUT.
- THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION OF EXISTING UTILITIES AND TOPOGRAPHICAL FEATURES WITH THE OWNERS AND FIELD-VERIFY PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES OR VARIATIONS FROM PLAN.
- ALL RCP FOR STORM SEWER SHALL BE CLASS III UNLESS OTHERWISE NOTED.
- ALL CATCH BASINS AND MANHOLES IN PAVED AREAS SHALL BE SUMPED 0.10 FEET. RIM ELEVATIONS ON PLANS REFLECT THE SUMPED ELEVATIONS.
- ALL CONSTRUCTION SHALL CONFORM TO LOCAL RULES.
- POSITIVE DRAINAGE FROM THE SITE MUST BE PROVIDED AT ALL TIMES.

EROSION CONTROL NOTES:

- ALL SILT FENCE AND OTHER EROSION CONTROL FEATURES SHALL BE IN-PLACE PRIOR TO ANY EXCAVATION/CONSTRUCTION AND SHALL BE MAINTAINED UNTIL VIABLE TURF OR GROUND COVER HAS BEEN ESTABLISHED. EXISTING SILT FENCE ON-SITE SHALL BE MAINTAINED AND OR REMOVED AND SHALL BE CONSIDERED INCIDENTAL TO THE GRADING CONTRACT. IT IS OF EXTREME IMPORTANCE TO BE AWARE OF CURRENT FIELD CONDITIONS WITH RESPECT TO EROSION CONTROL. TEMPORARY PONDING, DIKES, HAYBALES, ETC., REQUIRED BY THE CITY SHALL BE INCIDENTAL TO THE GRADING CONTRACT.
- ALL STREETS DISTURBED DURING WORKING HOURS MUST BE CLEANED AT THE END OF EACH WORKING DAY. A ROCK ENTRANCE TO THE SITE MUST BE PROVIDED ACCORDING TO DETAILS TO REDUCE TRACKING OF DIRT ONTO PUBLIC STREETS.

NOTE:

TO PREVENT SOIL COMPACTION, THE PROPOSED INFILTRATION AREAS SHALL BE STAKED OFF AND MARKED DURING CONSTRUCTION TO PREVENT HEAVY EQUIPMENT AND TRAFFIC FROM TRAVELING OVER IT. IF INFILTRATION FACILITIES ARE IN PLACE DURING CONSTRUCTION ACTIVITIES, SEDIMENT AND RUNOFF SHALL BE KEPT AWAY FROM THE FACILITY, USING PRACTICES SUCH AS DIVERSION BERMS AND VEGETATION AROUND THE FACILITY'S PERIMETER. INFILTRATION FACILITIES SHALL NOT BE EXCAVATED TO FINAL GRADE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN CONSTRUCTED AND FULLY STABILIZED. THE FINAL PHASE OF EXCAVATION SHALL REMOVE ALL ACCUMULATED SEDIMENT AND BE DONE BY LIGHT TRACKED EQUIPMENT TO AVOID COMPACTION OF THE BASIN FLOOR. TO PROVIDE A WELL-AERATED, HIGHLY POROUS SURFACE, THE SOILS OF THE BASIN FLOOR SHALL BE LOOSENEED TO A DEPTH OF AT LEAST 24 INCHES TO A MAXIMUM COMPACTION OF 85% STANDARD PROCTOR DENSITY PRIOR TO PLANTING.

LEGEND:

- DENOTES SOIL BORING
- DENOTES SILT FENCE
- DENOTES HEAVY DUTY SILT FENCE
- DENOTES EXISTING CONTOURS
- DENOTES PROPOSED CONTOURS
- DENOTES EXISTING STORM SEWER
- DENOTES PROPOSED STORM SEWER
- DENOTES EXISTING TREE LINE
- DENOTES APPROXIMATE TREE REMOVAL LIMITS

- DENOTES EXISTING SPOT ELEVATION
- DENOTES PROPOSED SPOT ELEVATION
- DENOTES BIO-ROLL EROSION CHECKS
- DENOTES EMERGENCY OVERFLOW ELEVATION
- DENOTES WETLANDS



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NOT FOR CONSTRUCTION

**Hammes
Property**

Lake Elmo, Minnesota

Overall Preliminary
Grading, Drainage &
Erosion Control Plan

Date: 02/20/14 Sheet: 6 OF 25



Westwood Professional Services, Inc.
 7999 Anagram Drive
 Eden Prairie, MN 55344
 PHONE 952-937-5150
 FAX 952-937-5622
 TOLL FREE 1-866-937-5150
 www.westwoodps.com

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly licensed PROFESSIONAL ENGINEER under the laws of the State of Minnesota.

Ryan M. Bluhm
 Date: 06/06/14 License No. 41257

Revisions:

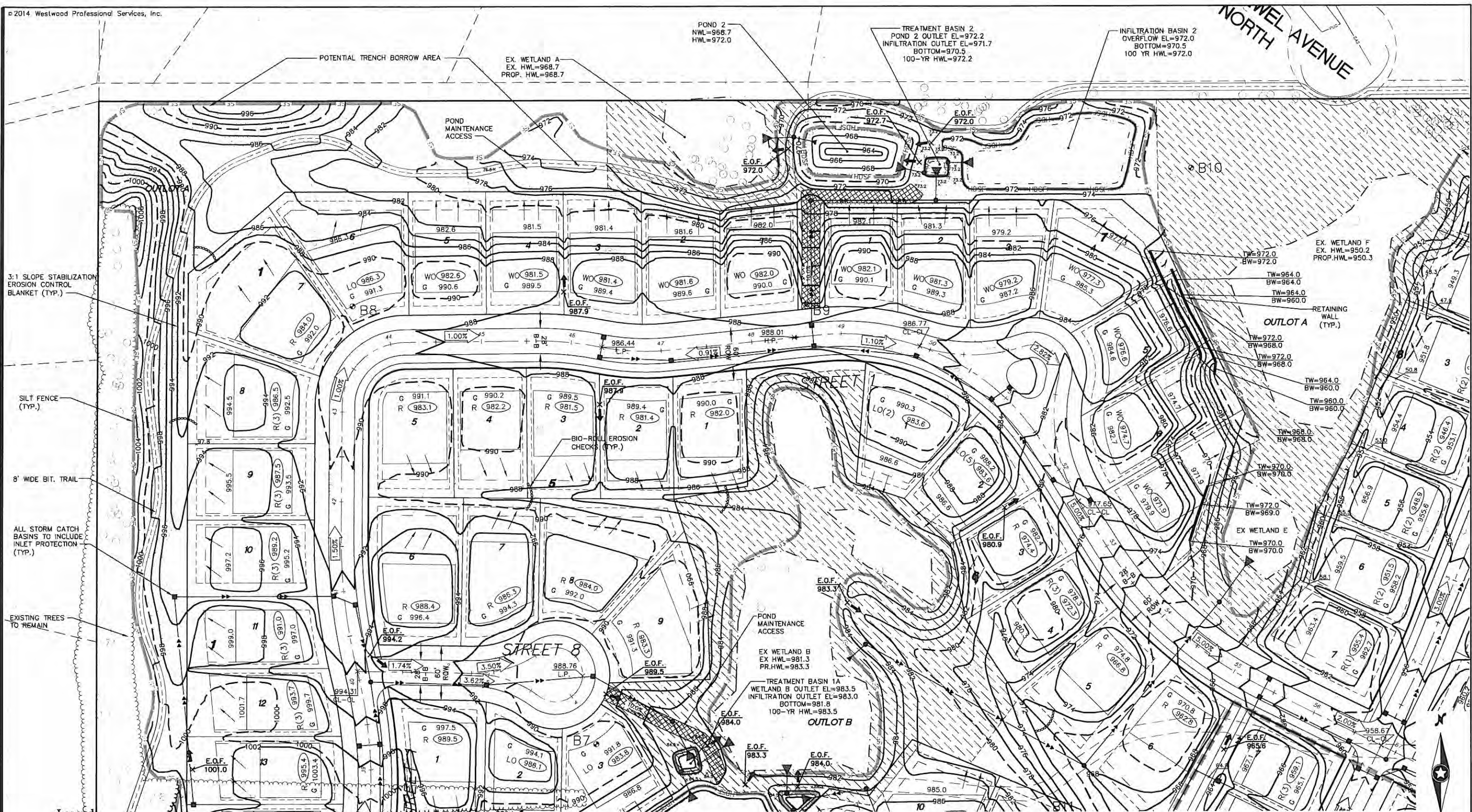
04/08/14 CITY COMMENTS
 05/02/14 CITY COMMENTS
 06/06/14 CITY COMMENTS

Designed: CLJ
 Checked: RMB
 Drawn: HW
 Record Drawing, by/date:

Prepared for:

Ryland Homes
 7599 Anagram Drive
 Eden Prairie, Minnesota 55344

0002905GDP01.dwg



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 Common Ground Alliance

NOT FOR CONSTRUCTION

**Hammes
Property**

Lake Elmo, Minnesota

Date: 02/20/14 Sheet: 7 OF 25

**Preliminary Grading,
Drainage & Erosion
Control Plan**



Westwood Professional Services, Inc.
 7999 Anagram Drive
 Eden Prairie, MN 55344
 PHONE: 952-937-5150
 FAX: 952-937-5622
 TOLL FREE: 1-888-937-5150
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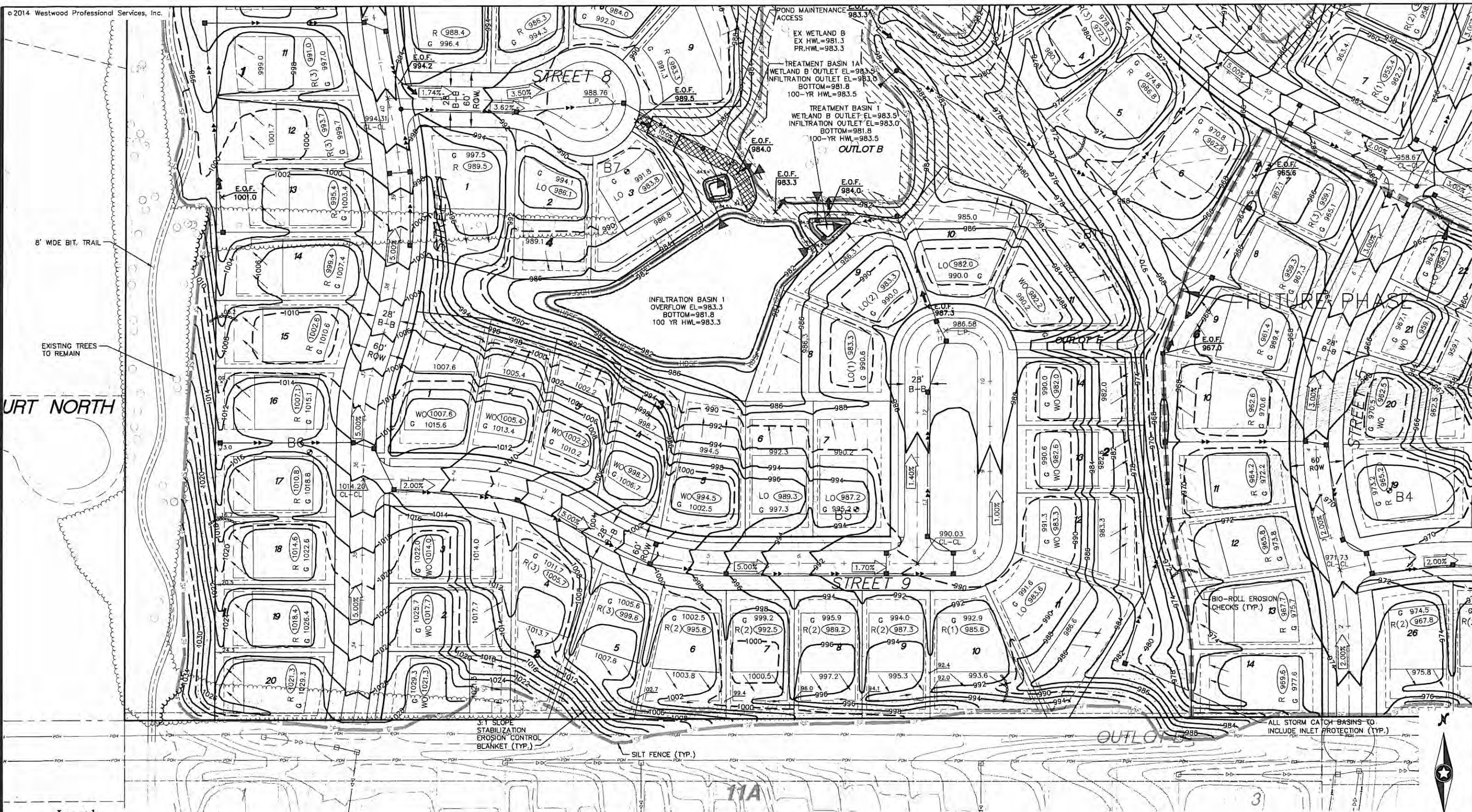
Revisions:

04/08/14 CITY COMMENTS
 05/02/14 CITY COMMENTS
 05/06/14 CITY COMMENTS

Designed: CLJ
 Checked: KMB
 Drawn: HW
 Record Drawing by/date:

Prepared for:

Ryland Homes
 7599 Anagram Drive
 Eden Prairie, Minnesota 55344



Legend:

- | | | | | | |
|--|-----------------------------------|--|---------------------------------|--|---|
| | DENOTES SOIL BORING | | DENOTES FUTURE STORM SEWER | | DENOTES BIO-ROLL EROSION CHECKS |
| | DENOTES SILT FENCE | | DENOTES EXISTING STORM SEWER | | DENOTES EMERGENCY OVERFLOW ELEVATION |
| | DENOTES HEAVY DUTY SILT FENCE | | DENOTES PROPOSED STORM SEWER | | 3:1 SLOPE STABILIZATION EROSION CONTROL BLANKET |
| | DENOTES EXISTING CONTOURS | | DENOTES EXISTING TREE LINE | | DENOTES WETLANDS |
| | DENOTES PROPOSED CONTOURS | | DENOTES EXISTING SPOT ELEVATION | | |
| | DENOTES PROPOSED INTERIM CONTOURS | | DENOTES PROPOSED SPOT ELEVATION | | |

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NOT FOR CONSTRUCTION

Hammes Property
7599 Anagram Drive
Lake Elmo, Minnesota

Date: 02/20/14 Sheet: 8 OF 25

Preliminary Grading,
Drainage & Erosion
Control Plan

Westwood
Westwood Professional Services, Inc.
7699 Anagram Drive
Eden Prairie, MN 55344
PHONE: 952-937-5150
FAX: 952-937-5822
TOLL FREE: 1-888-937-5150
www.westwoodps.com

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly licensed PROFESSIONAL ENGINEER under the laws of the State of Minnesota.
Ryan M. Buhm
Date: 06/06/14 License No. 41257

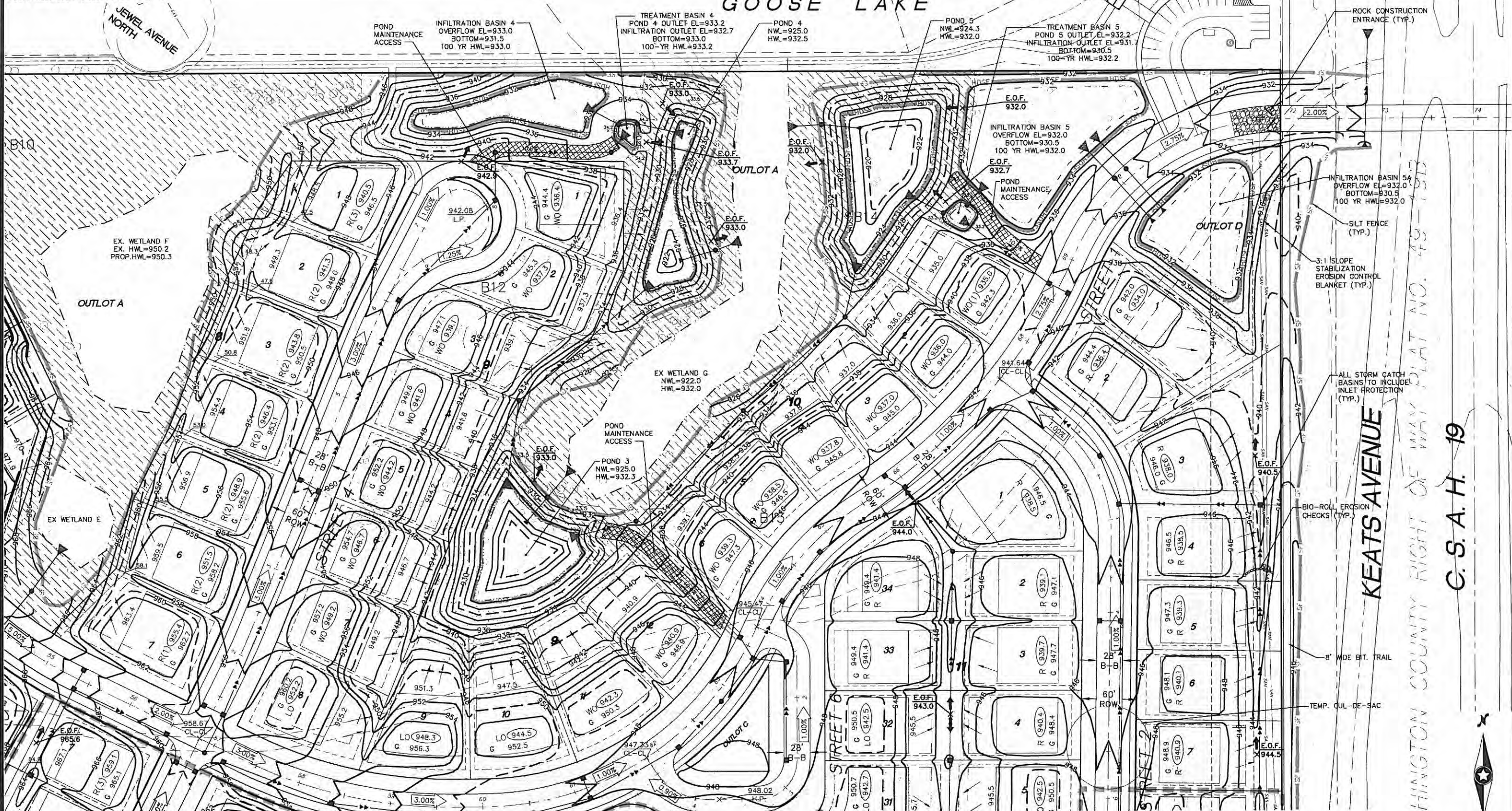
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04/08/14 CITY COMMENTS	
05/02/14 CITY COMMENTS	
06/06/14 CITY COMMENTS	

Designed:	CLJ
Checked:	KLM
Drawn:	HW
Record Drawing by/date:	

Prepared for:	
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Ryland Homes
7599 Anagram Drive
Eden Prairie, Minnesota 55344

GOOSE LAKE



Legend:

- | | | |
|---|--|---|
| <ul style="list-style-type: none"> ⊕ DENOTES SOIL BORING --- DENOTES SILT FENCE --- DENOTES HEAVY DUTY SILT FENCE --- DENOTES EXISTING CONTOURS --- DENOTES PROPOSED CONTOURS --- DENOTES PROPOSED INTERIM CONTOURS | <ul style="list-style-type: none"> --- DENOTES FUTURE STORM SEWER --- DENOTES EXISTING STORM SEWER --- DENOTES PROPOSED STORM SEWER --- DENOTES EXISTING TREE LINE --- DENOTES EXISTING SPOT ELEVATION --- DENOTES PROPOSED SPOT ELEVATION | <ul style="list-style-type: none"> E.O.F. DENOTES BIO-ROLL EROSION CHECKS --- DENOTES EMERGENCY OVERFLOW ELEVATION --- 3:1 SLOPE STABILIZATION EROSION CONTROL BLANKET --- DENOTES WETLANDS |
|---|--|---|

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**Hammes
Property**

Lake Elmo, Minnesota

Date: 02/20/14 Sheet: 9 OF 25

**Preliminary Grading,
Drainage & Erosion
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Revisions:

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 06/06/14 CITY COMMENTS

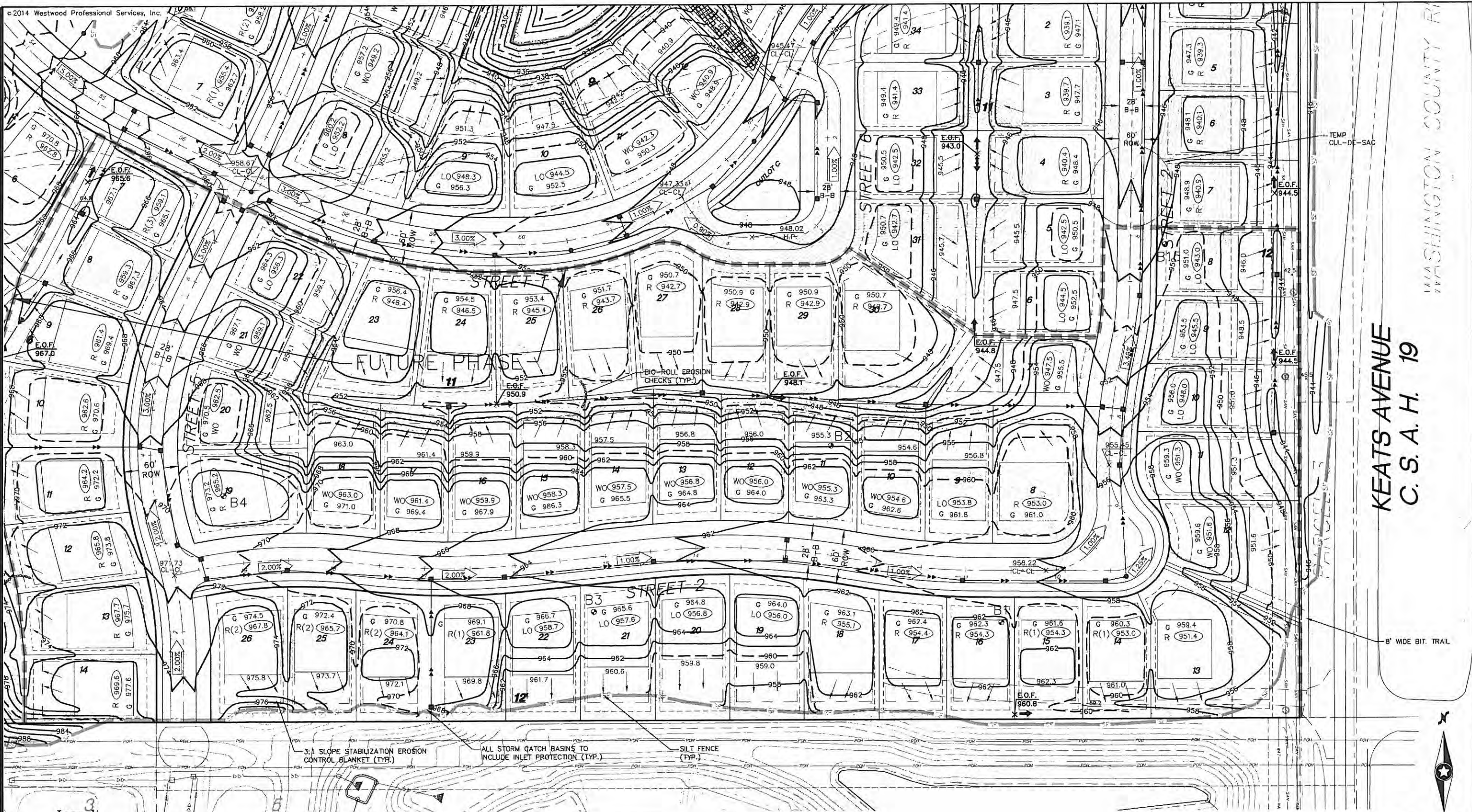
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Prepared for:

Ryland Homes
 7599 Anagram Drive
 Eden Prairie, Minnesota 55344



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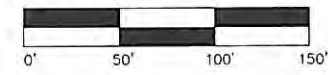


KEATS AVENUE
C.S.A.H. 19

WASHINGTON COUNTY ROAD

Legend:

- | | | |
|--|--|---|
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|--|--|---|



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**Hammes
Property**

Lake Elmo, Minnesota

Date: 02/20/14 Sheet: 10 OF 25

Preliminary Grading,
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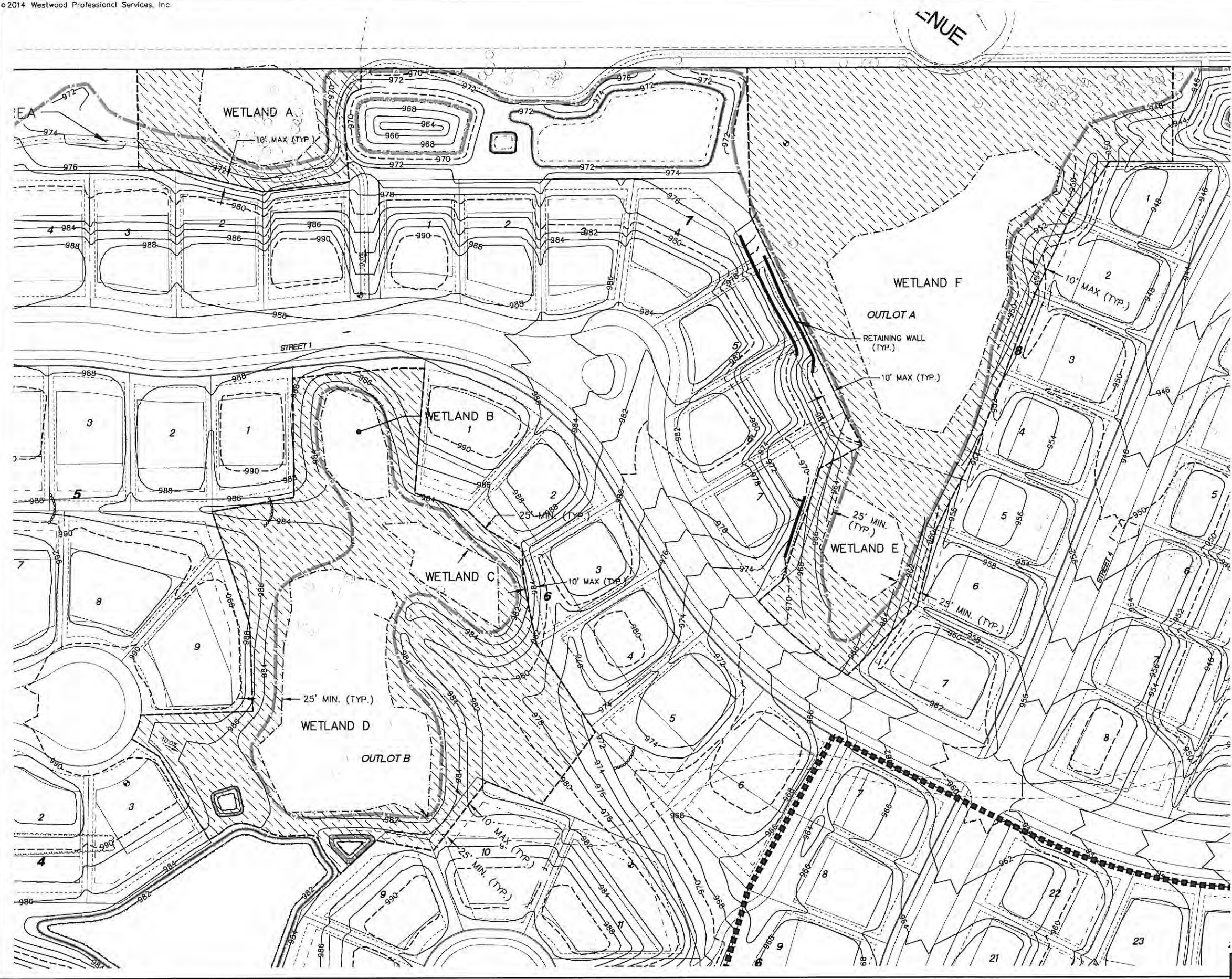
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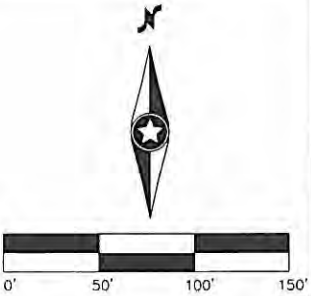
Ryland Homes
7599 Anagram Drive
Eden Prairie, Minnesota 55344



LEGEND:

- DENOTES SOIL BORING
- DENOTES SILT FENCE
- DENOTES HEAVY DUTY SILT FENCE
- DENOTES EXISTING CONTOURS
- DENOTES PROPOSED CONTOURS
- DENOTES EXISTING STORM SEWER
- DENOTES PROPOSED STORM SEWER
- DENOTES EXISTING TREE LINE
- DENOTES APPROXIMATE TREE REMOVAL LIMITS
- DENOTES EXISTING SPOT ELEVATION
- DENOTES PROPOSED SPOT ELEVATION
- DENOTES BIO-ROLL EROSION CHECKS
- DENOTES EMERGENCY OVERFLOW ELEVATION
- PROPOSED WETLAND BUFFER AREA

Wetland	Existing Buffer	Proposed Buffer	Average Buffer Width
A	17,320 SF	14,371 SF	51.0 LF
B, C & D	78,945 SF	82,261 SF	58.0 LF
E	23,952 SF	20,152 SF	66.4 LF
F	72,409 SF	76,580 SF	95.2 LF



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**Hammes
Property**

Lake Elmo, Minnesota

Preliminary Wetland
Buffer Plan

Date: 02/20/14 Sheet: 11 OF 25

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06/06/14 CITY COMMENTS	

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Drawn: **RW**
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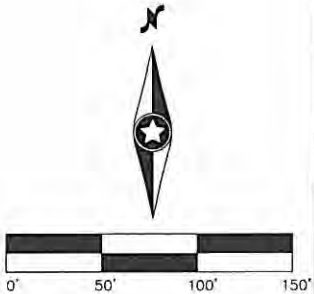
Ryland Homes
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Eden Prairie, Minnesota 55344



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- DENOTES BIO-ROLL EROSION CHECKS
- DENOTES EMERGENCY OVERFLOW ELEVATION
- PROPOSED WETLAND BUFFER AREA

Wetland	Existing Buffer	Proposed Buffer	Average Buffer Width
G	92,054 SF	95,313 SF	85.3 LF



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**Hammes
Property**

Lake Elmo, Minnesota

Preliminary Wetland
Buffer Plan

Date: 02/20/14 Sheet: 12 OF 25

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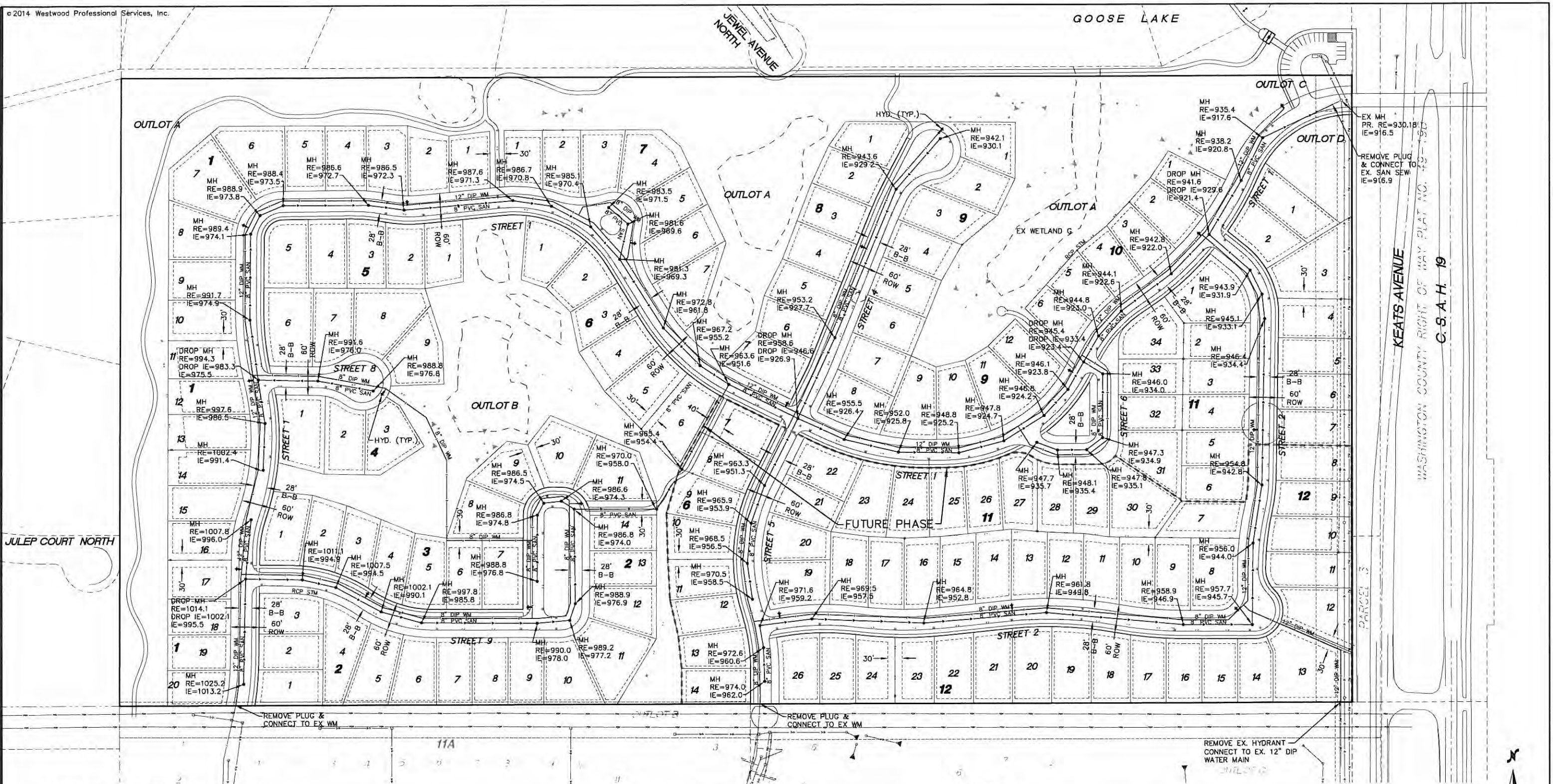
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Record Drawing by/date:

Prepared for:

Ryland Homes
7599 Anagram Drive
Eden Prairie, Minnesota 55344

**GENERAL UTILITY NOTES:**

- THE CONTRACTOR SHALL VERIFY ALL EXISTING CONDITIONS PRIOR TO CONSTRUCTION AND NOTIFY THE OWNER OF ANY DIFFERENCES.
- UNLESS OTHERWISE NOTED, ALL MATERIALS, CONST. TECHNIQUES AND TESTING SHALL CONFORM TO THE 1999 ED. OF THE "STANDARD UTILITIES SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION AND SANITARY SEWER AND STORM SEWER INSTALLATION BY THE CITY ENGINEERING ASSOCIATION OF MINN." AND TO THE "STANDARD SPECIFICATION FOR HIGHWAY CONSTRUCTION" MINN. DEPT. OF TRANS., AUGUST 31, 2005 INCLUDING THE CURRENT ADDENDUM. THE CONTRACTOR SHALL BE REQUIRED TO FOLLOW ALL PROCEDURES AS OUTLINED BY THE LOCAL AGENCY.
- THE CONTRACTOR SHALL RECEIVE THE NECESSARY PERMITS FOR ALL WORK OUTSIDE OF THE PROPERTY LIMITS.
- VERIFY EXISTING INVERT LOC. & ELEV. PRIOR TO BEGINNING CONSTRUCTION.
- THE WATER SERVICE SHALL BE INSTALLED WITH A MIN. OF 7.5 FT. OF COVER. THE CONTRACTOR SHALL VERIFY THE CITY AS-BUILT CONSTRUCTION PLANS TO CHECK LOCATION AND MATERIAL TYPE.
- THE CONTRACTOR SHALL CONTACT "GOPHER STATE ONE CALL" FOR UTILITY LOCATIONS PRIOR TO UTILITY INSTALLATION.

LEGEND

FUTURE	EXISTING	PROPOSED
Sanitary Sewer	Sanitary Sewer	Sanitary Sewer
Water	Water	Water
Hyd. w/Valve	Hyd. w/Valve	Hyd. w/Valve
Storm Sewer	Storm Sewer	Storm Sewer

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**Hammes
Property**

Lake Elmo, Minnesota

Date: **02/20/14** Sheet: **13 OF 25**
**Preliminary Utility
Plan**



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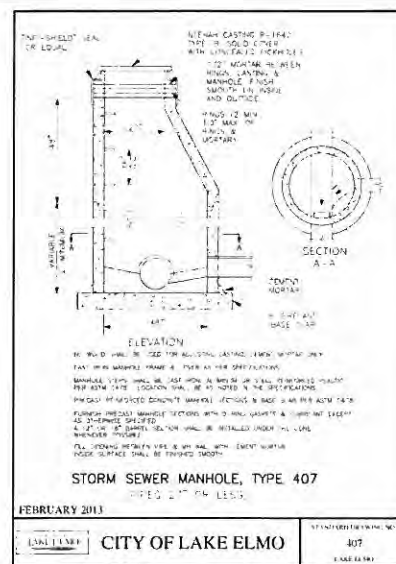
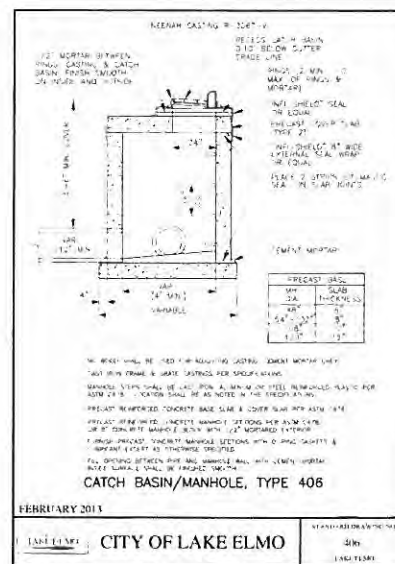
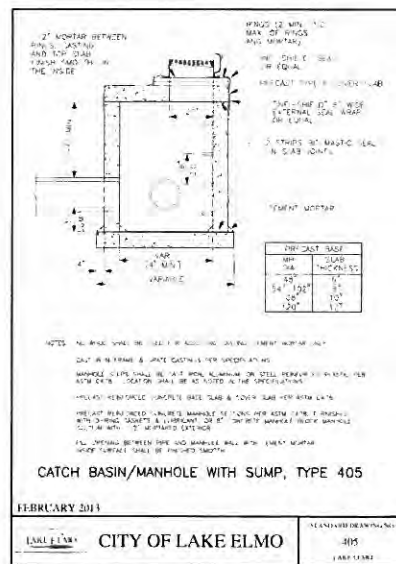
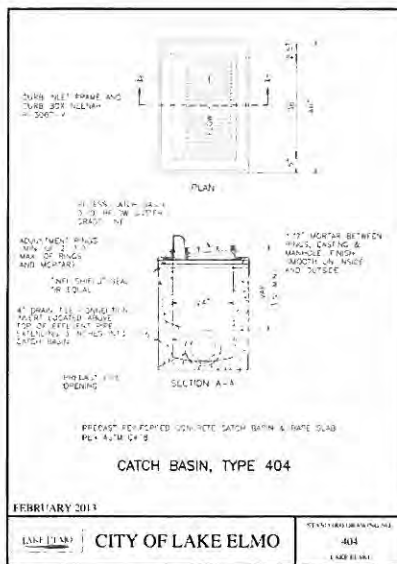
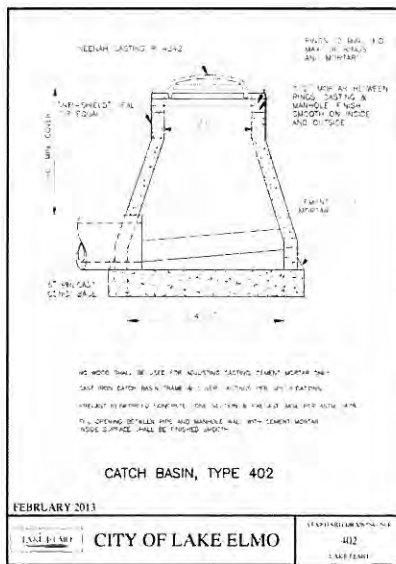
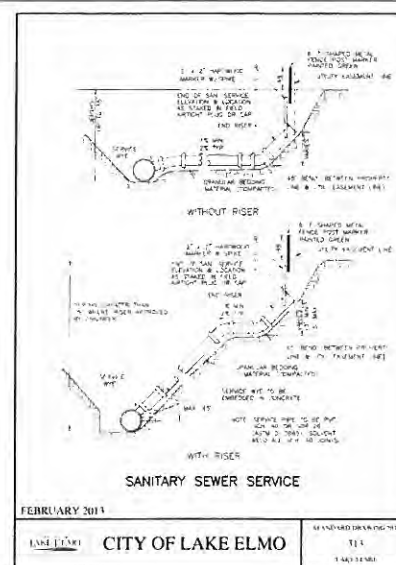
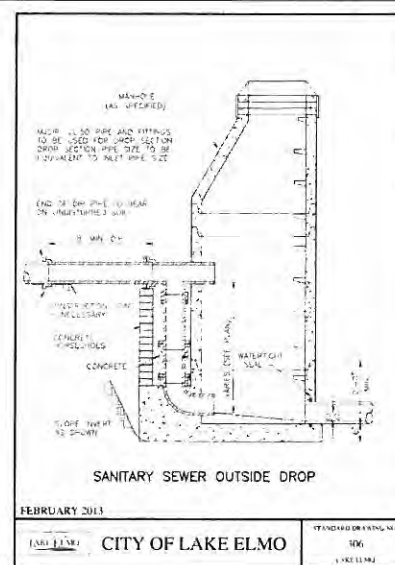
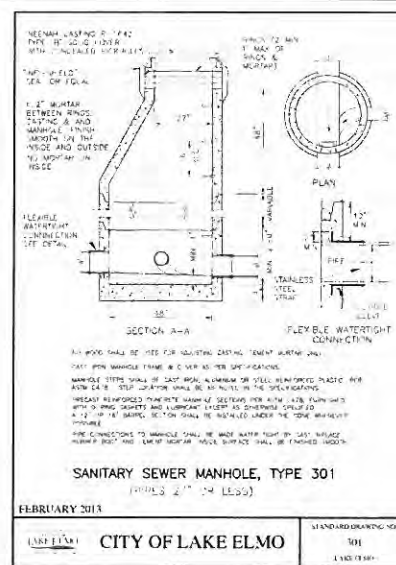
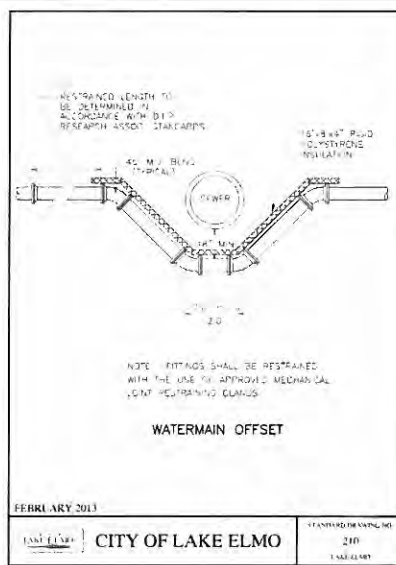
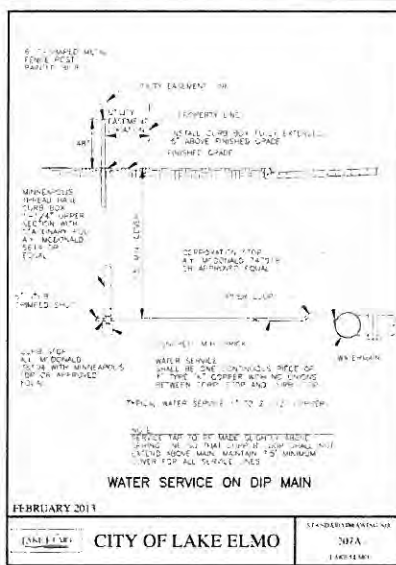
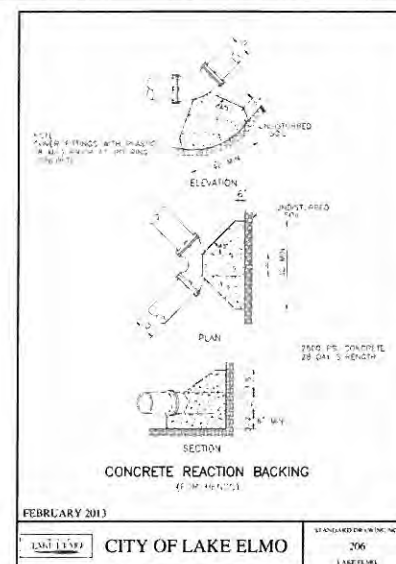
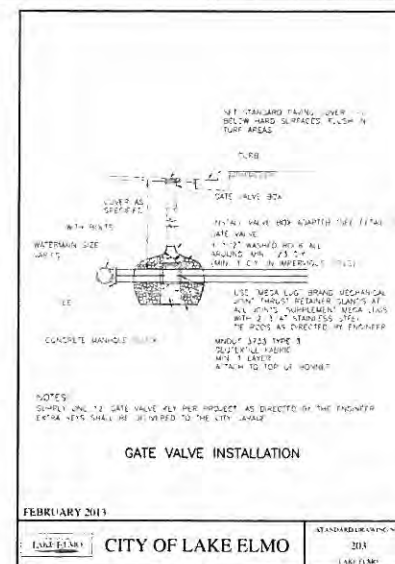
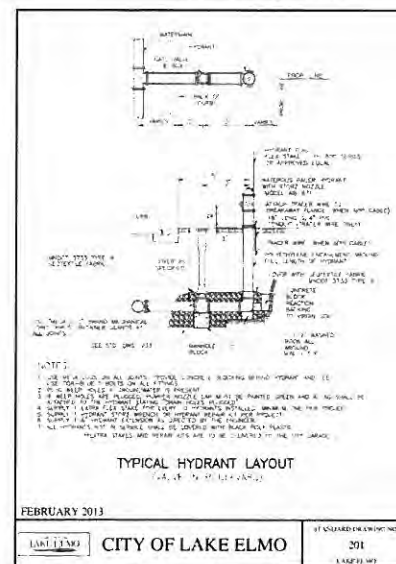
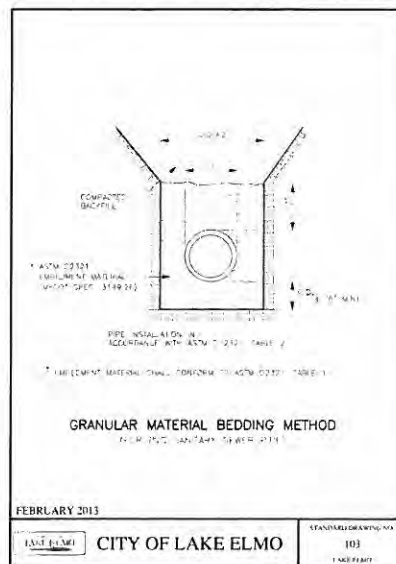
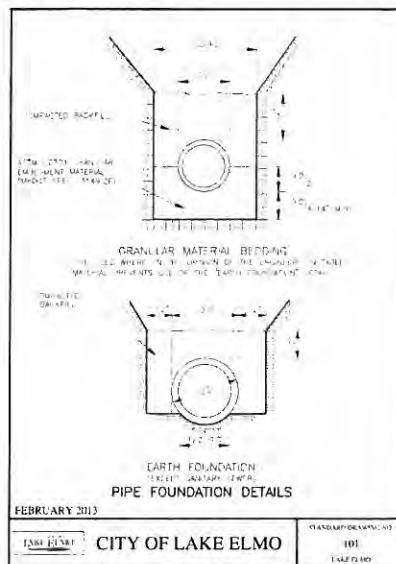
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Ryland Homes
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0' 100' 200' 300'

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Date: 06/06/14 License No. 41257

Revisions:

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06/06/14 CITY COMMENTS

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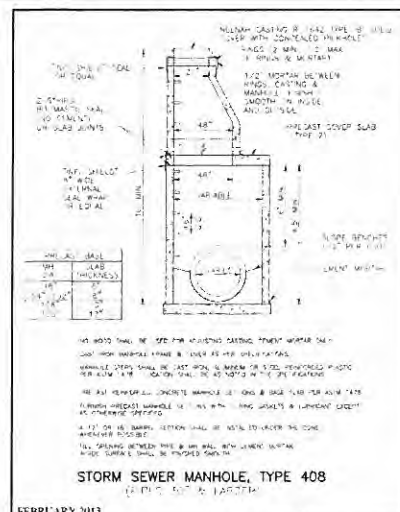
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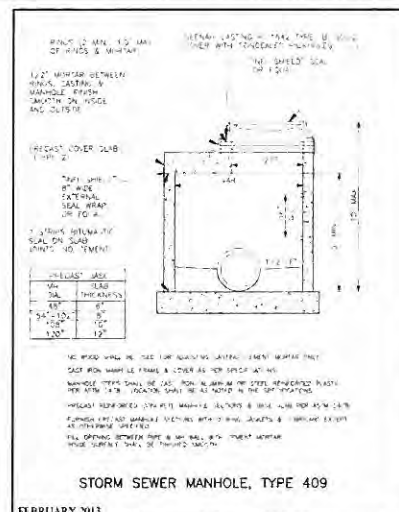
Hammes Property
Lake Elmo, Minnesota

Date: 02/20/14 Sheet: 14 OF 25

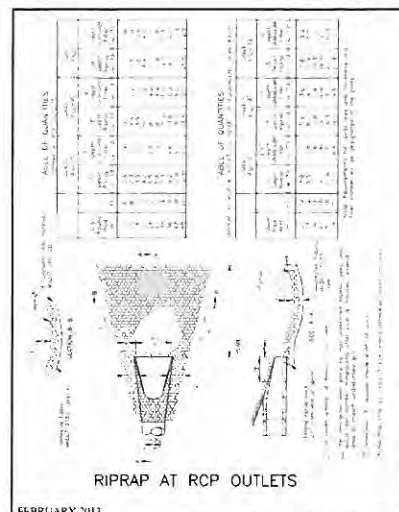
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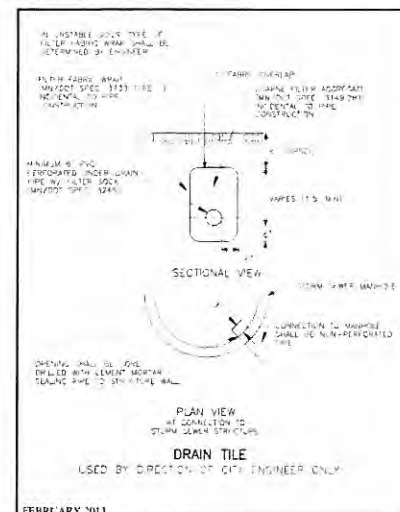
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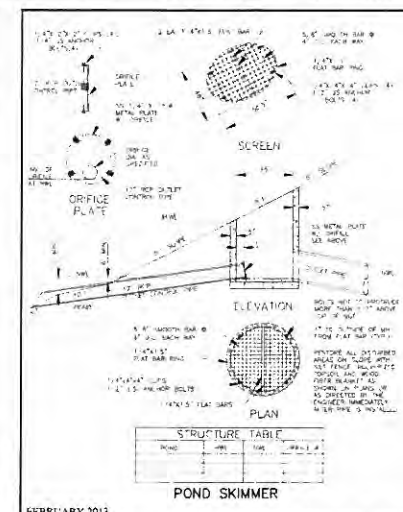
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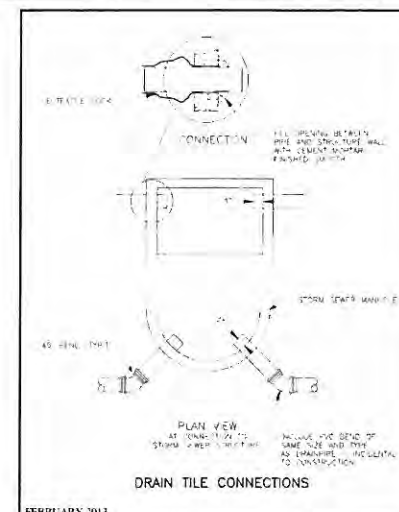
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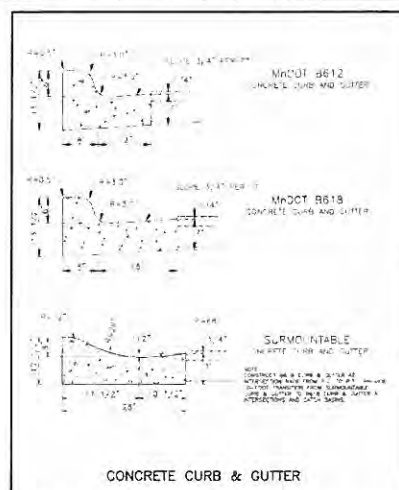
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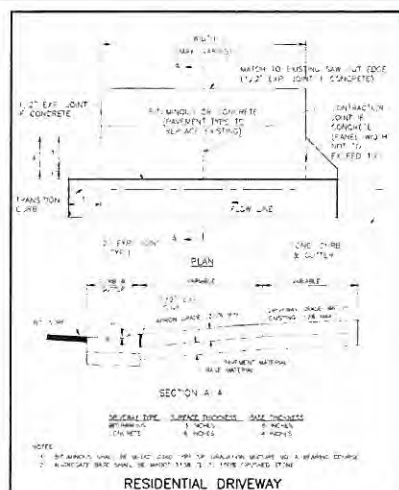
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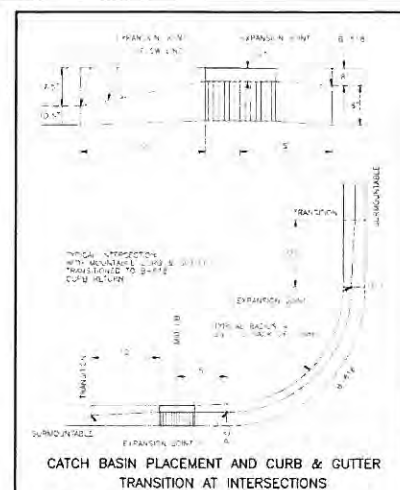
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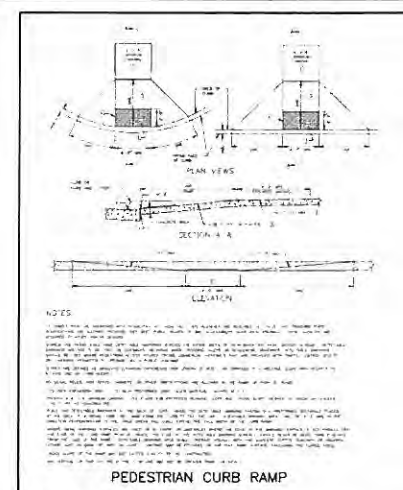
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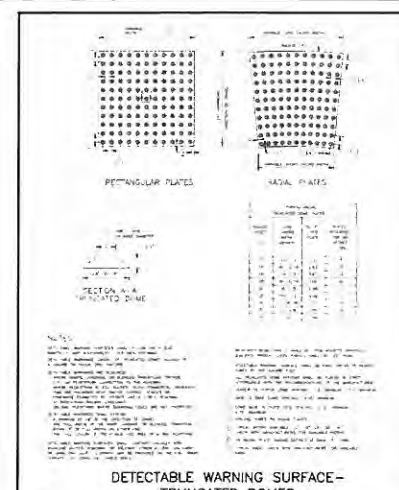
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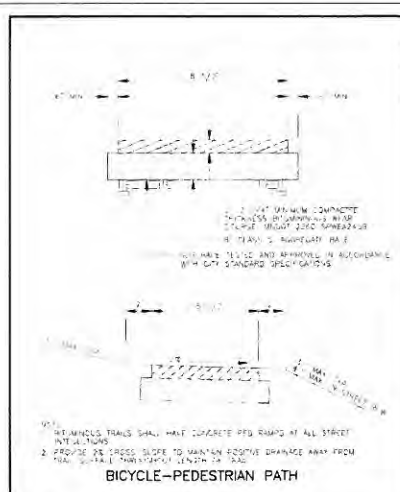
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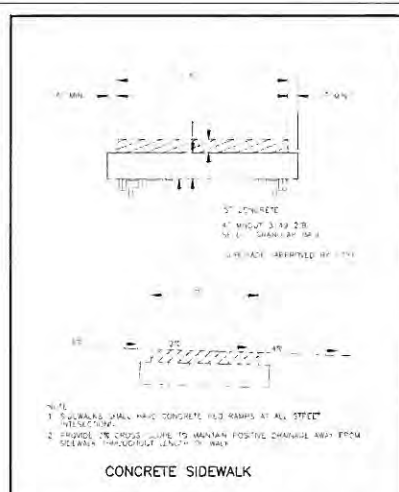
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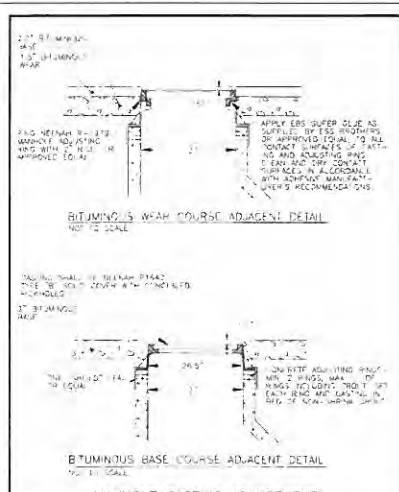
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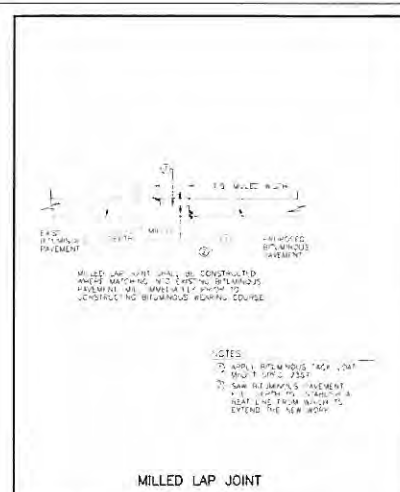
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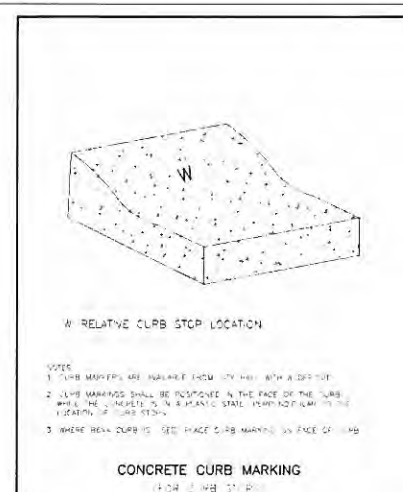
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FEBRUARY 2013
CITY OF LAKE ELMO
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FEBRUARY 2013
CITY OF LAKE ELMO
STANDARD DRAWING NO. 512
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FEBRUARY 2013
CITY OF LAKE ELMO
STANDARD DRAWING NO. 513
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**Hammes
Property**

Lake Elmo, Minnesota

Date 02/20/14 Sheet 15 OF 25

Details



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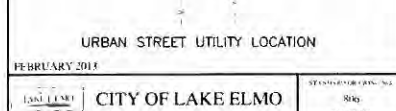
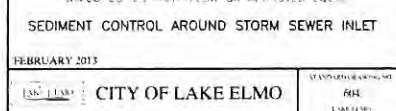
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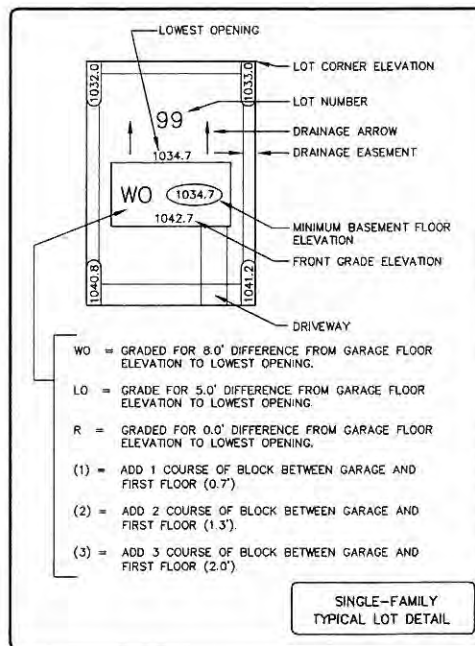
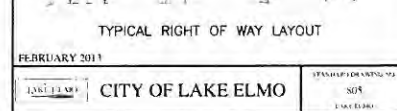
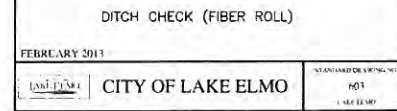
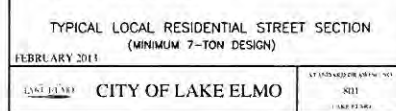
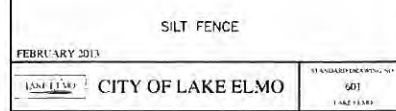
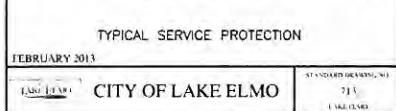
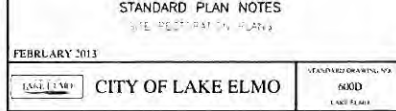
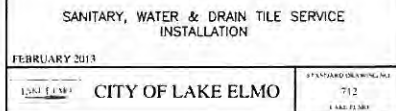
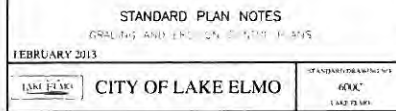
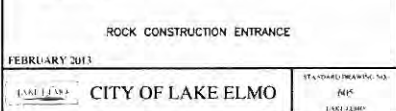
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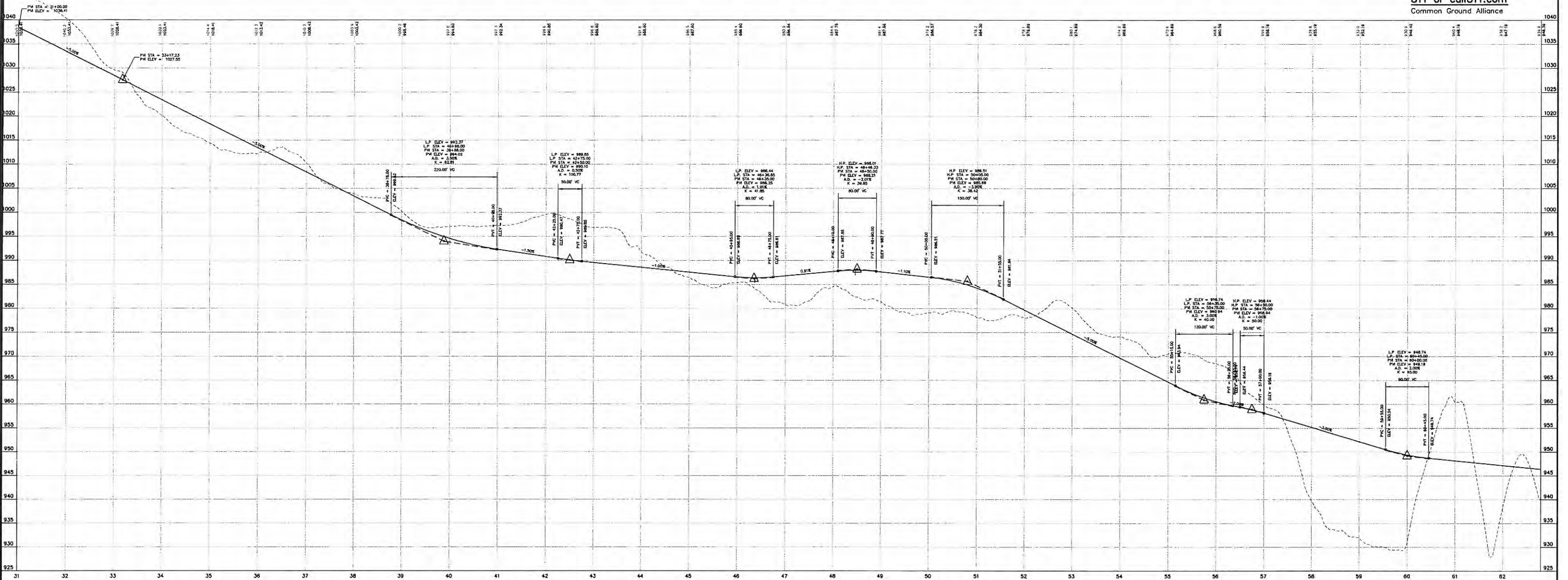
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Eden Prairie, Minnesota 55344

STANDARD PLAN NOTES		
GRADING AND EROSION CONTROL PLANS		
FEBRUARY 2013		
	CITY OF LAKE ELMO	ST. LOUIS DESIGN INC. 6004 LAKE ELMO, MO

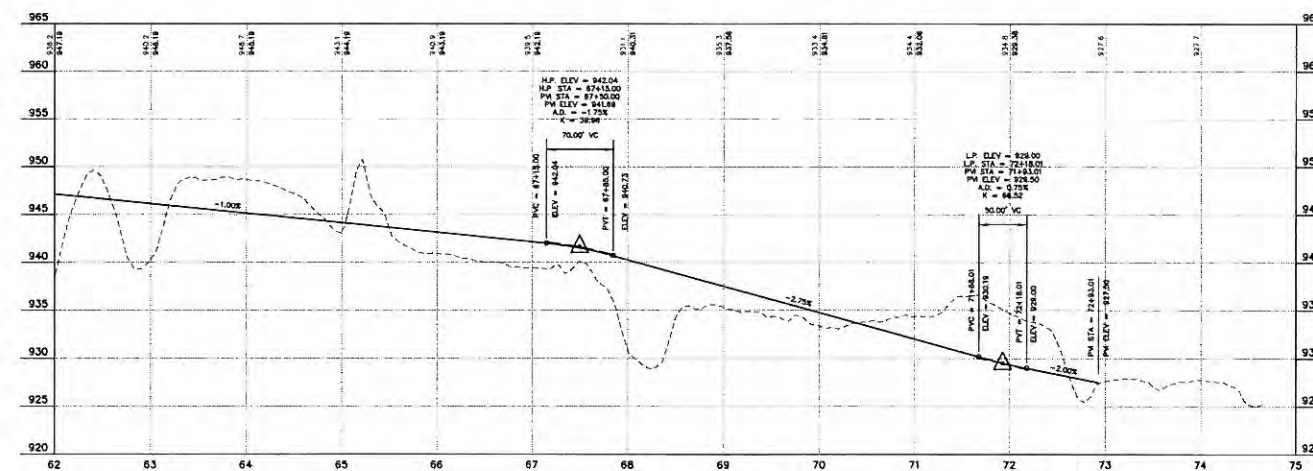


STANDARD PLAN NOTES	
GRABENT AND EROSION CONTROL PLANS	
FEBRUARY 2013	
LAKE ELMO	CITY OF LAKE ELMO
	STANDARD DRAWING NO.
	600B
	LAKE ELMO

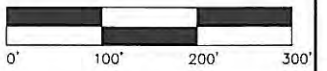




Street 1



Street 1



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Date: 02/20/14 Sheet: 17 OF 25

**Hammes
Property**

Lake Elmo, Minnesota

Street Profiles



Westwood

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7899 Anagram Drive
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PHONE 952-937-5150
FAX 952-937-5822
TOLL FREE 1-888-937-5150

www.westwoodps.com

I hereby certify that this plan was prepared by me or under my
direct supervision and that I am a duly licensed PROFESSIONAL
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Ryan M. Bluhm
Date: 06/06/14 License No. 41257

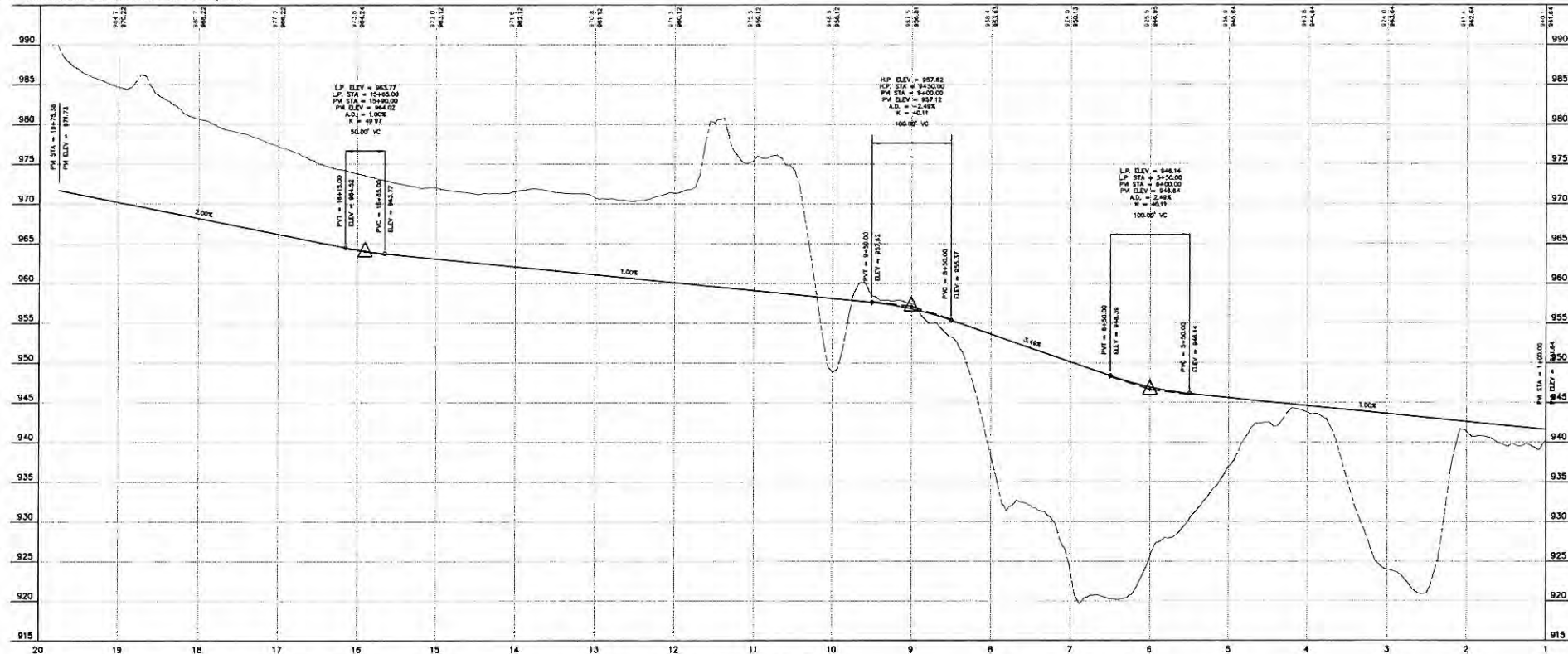
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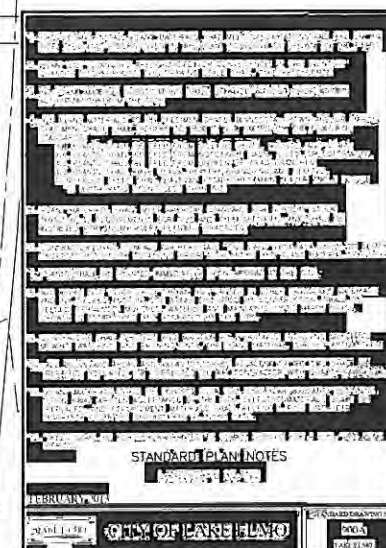
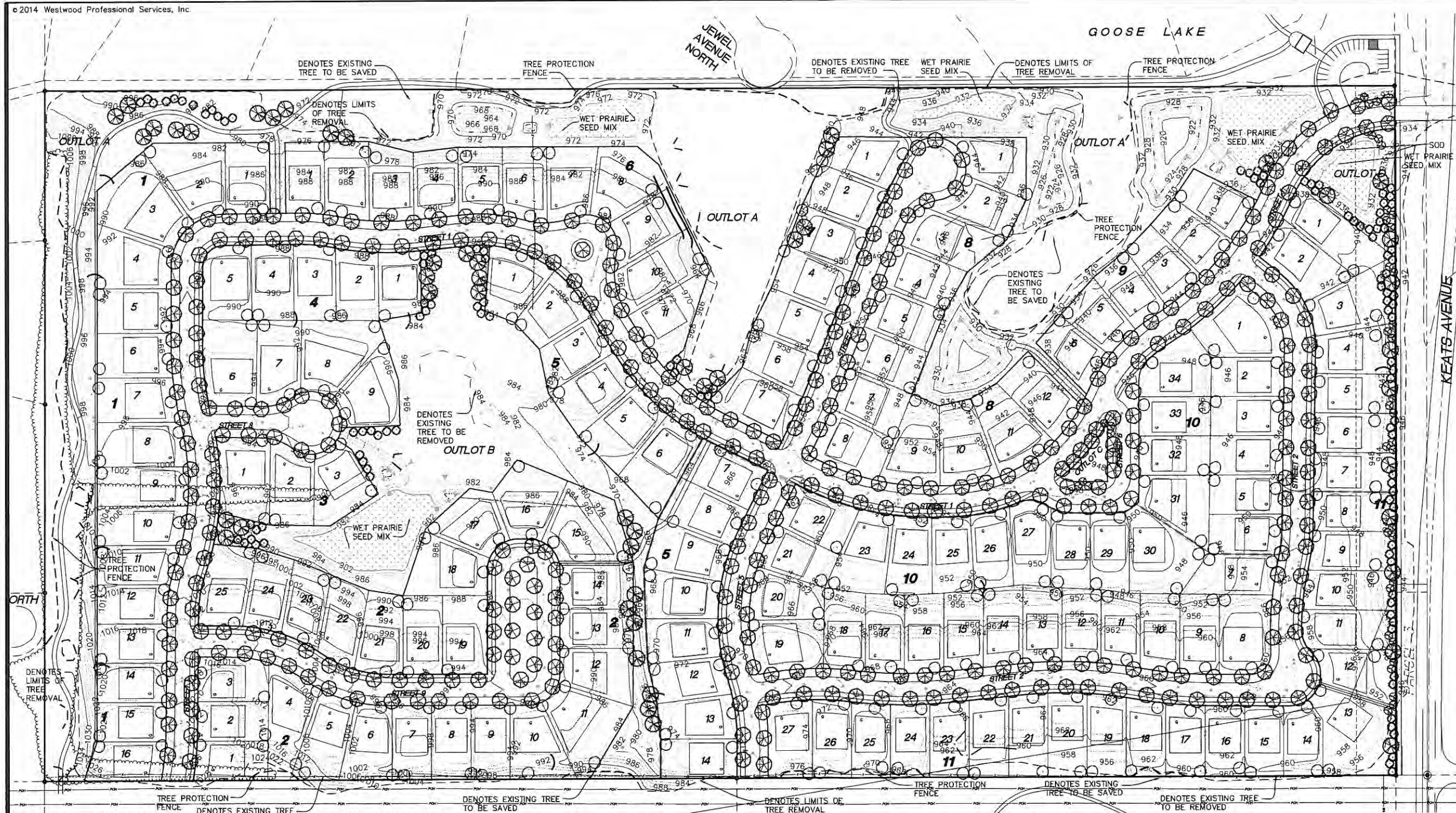
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Designed: CLJ
Checked: RMB
Drawn: HW
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Eden Prairie, Minnesota 55344





***ADDITIONAL NOTES:
STREET TREE LOCATIONS AND SPACING ARE
SUBJECT TO CHANGE BASED ON CONSTRUCTED
DRIVEWAY AND UTILITY LOCATIONS.

Preliminary Plant Schedule

SYMBOL	QTY	COMMON/BOTANICAL NAME	SIZE
OVERSTORY - STREET TREE	348	Sugar Maple / <i>Acer saccharum</i>	2.5" BB
		Autumn Blaze Maple / <i>Acer x freemanii</i> 'Jeffers Red'	2.5" BB
		Northwood Maple / <i>Acer rubrum</i> 'Northwood'	2.5" BB
		Skyline Honeylocust / <i>Gleditsia triacanthos inermis</i> 'Skycole'	2.5" BB
OVERSTORY - OTHER	50	Northern Pin Oak / <i>Quercus ellipsoidalis</i>	2.5" BB
		Red Oak / <i>Quercus rubra</i>	2.5" BB
		Swamp White Oak / <i>Quercus bicolor</i>	2.5" BB
		Bur Oak / <i>Quercus macrocarpa</i>	2.5" BB
		Hackberry / <i>Celtis occidentalis</i>	2.5" BB
OVERSTORY - YARD TREE	328	Boulevard Linden / <i>Tilia americana</i> 'Boulevard'	2.5" BB
		Greenspire Linden / <i>Tilia cordata</i> 'Greenspire'	2.5" BB
		Discovery Elm / <i>Ulmus davidiana</i> var. <i>japonica</i> 'Discovery'	2.5" BB
		Princeton Elm / <i>Ulmus americana</i> 'Princeton'	2.5" BB

NOTE: QUANTITIES ON PLAN SUPERSEDE LIST QUANTITIES IN THE EVENT OF A DISCREPANCY

Preliminary Plant Schedule

SYMBOL	QTY	COMMON/BOTANICAL NAME	SIZE
ORNAMENTAL	39	River Birch / <i>Betula nigra</i>	6' HT., BB CLUMP
		Thornless Cockspur Hawthorn / <i>Crataegus crusgalli</i> 'Inermis'	2" BB
		Japanese Tree Lilac / <i>Syringa reticulata</i>	6' HT., BB CLUMP
		Allegheny Serviceberry / <i>Amelanchier laevis</i>	6' HT., BB CLUMP
		Whitespire Birch / <i>Betula populifolia</i> 'Whitespire'	2" BB
		Prairiefire Crab / <i>Malus 'Prairiefire'</i>	2" BB
		Profusion Crab / <i>Malus 'Profusion'</i>	2" BB
		Spring Snow Crab / <i>Malus 'Spring Snow'</i>	2" BB
EVERGREEN	113	Black Hills Spruce / <i>Picea glauca densata</i>	6' HT., BB
		Colorado Green Spruce / <i>Picea pungens</i>	6' HT., BB
		Austrian Pine / <i>Pinus nigra</i>	6' HT., BB
		Norway Pine / <i>Pinus resinosa</i>	6' HT., BB
		Scotch Pine / <i>Pinus sylvestris</i>	6' HT., BB
		White Pine / <i>Pinus strobus</i>	6' HT., BB

NOTE: QUANTITIES ON PLAN SUPERSEDE LIST QUANTITIES IN THE EVENT OF A DISCREPANCY

Landscape Requirement Calculations

GENERAL LANDSCAPE REQUIREMENTS:	
FIVE TREES PER ACRE X SITE AREA (67.1 AC)	= 336 TREES REQUIRED
(2-1/2" cal. inches)	
STREET TREE REQUIREMENTS:	
1 TREE PER 50 STREET FRONTAGE (16,381 LF)	= 328 TREES REQUIRED
(2-1/2" cal. inches)	
SUBTOTAL LANDSCAPE REQUIREMENTS:	= 664 TREES REQUIRED
TREE REPLACEMENT REQUIREMENTS:	= 345 TREES REQUIRED
(863 cal. inches / 2.5" cal. in. per tree)	
OVERALL LANDSCAPING REQUIREMENTS:	= 1009 TREES REQUIRED
OVERALL LANDSCAPING PROVIDED:	= 878 TREES PROVIDED
(INCLUDES ALL PLANTINGS ON-SITE)	

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**Hammes
Property**

Lake Elmo, Minnesota

Latest Revision Date: 05/02/14

Date: 02/20/14 Sheet: 19 OF 25

Preliminary Landscape
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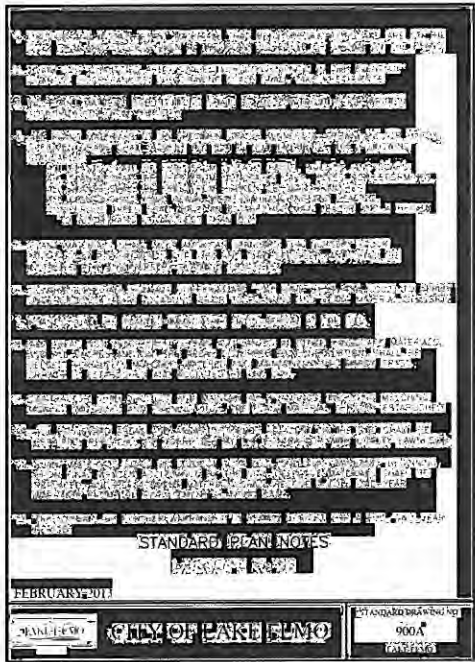
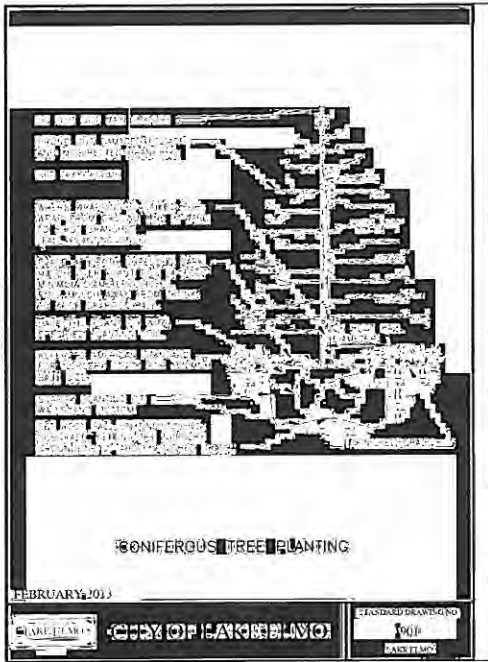
Cory Meyer
Date: 05/02/14 License No. 26971

Revisions:
04/08/14 CITY COMMENTS
05/02/14 CITY COMMENTS

Designed: CLM
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Drawn: SYB
Record Drawing by/date:

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Eden Prairie, Minnesota 55344



Supplemental Planting Notes

1. ACTUAL LOCATION OF PLANT MATERIAL IS SUBJECT TO FIELD AND SITE CONDITIONS.
2. NO PLANTING WILL BE INSTALLED UNTIL ALL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
3. PRUNE PLANTS AS NECESSARY – PER STANDARD NURSERY PRACTICE AND TO CORRECT POOR BRANCHING OF EXISTING AND PROPOSED TREES
4. STAKING OF TREES AS REQUIRED, REPOSITION, PLUMB AND STAKE IF NOT PLUMB AFTER ONE YEAR
5. THE NEED FOR SOIL AMENDMENTS SHALL BE DETERMINED UPON SITE SOIL CONDITIONS PRIOR TO PLANTING. LANDSCAPE CONTRACTOR SHALL NOTIFY LANDSCAPE ARCHITECT FOR THE NEED OF ANY SOIL AMENDMENTS
6. BACKFILL SOIL AND TOPSOIL TO ADHERE TO MN/DOT STANDARD SPECIFICATION 3877 (SELECT TOPSOIL BORROW) AND TO BE EXISTING TOP SOIL FROM SITE FREE OF ROOTS, ROCKS LARGER THAN ONE INCH, SUBSOIL DEBRIS, AND LARGE WEEDS UNLESS SPECIFIED OTHERWISE. MINIMUM 4" DEPTH TOPSOIL FOR ALL LAWN GRASS AREAS AND 12" DEPTH TOPSOIL FOR TREE, SHRUBS, AND PERENNIALS
7. MULCH TO BE AT ALL TREE, SHRUB, PERENNIAL, AND MAINTENANCE AREAS. SHREDDED HARDWOOD MULCH TO BE USED AROUND ALL PLANTS WITHIN TURF AREAS. PERENNIAL AND ORNAMENTAL GRASS BEDS SHALL HAVE 2" DEPTH SHREDDED HARDWOOD MULCH. MULCH TO BE FREE OF DELETERIOUS MATERIAL
8. EDGING TO BE SPADED EDGE, UNLESS OTHERWISE INDICATED. SPADED EDGE TO PROVIDE V-SHAPED DEPTH AND WIDTH TO CREATE SEPARATION BETWEEN MULCH AND GRASS. INDIVIDUAL TREE, SHRUB, OR RAIN-GARDEN BEDS TO BE SPADED EDGE, UNLESS NOTED OTHERWISE
9. ALL TOPSOIL AREAS TO BE RAKED TO REMOVE DEBRIS AND ENSURE DRAINAGE. SLOPES OF 3:1 OR GREATER SHALL BE STAKED. NATIVE SEEDING PER MN/DOT SPECIFICATIONS IN OUTLOT C & I TO BE SEEDED WITH SEED MIX 33-262. PORTIONS OF OUTLOTS G & H TO BE SEEDED WITH SEED MIX 35-621 PER MN/DOT SPECIFICATIONS
10. PROVIDE IRRIGATION TO ALL STREET TREE AND BUFFER LANDSCAPING ON SITE. REAR YARD AND OPEN SPACE PLANTINGS WILL NOT BE IRRIGATED. IRRIGATION SYSTEM TO BE DESIGN/BUILD BY LANDSCAPE CONTRACTOR. LANDSCAPE CONTRACTOR TO PROVIDE SHOP DRAWINGS TO CITY OF LAKE ELMO FOR APPROVAL PRIOR TO INSTALLATION OF IRRIGATION SYSTEM. CONTRACTOR TO PROVIDE OPERATION MANUALS, AS-BUILT PLANS, AND NORMAL PROGRAMMING. SYSTEM SHALL BE WINTERIZED AND HAVE SPRING STARTUP DURING FIRST YEAR OF OPERATION. SYSTEM SHALL HAVE ONE-YEAR WARRANTY ON ALL PARTS AND LABOR. ALL INFORMATION ABOUT INSTALLATION AND SCHEDULING CAN BE OBTAINED FROM THE GENERAL CONTRACTOR
11. REPAIR ALL DAMAGE TO PROPERTY FROM PLANTING OPERATIONS AT NO COST TO OWNER
12. ALL DISTURBED AREAS WITHIN OUTLOTS TO BE SEEDED WITH DRY PRAIRIE SEED GRASS MIX, UNLESS OTHERWISE NOTED. ALL AREAS WITHIN INDIVIDUAL LOTS TO BE SODDED. SOD TO BE STANDARD MINNESOTA GROWN AND HARDY BLUEGRASS MIX, FREE OF LAWN WEEDS. ALL TOPSOIL AREAS TO BE RAKED TO REMOVE DEBRIS AND ENSURE DRAINAGE. SLOPES OF 3:1 OR GREATER SHALL BE STAKED. SEED AS SPECIFIED PER MN STATE SEED MIX SPECIFICATIONS (SEE SEED MIX TABLES)

Dry Prairie Seed SE Grass Mix

35-621		Dry Prairie Southeast			
Common Name	Scientific Name	Rate (kg/ha)	Rate (lb/ac)	% of Mix (% by wt)	Seeds/ sq ft
side-oats grama	<i>Bouteloua curtipendula</i>	1.27	1.13	10.23%	2.48
blue grama	<i>Bouteloua gracilis</i>	0.76	0.68	6.19%	10.00
kalm's brome	<i>Bromus kalmii</i>	0.35	0.31	2.78%	0.90
nodding wild rye	<i>Elymus canadensis</i>	1.68	1.50	13.61%	2.86
slender wheatgrass	<i>Elymus trachycaulus</i>	1.32	1.18	10.76%	3.00
junegrass	<i>Koeleria macrantha</i>	0.46	0.41	3.71%	30.00
little bluestem	<i>Schizachyrium scoparium</i>	1.69	1.51	13.70%	8.30
sand dropseed	<i>Sporobolus cryptandrus</i>	0.25	0.22	1.98%	16.00
prairie dropseed	<i>Sporobolus heterolepis</i>	0.29	0.26	2.32%	1.50
Total Grasses		8.07	7.20	65.28%	75.04
butterfly milkweed	<i>Asclepias tuberosa</i>	0.07	0.06	0.52%	0.09
whorled milkweed	<i>Asclepias verticillata</i>	0.01	0.01	0.11%	0.05
bird's foot coreopsis	<i>Coreopsis palmata</i>	0.06	0.05	0.50%	0.20
white prairie clover	<i>Dalea candida</i>	0.10	0.09	0.78%	0.60
purple prairie clover	<i>Dalea purpurea</i>	0.17	0.15	1.32%	0.80
ox-eye	<i>Heliopsis helianthoides</i>	0.07	0.06	0.51%	0.13
round-headed bush clover	<i>Lespedeza capitata</i>	0.03	0.03	0.31%	0.10
rough blazing star	<i>Liatris aspera</i>	0.02	0.02	0.17%	0.11
dotted blazing star	<i>Liatris punctata</i>	0.02	0.02	0.23%	0.06
wild bergamot	<i>Monarda fistulosa</i>	0.03	0.03	0.30%	0.85
horsemint	<i>Monarda punctata</i>	0.02	0.02	0.22%	0.80
stiff goldenrod	<i>Oligoneuron rigidum</i>	0.07	0.06	0.59%	0.98
large-flowered beard tongue	<i>Pensilemon grandiflorus</i>	0.04	0.04	0.35%	0.20
black-eyed susan	<i>Rudbeckia hirta</i>	0.10	0.09	0.86%	3.20
gray goldenrod	<i>Solidago nemoralis</i>	0.01	0.01	0.14%	1.65
skyblue aster	<i>Symphotrichum oolentangiense</i>	0.01	0.01	0.06%	0.20
silky aster	<i>Symphotrichum sericeum</i>	0.02	0.02	0.19%	0.20
bracted spiderwort	<i>Tradescantia bracteata</i>	0.01	0.01	0.12%	0.05
heart-leaved alexanders	<i>Zizia aptera</i>	0.02	0.02	0.21%	0.10
Total Forbs		0.90	0.80	7.49%	10.37
Oats or winter wheat (see note at beginning of list for recommended dates)		3.36	3.00	27.23%	1.33
Total Cover Crop		3.36	3.00	27.23%	1.33
Totals:		12.33	11.00	100.00%	86.75
Purpose:		Regional dry prairie reconstruction for wetland mitigation, ecological restoration, or conservation program plantings			
Planting Area:		Eastern Broadleaf Forest Province excluding Hardwood Hills subsection: Mn/DOT Districts Metro & 6.			

Wet Prairie Seed Grass Mix

35-621		Dry Prairie Southeast			
Common Name	Scientific Name	Rate (kg/ha)	Rate (lb/ac)	% of Mix (% by wt)	Seeds/ sq ft
side-oats grama	<i>Bouteloua curtipendula</i>	1.27	1.13	10.23%	2.48
blue grama	<i>Bouteloua gracilis</i>	0.76	0.68	6.19%	10.00
kalm's brome	<i>Bromus kalmii</i>	0.35	0.31	2.78%	0.90
nodding wild rye	<i>Elymus canadensis</i>	1.68	1.50	13.61%	2.86
slender wheatgrass	<i>Elymus trachycaulus</i>	1.32	1.18	10.76%	3.00
junegrass	<i>Koeleria macrantha</i>	0.46	0.41	3.71%	30.00
little bluestem	<i>Schizachyrium scoparium</i>	1.69	1.51	13.70%	8.30
sand dropseed	<i>Sporobolus cryptandrus</i>	0.25	0.22	1.98%	16.00
prairie dropseed	<i>Sporobolus heterolepis</i>	0.29	0.26	2.32%	1.50
Total Grasses		8.07	7.20	65.28%	75.04
butterfly milkweed	<i>Asclepias tuberosa</i>	0.07	0.06	0.52%	0.09
whorled milkweed	<i>Asclepias verticillata</i>	0.01	0.01	0.11%	0.05
bird's foot coreopsis	<i>Coreopsis palmata</i>	0.06	0.05	0.50%	0.20
white prairie clover	<i>Dalea candida</i>	0.10	0.09	0.78%	0.60
purple prairie clover	<i>Dalea purpurea</i>	0.17	0.15	1.32%	0.80
ox-eye	<i>Heliopsis helianthoides</i>	0.07	0.06	0.51%	0.13
round-headed bush clover	<i>Lespedeza capitata</i>	0.03	0.03	0.31%	0.10
rough blazing star	<i>Liatris aspera</i>	0.02	0.02	0.17%	0.11
dotted blazing star	<i>Liatris punctata</i>	0.02	0.02	0.23%	0.06
wild bergamot	<i>Monarda fistulosa</i>	0.03	0.03	0.30%	0.85
horsemint	<i>Monarda punctata</i>	0.02	0.02	0.22%	0.80
stiff goldenrod	<i>Oligoneuron rigidum</i>	0.07	0.06	0.59%	0.98
large-flowered beard tongue	<i>Pensilemon grandiflorus</i>	0.04	0.04	0.35%	0.20
black-eyed susan	<i>Rudbeckia hirta</i>	0.10	0.09	0.86%	3.20
gray goldenrod	<i>Solidago nemoralis</i>	0.01	0.01	0.14%	1.65
skyblue aster	<i>Symphotrichum oolentangiense</i>	0.01	0.01	0.06%	0.20
silky aster	<i>Symphotrichum sericeum</i>	0.02	0.02	0.19%	0.20
bracted spiderwort	<i>Tradescantia bracteata</i>	0.01	0.01	0.12%	0.05
heart-leaved alexanders	<i>Zizia aptera</i>	0.02	0.02	0.21%	0.10
Total Forbs		0.90	0.80	7.49%	10.37
Oats or winter wheat (see note at beginning of list for recommended dates)		3.36	3.00	27.23%	1.33
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Totals:		12.33	11.00	100.00%	86.75
Purpose:		Regional dry prairie reconstruction for wetland mitigation, ecological restoration, or conservation program plantings			
Planting Area:		Eastern Broadleaf Forest Province excluding Hardwood Hills subsection: Mn/DOT Districts Metro & 6.			

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**Hammes
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Lake Elmo, Minnesota

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Cory Meyer
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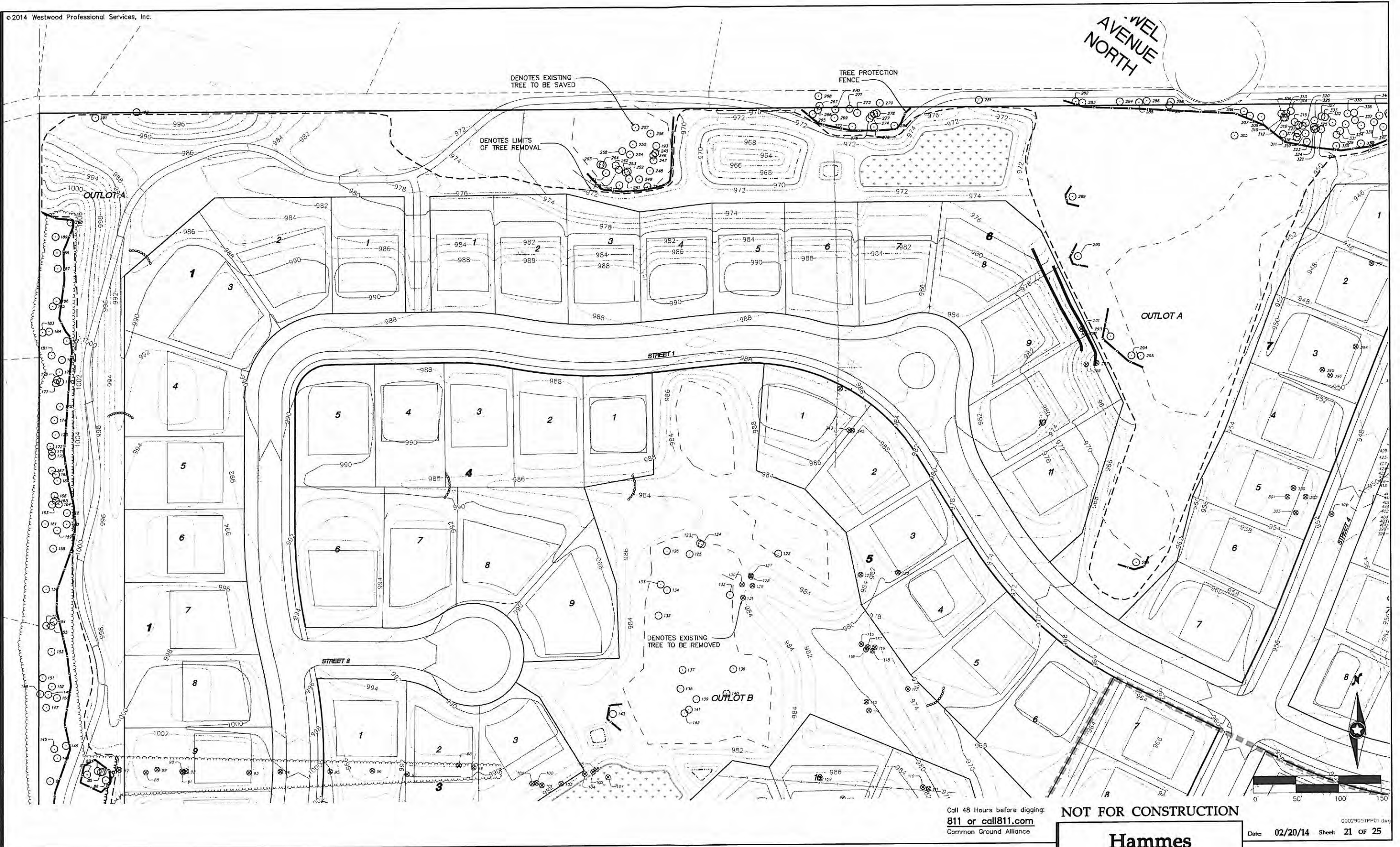
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**Preliminary Tree
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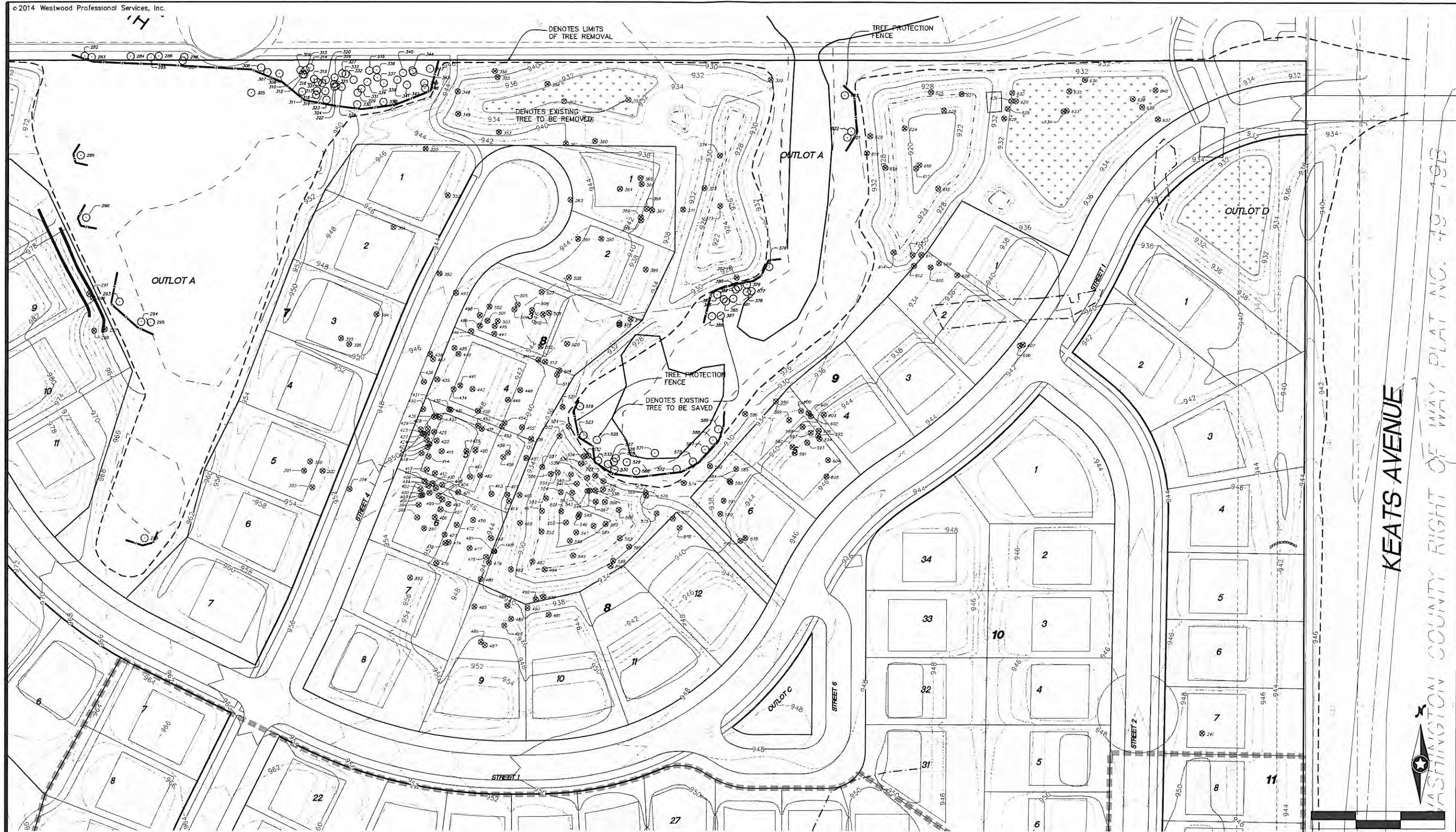
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Lake Ilmo, Minnesota

**Preliminary Tree
 Preservation Plan**



Call 48 Hours before digging: **811 or call811.com**
Common Ground Alliance

NOT FOR CONSTRUCTION

0002905TPP04.dwg

Date: 02/20/14 Sheet 24 OF 25



Westwood Professional Services, Inc.
7699 Anagram Drive
Eden Prairie, MN 55344
PHONE 952-937-6190
FAX 952-937-5822
TOLL FREE 1-888-937-5150
www.westwoodpl.com

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly licensed LANDSCAPE ARCHITECT under the laws of the State of Minnesota.
Cory Meyer
Date: 05/02/14 License No. 26971

Revisions:
04/08/14 CITY COMMENTS
05/02/14 CITY COMMENTS

Designed: _____
Checked: _____
Drawn: SYR
Record Drawing by/date: _____

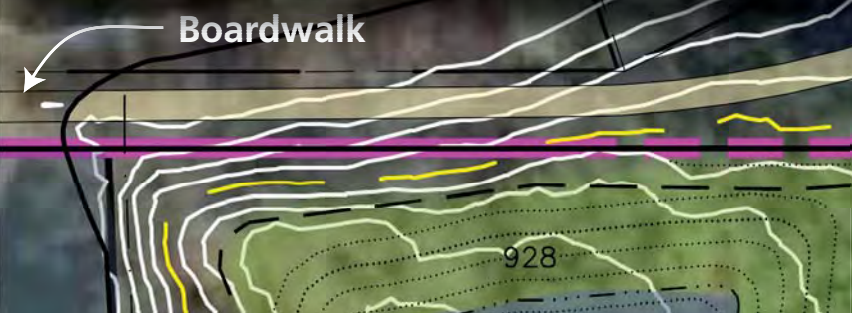
Prepared for:

Ryland Homes
7599 Anagram Drive
Eden Prairie, Minnesota 55344

**Hammes
Property**
Lake Elmo, Minnesota

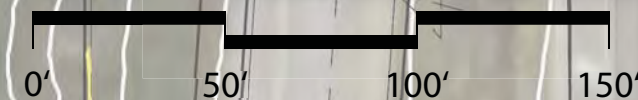
**Preliminary Tree
Preservation Plan**

ILLUSTRATIVE PARK PLAN



KEATS AVENUE

HAMMES PROPERTY
LAKE ELMO, MN



Westwood Professional Services
7699 Anagram Drive
Eden Prairie, MN 55344
PHONE 952-937-5150
FAX 952-937-5822
TOLL FREE 1-888-937-5150
www.westwoodps.com

6/03/2014
0002905

RYLAND
HOMES

LAND CREDIT EXHIBIT



KEATS AVENUE
WASHINGTON COUNTY RIGHT OF WAY PLAT NO. 49-19B
C.S.A.H. 19

PARCEL 3

From: [Ryan M. Bluhm](#)
To: [Nick Johnson](#)
Subject: FW: 0002905 Land Credit Exhibit Update
Date: Tuesday, June 17, 2014 4:35:29 PM
Attachments: [0002905 Land Credit Exhibit 200sc 14-06-17.pdf](#)

Attached is the revised plan. Thanks

-----Original Message-----

From: Steven Eggert
Sent: Tuesday, June 17, 2014 3:26 PM
To: Ryan M. Bluhm
Subject: RE: 0002905 Land Credit Exhibit Update

Total Area: 250,100 sf
5.7 ac

Steven Eggert
Project Planner
Landscape Architecture

Westwood Professional Services
Serving clients across the Nation

DIRECT 952-906-7458
MAIN 952-937-5150
FAX 952-937-5822
WEB www.westwoodps.com
EMAIL steven.eggert@westwoodps.com

-----Original Message-----

From: Ryan M. Bluhm
Sent: Tuesday, June 17, 2014 2:57 PM
To: Steven Eggert
Subject: FW: 0002905 Land Credit Exhibit Update

FYI, can you make the changes

-----Original Message-----

From: Nick Johnson [<mailto:NJohnson@lakeelmo.org>]
Sent: Tuesday, June 17, 2014 2:38 PM
To: Ryan M. Bluhm
Cc: Kyle Klatt
Subject: RE: 0002905 Land Credit Exhibit Update

Ryan,

Attached is a sketch of what I think is appropriate. Near the cul-de-sac trail, the portion of the trail that is on private lots is not eligible for land dedication credit. In addition, along the northern greenway, areas that are within wetland buffers do not qualify, as it is encumbered land that the City could not do improvements on. The same applies for land that is dedicated within a utility easement for example. If the land is encumbered, it is not eligible per our ordinance.

Does this make sense? Hopefully we can square this figure away so we are all on the same page.

MEMORANDUM

FOCUS ENGINEERING, inc.

Cara Geheren, P.E. 651.300.4261
Jack Griffin, P.E. 651.300.4264
Ryan Stempski, P.E. 651.300.4267
Chad Isakson, P.E. 651.300.4285

Date: June 16, 2014

To: Nick Johnson, City Planner
Cc: Kyle Klatt, Planning Director

Re: Hammes Estates
Preliminary Plat Review

From: Jack Griffin, P.E., City Engineer

An engineering review has been completed for the Hammes Estates development. Preliminary Plans were received on June 11, 2014. The submittal consisted of the following documentation prepared by Westwood Professional Services, Inc.:

- Revised Preliminary Plans dated 06.06.2014.
- Revised preliminary Storm Water Runoff Narrative, dated 06.11.2014.
- Plan revision response letter dated 06.11.2014.

STATUS/FINDINGS: Engineering review comments are as outlined below. Comments that are underlined indicate potential site plan changes that may impact the preliminary or final plat:

UTILITY PLANS AND EASEMENTS

- A 12-inch watermain stub should be extended east along Street 1 to the intersection and County R/W of Keats Avenue for future extension to the east side of CSAH 19.
- The 8-inch watermain line from the Street 8 cul-de-sac to the Street 9 cul-de-sac passes directly under infiltration basin 1 and does not maintain the state required 10-foot offset from storm sewer pipe. An alternate alignment or alternate loop connection will need to be determined as part of the final construction plans.
- The sanitary sewer segment along the east side of Lot 6, Block 6 needs to move further east to maintain additional offset from the Lot 6 property line.
- Additional plan information is needed to evaluate impacts to the City's sewer, forcemain and lift station infrastructure by the proposed Park improvements.
- A few additional easements or pipe alignment adjustments will be needed. These adjustments do not appear to create site plan difficulties and can be completed as part of the final construction plans.
 - Along the 42-inch storm sewer pipe behind Lots 2-6, Block 10.
 - At the northeast corner of Lot 1, Block 10.
 - Along the rear yard of Lot 17, Block 11.

STORM SEWER SYSTEM

- The storm sewer system or grading plans must be revised to provide the City standard minimum pipe cover of 3.5 feet. Throughout the site plan the storm sewer minimum cover has not been provided. It appears that additional cover can be easily accomplished in most areas. However a few areas will require

site plan design changes to provide an acceptable storm sewer system design that integrates properly with the street section, drain tile connections and other utilities.

- Drain tile is required as part of the City standard street section at all localized low points in the street. Drain tile considerations may impact the storm sewer design and depth requirements.

CSAH 19 (KEATS AVENUE) IMPROVEMENTS

- Written documentation is required to demonstrate Washington County approval for the proposed access to Keats Avenue together with any County requirements. Turn lane, by-pass lane and other improvements on CSAH 19 as required by Washington County must be identified and incorporated into the plans.
- Street 1 improvements must extend into the County Road R/W and connect to CSAH 19 including turning radii and drainage provisions.

WETLANDS AND WETLAND BUFFERS

- VBWD requires a minimum 25-foot buffer when buffer averaging is used. In addition, the VBWD does not allow impervious surfaces to be placed on wetlands or wetland buffers.
- The wetland buffer for Wetland G encroaches over the proposed trail between Lot 6, Block 10 and Lot 12, Block 9.

GRADING AND STORM WATER MANAGEMENT

- Erosion Control Plans were not reviewed at this time. A detailed review will be completed with the review of the Final Construction Plans.
- The site plan is dependent upon and subject to the storm water management plan meeting the VBWD rules and regulations. Storm water facilities proposed as part of the site plan to meet VBWD permitting requirements must be constructed in accordance with the City Engineering Design Standards Manual. Plan modifications may be necessary to meet these requirements and standards and must be completed prior to grading operations or start of construction.
- Additional information is needed to complete a review of the proposed storm water management plan and to verify the proposed grading.
 - Wetland A area: The existing HWL for Wetland A appears to extend north over the entire existing adjacent property. More information is needed to describe and verify the existing conditions in this area including the existing elevation of the adjacent home.
 - Wetland A / Pond 2, Treatment Basin and Infiltration Basin 2 area: The proposed storm water plan does not identify a system EOF. More information is needed to describe and verify the proposed flood condition and emergency overflow path to ensure that flood conditions are not increased for adjacent properties.
 - Outlot A area including all wetlands, ponds and infiltration basins: More information is needed to describe and identify the proposed flood condition and emergency overflow path. All proposed ponds, treatment and infiltration basins appear to be connected to Goose Lake during flood conditions. If there is no emergency outlet, back to back storm events may need to be evaluated.
- Infiltration basin 5 and 5A on Outlot D: The HWL for these facilities (932.0) are above the adjacent Street 1 subgrade elevation. The infiltration basin HWL must be below the street subgrade and the interconnecting storm sewer pipes must be lowered to provide additional pipe cover. This will require additional horizontal and vertical separation from the street and infiltration basins.
- Storm water pond, infiltration basin, and wetland HWLs must be fully contained within Outlots. The 100-year HWL for Wetland G encroaches proposed Lots 4, 5 and 6, Block 10, and Lots 2 and 3, Block 9; and the 100-year HWL for Wetland F encroaches proposed Lots 1 and 2, Block 8.
- No HWL has been provided as required for Wetland E.
- The grading plans need to be extended to include the Park Improvements proposed near Goose Lake.



LANDSCAPE ARCHITECTURE INCORPORATED

WWW.LANDARCINC.COM

HAMMES PROPERTY – DESIGN REVIEW REPORT **LAKE ELMO, MN**

LANDSCAPE ARCHITECTURAL DESIGN REVIEW DATED MAY 7TH, 2014

REVIEWED PLAN SET DATED MAY 2ND, 2014

Required Action Items by Hammes Project Team

1. Plan does not meet minimum City requirements for landscape materials. City requirements are fair and reasonable therefore, one or a combination of the following recommendations must be met.

Recommendations:

- Revise design to preserve more existing trees. Therefore, reducing tree replacement requirements.
- Add more landscape materials on-site to meet landscape requirements.
- Increase caliper inches or height of trees already specified to comply with aggregate landscape requirements.
- Plant remaining required plant materials in a nearby City Park per City staff direction to meet landscape requirements.

(per tree preservation ordinance 154.257 8 a-d & landscape requirements 154.258)

2. Some of the suggested plant materials specified may or may not be appropriate for this site depending on where they are utilized. We look forward to a tagged landscape plan revealing the exact locations of all the specified plant species so we can better understand and review the proposed landscape plans.
3. Provide landscape irrigation plans.
4. Reduce the use of turf sod around storm water facilities and replace with an ecologically appropriate alternative such as native shrubs or prairie.

SINCERELY,

LANDSCAPE ARCHITECTURE, INC.

STEPHEN MASTEY, ASLA, CLARB, LEED AP BD+C
DIRECTOR OF DESIGN



Public Works Department

Donald J. Theisen, P.E.
Director

Wayne H. Sandberg, P.E.
Deputy Director/County Engineer

May 6, 2014

Kyle Klatt
Community Development Director
City of Lake Elmo
3600 Laverne Avenue North
Lake Elmo, MN 55042

RE: Washington County comments on the preliminary plans and plat for the Hammes Property Residential Development, City of Lake Elmo

Dear Mr. Klatt:

Thank you for providing the county with the preliminary plat and plans of the Hammes property subdivision in Section 34, Township 27, Range 21, City of Lake Elmo. The project will consist of 165 single family lots. Based on review of the plans, we offer the following comments and recommendations to consider as you process this subdivision application through the City of Lake Elmo:

- The proposed access point on County State Aid Highway (CSAH)19/Keats Avenue is acceptable to the County. There are two additional local street connections, that extend south through the Savona Subdivision and ultimately, to 5th Street North. The average vehicles trips generated from this site will be 1,579 trips per day according to the *Trip Generation 7th Addition, Institute of Transportation Engineers (ITE.)*
- Since the proposed project will generate additional traffic on CSAH 19/Keats Avenue, functionally classified as an "A" Minor Arterial Roadway, improvements to CSAH 19/Keats Avenue will need to be provided for access to the local street. These improvements include a right turn lane on the southbound lane and a left turn lane on the northbound lane on CSAH 19. The median crossing from the southbound lane to the northbound lane should be modified and as discussed previously, the access to the Hammes Mining property on the west side of CSAH 19 should be aligned with the new local street location. A pedestrian trail/sidewalk should incorporated into the south side of median.
- There should be consideration on moving the driveway to the parking lot for the park further to the west to allow for additional access spacing from the intersection.
- A Washington County Right-Of- Way permit will be required for any work in the CSAH 19 right-of way as it relates to the development. A plan set is required with

the application and include any grading, installation of culverts, installation of water and sewer services, left and right turn lanes on CSAH 19, pedestrian crossings, parallel trail grading and development, if required, paving and striping, center median crossing removals, new center median crossings, signage and any landscaping and other improvements within county right-of-way.

- The proposed project will generate pedestrian/bicycle traffic on CSAH 19/Keats Avenue which is identified in the Washington County Comprehensive Plan 2030 as the Central Greenway Regional Trail. Pedestrians from the future development will need to access the existing off road trail on the east side of CSAH 19/Keats Avenue, therefore a crossing on the north side of the new intersection will need to be designed and constructed to provide this connection.
- Although it is not in the City of Lake Elmo 2030 Comprehensive Trail Plan, an off road parallel trail on the west side of CSAH 19/Keats Avenue should be considered by the city as the development/subdivision proceeds through the city review process. This trail will link directly to the Lake Elmo Regional Park Reserve to the north and the City of Woodbury to the south.
- Removal of the existing medians on CSAH 19/Keats Avenue shall be included in the plans.
- A signage plan for CSAH 19/Keats Avenue shall be prepared as it relates to the traffic improvements.
- The developer or the city must submit the drainage report and calculations to our office for review of any downstream impacts to the county drainage system. Along with the drainage calculations, we will request written conclusions that the volume and rate of stormwater run-off into the county right-of way will not increase as part of the project.
- Access control must be dedicated to Washington County along the CSAH 19/Keats Avenue frontage, except for the opening corresponding to the City's right- of- way for the collector roadway.
- Washington County's policy is to assist local governments in promoting compatibility between land use and highways. Residential uses located adjacent to highways often result in complaints about traffic noise. Traffic noise from this highway could exceed noise standards established by the Minnesota Pollution Control Agency (MPCA), the U.S. Department of Housing and Urban Development, and the U.S. Department of Transportation. Minnesota Rule 7030.0030 states that municipalities are responsible for taking all reasonable measures to prevent land use activities listed in the MPCA's Noise Area Classification (NAC) where the establishment of the land use would result in violations of established noise standards. Minnesota Statute 116.07, Subpart 2a exempts County Roads and County State Aid Highways from noise thresholds.

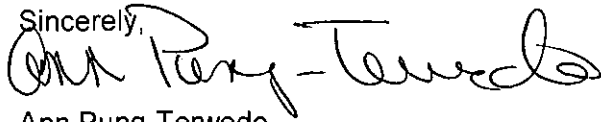
Hammes Subdivision
May 6, 2014

County policy regarding development adjacent to existing highways prohibits the expenditure of highway funds for noise mitigation measures in such areas. The developer should assess the noise situation and take any action outside of County right of way deemed necessary to minimize the impact of any highway noise.

- Finally, the new intersection as well as the intersection at CSAH 19/5th Street may warrant the installation of traffic signals in the future as development occurs along the I-94 corridor. It is Washington County Policy that cities provide funding for 50% of the costs of traffic signals.

If you have any questions or comments to the responses on the Hammes Property, please contact me at Ann.pung-terwedo@co.washington.mn.us.

Sincerely,

A handwritten signature in black ink, appearing to read "Ann Pung-Terwedo", written in a cursive style.

Ann Pung-Terwedo
Senior Planner

C: Carol Hanson, Office Specialist

R/Plat Reviews/City of Lake Elmo/Hammes/5-6-2014

Minnesota Department of Natural Resources

Division of Ecological and Water Resources
1200 Warner Road
Saint Paul, MN 55106-6793



May 28, 2014

Nick Johnson
City of Lake Elmo
3800 Laverne Ave. N.
Lake Elmo, MN 55042

RE: Shoreland Ordinance Revisions/Hammes Plat

Dear Mr. Johnson:

I have taken a preliminary look at the proposed revisions to your shoreland ordinance. It will take me more time to complete a thorough review without a redline version to compare to your old ordinance. It is also extremely time consuming to have to look in other parts of the ordinance for some of the standards. I may need to meet with you again so that you can highlight the changes.

The revision you are most interested in I think, is the riparian dedication so that you can apply it to the Hammes plat to increase density. You also mentioned that there may be other developments coming up. That is the section I will comment on at this time. Riparian buffers do have value when applied to undeveloped lakes so that the buffer functions to protect the riparian zone from individual property owner alterations that effect water quality and habitat. The city really only has 1 partially developed and one mostly undeveloped water body that these would be applicable to. The other water bodies are fully developed.

If approved, the riparian dedications areas must remain largely undeveloped and free of impervious surfaces. It seems like the standards for use contained in the ordinance would allow significant alteration for common spaces. For the Hammes Plat, the riparian dedication is small compared to the size of the lake. Such uses should be clustered for minimal impact and restricted to the greatest extent possible. Facilities and alterations must be setback the greatest amount possible to keep the buffer nearest the lake intact. It is also extremely important that the buffer areas be marked with monuments and signs to prevent yard creep. There should be deed restrictions and clear rules and enforcement.

Regarding the Hammes plat, we noticed that the riparian dedication does not cover the southern-most extension of the lake. As this is part of Goose Lake, the riparian dedication should also extend to protect the entire south end of the lake, including the extension. It is unknown to me whether this was natural or manmade, but at this point, it is considered part of the lake. It is unlikely that we could approve a flexibility request by the city to allow a riparian dedication of a 150' buffer without including the whole portion of the lake contained within the proposed plat.

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Another consideration for flexibility approval will be to evaluate the section of the ordinance regarding water oriented structures. We would still like to receive a written response to our letter regarding the Leonard structure. We appreciate that the city has revised the height to conform to the state standard in the new ordinance, but if our interpretations differ, we need to assure that we are on the same page going forward. This may involve inserting some additional language.

We likely need additional conversations regarding the steps forward in order to implement and approve flexibility for reduced standards. Unfortunately our time is very limited for land use related activities, but we are sensitive to the fact that the city needs to move forward and will try to prioritize reaching a conclusion of these issues.

Please contact me at (651) 259-5845 or molly.shodeen@state.mn.us to discuss your thoughts.

Sincerely,



Molly Shodeen
Area Hydrologist

cc: Kyle Klatt, City Planning Director
Dan Petrik, DNR EWR Land Use Unit

From: [Shodeen, Molly \(DNR\)](#)
To: [Nick Johnson](#)
Cc: [Kyle Klatt](#); [Dean Zuleger](#); [Petrik, Daniel \(DNR\)](#)
Subject: RE: June Land Use Review
Date: Monday, June 16, 2014 12:07:19 PM
Attachments: [image002.jpg](#)

Thanks Nick, as I said in my last letter, we believe that the 150' riparian dedication must be applied to the manmade channel/southern extension of the lake. DNR permit rules consider anything that is dredged and attached to the lake to be part of the lake, and as such must meet any setback requirements, as well as in this case, the 150' buffer requirement. We do not see that there are practical difficulties beyond financial for issuing the variance and we recommend that the variance be denied. We consider the Met Council argument to be a bit weak as there are other developments coming up that will get you to your projections.

As an alternative, we would request that a berm be placed across the access channel to restore the Goose Lake basin to what it was. The photos show that it was excavated sometime between the 60's and 90's without any DNR permits. A permit would be needed to close it off, but we would consider it to be a restoration. The photos also show that originally in 1991 there was a very narrow connection which was again illegally widened since 1991 to its current configuration.

As far as the ordinance goes, we need to meet to discuss any and all changes that you have made unless you have a strike through version to show the changes. I need to discuss your reaction to my suggested changes that were not made in the final ordinance. For any buffer implementation, we request that it be marked and monumented to prevent encroachment over time. As previously stated, we would like to see that right in the ordinance.

You also need to request implementation flexibility as part of the request to approve the ordinance. It is a letter asking that we consider allowing flexibility for the city to deviate from the statewide standards. The letter needs to detail what the request is, and how it will afford additional protection for the resources to justify the flexibility.

From: Nick Johnson [<mailto:NJohnson@lakeelmo.org> <<mailto:NJohnson@lakeelmo.org>>]
Sent: Tuesday, June 10, 2014 12:07 PM
To: Shodeen, Molly (DNR); John Hanson (jhanson@barr.com <<mailto:jhanson@barr.com>>)
Cc: Kyle Klatt; Dean Zuleger
Subject: June Land Use Review

Molly and John,

Please see the attached land use review for the June 23rd Planning Commission meeting. If possible, please send review comments by Wednesday, June 18th. Hard copies are being placed in the mail today to your office.

Thanks for your attention to this matter.

Nick M. Johnson | City Planner

City of Lake Elmo, Minnesota

njohnson@lakeelmo.org <<mailto:njohnson@lakeelmo.org>>

(w) 651-747-3912 | (f) 651-747-3901

www.lakeelmo.org <<http://www.lakeelmo.org>>



MAYOR & COUNCIL COMMUNICATION

DATE: July 1, 2014
CONSENT
ITEM # 14

AGENDA ITEM: Planning Commission Appointments

SUBMITTED BY: Nick M. Johnson, City Planner

THROUGH: Dean Zuleger, City Administrator

REVIEWED BY: Kyle Klatt, Community Development Director

SUGGESTED ORDER OF BUSINESS (if removed from the Consent Agenda):

- Introduction of Item.....Community Development Director
- Report/Presentation.....Community Development Director
- Questions from Council to Staff..... Mayor Facilitates
- Call for Motion Mayor & City Council
- Discussion..... Mayor & City Council
- Action on Motion..... Mayor Facilitates

POLICY RECCOMENDER: Staff

FISCAL IMPACT: None

SUMMARY AND ACTION REQUESTED: The City has received notification from Commissioner Sara Yocum that she will be stepping down from the Planning Commission due to a significant amount of professional and family commitments. Per her request, staff is recommending that the City Council vacate her appointment to the Planning Commission. In addition, Commissioner Jay Morreale has been unable to attend Planning Commission meetings since December of 2013. Per the procedural requirements for the Planning Commission in the City Code (§32.029), the City Council may vacate an appointment if the member has three consecutive absences or if they are absent from more than 1/3 of the total meetings during a calendar year. Staff is recommending vacating Commissioner Morreale's appointment so that a new member may be appointed who is more able to regularly attend the meetings. If the Council vacates the appointments of Commissioners Yocum and Morreale, there would be a vacancy of one Full Voting Member (Morreale's term). As a result, staff is recommending that the City Council appoint Commissioner Jill Lundgren (1st Alternate) to Full Voting Member status to fill the remainder of Commissioner Morreale's term.

Staff is recommending that the City Council vacate the appointments of Sara Yocum and Jay Morreale to the Planning Commission and appoint Jill Lundgren as a Full Voting Member of the Planning Commission as part of the Consent Agenda. If removed from the Consent Agenda, the recommended action can be approved through the following motion:

“Move to vacate the appointments of Sara Yocum and Jay Morreale to the Planning Commission. In addition, move to appoint Jill Lundgren as a Full Voting Member of the Planning Commission.”

LEGISLATIVE HISTORY/BACKGROUND INFORMATION:

The Planning Commission consists of 7 full voting members and 2 alternates. At this time, the roster is full. However, Commissioner Yocum has notified the City that she intends to step down. In addition, Commissioner Morreale has been unable to attend Planning Commission meetings for some time. As the orientation, training and acclimation of a Planning Commissioner takes a significant amount of time, staff is recommending that the City work to appoint two new candidates as soon as possible. As the City encounters more and more land use application associated with upcoming growth and development, it is important to have a full roster of experienced Planning Commissioners to review the multitude of various requests that the city receives.

BACKGROUND INFORMATION (SWOT):

Strengths: Proceeding through the vacation of the two appointments will allow the City to recruit two new members. In addition, Commissioner Lundgren's growth and experience as an alternate has made her a qualified candidate to become a Full Voting Member.

Weaknesses: None

Opportunities: Appointing two new members to the Planning Commission would allow them to start the orientation and training process as soon as possible.

Threats: None

RECOMMENDATION:

Based on the aforementioned, Staff is recommending that the City Council vacate the appointments of Sara Yocum and Jay Morreale to the Planning Commission and appoint Jill Lundgren as a Full Voting Member of the Planning Commission as part of the Consent Agenda. If removed from the Consent Agenda, the recommended action can be approved through the following motion:

“Move to vacate the appointments of Sara Yocum and Jay Morreale to the Planning Commission. In addition, move to appoint Jill Lundgren as a Full Voting Member of the Planning Commission.”

ATTACHMENTS:

1. Sara Yocum's Letter of Resignation

From: [Klepac, Jodi and Sara Yocum](#)
To: [Kyle Klatt; Nick Johnson](#)
Subject: Planning Commission
Date: Monday, June 09, 2014 7:38:23 PM
Attachments: [image003.png](#)

Kyle & Nick,

It is with sincere regret that I write this note. I have to step down from the Planning Commission at this time. My husband accepted a new job that involves quite a bit of travel and long work days which means that I am taking on a lot more responsibility at home with our three very busy children. It is nearly impossible for me to balance home life and a full time job with another commitment at this time.

I would love to be considered for the Commission again in the future when things settle down for us. For now, I want to sincerely thank you for the opportunity to learn and assist with the tremendous growth of the City of Lake Elmo.

Sincerely,

Sara Yocum

Sara Joynson Yocum (Attorney)

651.334.4740

Jodi Joynson Klepac (Licensed in MN and WI)

651.226.2672

Barb Joynson

651.428.8643

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520 Commons Drive- Woodbury, MN 55125

YocumRealEstateGroup@EdinaRealty.com

www.YocumRealEstateGroup.com

<<http://www.linkedin.com/in/jodiklepac/>>

<<http://www.linkedin.com/pub/sara-yocum/6/b77/196/>>



MAYOR & COUNCIL COMMUNICATION

DATE: July 1, 2014
CONSENT
ITEM # 15

AGENDA ITEM: Easement Encroachment Agreement – 9954 Tapestry Road

SUBMITTED BY: Joan Ziertman, Planning Program Assistant

THROUGH: Dean Zuleger, City Administrator

REVIEWED BY: Rick Chase, Building Official
Adam Bell, City Clerk

SUGGESTED ORDER OF BUSINESS(if removed from consent agenda):

- Introduction of ItemStaff
- Report/Presentation.....Staff
- Questions from Council to Staff..... Mayor Facilitates
- Call for Motion Mayor & City Council
- Discussion..... Mayor & City Council
- Action on Motion..... Mayor Facilitates

POLICY RECCOMENDER: Staff recommends that the City Council approve the encroachment agreement for Matthew & Cynthia Fanta at 9954 Tapestry Road North as part of tonight's consent agenda.

FISCAL IMPACT: None

SUMMARY AND ACTION REQUESTED: The City Council is respectfully requested to authorize as part of tonight's consent agenda, the execution of an easement encroachment agreement. The City has received a request to install a fence within a drainage and utility easement area at 9954 Tapestry Road North from Matthew & Cynthia Fanta. Approval of the requested improvement within the City's drainage and utility easements would allow the property owners to construct the requested improvement within the City's drainage and utility easements located on their private property.

Staff is recommending that the City Council approve the easement encroachment agreement as part of the Consent Agenda. If the City Council removes the item from the Consent Agenda, the recommended action can be completed through the following motion:

“Move to approve the easement encroachment agreement for Matthew & Cynthia Fanta at 9954 Tapestry Road to install a driveway and retaining wall within the City’s drainage and utility easement.”

LEGISLATIVE HISTORY: The City holds easements of different sizes and for different purposes on many residential and commercial properties throughout the City. When a resident is interested in putting a structure within an easement, the city has requested the property owner provide a site plan showing where the improvement is proposed to be located, a detail of what the improvement will look like and how it will function. After that information is received, staff reviews the proposed improvement and the use of the easement to determine if the proposed improvement will impede the functionality of the easement. If staff determines that the improvement will not negatively impact the functionality of the easement, an approved building permit showing the requested work and an Easement Encroachment Agreement is needed before the work may commence.

The Easement Encroachment Agreement that has been submitted for Council consideration is for a fence and has been reviewed by planning staff. The proposed fence meets all city code requirements and Staff would have otherwise authorized construction of the retaining wall and driveway if they did not encroach into a drainage and utility easement.

BACKGROUND INFORMATION (SWOT):

Strengths: The easement encroachment agreement is a legal document that has been signed by all property owners seeking to install improvements within an easement. The document, among other things, indemnifies the city from responsibility if damage occurs to the improvement or if it needs to be removed at some point in the future.

Weaknesses: None

Opportunities: None

Threats: None

RECOMMENDATION:

Based on the aforementioned, Staff is recommending that the City Council approve the easement encroachment agreement as part of the Consent Agenda. If the City Council removes the item from the Consent Agenda, the recommended action can be completed through the following motion:

“Move to approve the easement encroachment agreement for Matthew & Cynthia Fanta to install a fence within the City’s drainage and utility easement.”



MAYOR & COUNCIL COMMUNICATION

DATE: July 1, 2014
REGULAR
ITEM # 16

AGENDA ITEM: 2015 Street and Utility Improvements – Authorize Preparation of a Feasibility Report; Approve Task Order No. 1 for Engineering Support Services

SUBMITTED BY: Ryan Stempski, Assistant Engineer

THROUGH: Dean A. Zuleger, City Administrator

REVIEWED BY: Jack Griffin, City Engineer
Cathy Bendel, Finance Director
Mike Bouthilet, Public Works

SUGGESTED ORDER OF BUSINESS:

- Introduction of Item..... City Engineer
- Report/Presentation..... City Engineer
- Questions from Council to Staff..... Mayor Facilitates
- Public Input, if Appropriate..... Mayor Facilitates
- Call for Motion..... Mayor & City Council
- Discussion..... Mayor & City Council
- Action on Motion..... Mayor Facilitates

POLICY RECOMMENDER: Engineering

FISCAL IMPACT: \$15,500

If authorized, FOCUS Engineering Inc., together with Hydromethods, LLC will prepare a feasibility report in the not to exceed amount of \$15,500 for the 2015 Street and Utility Improvements. If the improvements are ordered, the report costs will be charged against the project fund and become assessable to the benefiting properties. Should the project not be constructed, the report costs cannot be assessed.

SUMMARY AND ACTION REQUESTED:

The City Council is respectfully requested to consider approving Resolution No. 2014-56, Ordering Preparation of a Feasibility Report; and approving Task Order No. 1 to Hydromethods, LLC. The recommended motions for these actions are as follows:

“Move to approve Resolution No. 2014-56, Ordering preparation of a Feasibility Report for the 2015 Street and Utility Improvements to FOCUS in the not to exceed amount of \$12,300.”
and

“Move to approve a Task Order No. 1 to Hydromethods, LLC. for Engineering Support Services in the not to exceed amount of \$3,192.”

LEGISLATIVE HISTORY/BACKGROUND INFORMATION:

In accordance with the proposed 2014-2018 Street Capital Improvement Plan (Street CIP), the following local streets represent the most essential street improvements for implementation in 2015. The streets programmed for improvement in 2015 include:

- 36th Street North, from Jamaca Avenue to Irwin Avenue,
- 37th Street North, from Jamaca Avenue to Irwin Avenue,
- Irwin Avenue North,
- 38th Street North, from Ideal Avenue to Innsdale Avenue,
- 39th Street North, from Ideal Avenue to Innsdale Avenue,
- Innsdale Avenue North, and
- Kelvin Avenue North, from Trunk Highway 5 to cul-de-sac (including the private drive north of Kelvin Court).

In order to initiate this improvement the Council must direct the preparation of a feasibility report as required by the statutory process for public improvements that are specially assessed. The report will advise on the scope of recommended improvements for each local street; provide estimated project costs of the recommended improvements; identify easement and right-of-way, permits, and other requirements of other local agencies needed to implement the improvements; recommend whether the improvements should be best made as proposed or in connection with some other improvement; and advise if the improvements are necessary, cost effective, and feasible. The Report will also provide a proposed preliminary assessment roll for each of the benefiting properties based upon the City's Special Assessment Policy for Local Improvements, dated June 2014.

The improvements will likely include full street reconstruction, storm sewer and drainage improvements, and will consider watermain replacement or watermain extension consistent with the City's 2030 Comprehensive Plan. Water quality improvements will be evaluated and incorporated as necessary to meet VBWD permitting requirements. To assist with storm water management, it is recommended that Hydromethods, LLC. be retained to model the existing and proposed conditions and provide the recommended storm sewer system improvements for the project areas.

A project schedule for the 2015 Street and Utility Improvements is attached and outlines the implementation timeframe necessary to ready the improvements for construction in 2015. The proposed schedule provides the necessary time to address public involvement, acquire project related easements, acquire field survey before snowfall, and to receive contractor bids early in 2015 to promote a competitive bid environment.

RECOMMENDATION:

Staff is recommending that the City Council authorize FOCUS Engineering, Inc. to prepare a Feasibility Report for the 2015 Street and Utility Improvements in the estimated amount of \$12,300. The recommended motion for this action is as follows:

“Move to approve Resolution No. 2014-56, Ordering preparation of a Feasibility Report for the 2015 Street and Utility Improvements to FOCUS in the not to exceed amount of \$12,300.”

Staff is also recommending that the City Council approve Task Order No. 1 to Hydromethods, LLC. to complete a Storm Sewer System Improvement plan for the 2015 Street and Utility Improvements Feasibility Report. The recommended motion for this action is as follows:

“Move to approve a Task Order No. 1 to Hydromethods, LLC. for Engineering Support Services in the not to exceed amount of \$3,192.”

ATTACHMENT(S):

1. Resolution No. 2014-56, Ordering Preparation of a Feasibility Report.
2. 2015 Street and Utility Improvements – Location Map.
3. 2015 Street and Utility Improvements – Project Schedule.
4. Hydromethods,, LLC. Task Order No. 1 – 2015 Street and Utility Improvements: Storm Sewer System Improvments.

**CITY OF LAKE ELMO
WASHINGTON COUNTY
STATE OF MINNESOTA**

RESOLUTION NO. 2014-56

**A RESOLUTION ORDERING PREPARATION OF A FEASIBILITY
REPORT FOR THE 2015 STREET AND UTILITY IMPROVEMENTS**

WHEREAS, the City maintains a 5-year revolving Street Capital Improvement Program (Street CIP) to promote the strategic use of limited financial resources while maintaining, developing or replacing the City's local street network; and

WHEREAS, the Street CIP represents the City's long-range street improvement program that identifies the present and future needs and priorities, and develops a schedule of public improvements to address the most essential improvements first; and

WHEREAS, with a street reconstruction project the City reviews and considers the potential replacement or need for utility improvements in accordance with the City's 2030 Comprehensive Plan, including watermain or sanitary sewer, to be constructed in conjunction with the street improvement; and

WHEREAS, it is proposed to initiate the 2015 Street and Utility Improvement project to improve the following local streets;

- 36th Street North, from Jamaca Avenue to Irwin Avenue,
- 37th Street North, from Jamaca Avenue to Irwin Avenue,
- Irwin Avenue North,
- 38th Street North, from Ideal Avenue to Innsdale Avenue,
- 39th Street North, from Ideal Avenue to Innsdale Avenue,
- Innsdale Avenue North,
- Kelvin Avenue North, from Trunk Highway 5 to cul-de-sac; and

WHEREAS, it is proposed to assess the benefiting properties for all or a portion of the cost of the improvement, pursuant to the City's Special Assessment Policy and Minnesota Statutes, Chapter 429.

NOW, THEREFORE, IT IS HEREBY RESOLVED,

That the proposed improvement, called the 2015 Street and Utility Improvements, be referred to the City Engineer and FOCUS Engineering, and that FOCUS Engineering is instructed to complete a feasibility report in accordance with Minnesota Statutes, Chapter 429 for the proposed improvements, and to report to the council with all convenient speed advising the council in a preliminary way as to whether the proposed improvement is necessary, cost-effective, and feasible; whether it should be best made as proposed or in connection with some other

improvement; the estimated cost of the improvement as recommended; and a description of the methodology used to calculate individual assessments for affected parcels.

ADOPTED BY THE LAKE ELMO CITY COUNCIL ON THE FIRST DAY OF JULY 2014.

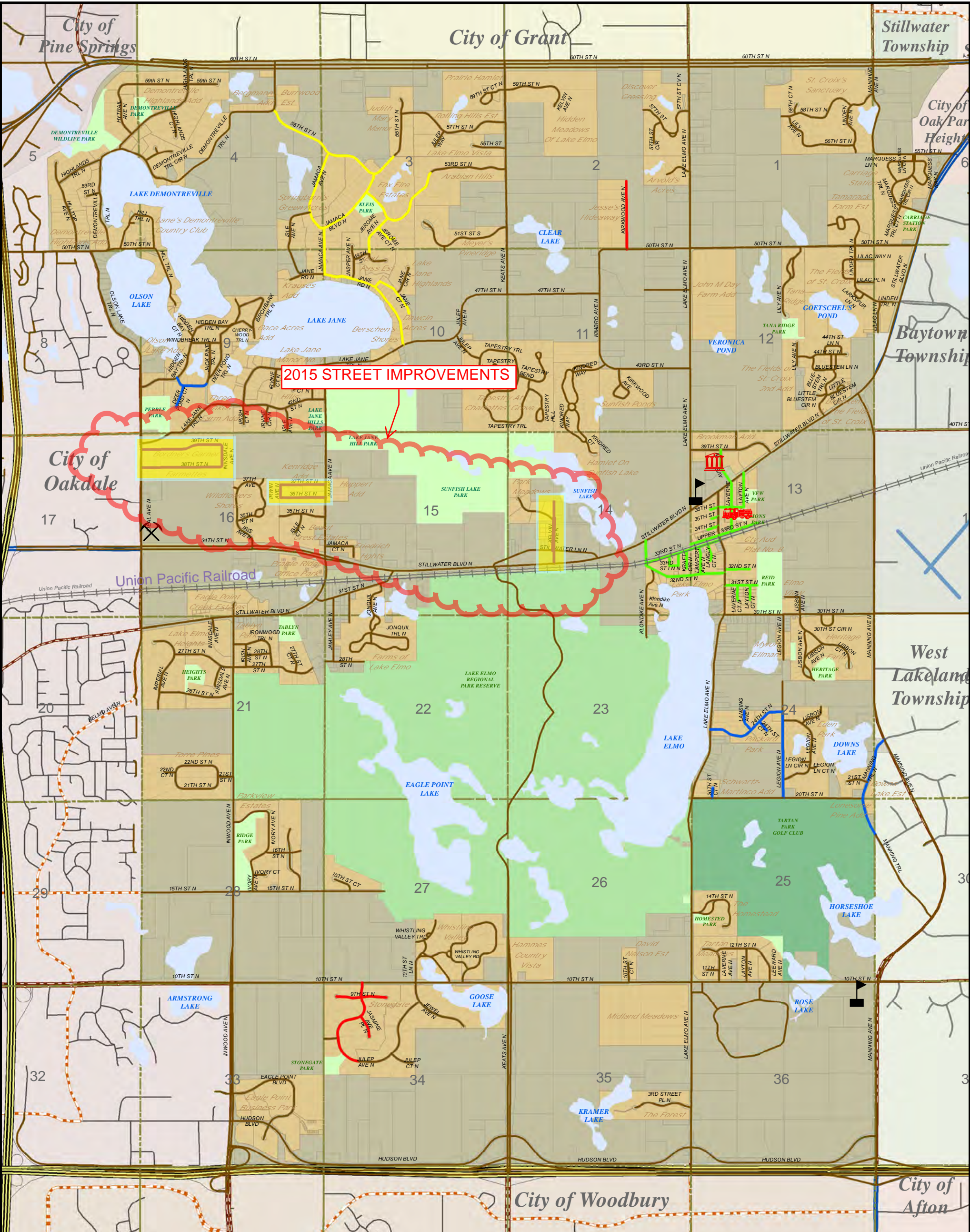
CITY OF LAKE ELMO

By: _____
Mike Pearson
Mayor

(Seal)

ATTEST:

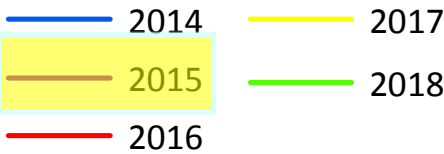
Adam Bell
City Clerk



2014 - 2018 RESIDENTIAL STREET CAPITAL IMPROVEMENT PLAN



Improvement Year



City Hall
3800 Laverne Avenue N
651/777-5510
www.lakeelmo.org



Public Works Building
3445 Ideal Ave N
651/233-5414



Fire Stations
Station No. 1
3510 Laverne Ave N
651/770-5006
Station No. 2
4259 Jamaica Ave N
651/779-8882



Schools



Parcels



Subdivisions



City Parks



Golf Course



**Lake Elmo Regional
Park Reserve
Park Office
651/430-8368
Lakes**



Map Date: October 2013

Created By:
FOCUS ENGINEERING, inc.

PRELIMINARY PROJECT SCHEDULE
CITY OF LAKE ELMO

FOCUS ENGINEERING, inc.

Cara Geheren, P.E. 651.300.4261
Jack Griffin, P.E. 651.300.4264
Ryan Stempski, P.E. 651.300.4267
Chad Isakson, P.E. 651.300.4283

2015 STREET & UTILITY IMPROVEMENTS
PROJECT NO. 2014.136

JUNE 2014

July 1, 2014	Council authorizes Feasibility Report.
September 2, 2014	Presentation of Feasibility Report. Council accepts Report and Calls Hearing.
September, 2014	Property owner meeting. Presentation of Report findings and recommendations.
October 7, 2014	Public Improvement Hearing. Council orders Preparation of plans and specifications.
January 20, 2015	Council approves Plans and Specifications; Orders Advertisement for Bids.
February 19, 2015	Receive Contractor Bids.
March 3, 2015	Council accepts bids and awards Contract.
April 15, 2015	Conduct Pre-construction Meeting and Issue Notice to Proceed.
August 28, 2015	Substantial completion.
October 16, 2015	Final completion.

In accordance with ARTICLE 1 of the Master AGREEMENT between the City of Lake Elmo ("CITY") and Hydromethods, LLC ("ENGINEER"), dated JULY 1, 2014 ("AGREEMENT"), the ENGINEER agrees to provide Professional Engineering Support Services as follows:

**STORM SEWER SYSTEM IMPROVEMENTS FOR THE
2015 STREET AND UTILITY IMPROVEMENTS**

PROJECT OVERVIEW: The City of Lake Elmo is completing a Feasibility Report to determine the scope of improvements and costs associated with the road reconstruction, drainage and utility improvements of the Bordners Garner Neighborhood, including 38th Street, 39th Street, and Innsdale Avenue; and the Kenridge Addition Neighborhood, including 36th Street, 37th Street, and Irwin Avenue. The improvements are planned for construction to occur in 2015. The scope of services requested as a part of this Task Order includes Engineering Services to complete the preliminary storm sewer design for the neighborhoods identified above.

SERVICES TO BE PROVIDED BY ENGINEER: Hydromethods, LLC shall provide the following Professional Engineering Services:

- Review and identify watershed district rules and regulations applicable to the projects.
- Complete HydroCAD or similar modeling for existing and proposed rate control at each proposed discharge location for each neighborhood.
- Identify proposed storm sewer system and storm water management improvements necessary to be incorporated into the street improvement projects to address neighborhood drainage and to meet VBWD permitting requirements for rate and volume control.
- The storm sewer system engineering evaluations will include flow routing and ponding recommendations and will include:
 - Review of downstream pond models if they're required to be included in ultimate rate control for the project.
 - Verification of downstream flow paths and to ensure that nuisance issues at outlet locations are not created.
 - Quantify Valley Branch Watershed District required volume abstraction and identify potential locations and BMPs.
 - Drainage Analysis with recommendations for the existing agriculture field outlet retrofit south of Bordners Garner Neighborhood.

DELIVERABLES:

- Storm sewer design calculations for inlet, manhole, pipe sizing, and spacing.
- Proposed storm sewer system layout in AutoCAD and PDF format.
- Technical Summary memo and storm sewer quantities.
- Engineer's Estimate of Cost for the proposed storm sewer system improvements.

CITIES RESPONSIBILITIES: The City (or its consultants) will provide the following:

- Coordination with the public and conduct public meetings.
- Presentation(s) to the City Council.
- Provide Valley Branch Watershed District modeling for the ponds west of Irwin Avenue.

CITY'S REPRESENTATIVE AND CONTRACT ADMINISTRATION: The CITY's representative with respect to services rendered by ENGINEER under this TASK ORDER shall be the City Engineer. Project correspondence must be addressed to:

Jack Griffin, P.E., City Engineer
City of Lake Elmo
3800 Laverne Avenue North
Lake Elmo, MN 55042
651.300.4264
Email: Jack.griffin@focusengineeringinc.com

COMPENSATION: Compensation to ENGINEER shall be on an hourly rate basis using the hourly billing rates and assigned personnel in a not to exceed amount of \$3,192.00.

Payment for Services shall be in accordance with ARTICLE 3.2 of the Master AGREEMENT. Invoices should be sent to the attention of the City Engineer.

ATTACHMENTS: The following documents are incorporated by reference: None.

APPROVAL AND ACCEPTANCE: Approval and Acceptance of this Task Order, including the attachment(s) listed above, shall incorporate this document as part of the AGREEMENT. ENGINEER is authorized to begin performance of services upon receipt of a copy of this Task Order signed by CITY.

The Effective Date of this Task Order is **JULY 1, 2014**.

HYDROMETHODS, LLC

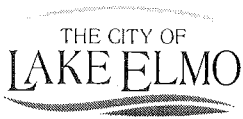
CITY OF LAKE ELMO, MINNESOTA

By _____

(Authorized Principal of the Firm)

By _____

City Administrator



MAYOR & COUNCIL COMMUNICATION

DATE: July 1, 2014

REGULAR

ITEM # 17

AGENDA ITEM: Old Village Phase 1 Street and Utility Improvements – Authorize Preparation of a Feasibility Report

SUBMITTED BY: Jack Griffin, City Engineer

THROUGH: Dean A. Zuleger, City Administrator

REVIEWED BY: Ryan Stempski, Assistant City Engineer
Cathy Bendel, Finance Director
Nick Johnson, City Planner

SUGGESTED ORDER OF BUSINESS:

- Introduction of Item City Engineer
- Report/Presentation..... City Engineer
- Questions from Council to Staff..... Mayor Facilitates
- Public Input, if Appropriate.....Mayor Facilitates
- Call for Motion Mayor & City Council
- Discussion..... Mayor & City Council
- Action on Motion..... Mayor Facilitates

POLICY RECOMMENDER: Engineering

FISCAL IMPACT: \$20,700.

If authorized, FOCUS Engineering Inc. will prepare a feasibility report in a not to exceed amount of \$20,700 for the Old Village Phase 1 Street and Utility Improvements. If the improvements are ordered, the report costs will be charged against the project fund and become assessable to the benefiting properties. Should the project not be constructed, the report costs cannot be assessed.

SUMMARY AND ACTION REQUESTED:

The City Council is respectfully requested to consider approving Resolution No. 2014-57, Ordering Preparation of Feasibility Report. The recommended motion for this action is as follows:

“Move to approve Resolution No. 2014-57, Ordering preparation of a Feasibility Report for the Old Village Phase 1 Street and Utility Improvements in a not to exceed amount of \$20,700.”

LEGISLATIVE HISTORY/BACKGROUND INFORMATION:

Over the past six months, the City of Lake Elmo and Washington County have been working jointly on the Lake Elmo Avenue (County Road 17) Corridor Management and Safety Improvement Project to improve Lake Elmo Avenue from 30th Street to Trunk Highway 5 while extending sanitary sewer service into the Downtown Village area. While Washington County is the lead agency for the project, each entity will be responsible for sharing in the project costs.

At the April 8 Transportation Workshop, the project team presented a proposed utility and project phasing plan that would phase the improvements over the 2015 and 2016 construction seasons. In 2015, street, drainage and utility improvements, including the extension of sanitary sewer service, would be completed along 30th Street from the Reid Park lift station to Lake Elmo Avenue, then north along Lake Elmo Avenue to the north side of the UP Railroad, then east along Upper 33rd Street to Laverne Avenue, then north along Laverne Avenue to Trunk Highway 5. Then in 2016, street, drainage and utility improvements would be constructed along Lake Elmo Avenue from the UP Railroad to Trunk Highway 5 together with Downtown streetscape improvements (see attached project Location Map).

Consistent with most public street and utility improvement projects the use of special assessments has been identified as one of the available funding mechanisms for the improvements, where the benefiting properties are specially assessed all or a portion of the cost of the improvement, pursuant to the City's Special Assessment Policy and Minnesota Statutes, Chapter 429. In order to maintain special assessments as a viable funding alternative, the City Council must direct the preparation of a feasibility report meeting the statutory process for public improvements that are specially assessed.

The report will advise on the scope of recommended improvements along each corridor, provide estimated project costs of the City cost share for the recommended improvements, identify easement and right-of-way, permits, and other requirements of other local agencies needed to implement the improvements, recommend whether the improvements should be made as proposed or in connection with some other improvement, and advise if the improvements are necessary, cost effective, and feasible. The Report will also provide a proposed preliminary assessment roll for each of the benefitting properties based upon the City's Special Assessment Policy for Local Improvements, dated June 2014. It is anticipated that a Council workshop will

be necessary once a draft preliminary assessment roll has been developed in order to address the more unique assessment aspects for a Downtown revitalization type project.

The report will address street, drainage and utility improvements for the following alignments:

- 30th Street North, from the Reid Park lift station to Lake Elmo Avenue.
- Lake Elmo Avenue from 30th Street to Upper 33rd Street.
- Upper 33rd Street North, from Lake Elmo Avenue to Laverne Avenue North.
- Laverne Avenue North, from Upper 33rd Street North to Trunk Highway 5.

Utility improvements will address both sanitary sewer service to replace individual on-site treatment systems, and replacement of aging watermain infrastructure. Costs will be developed in conjunction with Washington County and their consultants and the Washington County cost participation policy will be used to identify the City cost share for the improvements along the County road system.

Streetscape improvements (landscaping, street lighting, etc.) together with other ancillary costs such as overhead utilities are not included in the scope of this report since these project components are being developed under a separate contract. However, it is anticipated that the total project costs for these additional improvements may need to be incorporated as part of the project assessment policy and preliminary assessment roll and carried forward as part of a joint improvement hearing process.

RECOMMENDATION:

Staff is recommending that the City Council authorize FOCUS Engineering, Inc. to prepare a Feasibility Report for the Old Village Phase 1 Street and Utility Improvements in the estimated amount of \$20,700. The recommended motion for this action is as follows:

“Move to approve Resolution No. 2014-57, Ordering preparation of a Feasibility Report for the Old Village Phase 1 Street and Utility Improvements in a not to exceed amount of \$20,700.”

ATTACHMENT(S):

1. Resolution No. 2014-57, Ordering Preparation of a Feasibility Report.
2. Project Location Map.
3. Project Schedule.

**CITY OF LAKE ELMO
WASHINGTON COUNTY
STATE OF MINNESOTA**

RESOLUTION NO. 2014-57

**A RESOLUTION ORDERING PREPARATION OF A FEASIBILITY REPORT FOR
THE OLD VILLAGE PHASE 1 STREET AND UTILITY IMPROVEMENTS**

WHEREAS, the City of Lake Elmo and Washington County have been working jointly on the Lake Elmo Avenue (County Road 17) Corridor Management and Safety Improvement Project to improve Lake Elmo Avenue from 30th Street to Trunk Highway 5 while extending sanitary sewer service into the Downtown Village area; and

WHEREAS, each entity will be responsible for sharing in the project costs as jointly agreed to; and

WHEREAS, the improvements will be implemented in two phases with the Phase 1 Street and Utility Improvements proposed for construction in 2015; and the remaining improvements constructed in 2016; and

WHEREAS, the Phase 1 Improvements includes street, drainage and utility improvements, including the extension of sanitary sewer service and replacement of aging watermain infrastructure along the following corridors;

- 30th Street North, from the Reid Park lift station to Lake Elmo Avenue.
- Lake Elmo Avenue, from 30th Street North to Upper 33rd Street.
- Upper 33rd Street North, from Lake Elmo Avenue to Laverne Avenue North.
- Laverne Avenue North, from Upper 33rd Street North to Trunk Highway 5.

WHEREAS, it is proposed to assess the benefiting properties for all or a portion of the cost of the improvement, pursuant to the City's Special Assessment Policy and Minnesota Statutes, Chapter 429.

NOW, THEREFORE, IT IS HEREBY RESOLVED,

That the proposed improvement, called the Old Village Phase 1 Street and Utility Improvements, be referred to the City Engineer and FOCUS Engineering, and that FOCUS Engineering is instructed to complete a feasibility report in accordance with Minnesota Statutes, Chapter 429 for the proposed improvements, and to report to the council with all convenient speed advising the council in a preliminary way as to whether the proposed improvement is necessary, cost-effective, and feasible; whether it should be best made as proposed or in connection with some other improvement; the estimated cost of the improvement as recommended; and a description of the methodology used to calculate individual assessments for affected parcels.

**ADOPTED BY THE LAKE ELMO CITY COUNCIL ON THE FIRST DAY OF JULY 2014.
CITY OF LAKE ELMO**

By: _____
Mike Pearson
Mayor

(Seal)

ATTEST:

Adam Bell
City Clerk



Lake Elmo Ave Corridor Project



Project Phasing

- 2015
- 2016

City of Lake Elmo
5-13-2014
Data Source:
Washington County, MN



PROJECT SCHEDULE
CITY OF LAKE ELMO

FOCUS ENGINEERING, inc.

Cara Geheren, P.E.	651.300.4261
Jack Griffin, P.E.	651.300.4264
Ryan Stempski, P.E.	651.300.4267
Chad Isakson, P.E.	651.300.4283

OLD VILLAGE PHASE 1 STREET & UTILITY IMPROVEMENTS
PROJECT NO. 2014.137

JUNE 2014

July 1, 2014 Council authorizes Feasibility Report.

September 16, 2014 Presentation of Feasibility Report. Council accepts Report and Calls Hearing.

October, 2014 Property owner meeting. Presentation of Report findings and recommendations.

October 21, 2014 Public Improvement Hearing. Council orders Preparation of plans and specifications.

*PLANS AND SPECIFICATIONS WILL BE PREPARED IN CONJUNCTION WITH THE CSAH 17 (LAKE ELMO AVENUE) CORRIDOR IMPROVEMENT PROJECT – A COUNTY-CITY JOINT PROJECT.

*PROJECT IS BEING IMPLEMENTED FOR CONSTRUCTION IN 2015.



MAYOR & COUNCIL COMMUNICATION

DATE: July 1, 2014
REGULAR
ITEM # 18

AGENDA ITEM: Wildflower at Lake Elmo (Robert Engstrom Companies) Comprehensive Plan Amendment

SUBMITTED BY: Kyle Klatt, Community Development Director

THROUGH: Dean Zuleger, City Administrator

REVIEWED BY: Planning Commission
Nick Johnson, City Planner

SUGGESTED ORDER OF BUSINESS:

- Introduction of ItemCommunity Development Director
- Report/Presentation.....Community Development Director
- Questions from Council to Staff Mayor Facilitates
- Call for Motion Mayor & City Council
- Discussion..... Mayor & City Council
- Action on Motion..... Mayor Facilitates

POLICY RECCOMENDER: The Planning Commission reviewed a PUD Concept Plan and Comprehensive Plan Amendment related to the proposed Wildflower at Lake Elmo development at its June 9, 2014 meeting and recommended approval of both requests with conditions. The City Council approved the PUD Concept Plan at its June 17th meeting, but tabled discussion concerning the proposed Comprehensive Plan Amendment due the absence of two Council members.

FISCAL IMPACT: TBD – The Comprehensive Plan Amendment is necessary for the development project to move forward as proposed. If the amendment is not approved, the applicant will need to submit a revised concept plan.

SUMMARY AND ACTION REQUESTED: The City Council is being asked to consider a Comprehensive Plan Amendment to allow residential development to occur on two small areas within the proposed Wildflower at Lake Elmo subdivision that are currently guided for RAD – Rural Area Development and Open Space. The City Council approved the PUD Concept Plan for this development at its last meeting, but could not take action on the related Comprehensive Plan amendment because the proposed amendment requires a 4/5ths majority of the Council to

pass and two Council members were absent from this meeting. Action on this part of the applicant's original request has been postponed until the July 1st meeting.

The Planning Commission has recommended approval of the Comprehensive Plan amendment. The suggested motion to adopt the Planning Commission recommendation is as follows:

***“Move to adopt Resolution No. 2014-046 approving a Comprehensive Plan Amendment to change the future land use designation of two areas within the Wildflower at Lake Elmo development from RAD and OP to LDR and MDR.*”**

LEGISLATIVE HISTORY/PLANNING COMMISSION REPORT: The information attached to the previously Council agenda packet for this item included detailed plans, reports, and other information concerning the Wildflower at Lake Elmo Development. In the interest of avoiding additional copying for the July 1st Council meeting, Staff has not provided information included in the previous packet except for the proposed resolution of approval and related map. All of the previous information is available upon request (and still available on-line).

As part of its approval of the Wildflower Concept Plan, the City Council added two conditions to the Planning Commission recommendation based on feedback from the surrounding property owners, include the Fields of St. Croix Homeowner's Association. These conditions requested the following:

- That prior to approval of the Comprehensive Plan Amendment the Fields of St. Croix Association and Robert Engstrom Companies would submit their written agreement to the City concerning the proposed development on Outlot P and proposed amendments to the conservation easement over Outlot P.
- That prior to approval of the Comprehensive Plan Amendment the three property owners to the east of Wildflower that have submitted written statements to the City concerning the development (Eischen, Dupuis, and Smith) would work out an agreement with the developer concerning buffering and screening of their properties.

Since the Council meeting, Staff has received the written agreement between Robert Engstrom Companies and the Fields Association. This agreement is attached for consideration by the Council. In addition, the developer has met on site with the three eastern property owners and also participated in a meeting at City Hall with Staff and the Mayor present to further discuss their concerns. The result of this meeting is the attached landscape plan that documents the types of planting and location for the plantings that was deemed acceptable to all parties. Furthermore, the developer has agreed to the following actions to further address neighbor concerns:

- To conduct a further investigation of wooded area to the east of the Eischen's home that extends into Outlot P. This investigation is intended to identify any work needed (removal of dead trees, removal of invasive species, additional plantings) to allow this area to provide an effective screen and keep the area in a natural state.

- To revise the parcel layout in front of the Smith property to remove one buildable lot and to reconfigure the adjacent parcels so that they only abut Smith's land at one point. All land between the Smith property and public roadway would be platted as an outlot to be owned and maintained by the future HOA. This revised layout is depicted in the attached updated landscape plan for this area.

With the submission of this information, the relevant conditions of approval attached to the concept plan appear to have been addressed. The developer was still reviewing some of the details of the updated plan with the property owners, any further updates will be discussed at the City Council meeting.

As noted in the previous Staff report on this item, the Planning Commission discussed the request, and unanimously recommended approval of the comprehensive plan amendment as presented with the one condition as recommended by Staff.

BACKGROUND INFORMATION (SWOT) FROM PREVIOUS RERPOT:

Strengths

- The PUD Concept Plan is consistent with the City's Comprehensive Plan for the Village Planning Area (with the exception of the plan amendments requested by the developer).
- The project has been designed to comply with the City's zoning regulations and development standards for the Village Medium Density district.
- The project addresses several of the Village Planning Principles adopted as part of the Comprehensive Plan.

Weaknesses

- The concept plan will require the removal of a portion of the existing conservation easement over Outlot P of the Fields of St. Croix Second Addition.

Opportunities

- The development will include 145 REC units and will pay connection fees for sewer and water service.
- The project includes a large conservation area that will ensure the permanent protection of a large portion of the planned Village Open Space/Buffer area.
- The development will bring sewer to the extreme northeastern portion of the Village Planning Area and will be designed to allow for future connections in this part of the City.

Threats

- The developer will need to work with the City on establishing a plan for management and oversight of the conservation area in a manner that will not overburden the City.

RECOMMENDATION: Based upon the above report and analysis, Staff and the Planning Commission are recommending that the City Council approve the request from Robert Engstrom Companies for a Comprehensive Plan Amendment related to a residential subdivision to be called Wildflower at Lake Elmo. The suggested motion to adopt the Planning Commission recommendation are as follows:

***“Move to adopt Resolution No. 2014-046 approving a Comprehensive Plan Amendment to change the future land use designation of two areas within the Wildflower at Lake Elmo development from RAD and OP to LDR and MDR.*”**

ATTACHMENTS:

1. Resolution No. 2014-46 (Comprehensive Plan Amendment)
2. Proposed Comprehensive Plan Amendments
3. Updated Landscaping Sketch Plan – Wildflower at Lake Elmo
4. Planting List and Details
5. Aerial Photograph – Smith, Eischen, and Dupuis Property
6. Fields of St. Croix and Engstrom Written Agreement
7. Letter From Robert and Marcella Eischen 6/24/14

**CITY OF LAKE ELMO
WASHINGTON COUNTY, MINNESOTA**

RESOLUTION NO. 2014-46

*RESOLUTION APPROVING AN AMENDMENT TO THE CITY OF LAKE ELMO
COMPREHENSIVE PLAN*

WHEREAS, the City of Lake Elmo has established a Comprehensive Plan that provides a compilation of background data, policy statements, standards, and maps, which help to guide the future physical, social, and economic development of the City; and

WHEREAS, Robert Engstrom Companies, 4801 West 81st Street, #101, Bloomington, MN, (“Applicant”) has submitted an application to the City of Lake Elmo (“City”) to amend the Lake Elmo Comprehensive Plan, a copy of which is on file in the City Planning Department; and

WHEREAS, the request to amend the Comprehensive Plan was submitted along with a Planned Unit Development concept plan for a proposed single-family residential subdivision to be called Wildflower at Lake Elmo; and

WHEREAS, the Lake Elmo Planning Commission held a public hearing on June 9, 2014 to consider the request to amend the Comprehensive Plan; and

WHEREAS, on June 9, 2014 the Lake Elmo Planning Commission adopted a motion to recommend that the City Council approve the request to amend the Comprehensive Plan; and

WHEREAS, the City Council reviewed the recommendation of the Planning Commission and the proposed amendment to the Comprehensive Plan at meetings on June 17, 2014 and July 1, 2014; and.

NOW, THEREFORE, based upon the testimony elicited and information received, the City Council makes the following:

FINDINGS

- 1) That the Applicant has submitted a request to amend the Comprehensive Plan in accordance with the procedures as established by the Lake Elmo Planning Department and Lake Elmo Planning Commission.
- 2) That the request to is to amend the Future Land Use Map (Map 3-3 in Chapter III – *Land Use Plan*) and Village Planned Land Use Map (Map 3-5 in Chapter III – *Land Use Plan*) in

the Lake Elmo Comprehensive Plan, and to specifically change the future land use designation of a portion of two parcels of land located within the Wildflower at Lake Elmo development as depicted in the attached Exhibit A and described as follows:

- a) To change the western portion of Outlot P of the Fields of St. Croix Second Addition from RAD – Rural Area Development to V-MDR Village Urban Medium Density Residential (a portion of PID 12.029.21.43.0013).
- b) To change the approximately eight acres immediately east of the intersection of 43rd Street North and Lake Elmo Avenue (the area depicted for the westernmost single family residential lots on the Wildflower at Lake Elmo PUD Concept Plan approved by the City Council on June 9, 2014) from RAD – Rural Area Development and Village Open Space Overlay to V-LDR Village Urban Low Density Residential (a portion of PID 12.029.21.32.0001).
- 3) That the proposed area impacted by the proposed amendment is relatively small and will not have a significant impact on the City's 2030 household and population forecasts.
- 4) That the proposed amendments are consistent with the overall goals and objectives of the Village Land Use Plan.

NOW, THEREFORE, BE IT RESOLVED, that based on the foregoing, the Lake Elmo City Council hereby approves the Applicant's request to amend the Lake Elmo Comprehensive Plan, subject to and contingent upon the following:

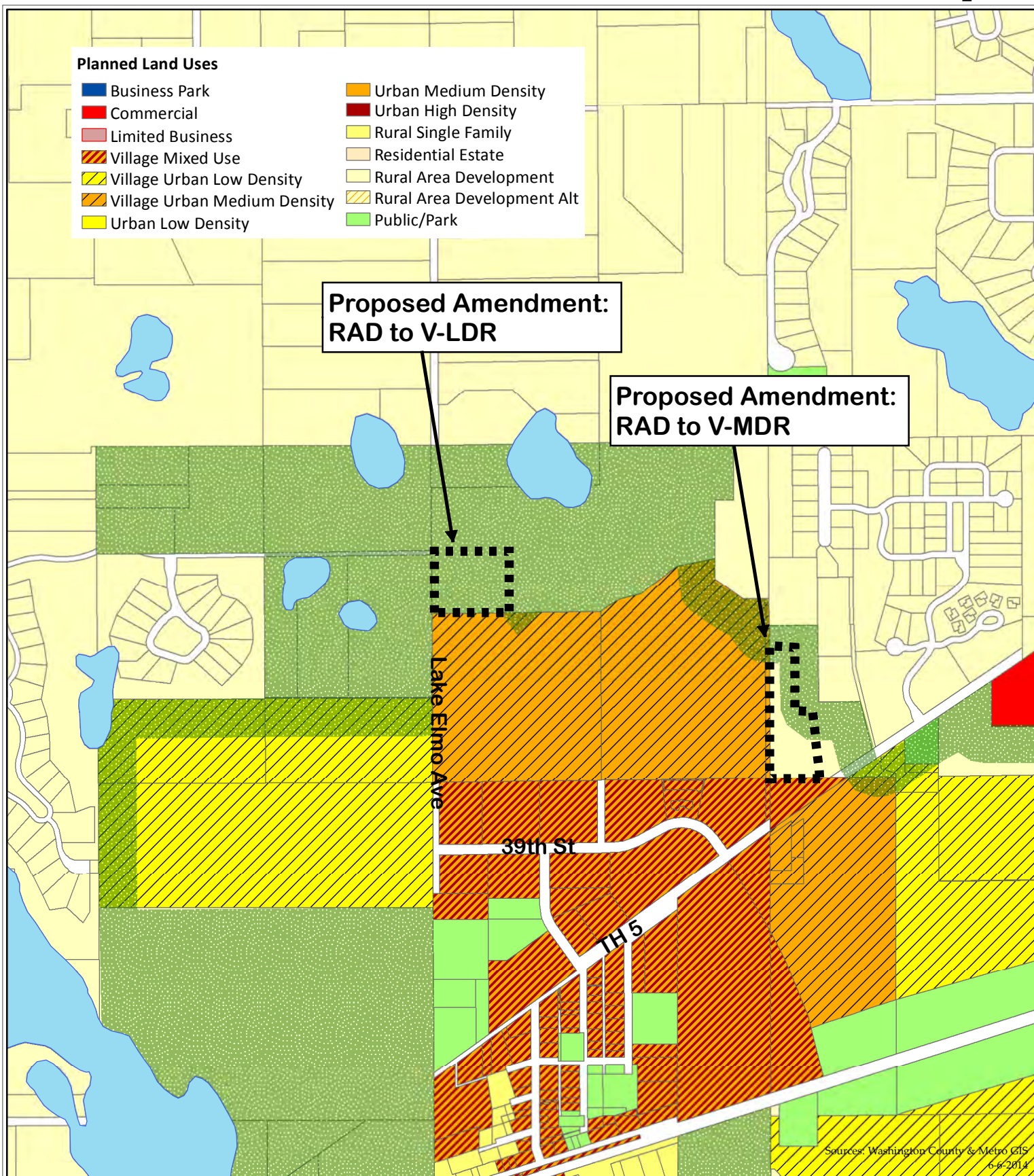
- 1) Submission of the Comprehensive Plan Amendment to the Metropolitan Council and the receipt of formal notification from the Metropolitan Council that its review has been completed and approved.

Passed and duly adopted this 1st day of July 2014 by the City Council of the City of Lake Elmo, Minnesota.

Mike Pearson, Mayor

ATTEST:

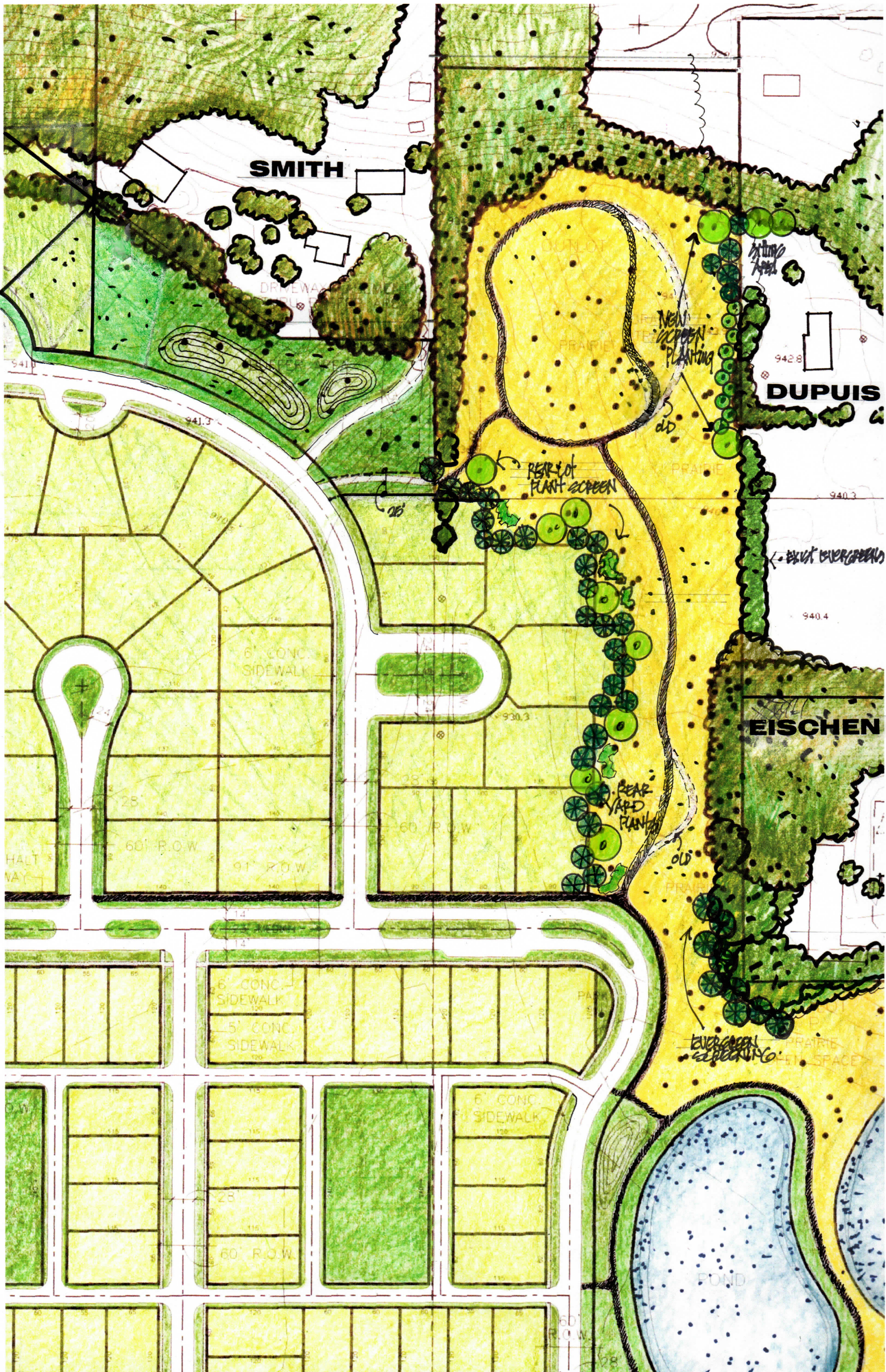
Adam Bell, City Clerk



Proposed Comprehensive Plan Amendment

Wildflower at Lake Elmo Concept Plan





WILDFLOWER

PLANT LIST

(Dry mesic soils)

- A. Sugar Maple – 1” – 1 ½”
- B. American Basswood - #2
- C. Swamp Oak
- D. Crab Apple
 - Donald Wyman
 - Spring Snow
 - Red Splendor
- E. Japanese Lilac
- F. Black Hills Spruce and/or Colorado Spruce – 5’-6’
- G. Shrubs
 - 1. Chokecherry
 - 2. Highbush Cranberry
 - 3. Dogwood
 - 4. Lilac
 - 5. Service Berry (multi-stem) - #2
 - 6. Fragrant Sumac - #2
 - 7. Grow Low Sumac (ground cover) - #1
 - 8. Arrowwood Viburnum - #2
 - 9. Nannyberry Viburnum - #2
 - 10. Big Bluestem (grass)

WILDFLOWER

Plant Quantities & Layout

Note:

Plans as shown are illustrated with plant materials deemed appropriate for soil conditions existing at sites (dry mesic soils). The attached list names these materials. The developer would like to have the flexibility to substitute within this group if better or larger selections are available at time of planting. The homeowner will be asked for substitution approval. Planting schedule will occur at such time when the site is in final graded condition and weather is cooperative. Actual tree planting locations will be done in the field with property owner's input.

Dupuis Property:

Developer will plant a screen grouping at NE corner of property to screen an existing sitting area. Plant screen will wrap lot corner west and north.

Recommended plant materials will be spruce trees 5'-6' ht and sugar maple 1 1/2" cal. An additional understory plant screen will be planted in front of existing norway pines to screen approx. 6'-8' of open area between ground and existing top mass of trees. Plant material selection will be selected from group G.

Eischen Property:

Developer will plant spruce evergreen trees at SW corner of lot to provide screening of car head lights. Screen will wrap corner as shown on site plan. Other plantings along west property line will be determined after an assessment of existing trees is made and determined what plants will be appropriate.

Smith Property:

Developer has provided a south buffer area along the total south line of property. Exact berming and plantings along with a new road easement is in the process of final negotiations.



June 9, 2014

The following is a summary of a tentative agreement between the Fields of St Croix Community Association (FoSC) and Robert Engstrom Companies (RECo) regarding RECo's proposed Wildflower development. This summary is provided for summary purposes only and is not meant to be a complete statement of the proposed agreement and is subject to change.

Subject: The Fields of St Croix Community Association (FoSC) and Robert Engstrom Companies (RECo) Wildflower development

Background: The Open Space Agreement(s) is/are key to this discussion because it explicitly prohibits the Wildflower development proposed for Outlots O & P, which are part of the FoSC. The express intent of the Open Space Agreement is in summary to:

- Permanently retain the protected land in its predominately natural, scenic, agricultural, forested and open space condition;
- Prevent or remedy action or use that significantly impairs or interferes with the Conservation Values of the land;
- Expressly prohibits the Owner (RECo) from subdividing all or part of the protected lands for residential, commercial or industrial development;
- Prohibits construction of roads, driveways and the alteration of the surface of the protected land, including excavation.

FoSC Homeowners

- Purchased lots and built homes understanding FoSC was a conservation community with Open Space Agreement protection.
- Do not want development on Outlot O & Outlot P, but will support *limited* residential development on part of Outlot P in turn for "additional" legal protection preventing development on the other Outlots associated with FoSC.
- Requested that representatives of FoSC conduct negotiations with RECo. Tentative agreement between FoSC and RECo was reached, pending City approval to allow limited residential development on Outlot P, given the terms of the agreement, which are attached.

The FoSC Community Association requests this agreement become part of Wildflower's development plan, and its terms enforced by the City of Lake Elmo.

Respectfully submitted on behalf of the FoSC Association homeowners:

John W. Hodler
Fields of St Croix Board President

The following are highlights of the tentative agreement:

- No development will occur on Outlot O.
 - Retain the existing Open Space Easement condition;
 - Fields of St Croix Community Association (FoSC) will be added as a party to the easement, thereby requiring FoSC's approval for any future changes thereto.
- Outlot P will have *limited* development on its west boundary (plan on page #4):
 - Includes small number of RECo-constructed homes along the west boundary of Outlot P;
 - No coffee shop or other retail/commercial facilities located on Outlot P;
 - RECo will install a path around part of the storm water system on Outlot P. This path will not connect to the FoSC pathways;
 - FoSC will have easement access rights for pedestrian and bicycles to Outlots O & P, and any Outlot created out of Outlot P for the storm water system;
 - FoSC will be added as a party to the existing easement on the title to the section of Outlot P not included in the new development's homes;
 - The exact boundary of the re-sized Outlot P will be defined by a survey (paid by RECo).
- The City of Lake Elmo will ensure the design of the Wildflower development's storm water system does not include a storm water retention basin on Outlot O; and will not increase the current flow of storm water drained onto Outlot O or any FoSC II Outlot (exception Outlot P):
 - The storm water design will not allow drainage to adversely affect any FoSC II homeowner;
 - Lake Elmo City will ensure the contractor and developer install the storm water system as designed;
 - The City of Lake Elmo will be responsible for maintenance of any portion of the Wildflower storm water system installed on Outlot P, or any Outlot created from Outlot P for the storm water system.
- Road access to Wildflower development:
 - The existing FOSE, phase II, entry will remain "as is". This access road will not connect to the Wildflower development;
 - No access road for the new development will be constructed that connects to highway #5 through any Outlot.

- Ensure FoSC has the right to object to or modify any future development requests involving any one or more of the Outlots located in the Fields of St Croix development (see Outlots page #5).
 - FoSC will be added as a party to all Open Space Easements now in place for Phase II Outlots;
 - FOSC will be added as a party to the Minnesota Land Trust Conservation Easement now in place for Phase I Outlots.
- Phase I Outlots A & E's Minnesota Land Trust Conservation Easement revisions.
 - Rescind the right to construct a farm home;
 - Livestock operation or use of temporary farm buildings will only be granted if all parties to the easement (including FoSC) agree.
- RECo will transfer ownership of the Bluestem Village Green property (PIN1202921440009) if it is not under a 'public right of way restriction'.
- All easement updates or amendments and Outlot ownership transfers will be completed prior to approval of the new development's plans by the City of Lake Elmo.
- RECo's creation of the storm water system, path, natural setting, and plantings on Outlots O and P will be completed ~~in the beginning stage of the new development. The "beginning stage" is defined as a date equal to the completion of the first home.~~ *for the requirements and timeline established by Lake Elmo City and incorporated into the final plan*
- RECo coordinates with Lake Elmo City & FoSC during the design and installation of Wildflower's sewer pipe *Just REC 6/9/2014*
 - Ensure adequate capacity to handle the wastewater from FoSC's phase I, phase II (including Bluestem) and Tana Ridge homes;
 - Sewer easement across the Northeast corner of the new development and Outlots O & P will be granted to FoSC;
 - Final connection, if and when completed, between the Lake Elmo City sewer system and the FoSC wastewater system (currently serving the above homes) will be coordinated by the FoSC Association at a later time.

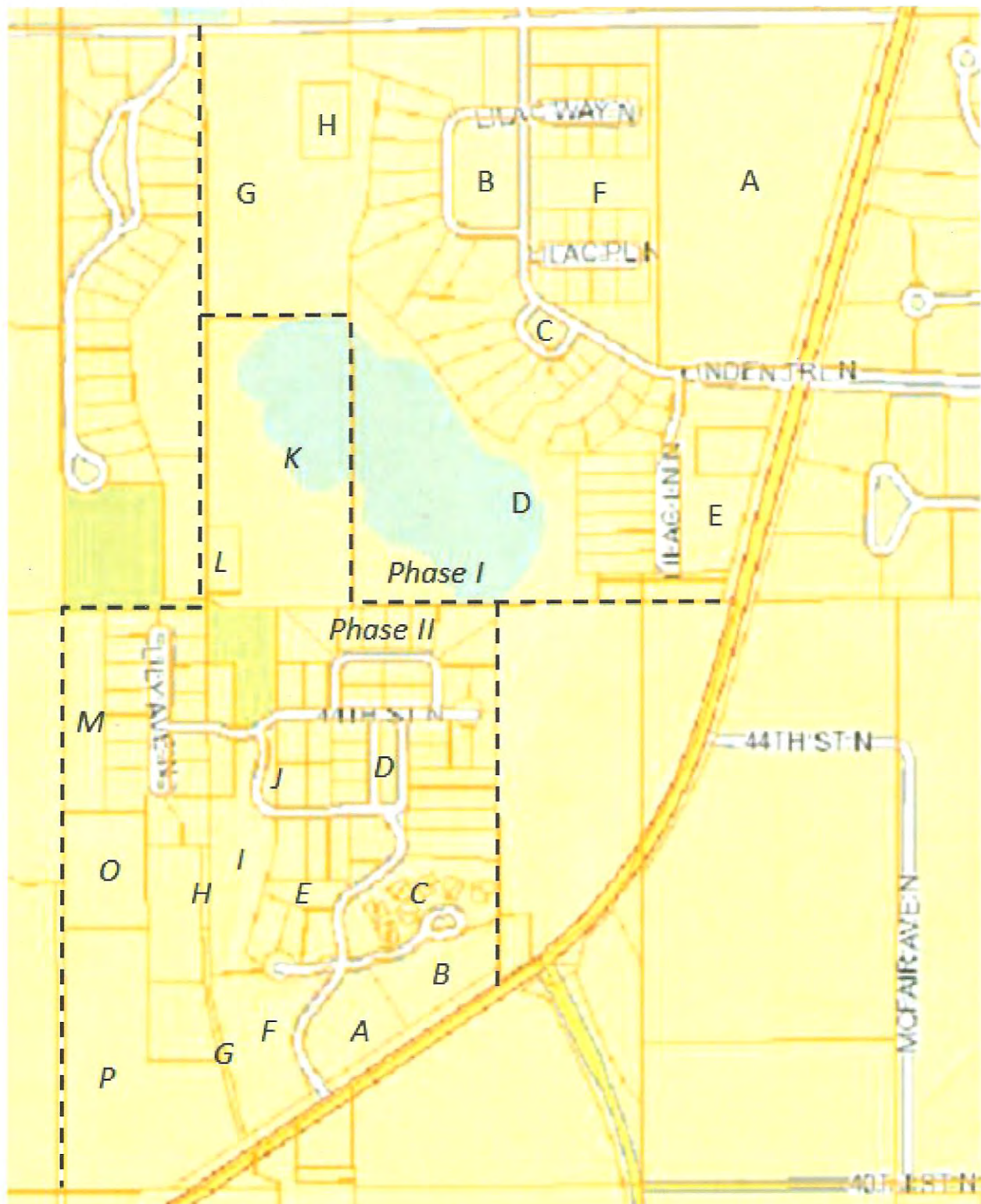
It is understood, that any agreements between RECo and FOSC will be subject to approval by the Lake Elmo City Council and other regulatory bodies.

The final agreement will include a provision that neither party may assign the agreement to any other person without the express prior written consent of the other party or its successor in interest.

REC
Robert E. Engstrom
 6/9/14

FOSC
John Holden
 6/9/2014

FoSC Phase I & II outlots



ROBERT & MARCELLA EISCHEN

Date: June 24, 2014
To: Kyle Klatt
Community Development Director
Subject: Wildflower Proposal

As we are adjoining property owners to the proposed Engstrom Wildflower Development, my wife Marcy and I met with Bob Engstrom and architect Jerry Mazarra on June 19th to review our proposals for changes to the most recently proposed changes to the conservation easement adjacent to our land. As we previously agreed:

- ✚ Remove large parking and small Public Park from the close proximity to the S/E portion of our property.
- ✚ Straighten the walking path away from the western edge of our property and more toward the middle of the conservation easement.
- ✚ Straighten Wildflower property road with circular drive and remove pie shaped circular turn to help eliminate vehicle lights shining and flashing on our home.
- ✚ As proposed by the developer, plant a buffer of evergreen trees near S/W corner of our property. We request two rows of 5' trees planted on the S/W corner estimated some 40 feet long. We feel this is a satisfactory solution for the S/W corner of our property.

We withdraw our previous request for a 5' earthen berm with trees and shrubs planted on the berm's western side of our property.

The developer now proposes trees and shrubs be planted in front of homes located adjacent to the west side of our property. We agree this is a good idea but not an alternative to our request for trees and shrubs, i.e., Black Hills and Colorado Spruce interspaced with Maple Trees and shrubs such as High Bush

Cranberry, Bridal Wreath and shrubs compatible with butterfly conservation issues. These plantings should be adjacent to the western edge of the Eischen property line and extending north from the evergreens planted to serve as a light buffer to the N/W Eischen property line marker. These plantings should be further extended north through the western edge of the Dupuis property as recommended by Mary Jean. We feel the developer and/or property association is further responsible for maintenance of these plantings.

We request these agreements and/or lack thereof be recorded and put in writing. Where reasonable requests and/or agreements can't be agreed upon, we would suggest a further opportunity to review these issues with a mediator and/or arbitrator.

Respectfully

Robert Eischen

Robert Eischen

Marcela Eischen

Marcella Eischen
