



MAYOR & COUNCIL COMMUNICATION

DATE: July 22, 2014
REGULAR
ITEM #1
RESOLUTION 2014-61

AGENDA ITEM: Lakewood Crossing Preliminary Plat and Final Plat; Kwik Trip Conditional Use Permit

SUBMITTED BY: Nick M. Johnson, City Planner

THROUGH: Dean Zuleger, City Administrator

REVIEWED BY: Planning Commission
Kyle Klatt, Community Development Director
Jack Griffin, City Engineer
Ann Pung-Terwedo, Washington County Public Works
Tod Sherman, MnDOT

SUGGESTED ORDER OF BUSINESS:

- Introduction of ItemCommunity Development Director
- Report/Presentation.....Community Development Director
- Questions from Council to Staff Mayor Facilitates
- Call for Motion Mayor & City Council
- Discussion Mayor & City Council
- Action on Motion..... Mayor Facilitates

POLICY RECCOMENDER: The Planning Commission and staff are recommending that the City Council approve a request by CM Properties 94 LP and Kwik Trip, Inc. for a preliminary plat, final plat, and conditional use permit to construct a Kwik Trip gasoline station at the southwest corner of Keats Ave. N. and Hudson Blvd.

FISCAL IMPACT: None – City costs related to the review of the proposed plat and gasoline station are reimbursed by application fees and development escrow. The private parties will be responsible for constructing the improvements associated with the gasoline station.

SUMMARY AND ACTION REQUESTED: CM Properties 94 LP and Kwik Trip, Inc. have submitted an application for a preliminary plat and final plat to subdivide an approximately 6-acre parcel of land for future commercial development, including a Kwik Trip gasoline station. The proposed plat and gasoline station are located at the southwest corner of Keats Avenue

North (CSAH 19) and Hudson Boulevard North in Stage 1 of the I-94 Corridor Planning Area. In addition the request includes a conditional use permit, as gasoline stations (and car washes) are a conditional use in the City's Commercial zoning district.

The Planning Commission and Staff are recommending that the City Council approve the Preliminary Plat, Final Plat and Conditional Use Permit request with 8 conditions of approval through the following motion:

“Move to adopt Resolution No. 2014-61, approving the Lakewood Crossing Preliminary Plat and Final Plat, and approving the Conditional Use Permit for the Kwik Trip gasoline station, subject to 8 conditions of approval.”

BACKGROUND INFORMATION:

Attached is the original detailed Staff Report that was provided to the Planning Commission on 7/14/14 regarding the applicant's request for Preliminary Plat, Final Plat and Conditional Use Permit approval to allow for the construction of a Kwik Trip gasoline station. The Staff Report includes general information about the application, a summary of the relevant planning and zoning issues, a thorough review of the site plan, draft findings, and the staff recommendation to the Planning Commission.

In reviewing the site plan for the proposed gasoline station, the most critical aspect of review related to the proposed accesses to Hudson Boulevard North. Hudson Blvd. is a collector road, requiring required access spacing and additional sensitivity to traffic management and design. In reviewing the proposed accesses provided, staff identified three items where modifications were warranted: 1) the design of the right-out only northeastern access needed modification to improve sightlines, 2) the taper of the proposed westbound left-hand turn lane in Hudson Blvd. needed to be tweaked to include increased tapering due to the 50 mph design speed on Hudson Blvd., and finally 3) the full southwestern access needed to be widened to 40 feet to support a 16-foot inbound lane and two outbound 12-foot lanes (left and right turn lanes). Staff included these modifications and conditions of approval. The applicant noted no opposition to the design modifications. In making their recommendation, the Planning Commission included these conditions in the recommendation for approval.

PLANNING COMMISSION REPORT:

The Planning Commission reviewed the Preliminary Plat, Final Plat and Conditional Use Permit request at its July 14, 2014 meeting and conducted a public hearing at this time. Before the public hearing was opened, Scott Teigen of Kwik Trip, Inc. provided background information about the company and its operational principles. The applicant also noted no opposition to any of the proposed conditions of approval as recommended by staff. After the applicant spoke, the public hearing was opened. During the public hearing, no one from the public spoke. In addition, staff received no written testimony. The Planning Commission then closed the public hearing.

The Planning Commission recommended approval of the Preliminary Plat, Final Plat and Conditional Use Permit as submitted with the 8 conditions of approval as recommended in the Staff Report. The vote to recommend approval was unanimous (Vote: 4-0).

STRENGTHS, WEAKNESSES, OPPORTUNITIES, THREATS:

Strengths: Approval of the Preliminary Plat, Final Plat and Conditional Use Permit will allow for the construction of a Kwik Trip gasoline station at the southwest corner of Keats Ave. N. and Hudson Boulevard. Inclusion of the recommended conditions of approval surrounding access to Hudson Blvd. will ensure proper access spacing and traffic management on the collector road. The proposed Kwik Trip meets all the guidelines in the Lake Elmo Design Guidelines and Standards Manual.

Weaknesses: None

Opportunities: Approval of the Lakewood Crossing Preliminary Plat and Final Plat allows for additional land guided for Commercial to be prepared for development. The proposed commercial development will help finance the City's investments in sewer and water infrastructure.

Threats: None

RECOMMENDATION:

Based on the aforementioned, the Planning Commission and Staff and are recommending that the City Council approve the Preliminary Plat, Final Plat and Conditional Use Permit request with 8 conditions of approval through the following motion:

“Move to adopt Resolution No. 2014-61, approving the Lakewood Crossing Preliminary Plat and Final Plat, and approving the Conditional Use Permit for the Kwik Trip gasoline station, subject to 8 conditions of approval.”

ATTACHMENTS:

1. Resolution 2014-61
2. Staff Report to the Planning Commission, 7/14/14
3. Location Map
4. Application Forms and Project Narrative
5. Kwik Trip Site Plan and Supporting Plans
6. Kwik Trip Architectural Plans
7. Kwik Trip Architectural Renderings

8. Kwik Trip Signage Plan
9. City Engineer Review Memorandum, dated 7/9/14
10. Washington County Review Memorandum, dated 7/9/14
11. MnDOT Review Email and Letter
12. Design Guidelines and Standards - Commercial
13. Site Visit Photos

**CITY OF LAKE ELMO
WASHINGTON COUNTY
STATE OF MINNESOTA**

RESOLUTION NO. 2014-61

***RESOLUTION APPROVING A THE LAKEWOOD CROSSING PRELIMINARY PLAT AND
FINAL PLAT, AND APPROVING A CONDITIONAL USE PERMIT FOR A GASOLINE
STATION IN STAGE 1 OF THE I-94 CORRIDOR PLANNING AREA***

WHEREAS, the City of Lake Elmo is a municipal corporation organized and existing under the laws of the State of Minnesota; and

WHEREAS, CM Properties 94 LP, 3470 Washington Drive, Suite 102, Eagan, MN and Kwik Trip, Inc., PO Box 2107, La Crosse, WI, have submitted an application to the City of Lake Elmo (City) for a Preliminary Plat and Final Plat to be called Lakewood Crossing on an approximately 6 acre in Stage 1 of the I-94 Corridor Planning Area (PID: 34.029.21.44.0007), a copy of which is on file in the City of Lake Elmo Planning and Zoning Department; and

WHEREAS, CM Properties 94 LP, 3470 Washington Drive, Suite 102, Eagan, MN and Kwik Trip, Inc., PO Box 2107, La Crosse, WI, have submitted an application to the City of Lake Elmo (City) for a Conditional Use Permit to construct a gasoline station and car wash; and

WHEREAS, notice has been published, mailed and posted pursuant to the Lake Elmo Zoning Ordinance, Section 154.102; and

WHEREAS, the Lake Elmo Planning Commission held public hearing on July 14, 2014 to consider the Preliminary Plat, Final Plat and Conditional Use Permit request; and

WHEREAS, the Lake Elmo Planning Commission adopted a motion recommending approval of the Preliminary Plat, Final Plat and Conditional Use Permit subject to 8 conditions of approval; and

WHEREAS, the Lake Elmo Planning Commission has submitted its report and recommendation concerning the Preliminary Plat, Final Plat and Conditional Use Permit as part of a memorandum to the City Council from City Planner Nick Johnson for the July 22, 2014 Council Meeting; and

WHEREAS, the City Council reviewed the application at its meeting held on July 22, 2014 and made the following findings of fact:

- 1) That the Lakewood Crossing preliminary and final plat are consistent with the Lake Elmo Comprehensive Plan and the Future Land Use Map for this area.

- 2) That the Lakewood Crossing preliminary and final plat comply with the City's C - Commercial zoning district.
- 3) That the Lakewood Crossing preliminary and final plat comply with the City's subdivision ordinance.
- 4) That the Lakewood Crossing preliminary and final plat meet other City zoning ordinances, such as landscaping, erosion and sediment control, and other ordinances.
- 5) That the Lakewood Crossing preliminary and final plat is consistent with the City's engineering standards provided the plans are updated to address the City Engineer's comments documented in a letter July 9, 2014.
- 6) That the proposed architectural design of the gasoline station, canopy and car wash is consistent with the Lake Elmo Design Guidelines and Standards Manual.
- 7) That the proposed use will not be detrimental to or endanger the public health, safety, comfort, convenience or general welfare of the neighborhood or City.
- 8) That the use or development conforms to the City of Lake Elmo Comprehensive Plan.
- 9) That the use or development is compatible with the existing neighborhood.
- 10) That the proposed use meets all specific development standards for such use listed in Article 7 of the Zoning Code.
- 11) That the proposed use will be designed, constructed, operated and maintained so as to be compatible in appearance with the existing or intended character of the general vicinity and will not change the essential character of the area.
- 12) That the proposed use will not be hazardous or create a nuisance as defined under the Zoning Code to existing or future neighboring structures.
- 13) That the proposed use will be served adequately by essential public facilities and services, including streets, police and fire protection, drainage structures, refuse disposal, water and sewer systems.
- 14) That the proposed use will not create excessive additional requirements at public cost for public facilities and services and will not be detrimental to the economic welfare of the community.
- 15) That the proposed use will not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons, property or the general welfare because of excessive production of traffic, noise, smoke, fumes, glare or odors.
- 16) That vehicular approaches to the property will not create traffic congestion or interfere with traffic on surrounding public thoroughfares.
- 17) That the proposed use will not result in the destruction, loss or damage of a natural or scenic feature of major importance.

NOW, THEREFORE, BE IT RESOLVED THAT the City Council does hereby approve the Lakewood Crossing Preliminary Plat, Final Plat and Conditional Use Permit subject to the following conditions:

- 1) All required modifications to the plans as requested by the City Engineer in a review letter dated July 9, 2014 shall be incorporated into the plans prior to the approval of the building permit for the gasoline station. Required modifications include, but are not limited to, the following:
 - a. The design of the west-bound left turn lane proposed for the southwest access on Hudson Boulevard must be revised to incorporate a 50:1 taper to account for the 50mph design speed of the road as suggested by the City Engineer and the supporting consultant.
 - b. The design of the northeast right-out only access must be revised per the recommendation of the City Engineer and Washington County. The design of the northeast access must be approved by the City prior to the approval of the building permit.
 - c. The width of the western full access road must be modified to 40 feet to allow for one inbound lane (16 feet wide) and two outbound lanes (left and right turn lanes, 12 feet wide).
- 2) The applicant shall provide for an access easement over the entire portion of the shared western driveway located on Lot 1, Block 1 to provide dedicated access for Outlot A.
- 3) The developer shall follow all of the rules and regulations spelled out in the Wetland Conservation Act, and shall acquire the needed permits from Valley Branch Watershed District prior to the commencement of any grading or development activity on the site.
- 4) The applicant shall enter into a maintenance agreement with the City concerning the storm water infiltration areas prior to the issuance of a building permit for the gasoline station.
- 5) The applicant shall pay a parkland dedication fee in the amount of \$9,225.00 prior to the final plat being released for recording.
- 6) The Landscape Plan shall be reviewed by the City's Landscape Consultant. Any modifications requested by the Landscape Consultant shall be incorporated into the Final Landscape Plan prior to the approval of the building permit for the gasoline station.
- 7) The applicant shall secure a sign permit for all signage associated with the proposed gasoline station.
- 8) The applicant shall secure any necessary MnDOT permits for any work impacting the MnDOT right-of-way.

Passed and duly adopted this 22nd day of July 2014 by the City Council of the City of Lake Elmo, Minnesota.

Mike Pearson, Mayor

ATTEST:

Adam Bell, City Clerk



PLANNING COMMISSION
DATE: 7/14/2014
AGENDA ITEM: 4A – PUBLIC HEARING
CASE # 2014-37

ITEM: Kwik Trip Gasoline Station – Preliminary Plat, Final Plat and Conditional Use Permit

SUBMITTED BY: Nick Johnson, City Planner

REVIEWED BY: Kyle Klatt, Community Development Director
Jack Griffin, City Engineer
Ann Pung-Terwedo, Washington County
Greg Malmquist, Fire Chief
Stephen Mastey, Landscape Architecture, Inc.

SUMMARY AND ACTION REQUESTED:

The Planning Commission is being asked to consider a request from CM Properties 94 LP and Kwik Trip, Inc. to plat a portion of a property located immediately west of Keats Avenue (CSAH 19) and immediately south of Hudson Boulevard North within Stage 1 of the I-94 Corridor Planning Area with the intent of constructing a Kwik Trip gasoline station. In addition to the platting application, the request includes application for a Conditional Use Permit, as gasoline stations and car washes require a conditional use permit in the Commercial zoning district. Staff is recommending approval of the request subject to compliance with 8 conditions as noted in this report.

GENERAL INFORMATION

Applicant: CM Properties 94 Limited Partnership (Bruce Miller); 3470 Washington Drive, Suite 102, Eagan, MN 55112 and Kwik Trip, Inc. (Scott Teigen); PO Box 2107, La Crosse, WI 54603.

Property Owners: CM Properties 94 LP (Bruce Miller); 3470 Washington Drive, Suite 102, Eagan, MN 55112

Location: Part of Sections 34, Township 29 North, Range 21 West in Lake Elmo, immediately west of Keats Avenue North (CSAH 19), immediately south of Hudson Boulevard North and immediately north of Interstate Highway 94. PID Number: 34.029.21.44.0007.

Request: Application for preliminary plat, final plat and conditional use permit approval of a gasoline station in Stage 1 of the I-94 Corridor Planning Area.

Existing Land Use and Zoning: Vacant agricultural land. Current Zoning: RT – Rural Development Transitional Zoning District; Proposed Zoning: C – Commercial District

Surrounding Land Use and Zoning: North – vacant/agricultural land, guided for C - Commercial; west – vacant/agricultural land, guided for C- Commercial;

south – Interstate Highway 94; east – Keats Avenue North (CSAH 19)

Comprehensive Plan: Commercial

History: Utilities (sewer and water) were extended to the subject property as part of the Section 34 Public Utility Project, which is now complete.

Deadline for Action: Application Complete – 5/28/2014
60 Day Deadline – 7/28/14
Extension Letter Mailed – No
120 Day Deadline – 9/26/14

Applicable Regulations: Chapter 153 – Subdivision Regulations
§154.106 Conditional Use Permits
Article 12 – Commercial Districts (C)
Article 7 – Specific Development Standards (§154.305)
§150.270 Storm Water, Erosion, and Sediment Control
§150.035 Lighting, Glare Control and Exterior Lighting Standards

REQUEST DETAILS

The City of Lake Elmo has received a request from CM Properties 94 Limited Partnership and Kwik Trip, Inc. for a preliminary and final plat to subdivide 5.87 acres of land located within Stage 1 of the I-94 Corridor Planning Area into 2 lots with the intention of constructing a Kwik Trip gasoline station. The proposed plat would be located on property currently owned by CM Properties 94 Limited Partnership, and would be located immediately west of Keats Avenue (CSAH 19), immediately south of Hudson Boulevard, and immediately north of Interstate Highway 94. The parcel has historically been used for agricultural purposes.

The proposed plat and conditional use permit application have been developed in response to the City's Comprehensive Plan, which identifies the applicant's property for Commercial development. The proposed Kwik Trip would be the first commercial development in the eastern portion of Stage 1 of the I-94 Corridor Planning Area since the adoption of the City's current Land Use Plan.

In terms of access, the proposed site plan shows two connections to Hudson Blvd. along the northern boundary of the site. To account for the appropriate access spacing necessary for Hudson Blvd., the applicants are showing the western access to be a full access, supporting all turning movements, while the eastern access shall serve only as a right-out exit to Hudson. The City Engineer has reviewed the proposed access locations and found them to be acceptable with the proposed access spacing.

PLANNING AND ZONING ISSUES

The proposed Kwik Trip site is guided for Commercial development in the City's Comprehensive Plan. The overall subdivision plan has therefore been prepared in order to comply with the district standards for the Commercial zoning district in terms of lot size, lot widths, building setbacks, and other design criteria. It should be noted that while a neighborhood convenience store is a permitted use in the Commercial zoning district, gasoline stations are considered a conditional use. In addition, car washes, while accessory to the gasoline station use, also are a conditional use. For these reasons,

the applicant must apply for a conditional use permit for approval of the gasoline station and car wash land uses.

The proposed plat would subdivide the subject property into two parcels; one parcel (Lot 1, Block 1) for the proposed Kwik Trip and an outlot (Outlot A) for a future commercial development adjacent to the proposed Kwik Trip. The proposed size of the site (Lot 1) for the Kwik Trip is 89,355 square feet, or 2.05 acres. The remaining area for Outlot A would be 3.82 acres, which will likely be used for future commercial development and additional storm water management. It should be noted that the applicant is requesting to proceed with Preliminary Plat and Final Plat concurrently. From staff's perspective, if the applicant is meeting all the submission requirements for both preliminary and final plat, and the proposed plat is limited in scope and scale, then proceeding through both steps concurrently is acceptable. To better define what constitutes a project of limited scope or scale in staff's judgment, a limited scale plat would involve subdivision of a limited number of parcels without the need for major infrastructure improvements (utility extensions, new public roads, etc.). In this context, the proposed plat is of limited scale and scope according to staff.

In reviewing the applicable requirements from the City's zoning and subdivision regulations, staff has found that the proposed project is in conformance with these requirements. More specifically, staff reviewed the proposed gasoline station in light of the following:

- **Lot Size.** The proposed lot being platted for the commercial use is 89,355 square feet, which meets the minimum lot size requirement of 20,000 square feet per the Commercial zoning district.
- **Building Setback Requirements.** The proposed gasoline station/convenience store meets the required building setbacks (front: 30 feet, corner side yard: 25 feet, interior side yard: 10 feet and rear yard: 10 feet) for the Commercial district. In addition, the car wash meets the 10-foot setback requirement for accessory buildings.
- **Parking Setbacks.** The proposed parking stalls all meet the required parking setback requirements per the Commercial zoning district.
- **Impervious Surface.** The maximum amount of impervious surface in the Commercial zoning district is 75%. The proposed project complies with this requirement, as there is 62,010 square feet of pervious surface proposed, which is 69% of lot area.
- **Parking Stalls.** Per the City's off-street parking requirements, gasoline stations are required to have one parking stall for every 250 square feet of gross floor area used for sales. Using this calculation, staff determined that 23 parking stalls would be required. The applicants are proposing 28 standard and 2 handicap parking stalls, easily meeting the City's parking requirement.
- **Specific Development Standards – Gasoline Stations and Car Washes.** The City adopted specific development standards (§154.305) for gasoline stations and car washed, both of which are included in this proposal. In reviewing these standards, staff found that the application was in compliance with the specific development standards related to these two uses, including the following:
 - **Access to a Collector.** The proposed gas station meets the minimum lot size requirements and has access to a collector street (Hudson Blvd.).

- **Canopies.** The proposed canopy meets the required setback of 20 feet from all property lines. In addition the canopy columns reflect the design and building materials of the principal building.
- **Outdoor Displays.** Proposed outdoor merchandise is either located under the canopy or adjacent to the principal structure, meeting all setback requirements.
- **Car Wash Enclosure.** The proposed car wash has doors that enclose the car wash while in operation.
- **Sounds from Speakers.** The car wash area is not within close proximity to any residential districts or properties. Any sounds from speakers related to the car wash should not negatively impact any residential properties.

Based on Staff's review of the Site Plan and Supporting Plans for the proposed gasoline station and car wash, the applicant has demonstrated compliance with all applicable Zoning Code requirements related to gasoline stations in Commercial districts.

In terms of parkland dedication, the Subdivision Ordinance requires any property being developed in Commercial districts to pay a fee as determined by Resolution by the City Council. Currently, the fee for parkland dedication for commercial properties is \$4,500 per acre of land subdivided for commercial use. In this case, the applicant is proposing to subdivide 2.05 acres of land for commercial use. The remaining land is being left in outlot. Using the \$4,500 fee per acre, the applicant will be required to provide \$9,225 (2.05 acres x \$4,500 per acre = \$9,225) to the City to comply with the City's parkland dedication requirement. Staff would recommend that this fee be provided to the City in advance of releasing the final plat for recording (Condition #4).

Finally, as the gasoline station and car wash is considered a conditional use in the Commercial zoning district, the applicant has applied for a conditional use permit (CUP). Per the procedure described in Article 3 of the Zoning Code (§154.106), the City is required to make findings related to the proposed conditional use. Generally speaking, these findings are required to ensure that no negative impacts to the adjacent properties or broader community are associated with the proposed use. Staff reviewed all 12 required findings per the procedure for CUPs and found that the proposed use meets all the criteria to be granted a conditional use permit. As part of the draft findings for recommending approval of the request, staff has included the required findings that relate to the conditional use permit.

REVIEW AND ANALYSIS

City Staff has reviewed the proposed plat and conditional use permit, which has gone through multiple versions in advance of the formal application being accepted as complete by the City. As currently submitted, the proposed plat will meet all applicable City requirements for conditional approval, and any deficiencies or additional work that is needed is identified in the recommended conditions of approval.

Also, the City has received a detailed list of comments from the City Engineer and Washington County concerning the proposed gasoline station, in addition to informal review by the City's Fire Chief and Building Official. The written review comments from the City Engineer and Washington County are attached for consideration by the Planning Commission.

In addition to the general comments that have been provided in the preceding sections of this report, Staff would like the Planning Commission to consider the following discussion areas as well:

- **Comprehensive Plan.** The proposed subdivision and commercial use is consistent with the Lake Elmo Comprehensive Plan for this area. The parcel is within Stage 1 of the I-94 Corridor Planning Area and has access to City sanitary sewer and water.
- **Zoning.** The proposed zoning for the Lakewood Crossing site will be C – Commercial. The submitted development plans demonstrate compliance with the City’s Commercial zoning district standards. Neighborhood Convenience Store is a permitted use in the Commercial zoning district, while gasoline station and car wash are conditional uses. The applicants have applied for a conditional use permit in conjunction with the platting request.
- **Subdivision Requirements.** The City’s Subdivision Ordinance includes a fairly lengthy list of standards that must be met by all new subdivisions, and include requirements for blocks, lots, easements, erosion and sediment control, drainage systems, monuments, sanitary sewer and water facilities, streets, and other aspects of the plans. Staff, as well as the City Engineer, have not identified any existing conflicts with the City’s Subdivision Ordinance.
- **Access.** Staff has the following review comments related to access:
 - **Eastern Right-Out Only Access.** To provide access to the proposed gasoline station, the applicant is proposing two access locations (west and east) along Hudson Boulevard. The City Engineer reviewed the proposed access locations and found the spacing to be acceptable. However, staff is recommending that the eastern access be permitted only as a right-out only. The applicants have updated their plans to reflect this design. After reviewing the proposed right-out eastern access, staff and Washington County found that the proposed right turn is too sharp, necessitating a different design or an acceleration lane on Hudson. Staff is comfortable working with the applicant to arrive at the best suited design for the right-out eastern access (Condition #1b).
 - **Left Turn Lane on Hudson Blvd.** In addition to the right-out access (eastern access), City staff also requested a left turn lane to serve the full western access of the gas station. It is important to note that Hudson Boulevard is classified as a collector road and it anticipated to serve a substantial amount of traffic as the I-94 Corridor develops. The City does not plan to expand Hudson to a 4-lane roadway. In addition, the western full access will also be designed to serve Outlot A in addition to the gasoline station. For these reasons, staff recommended a left turn lane on westbound Hudson Blvd for the full shared access between the proposed gasoline station and Outlot A. The applicants have provided the left turn lane to respond to the staff request. However, in reviewing the proposed design, the City Engineer found that the taper is too abrupt for a 50mph design speed. Staff is recommending that the taper be enlarged to allow for a more appropriate design for the existing conditions of Hudson Boulevard (Condition #1a).
 - **Western Shared Access.** Finally, staff is also recommending that the western shared full access be expanded to 40 feet in width (Condition #1c), which would allow for both right and left turn lanes to exit the site. The current width as proposed is approximately 36 feet. The requested expansion would include approximately 4 feet of additional width to allow better circulation in and out of the proposed gasoline station, as well as whatever future use is located on Outlot A. In addition to the

requested modification to the design of the western access drive, staff is also recommending that the applicant provide an access easement along the portion of shared access driveway that is located on the Kwik Trip site (Condition #2). The goal is to ensure that the future use on Outlot A has dedicated full access to the western driveway.

- **Landscaping.** The applicant has submitted a Landscape Plan (Sheet L1) as part of the proposed gasoline station. The City's landscaping provisions (§154.258) require 1 tree per 50 feet of street frontage. In addition to street plantings, the City's requirements include 5 trees per developed acre. Given these requirements, staff calculated that 15 street trees are required (based on approximately 780 feet of street frontage on Hudson and Keats) and 10 interior trees are required (5 trees per acre). The total required amount of trees per staff's calculation is 25 trees. The applicants are proposing 25 trees of both deciduous and coniferous varieties, meeting the City's requirement. It should be noted that the coniferous trees are being utilized to screen the trash or refuse area. In addition to the trees, the applicants are proposing a variety of shrubs and planted beds. Finally, staff has distributed the Landscape Plan to the City's Landscape Consultant, Stephen Mastey. Staff would recommend as a condition of approval (Condition #5) that the Landscape Plan be reviewed by the Landscape Consultant. In addition to review, staff would recommend that any modifications related to plant species or location be incorporated in the Final Landscape Plan prior to issuance of the building permit.
- **Theming and Branding.** As part of the improvements proposed for the gasoline station, the applicants are proposing to install decorative fencing that is consistent with the design recommended by the City's Theming and Branding Study. The decorative fencing is a white three-rail fence that is often used on agricultural properties for horses. The applicants are proposing to install this fencing along the northeastern portions of the property, which should be visible from Keats Ave. N. (CSAH 19).
- **Architectural Design Review.** City staff reviewed the architectural plans and architectural renderings of the proposed Kwik Trip gasoline station and car wash for consistency with the Lake Elmo Design Guidelines and Standards Manual (Attachment #11). Overall, Staff finds that the proposed building meets the guidelines and standards for Commercial development contained within the manual and would offer the following review comments:
 - **Building Materials.** The proposed building is chiefly constructed of red modular brick. Accent materials include limestone and tan brick soldier course. The proposed building materials meet the City standards for building materials in the Commercial district.
 - **Streetscape.** A significant portion of the proposed landscaping are located along the public right-of-way for Hudson Boulevard and Keats Avenue (CSAH 19). In addition, the applicants are proposing to install a three-rail decorative fence that is consistent with the City's Branding and Theming Study. In staff's judgment, the proposed design will be contributing to a positive streetscape.
 - **Storage Areas.** The Manual requires that trash collection area be located out of the view of the public right-of-way. The proposed site plan indicated that the trash

collection area will be enclosed and located in the far eastern portion of the site. The proposed trash collection area meets the intent of the standard.

- **Entry.** The proposed convenience store has accent treatments of both limestone and tan brick soldier course around the entryway. In addition, the peaked roof at the entry provides an additional architectural feature.
- **Photometric Plan.** To demonstrate compliance with the City's sign and exterior lighting ordinances, the applicant has submitted a Photometric Plan. The Photometric Plan includes information about the lighting fixtures proposed for the site. In addition, it provides information about the light intensity, measuring the foot candles across the entire site and beyond. Based on the plan, the applicants are proposing pole lights that are 18 feet in height and that include a 90 degree cutoff. In addition, the proposed lighting internal to the canopy are recessed LEDs. From staff's review of the photometric plan, the proposed lighting is consistent with the City's ordinance, as the plan does an effective job of limiting light from being directed outside of the property. Generally speaking, the foot-candle reading at or near the property boundaries are all very low and are consistent with the ordinance. In addition, the 18-foot pole lights do not exceed the maximum height allowed for such lighting (30 feet maximum height for lights in non-residential districts with cutoff 90 degrees or less). When City staff reviews the building permit for the proposed gasoline station, a more in-depth review of the proposed lighting will be completed to ensure conformance to the City's ordinance.
- **Signage Plan.** As part of the plat and conditional use permit applications, the applicants have submitted a signage plan. The signage plan includes multiple sign types, including wall signs, canopy signs and a monument or ground sign. From a high level review perspective, it appear that the proposed signage will meet the City's Sign Ordinance. However, the applicant will be required to submit sign permits for the proposed signage, at which time staff will conduct a more in-depth review. Staff is recommending that the applicant submit sign permits for any proposed signage (Condition #6)
- **City Engineer Review.** The City Engineer has provided the Planning Department with a detailed comment letter (Attachment #8) as a summary of his review of the proposed gasoline station. The majority of the Engineer's comments relate to traffic and access management. In addition, the Engineer identifies other aspects of the proposed plan that currently do not meet City Engineering Standard, including utilities, grading and erosion control. As a condition of approval, staff is recommending that all modification requested by the City Engineer in his memo dated 7/9/14 be incorporated into Final Plans prior to the approval of the building permit (Condition #1).
- **Fire Department Review.** The Fire Chief has reviewed the Utility Plan for the proposed gasoline station and found the proposed hydrant location to be acceptable. Additional review by the Fire Chief will be required upon the submission of the building permit.
- **Washington County Review.** County Staff has reviewed the Lakewood Crossing plat and proposed gasoline station, providing comments in a written memorandum dated 7/9/14 (Attachment #9). In the memo, they note that no additional right-of-way for Keats Ave. N. is required as part of the proposed plat. In addition, the County recommends that the City monitor the traffic situation along the Hudson Boulevard corridor with the possibility that the

eastern right-out only access be removed at some point in the future. To account for this consideration, the City Engineer has recommended that a center median be installed adjacent to the right-out only access at some point in the future as opposed to eliminating the access. Finally, the memo from Washington County also includes notification that the Hudson Blvd. corridor in Lake Elmo is currently being considered as one the alternate alignments (D2 Alignment) for the Gateway Corridor transit project to be constructed at some point in the future. City staff have also notified the applicants of this possibility in previous meetings.

- ***MnDOT Review.*** The Minnesota Department of Transportation owns a substantial amount of right-of-way surrounding the proposed gasoline station site. In a review email dated 7/10/14, MnDOT noted that the applicant may not include any ponding within the MnDOT right-of-way. In addition, any work affecting or impacting the MnDOT right-of-way will require a permit. Staff is recommending that the applicant obtain necessary permits from MnDOT for any work impacting the MnDOT right-of-way (Condition #8)
- ***Watershed Districts.*** The project area lies within the Valley Branch Watershed District (VBWD). It should be noted that the developer must meet all the rules of the Wetland Conservation Act and VBWD and will need to secure permits from the VBWD in order to proceed with the development as planned (Condition #3).

Based on the above Staff report and analysis, Staff is recommending approval of the preliminary plat, final plat and conditional use permit with 8 conditions of approval. The recommended conditions are as follows:

Recommended Conditions of Approval:

- 1) All required modifications to the plans as requested by the City Engineer in a review letter dated July 9, 2014 shall be incorporated into the plans prior to the approval of the building permit for the gasoline station. Required modifications include, but are not limited to, the following:
 - a. The design of the west-bound left turn lane proposed for the southwest access on Hudson Boulevard must be revised to incorporate a 50:1 taper to account for the 50mph design speed of the road as suggested by the City Engineer and the supporting consultant.
 - b. The design of the northeast right-out only access must be revised per the recommendation of the City Engineer and Washington County. The design of the northeast access must be approved by the City prior to the approval of the building permit.
 - c. The width of the western full access road must be modified to 40 feet to allow for one inbound lane (16 feet wide) and two outbound lanes (left and right turn lanes, 12 feet wide).
- 2) The applicant shall provide for an access easement over the entire portion of the shared western driveway located on Lot 1, Block 1 to provide dedicated access for Outlot A.

- 3) The developer shall follow all of the rules and regulations spelled out in the Wetland Conservation Act, and shall acquire the needed permits from Valley Branch Watershed District prior to the commencement of any grading or development activity on the site.
- 4) The applicant shall enter into a maintenance agreement with the City concerning the storm water infiltration areas prior to the issuance of a building permit for the gasoline station.
- 5) The applicant shall pay a parkland dedication fee in the amount of \$9,225.00 prior to the final plat being released for recording.
- 6) The Landscape Plan shall be reviewed by the City's Landscape Consultant. Any modifications requested by the Landscape Consultant shall be incorporated into the Final Landscape Plan prior to the approval of the building permit for the gasoline station.
- 7) The applicant shall secure a sign permit for all signage associated with the proposed gasoline station.
- 8) The applicant shall secure any necessary MnDOT permits for any work impacting the MnDOT right-of-way.

DRAFT FINDINGS

Staff is recommending that the Planning Commission consider the following findings with regards to the proposed Lakewood Crossing preliminary plat, final plat and conditional use permit:

- That the Lakewood Crossing preliminary and final plat is consistent with the Lake Elmo Comprehensive Plan and the Future Land Use Map for this area.
- That the Lakewood Crossing preliminary and final plat complies with the City's C – Commercial zoning district.
- That the Lakewood Crossing preliminary and final plat complies with the City's subdivision ordinance.
- That the Lakewood Crossing preliminary and final plat meets other City zoning ordinances, such as landscaping, erosion and sediment control, and other ordinances.
- That the Lakewood Crossing preliminary and final plat is consistent with the City's engineering standards provided the plans are updated to address the City Engineer's comments documented in a letter July 9, 2014.
- That the proposed architectural design of the gasoline station, canopy and car wash is consistent with the Lake Elmo Design Guidelines and Standards Manual.
- That the proposed use will not be detrimental to or endanger the public health, safety, comfort, convenience or general welfare of the neighborhood or City.
- That the use or development conforms to the City of Lake Elmo Comprehensive Plan.
- That the use or development is compatible with the existing neighborhood.
- That the proposed use meets all specific development standards for such use listed in Article 7 of the Zoning Code.

- That the proposed use will be designed, constructed, operated and maintained so as to be compatible in appearance with the existing or intended character of the general vicinity and will not change the essential character of the area.
- That the proposed use will not be hazardous or create a nuisance as defined under the Zoning Code to existing or future neighboring structures.
- That the proposed use will be served adequately by essential public facilities and services, including streets, police and fire protection, drainage structures, refuse disposal, water and sewer systems.
- That the proposed use will not create excessive additional requirements at public cost for public facilities and services and will not be detrimental to the economic welfare of the community.
- That the proposed use will not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons, property or the general welfare because of excessive production of traffic, noise, smoke, fumes, glare or odors.
- That vehicular approaches to the property will not create traffic congestion or interfere with traffic on surrounding public thoroughfares.
- That the proposed use will not result in the destruction, loss or damage of a natural or scenic feature of major importance.

RECCOMENDATION:

Staff recommends that the Planning Commission recommend approval of the Lakewood Crossing Preliminary Plat and Final Plat. In addition, staff recommends that the Planning Commission recommend approval of the Conditional Use Permit for the proposed Kwik Trip gasoline station and car wash. Staff is recommending approval of the requests subject to 8 conditions of approval as listed in the Staff report. Suggested motion:

“Move to recommend approval of the Lakewood Crossing Preliminary and Final Plat and Conditional Use Permit with the 8 conditions of approval as drafted by Staff based on the findings of fact listed in the Staff Report.”

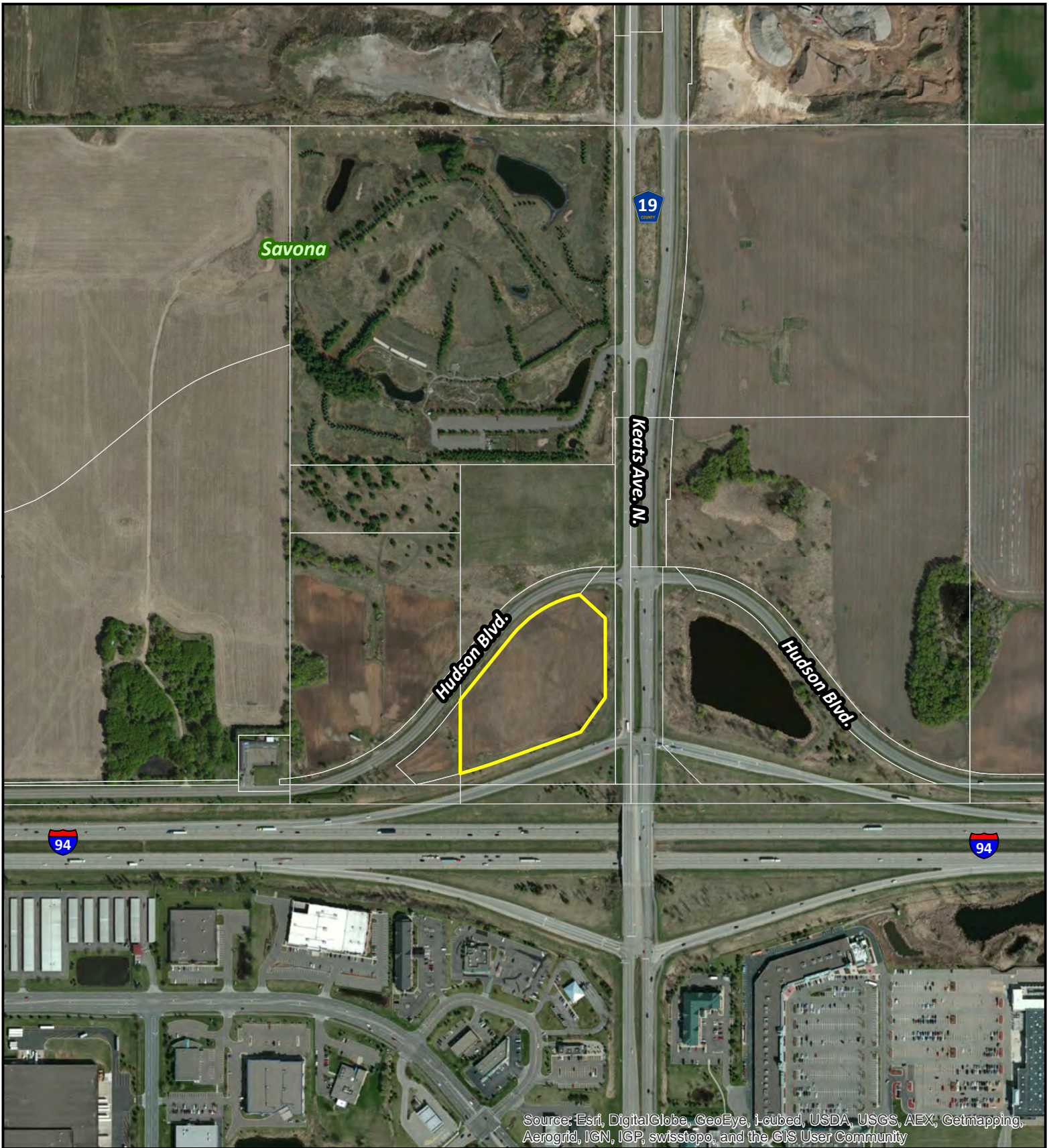
ATTACHMENTS:

1. Location Map
2. Application Forms and Narrative
3. Lakewood Crossing Preliminary and Final Plat
4. Kwik Trip Site Plan and Supporting Plans (12 sheets)
5. Kwik Trip Architectural Plans
6. Kwik Trip Architectural Renderings
7. Kwik Trip Signage Plan
8. City Engineer Review Memorandum, dated 7/9/14

9. Washington County Review Memorandum, dated 7/9/14
10. MnDOT Review Email, dated 7/10/14
11. Design Guidelines and Standards - Commercial

ORDER OF BUSINESS:

- IntroductionPlanning Staff
- Report by StaffPlanning Staff
- Questions from the Commission Chair & Commission Members
- Open the Public HearingChair
- Close the Public Hearing.....Chair
- Discussion by the Commission Chair & Commission Members
- Action by the Commission Chair & Commission Members



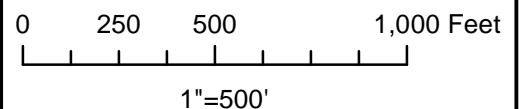
Site Plan: 34.029.21.44.0007



Data Source: Washington County, MN
7-10-2014



34.029.21.44.0007
(Proposed Kwik Trip Site)



Date Received: _____
Received By: _____
LU File #: _____



651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

PRELIMINARY PLAT APPLICATION

Applicant: CM PROPERTIES 94, LP
Address: 3470 WASHINGTON DRIVE, SUITE 102, ENSTADT, MN 55122
Phone #: 651-452-3303
Email Address: BRILLER@MFCPROPERTIES.COM

Fee Owner: _____
Address: SAME
Phone #: _____
Email Address: _____

Property Location (Address and Complete (long) Legal Description):
NNQ OF I-94/KENT AVENUE - SEE ATTACHED SURVEY
FOR LEGAL

General information of proposed subdivision:
1 LOT, 2 OUTLOT COMMERCIAL SUBDIVISION TO ACCOMMODATE
A KWIK TRIP.

Conducted pre-application meeting with Staff?



Yes



No

In signing this application, I hereby acknowledge that I have read and fully understand the applicable provisions of the Zoning Ordinance and current administrative procedures. I further acknowledge the fee explanation as outlined in the application procedures and hereby agree to pay all statements received from the City pertaining to additional application expense.

Signature of applicant: [Signature] Date: 4-7-14

City Use Only

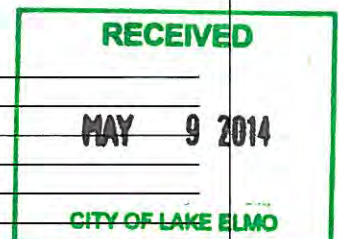
Planning: Zoning District: _____

Reviewed by: _____ Date: _____

Subject to the following conditions: _____

Engineering: Reviewed by: _____ Date: _____

Subject to the following conditions: _____





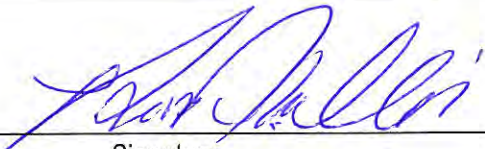
Lake Elmo City Hall
651-747-3900
3800 Lavene Avenue North
Lake Elmo, MN 55042

AFFIRMATION OF SUFFICIENT INTEREST

I hereby affirm that I am the fee title owner of the below described property or that I have written authorization from the owner to pursue the described action.

Name of applicant CM Properties 94, LP
(Please Print)

Street address/legal description of subject property NWQ of I-94/KUMS
AVENUE. SEE ATTACHED SURVEY FOR LEGAL.

 4-7-14
Signature Date

If you are not the fee owner, attach another copy of this form which has been completed by the fee owner or a copy of your authorization to pursue this action.

If a corporation is fee title holder, attach a copy of the resolution of the Board of Directors authorizing this action.

If a joint venture or partnership is the fee owner, attach a copy of agreement authorizing this action on behalf of the joint venture or partnership.



Lake Elmo City Hall
651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

ACKNOWLEDGEMENT OF RESPONSIBILITY

This is to certify that I am making application for the described action by the City and that I am responsible for complying with all City requirements with regard to this request. This application should be processed in my name and I am the party whom the City should contact regarding any matter pertaining to this application.

I have read and understand the instructions supplied for processing this application. The documents and/or information I have submitted are true and correct to the best of my knowledge. I will keep myself informed of the deadlines for submission of material and of the progress of this application.

I understand that this application may be reviewed by City staff and consultants. I further understand that additional information, including, but not limited to, traffic analysis and expert testimony may be required for review of this application. I agree to pay to the City upon demand, expenses, determined by the City, that the City incurs in reviewing this application and shall provide an escrow deposit to the City in an amount to be determined by the City. Said expenses shall include, but are not limited to, staff time, engineering, legal expenses and other consultant expenses.

I agree to allow access by City personnel to the property for purposes of review of my application.

Signature of applicant  Date 4-7-14

Name of applicant CM PROPERTIES 94, LP Phone 651-452-3303
(Please Print)

Name and address of Contact (if other than applicant) _____

Bruce Milne
3470 WISITROBEN DRIVE, SUITE 102
EDINA, MN 55122

Date Received: _____
Received By: _____
Permit #: _____



651-747-3900
3800 Laveme Avenue North
Lake Elmo, MN 55042

LAND USE APPLICATION

☐ Comprehensive Plan ☐ Zoning District Amend ☐ Zoning Text Amend ☐ Variance*(see below) ☐ Zoning Appeal

☒ Conditional Use Permit (C.U.P.) ☐ Flood Plain C.U.P. ☐ Interim Use Permit (I.U.P.) ☐ Excavating/Grading

☐ Lot Line Adjustment ☐ Minor Subdivision

Applicant: KWIK TRIP - BRADFORD FRY
Address: PO BOX 2107 LACROSSE WI 54603
Phone #: 608 793 6414
Email Address: bfry@kwiktrip.com

Fee Owner: _____
Address: _____
Phone #: _____
Email Address: _____

Property Location (Address and Complete (long) Legal Description: LOT 1 BLOCK 1

Detailed Reason for Request: CONSTRUCTION OF C-STONE W/ CANVASIT
AND FUEL

*Variance Requests: As outlined in Section 301.060 C. of the Lake Elmo Municipal Code, the applicant must demonstrate practical difficulties before a variance can be granted. The practical difficulties related to this application are as follows:

In signing this application, I hereby acknowledge that I have read and fully understand the applicable provisions of the Zoning ordinance and current administrative procedures. I further acknowledge the fee explanation as outlined in the application procedures and hereby agree to pay all statements received from the City pertaining to additional application expense.

Signature of applicant: [Signature] Date: 2014-05-01

City Use Only

Planning: Zoning District: _____ Date: _____

Reviewed by: _____ Subject to the following conditions: _____

Engineering: Reviewed by: _____ Date: _____

Subject to the following conditions: _____



City of Lake Elmo
Nick Johnson
3800 Laverne Avenue North
Lake Elmo, MN 55042

May 1, 2014

Mr. Johnson:

This letter is intended to accompany our submittal for our application to the City of Lake Elmo for the requested Conditional Use Permit for our proposed project at the southeast quadrant of Hudson Boulevard and Keats Avenue North. Included in the submittal is the Land Use Application along with the required civil and architectural plans for Lot 1, Block 1

Kwik Trip, Inc. is proposing the construction of a convenience store, carwash, and fueling facility at the currently undeveloped site. Attached are 50 copies (full, 11x17) of the existing conditions plan, site plan, keynote plan, grade and erosion control plan, utility plan, SWPPP, details, landscape plan, photometric plan, building floor plan and elevation, carwash floor plan and elevation, canopy elevation. The storm water management calculations are also attached.

The proposed method of operation for this development will be consistent with that for our existing convenience stores within the area. The requested hours of operation will be 24 hours for all uses. The type of products that will be sold will be similar to that of our existing stores: carwash, gasoline, in line diesel, E-85, groceries, bakery and dairy, hot and cold food and beverages, tobacco products, lotto, convenience store merchandise, ice, and propane. The outside merchandising of products is being requested next to the store (ice and propane) and miscellaneous merchandising under the gas canopy. The proposed store is projected to have between 15-20 full and part time employees, with 2-8 on staff at any given time.

The proposed architectural plan will consist of a brick facade with standing seam metal roof. The building and canopy fascia will tie in with franchise colors. The project will be constructed to the specifications set forth by the USGBC in order to be considered as a LEED "Green Build" project.

Kwik Trip would be happy to provide any additional information or answer any questions or concerns the City Lake Elmo may have with our submission. Please feel free to call Scott Teigen (715 236 6626) or myself with any questions.

Sincerely,

Bradford Fry, PE
Store Engineering
608 793 6414



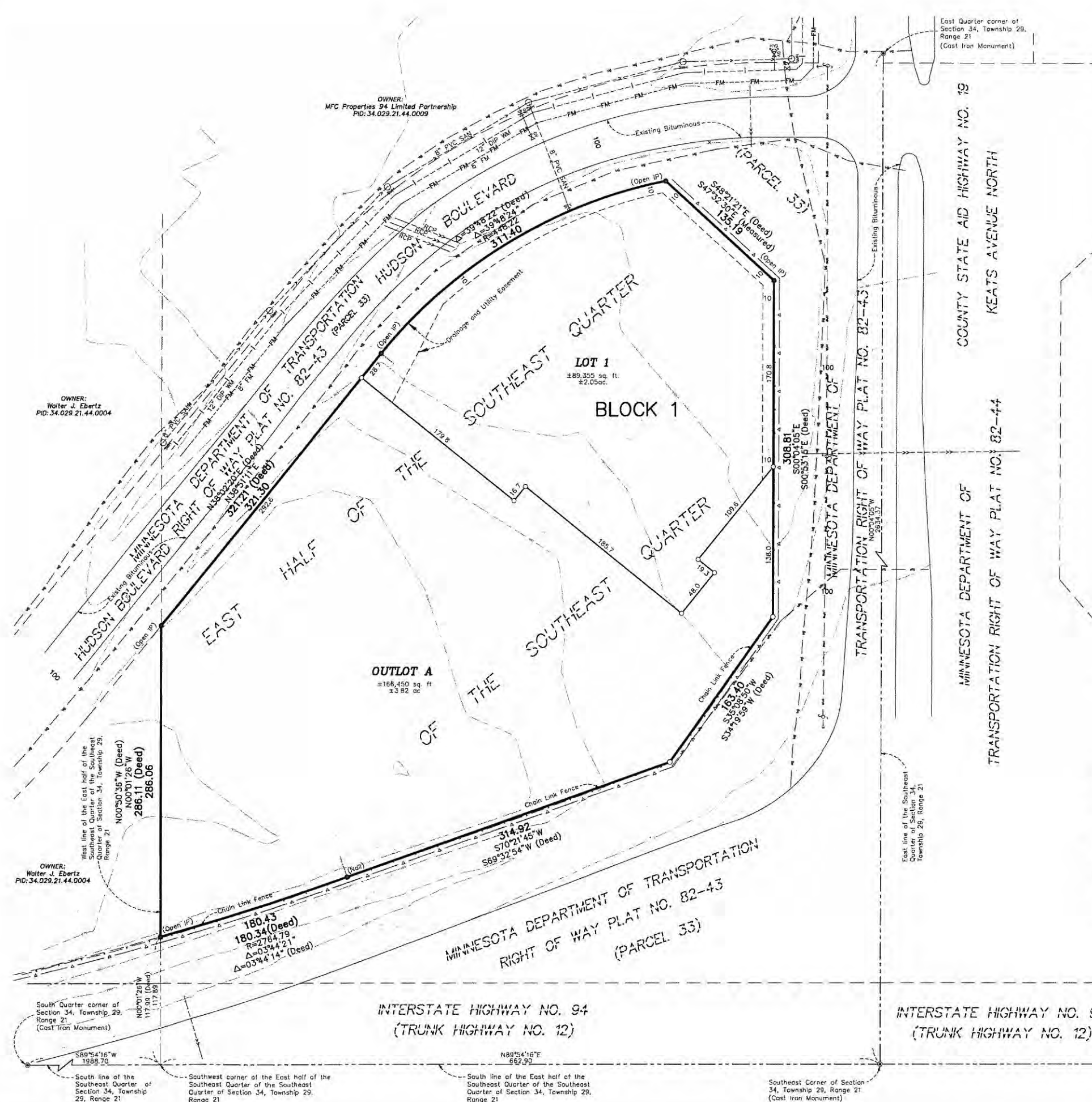
Our Mission: "To serve our customers and community more effectively than anyone else by treating our customers, co-workers and suppliers as we, personally, would like to be treated, and to make a difference in someone's life."

Kwik Trip & Kwik Star Stores • Tobacco Outlet Plus • Convenience Transportation, LLC • Hearty Platter Restaurants & Cafés



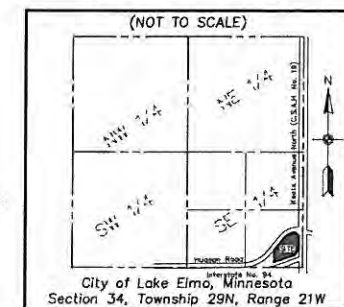
1. Land Use Application along with a check for \$1050 is attached
2. A. Seller: Bruce Miller, CM Properties 94 Limited Partnership, 3470 Washington Dr, Suite 102, Eagan, MN 55112
Contract applicant: Bradford Fry, Kwik Trip, 1626 Oak St, La Crosse, WI 54601
Engineer: Joe Radach, Carlson McCain, 248 Apollo Dr, Suite 100, Lino Lakes, MN 55014
B. Lot 1, Block 1, Lakewood Crossing (to be recorded), Zone C, 75,454 sf
C. The current property is a green field and is undeveloped
D. The proposed use of the property will be for the construction and operation of a convenience store with fuel and a double bay carwash. The site will have access into the development (one shared with future development) and 2 detention ponds (sediment and infiltration). We operate our facilities 24 hours a day, 7 days a week with approximately 15 to 20 employees.
E. The proposed use will provide fueling and carwash services to the community as well as a large array of products within the store. Many of the products sold in the store are created by our kitchens, dairy, bakery, and commissary located in La Crosse, WI. The development will have adequate access with 2 driveways, and sufficient parking in front of and along side the convenience store.
3. A copy of the current title commitment is attached
4. Address labels are attached from Washington County
5. 50 copies of the requested plan sets are attached
6. 50 copies of the landscape plan are attached
7. 50 copies of the architectural plans for the convenience store, carwash, and canopy





LAKEWOOD CROSSING

VICINITY MAP



SITE DATA

TOTAL SITE AREA	±5.87 AC.
TOTAL OUTLOT AREA	±3.82 AC.
TOTAL LOT AREA	±2.05 AC.
LOT 1	±89,355 S.F.
TOTAL NUMBER OF LOTS (including Outlot)	2
EXISTING ZONING	RT
UTILITIES	AVAILABLE
MINIMUM RT ZONING SETBACK DATA:	
FRONT	30 FT.
SIDE	10 FT.
CORNER	25 FT.
REAR	40 FT.

PARCEL DESCRIPTION

(Per Title Commitment File No. KTM00248, dated February 05, 2014 at 4:30 P.M. prepared by The Title Company, Inc., issuing agent for Chicago Title Insurance Company)

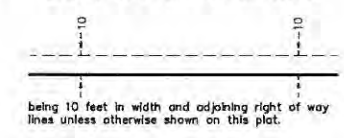
Part of the East half of the Southeast Quarter of the Southeast Quarter of Section 34, Township 29, Range 21, Washington County, Minnesota, described as follows: Beginning at a point on the West line of said East half of the Southeast Quarter of the Southeast Quarter of Section 34, distant 117.99 feet North of the Southwest corner thereof, said point also being in the boundary of Parcel 33, Minnesota Department of Transportation Right of Way Plat Numbered 82-43, according to the recorded plat thereof, thence North 0 degrees 50 minutes 36 seconds West, along said West line 286.11 feet to said Parcel 33 boundary, thence North 38 degrees 02 minutes 20 seconds East, along said boundary, 321.21 feet; thence Northeast, along said boundary, on a tangential curve concave to the Southeast, central angle 39 degrees 48 minutes 22 seconds and radius 448.22 feet, 311.40 feet; thence South 48 degrees 21 minutes 21 seconds East, along said boundary, 135.19 feet; thence South 0 degrees 53 minutes 15 seconds East, along said boundary, 308.81 feet; thence South 34 degrees 19 minutes 58 seconds West, along said boundary, 163.40 feet; thence South 60 degrees 32 minutes 32 seconds West, along said boundary, 314.92 feet; thence Southwesterly, along said boundary, on a tangential curve concave to the Northwest, central angle 3 degrees 44 minutes 14 seconds and radius 2784.79 feet, 180.34 feet to the point of beginning.

(Abstract Property)

GENERAL NOTES

- The field work for this survey was completed on February 19, 2014.
- Bearings shown are based on the East line of the Southeast Quarter of Section 34, Township 29N, Range 21W which is assumed to have a bearing of North 00 degrees 04 minutes 05 seconds West.
- Surveyed property address - unassigned, Lake Elmo, MN.
- Above ground utilities have been field located as shown. All underground locations shown hereon are APPROXIMATE. Prior to any excavations or digging, contact Gopher State One Call for an on-site location (651-454-0002).
- The temporary easement per Document No. 3953750 expires on December 31, 2014 and is not shown hereon.

PROPOSED DRAINAGE AND UTILITY EASEMENTS ARE SHOWN THUS:



LEGEND

- | | | | |
|---|---|--------|---|
| ● | - Denotes found Washington County Section Corner Monument, as noted | — | - Denotes Overhead Utilities |
| ● | - Denotes Found Iron Pipe, as noted | — | - Denotes Underground Fiber Optic |
| ○ | - Denotes Set Iron Pipe, Marked with RLS 40361 | --- | - Denotes Forcemain |
| ★ | - Denotes Light Pole | — | - Denotes Watermain |
| ○ | - Denotes Sanitary Manhole | — | - Denotes Sanitary Sewer |
| ○ | - Denotes Miscellaneous Sign | — | - Denotes Underground Gas |
| ○ | - Denotes Fire Hydrant | — | - Denotes Storm Sewer |
| ○ | - Denotes Gate Valve | — | - Denotes access control dedicated per MINNESOTA DEPARTMENT OF TRANSPORTATION RIGHT OF WAY PLAT NO. 82-43 |
| ○ | - Denotes Utility Pole | — | - Denotes Existing Fence as noted |
| ○ | - Denotes Service | — | - Denotes Existing 1 Ft. Contour |
| ○ | - Denotes Guy Wire | — | - Denotes Existing 5 Ft. Contour |
| | | (Deed) | - Denotes record distance or direction as described in the legal description |

BENCHMARK
1. MNDOT Brass Disk in the Southeast end of Bridge Rd, Station Name 8282 AF
Elevation = 974.47



Carlson McCain
ENVIRONMENTAL - ENGINEERING - SURVEYING
248 Apollo Dr, Suite 100, Lino Lakes, MN 55014
Phone: 763-489-7900 Fax: 763-489-7959

PRELIMINARY PLAT
LAKEWOOD CROSSING
Lake Elmo, Minnesota

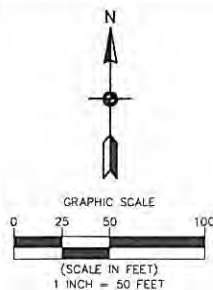
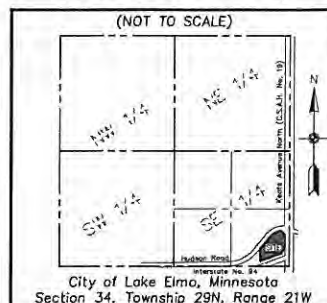
CM Properties 94 Limited Partnership
3470 Washington Drive
Suite 102
Eagan, MN 55122

REVISIONS	
1.	City Comments-6/27/14
2.	
3.	
4.	
5.	
6.	
DRAWN BY:	PJB
ISSUE DATE:	05/09/14
FILE NO:	813

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

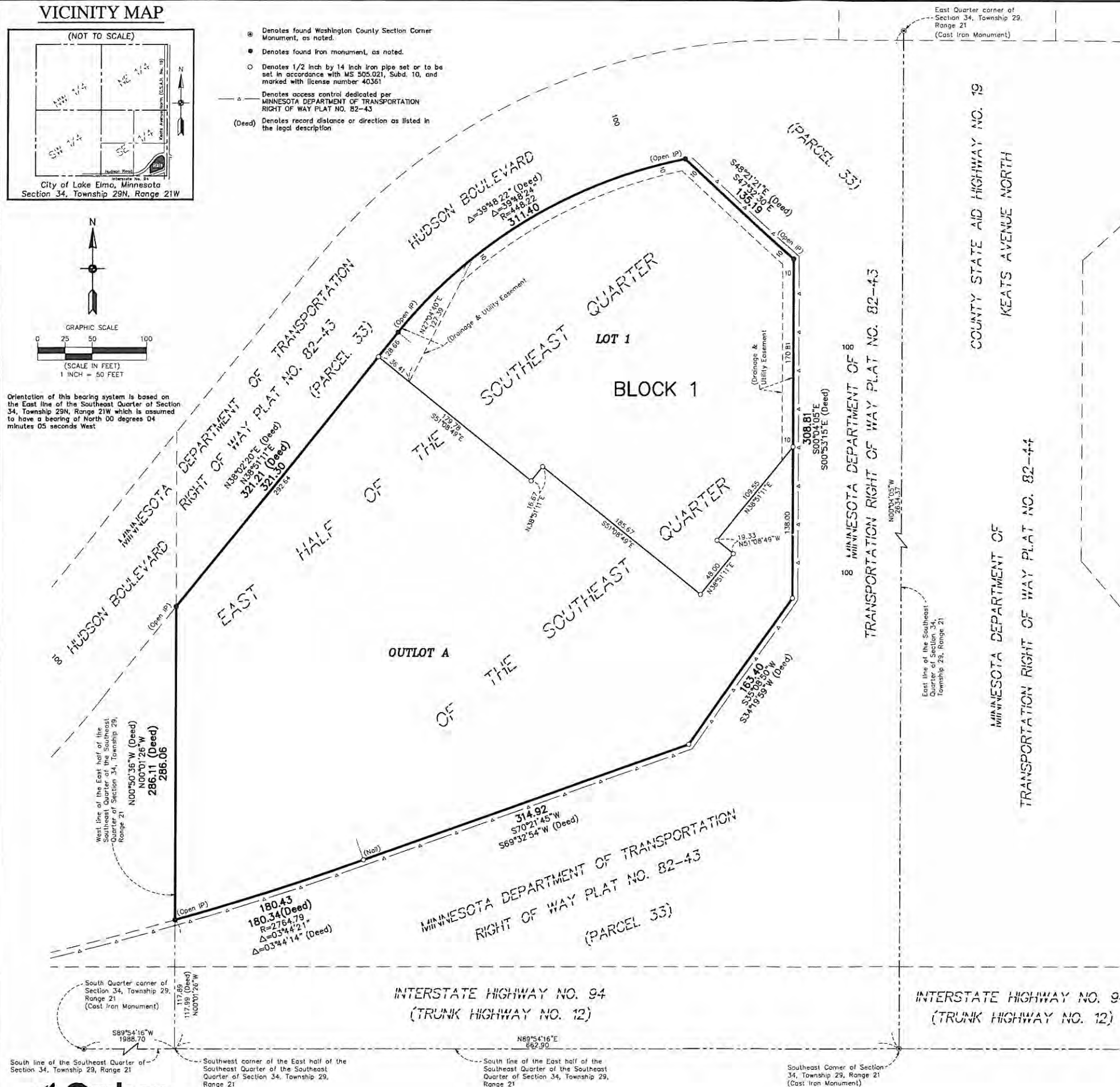
Name: Thomas R. Balluff
Signature: *Thomas R. Balluff*
Date: 05/09/14 License #: 40361

VICINITY MAP



Orientation of this bearing system is based on the East line of the Southeast Quarter of Section 34, Township 29N, Range 21W which is assumed to have a bearing of North 00 degrees 04 minutes 05 seconds West

- Denotes found Washington County Section Corner Monument, as noted.
- Denotes found Iron monument, as noted.
- Denotes 1/2 inch by 14 inch iron pipe set or to be set in accordance with MS 505.021, Subd. 10, and marked with license number 40361
- △ Denotes access control dedicated per MINNESOTA DEPARTMENT OF TRANSPORTATION RIGHT OF WAY PLAT NO. 82-43
- (Dead) Denotes record distance or direction as listed in the legal description



LAKEWOOD CROSSING

KNOW ALL BY THESE PRESENTS: That CM Properties 94 Limited Partnership, a Minnesota limited partnership, owner of the following described property situated in the County of Washington, State of Minnesota, to wit:

Part of the East half of the Southeast Quarter of Section 34, Township 29, Range 21, Washington County, Minnesota, described as follows: Beginning at a point on the West line of said East half of the Southeast Quarter of the Southeast Quarter of Section 34, distant 117.99 feet North of the Southwest corner thereof, said point also being in the boundary of Parcel 33, Minnesota Department of Transportation Right of Way Plat Numbered 82-43, according to the recorded plat thereof; thence North 0 degrees 50 minutes 36 seconds West, assumed basis for bearings, along said West line 286.11 feet to said Parcel 33 boundary; thence North 38 degrees 02 minutes 20 seconds East, along said boundary, 321.21 feet; thence Northeasterly, along said boundary, on a tangential curve concave to the Southeast, central angle 39 degrees 48 minutes 22 seconds and radius 448.22 feet, 311.40 feet; thence South 48 degrees 21 minutes 21 seconds East, along said boundary, 135.19 feet; thence South 0 degrees 53 minutes 15 seconds East, along said boundary, 308.81 feet; thence South 34 degrees 19 minutes 59 seconds West, along said boundary, 163.40 feet; thence South 69 degrees 32 minutes 54 seconds West, along said boundary, 314.92 feet; thence Southwesterly, along said boundary, on a tangential curve concave to the Northwest, central angle 3 degrees 44 minutes 14 seconds and radius 2764.79 feet, 180.34 feet to the point of beginning.

Has caused the same to be surveyed and platted as LAKEWOOD CROSSING and does hereby dedicate to the public for public use forever the easements created by this plat for drainage and utility purposes only.

In witness whereof said CM Properties 94 Limited Partnership, a Minnesota limited partnership has caused these presents to be signed by its proper partner this _____ day of _____, 20____.

CM Properties 94 Limited Partnership

Martin F. Colon, General Partner

STATE OF _____
COUNTY OF _____

This instrument was acknowledged before me on _____ by Martin F. Colon, General Partner of CM Properties 94 Limited Partnership, a Minnesota limited partnership on behalf of the partnership.

Notary Public, _____
My commission expires _____

I, Thomas R. Balluff, do hereby certify that I have surveyed and platted or directly supervised the survey and plating of the property described on this plat as LAKEWOOD CROSSING; that I am a duly licensed Land Surveyor in the State of Minnesota; that this plat is a correct representation of the boundary survey, that all mathematical data and labels are correctly designated on this plat; that all monuments depicted on the plat have been or will be correctly set within one year as indicated on the plat; that all water boundaries and wet lands as defined in MS 505.01, Subd. 3 existing as of the date of this certification are shown and labeled on the plat, and that all public ways are shown and labeled on the plat.

Dated this _____ day of _____, 20____.

Thomas R. Balluff, Licensed Land
Surveyor Minnesota License No. 40361

STATE OF _____
COUNTY OF _____

This instrument was acknowledged before me on _____ by Thomas R. Balluff, Licensed Land Surveyor.

Notary Public, _____
My commission expires _____

CITY OF LAKE ELMO PLANNING COMMISSION

Approved by the Planning Commission of the City of Lake Elmo, Minnesota, this _____ day of _____, 20____.

Signed: _____, Chairperson Signed: _____, Secretary

CITY COUNCIL, CITY OF LAKE ELMO, MINNESOTA

This plat was approved by the City Council of the City of Lake Elmo, Minnesota, this _____ day of _____, 20____, and hereby certifies compliance with all requirements as set forth in Minnesota Statutes, Section 505.03, Subd. 2.

Signed: _____, Mayor Signed: _____, Clerk

COUNTY SURVEYOR

Pursuant to Chapter 820, Laws of Minnesota, 1971, and in accordance with Minnesota Statutes, Section 505.021, Subd. 11, this plat has been reviewed and approved this _____ day of _____, 20____.

By: _____
Washington County Surveyor

By: _____

COUNTY AUDITOR/TREASURER

Pursuant to Minnesota Statutes, Section 505.021, Subd. 9, taxes payable in the year 20____ on the land hereinbefore described have been paid. Also, pursuant to Minnesota Statutes, Section 272.12, there are no delinquent taxes and transfer entered this _____ day of _____, 20____.

By: _____
Washington County Auditor/Treasurer

By: _____
Deputy

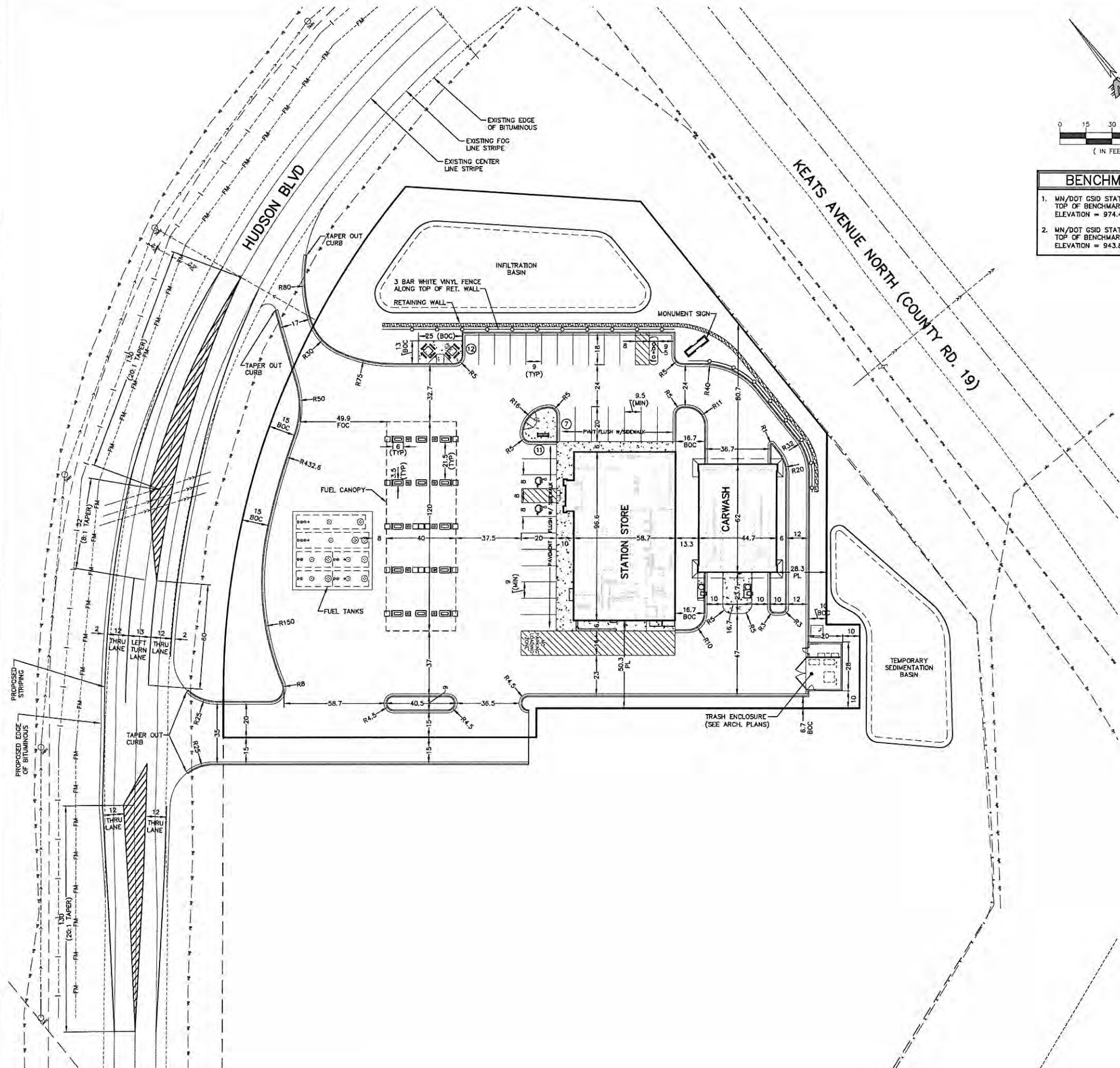
COUNTY RECORDER

Document Number: _____

I hereby certify that this instrument was recorded in the Office of the County Recorder for record on this _____ day of _____, 20____, at _____ o'clock _____ M., and was duly recorded in Washington County Records.

By: _____
Washington County Recorder

By: _____
Deputy



BENCHMARKS
1. MN/DOT GSD STATION NO. 33428
TOP OF BENCHMARK DISK
ELEVATION = 974.47 (NAVD 88)
2. MN/DOT GSD STATION NO. 33429
TOP OF BENCHMARK DISK
ELEVATION = 943.86 (NAVD 88)

SITE PLAN LEGEND	
EXISTING	PROPOSED
CURB	
BITUMINOUS	
CONCRETE WALK	
PROPERTY LINE	
EASEMENT LINE	
STORM SEWER	
SANITARY SEWER	
WATER MAIN	
UNDERGROUND GAS	
OVERHEAD ELECTRIC	
FORCE MAIN	
UNDERGROUND FIBER OPTICS	
MANHOLE	
HYDRANT	
GATE VALVE	
POWER POLE	
FENCE	
RETAINING WALL	

- SITE PLAN NOTES**
- ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
 - CONCRETE CURB AND GUTTER SHALL BE B612 UNLESS OTHERWISE NOTED.
 - IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES PRIOR TO THE START OF SITE WORK. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY OF DISCREPANCIES OR VARIATIONS FROM THE PLAN.
 - UNLESS OTHERWISE SHOWN ON THIS DRAWING, CONTRACTOR SHALL PROVIDE CONTROL JOINTS, CONSTRUCTION JOINT AND EXPANSION JOINTS IN SLAB ON GRADE, SIDEWALKS AND DRIVES PER THE FOLLOWING REQUIREMENTS:
CONTROL JOINT MAX. SPACING: WALKS-8' O.C.
ALL OTHERS-10' O.C.
SAW CUT CONTROL JOINTS MINIMUM 1/4 CONCRETE THICKNESS.
EXPANSION JOINT MAX. SPACING: WALKS-24' O.C.
*ALL OTHERS-40' O.C.
*ALL POINTS WHERE A CHANGE IN PAVEMENT THICKNESS OCCURS SHALL HAVE AN EXPANSION JOINT.
DOWELL ALL EXPANSION JOINTS: 24" O.C. MAX.

SITE DATA

ZONING:	C
PARCEL AREA:	89,355 SF
HARD SURFACE AREA:	
STATION STORE:	5,764 SF 6%
CARWASH:	2,775 SF 3%
PAVEMENT:	53,471 SF 60%
TOTAL:	62,010 SF 69%
PERVIOUS SURFACE AREA:	27,345 SF 31%
BUILDING HEIGHTS:	
STATION STORE:	24.0 FT
FUEL CANOPY:	15.5 FT
CARWASH:	14.0 FT

PARKING SUMMARY

STANDARD STALLS	28
CANOPY STALLS	20
HANDICAP STALLS	2
TOTAL STALLS	50

CALL BEFORE YOU DIG

Know what's below.
Call before you dig.

Kwik Trip
STORES

Kwik Star
STORES

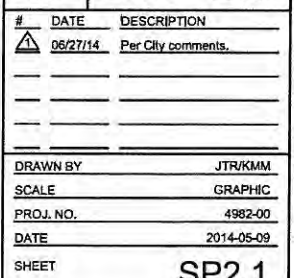
Kwik Trip, Inc.
P.O. BOX 2107
1626 OAK STREET
LA CROSSE, WI 54602-2107
PH. (608) 781-8988
FAX (608) 781-8960

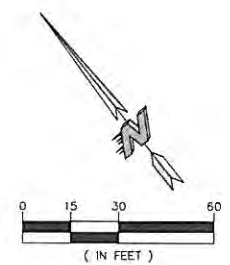
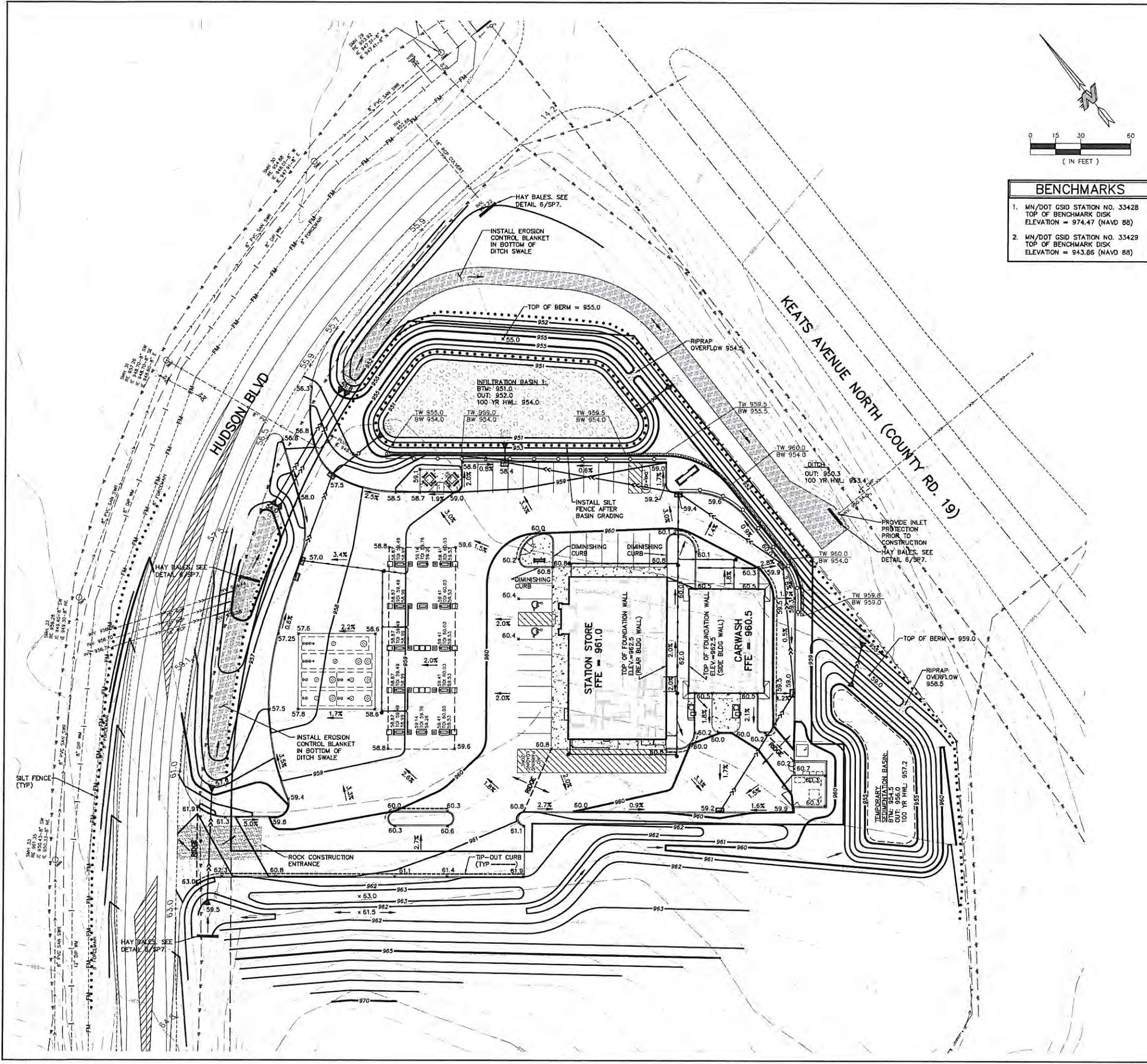
ENVIRONMENTAL - ENGINEERING - SURVEYING
248 Apollo Dr, Suite 100, Lino Lakes, MN 55014
Phone: 763-489-7900 Fax: 763-489-7959

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Name: Joseph T. Radach, P.E.
Signature:
Date: 05/09/14 License #: 45889

SITE PLAN (LAYOUT)	CONVENIENCE STORE #248 WITH DOUBLE BAY CARWASH		HUDSON BLVD & KEATS AVENUE NORTH LAKE ELMO, MINNESOTA
	#	DATE	
	DATE	DESCRIPTION	
	DATE	DESCRIPTION	
	DATE	DESCRIPTION	
DRAWN BY	JTR/KMM		
SCALE	GRAPHIC		
PROJ. NO.	4982-00		
DATE	2014-05-09		
SHEET	SP2		





BENCHMARKS	
1.	MN/DOT GSD STATION NO. 33428 TOP OF BENCHMARK DISK ELEVATION = 974.47 (NAVD 88)
2.	MN/DOT GSD STATION NO. 33429 TOP OF BENCHMARK DISK ELEVATION = 943.86 (NAVD 88)

GRADING PLAN LEGEND	
EXISTING	PROPOSED
CURB	
BITUMINOUS	
CONCRETE WALK	
PROPERTY LINE	
EASEMENT LINE	
STORM SEWER	
SANITARY SEWER	
WATER MAIN	
UNDERGROUND GAS	
OVERHEAD ELECTRIC	
FORCE MAIN	
UNDERGROUND FIBER OPTICS	
MANHOLE	
HYDRANT	
GATE VALVE	
POWER POLE	
FENCE	
RETAINING WALL	
5' CONTOUR	
1' CONTOUR	
SILT FENCE	
SPOT ELEVATION (CURB ELEVATIONS ARE TO GUTTER LINE)	
TOP OF ISLAND ELEVATION	

GOVERNING SPECIFICATIONS

- THE LATEST EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" AND THE CITY OF LAKE ELMO SPECIFICATIONS.
- THE LATEST EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD).
- THE LATEST EDITION OF THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM) STANDARD SPECIFICATIONS.

GRADING NOTES

- SILT FENCE AND EXISTING INLET PROTECTION SHALL BE INSTALLED PRIOR TO GRADING CONSTRUCTION, AND SHALL BE MAINTAINED UNTIL SITE HAS BEEN STABILIZED.
- CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES PRIOR TO THE START OF GRADING CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY OF DISCREPANCIES OR VARIATIONS FROM THE PLAN.
- CONTRACTOR SHALL STRIP, STOCKPILE AND RESPREAD SUFFICIENT TOPSOIL TO PROVIDE A MINIMUM OF 4" OF TOPSOIL OVER ALL DISTURBED AREAS THAT WILL BE SODDED, SEEDDED OR LANDSCAPED.



Kwik TRIP
STORES


Kwik STAR
STORES

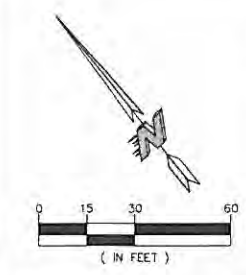
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Phone: 763-489-7900 Fax: 763-489-7959

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Name: Joseph T. Radach, P.E.
Signature: [Signature]
Date: 05/09/14 License #: 145889

GRADING & EROSION CONTROL
CONVENIENCE STORE #248
WITH DOUBLE BAY CARWASH
HUDSON BLVD & KEATS AVENUE NORTH
LAKE ELMO, MINNESOTA

#	DATE	DESCRIPTION
	06/27/14	Per City comments.
DRAWN BY		JTR/KMM
SCALE		GRAPHIC
PROJ. NO.		4982-00
DATE		2014-05-09
SHEET		SP3



BENCHMARKS	
1.	MN/DOT GSID STATION NO. 33428 TOP OF BENCHMARK DISK ELEVATION = 974.47 (NAVD 88)
2.	MN/DOT GSID STATION NO. 33429 TOP OF BENCHMARK DISK ELEVATION = 943.86 (NAVD 88)



UTILITY PLAN LEGEND	
EXISTING	PROPOSED
CURB	
BITUMINOUS	
CONCRETE WALK	
PROPERTY LINE	
EASEMENT LINE	
SANITARY SEWER	
WATER MAIN	
STORM SEWER	
OVERHEAD ELECTRIC	
FORCE MAIN	
UNDERGROUND FIBER OPTICS	
UNDERGROUND ELECTRIC	
UNDERGROUND TELEPHONE	
MANHOLE	
HYDRANT	
GATE VALVE	
CLEANOUT	
POWER POLE	
FENCE	
RETAINING WALL	

- SANITARY SEWER:**
- Pipe: Use solid-core, SDR-35, ASTM D3034 (or approved equal) Polyvinyl Chloride (PVC) pipe for all designated PVC sanitary sewer services unless otherwise noted. Pipe for services shall meet the requirements of ASTM D2665 for Schedule 40 or 80 pipe. All joints shall be push-on joints with molded rubber gaskets meeting the requirements of ASTM D2321 except for service lateral pipe connections to pipes, tees, wyes, and fittings which shall have solvent-cemented joints meeting the requirements of ASTM D2855 and ASTM D3212. Lay all PVC pipe on a continuous granular bed. Installation must comply with ASTM D2321.
 - Cleanouts: Install cleanouts on all sanitary sewer services. The distance between cleanouts in horizontal piping shall not exceed 100 feet for pipes 4-inch and over in size. Cleanouts shall be of the same nominal size as the pipe they serve and concrete frame and support. Install a meter box and frame and install a meter box frame and gate lid (Neenah R-1911-A, or approved equal) over all cleanouts, unless otherwise noted.
 - Testing: Pressure test all sanitary sewer lines per the City requirements. Test all flexible sanitary sewer lines for deflection after the sewer line has been installed and backfill has been in place for at least 30 days. No pipe shall exceed a deflection of 5%. If the test fails, make necessary repairs and retest.
 - Unless otherwise indicated, use reinforced, precast, concrete manhole boxes conforming to ASTM C478, furnished with precast boxes. Sanitary sewer manhole boxes shall be supplied with pre-formed inverts and flexible neoprene sleeve connections for all lateral lines 375 mm (15 inches) in diameter or less, unless otherwise indicated. Joints for all precast manhole box sections shall have confined, rubber "O"-ring gaskets in accordance with ASTM C923. The inside barrel diameter shall not be less than 48 inches.
 - Install flexible watertight frame/chimney seals on all sanitary sewer manhole boxes. Use either Manufactured Maintenance Hole Frame/Chimney Seals or Elastomeric Waterproofing Frame/Chimney Seals.
 - Use Neenah Foundry Co. R-1733 casting on all sanitary sewer manhole boxes unless otherwise noted. Covers shall bear the "Sanitary Sewer" label.

- WATER DISTRIBUTION SYSTEM:**
- Separation of Water and Sewer: Provide a minimum horizontal separation of 10 feet between all water and sewer lines. Provide a minimum separation of 18 inches at all water line and sewer line crossings.
 - Watermain Depth: Maintain 7 feet of cover over the top of the water lines to the finished grade. Verify elevation of proposed and existing water lines at all utility crossings. Install the water lines at greater depths in order to clear storm sewers, sanitary sewers, or other utilities as required. Include costs to lower water lines in the line bid.
 - Disinfection: Disinfect all completed watermain in accordance with AWWA Standard C651-05 and the City specifications. Water lines shall be flushed prior to disinfection, except when tablet method is used.
 - Testing: Pressure test and perform bacteriological tests on all water lines under the supervision of the City Public Works Department. Notify the City at least 24 working hours prior to any testing. Pressurize the waterline to 100 PSI (100 psi) gauge pressure (measured at the point of lowest elevation) by means of a pump connected to the pipe in a satisfactory manner. Maintain the test pressure for a minimum of 2 hours. Do not add water to the test line to maintain the required pressure during the watermain pressure testing. The test section of pipe is acceptable with a pressure drop of 7 PSI (1 psi) or less. Electrical conductivity testing shall be performed as directed by the City.
 - Use AWWA C151/A21.51 Thickness Class 52 Ductile Iron Pipe (DIP) with push-on joints for all watermain unless otherwise noted. All watermain fittings shall meet the requirements of AWWA C153, latest edition. Gaskets and nuts shall be "Cor-Loc" meeting AWWA C111, latest edition.
 - Use mechanical joint restraint devices for joint restraint on all watermain bends having a vertical or horizontal deflection of 22-1/2 degrees or greater, all valves, elbows, extensions, tees, crosses, plugs, all hydrant valves, and all hydrants in accordance with City requirements. Use "Sarnco 1100 Megaflow" manufactured by Sarnco Inc., Eastland, Texas or approved equal. Installed in accordance with the manufacturer's recommendations for restraint on Ductile Iron Pipe.
 - At all valve locations which require a 12" or smaller valve, install gate valves which are of the compression restrained (CRS) type. Gate valves shall conform to AWWA C153. Install cast iron valve boxes conforming to ASTM A48 at each valve location. Valve boxes shall be the best-quality screw type with 5-1/4" shorter and the word "water" on the lid. Use Tyler B550 with 4-inch drop lid, or approved equal. Valve boxes shall have at least 8" of adjustment above and below finished grade.
 - Copper service pipe shall be Type K, seamless copper water tubing, soft annealed temper and conform to the requirements of ASTM B88. If high density polyethylene (HDPE) water service will be used in lieu of copper, it shall conform to ASTM D2737, for "EndoPure" Water Service Tubing, PE 3408-200 psi, SDR 7 (PS), as manufactured by Endo Industries.
 - TRACER WIRE: Tracer wire for all non-metallic pipe shall be No. 8 AWG gauge, single or multiple strand plastic coated copper wire. Wire splices shall be made with a mechanical wire proof connection such as "3M DBP" or approved equal. Tracer wire shall be fastened to fire hydrant with "Hydrant Tracer Wire Bracket" as manufactured by Val Products or approved equal.

- STORM DRAINAGE:**
- Unless otherwise indicated, use reinforced, precast, concrete manhole boxes and catch basins conforming to ASTM C478, furnished with water stop gaskets and precast boxes. Joints for all precast manhole box sections shall have confined, rubber "O"-ring gaskets in accordance with ASTM C923. The inside barrel diameter shall not be less than 48 inches, unless otherwise noted.
 - All joints and connections to catch basins or manholes shall be watertight. Use resilient rubber seals, watertop gaskets, or approved equal. Cement mortar joints are not allowed.
 - Maintenance holes and catch basins in paved areas shall be sumped 0.05 feet. Catch basins in the gutter line shall be sumped 0.17 feet. Elevations shown on the plans represent the sumped elevations.
 - PVC Pipe: Use solid-core, SDR-35, ASTM D3034 Polyvinyl Chloride (PVC) pipe for designated PVC storm sewer 4 to 10 inches in diameter. Joints for all PVC storm sewer shall have push-on joints with molded rubber gaskets meeting the requirements of ASTM D2321 except for service lateral pipe connections to pipes, tees, wyes, and fittings which shall have solvent-cemented joints meeting the requirements of ASTM D2855 and ASTM D3212. Lay all PVC pipe on a continuous granular bed. Installation must comply with ASTM D2321.
 - RCP: Reinforced concrete pipe (RCP) and fittings shall conform to ASTM C76, Design G, with circular reinforcing for the area of pipe 24" and larger. Use Class V RCP for pipes 24" and larger. Use Class V RCP for pipes 18" and smaller. Joints shall be Bureau of Reclamation type R-4, with confined rubber "O"-ring gaskets in accordance with ASTM C561.
 - Manholes: Install a reinforced concrete manhole on the free end of all daylight RCP storm sewer pipes. The last three sections (including apron) of all daylight RCP storm sewers with a minimum of two feet bottom to bottom. This requirement applies to both upstream and downstream pipe joints and outlets. For concrete curbs, lay all joints. This is to be used only to hold the pipe sections together, not for pulling the sections tight. Install safety-trap racks on all concrete aprons.
 - Testing: Test all portions of storm sewer that are within 10 feet of buildings, within 10 feet of buried water lines, within 50 feet of water wells, or that pass through soil or water identified as being contaminated. Test all flexible storm sewer lines for deflection after the sewer line has been installed and backfill has been in place for at least 30 days. No pipe shall exceed a deflection of 5%. If the test fails, make necessary repairs and retest.
 - Use Neenah castings, or approved equal, of the type indicated in the Storm Sewer Schedule.
 - The minimum depth of cover for building and canopy roof drain leaders without insulation is 5 feet. Insulate roof drain leaders at all locations where the depth of cover is less than 5 feet. Provide a minimum insulation thickness of 2 inches. The insulation must be at least 4 feet wide and centered on the pipe. Install the insulation boards 6 inches above the top of the pipes on mechanically compressed and leveled pipe bedding material. Use high density, closed cell, rigid board equivalent to DOW Styrofoam H-40 plastic foam insulation.
 - Cleanouts: Install cleanouts on all roof drains in accordance with S.P.S. 382.35 (3)(c)(i). The distance between cleanouts in horizontal piping shall not exceed 100 feet for pipes 10-inches and under in size. Cleanouts shall be of the same nominal size as the pipe they serve. Install a meter box and frame and gate lid (Neenah R-1911-A, or approved equal) over all cleanouts unless otherwise noted.
 - HDPE Requirements:
 - Install dual-wall, smooth interior, corrugated high-density polyethylene (HDPE) pipe at locations indicated on the plan.
 - HDPE pipe shall conform to the requirements of AASHTO M239 and Section 12 of the AASHTO LRFD Bridge Design Specification.
 - All fittings must comply with ASTM Standard D3212.
 - Water-tight joints must be used at all connections including structures.
 - Lay all HDPE pipe on a continuous granular bed. Installation must comply with ASTM D3212. All sections of the corrugated HDPE pipe shall be coupled in order to provide water tight joints.
 - Testing shall be performed in accordance with the City specifications.

STORM SEWER SCHEDULE		
STRUCTURE	SIZE	NEENAH CASTING OR EQUAL
*CBMH-101	60" DIA.	R-3067-VB
CBMH-102	48" DIA.	R-3067-VB
CB-102A	24" X 36"	R-3067-VB
*CBMH-103	48" DIA.	R-3067-VB
CBMH-104	48" DIA.	R-3067-V
CBMH-105	48" DIA.	R-3067-V
CBMH-106	48" DIA.	R-3067-VB
CB-107	24" X 36"	R-3067-VB

*INSTALL 18R SNOUT OIL & DEBRIS STOP AS MANUFACTURED BY BMP, INC.

- GENERAL:**
- General Specifications:** The latest editions of the Minnesota Department of Transportation "Standard Specifications for Construction", the City Engineers Association of Minnesota (CEAM) Standard Specifications, the City of Lake Elmo Standard Specifications and the Minnesota Plumbing Code.
 - Comply with the work safety practices specified by the Occupational Safety and Health Administration (OSHA). Comply with all applicable local, state, and federal safety regulations. OSHA prohibits entry into "confined spaces", such as manholes and inlets (see 29CFR Section 1910.146) without undertaking certain specific practices and procedures. Construction safety is solely the responsibility of the Contractor, who is also solely responsible for the means, methods, and sequencing of the construction operations.
 - Perform all utility work in accordance with State and City requirements.
 - Contact the City of Lake Elmo's Public Works Department at (651) 747-3940 for inspection of all utility work.
 - Contact to existing sanitary sewer M/W by coring. Connect to existing storm sewer M/W by either sawcutting or coring. Meet all City standards and specifications for the connection. Reinstall twelve after installation. Use water stop gaskets in order to provide watertight seals when penetrating a structure wall with a pipe. Take measures before beginning construction to ensure that service connections do not cut into maintenance access structure joints or pipe barrel joints.
 - Perform trench excavations for all utilities in accordance with the requirements of OSHA 29 CFR, Part 1926, Subpart R, "Excavations and Trenches". (www.osha-slc.gov)
 - Coordinate building utility connection locations at 5 ft. out from the proposed building with the interior plumbing contractor prior to construction. Verify water and sewer service locations and elevations with the Mechanical Engineer prior to construction.

- The locations of existing utilities shown on this plan are from record information and field as-built data. The Engineer does not guarantee that all existing utilities are shown or, if shown, exist in the locations indicated on the plan. It is the Contractor's responsibility to verify the vertical and horizontal location of all existing utilities (including water service lines and appurtenances). Notify the Engineer of any discrepancies.
- Contact utility companies for locations of all public and private utilities within the work area prior to beginning construction. Contact 811 for locations of existing utilities at least 72 hours (not including weekends or holidays) before beginning any construction. Obtain ticket number and meet with representatives of the various utilities at the site. Provide the Owner with the ticket number information.
- Potential to verify the location of existing underground facilities at a sufficient number of locations in order to ensure that no conflict with the proposed work exists and that sufficient clearance is maintainable.
- Where existing gas, electric, cable, or telephone utilities conflict with the work, coordinate the abandonment, relocation, offset, or support of the existing utilities with the appropriate utility companies. Coordinate new gas meter and gas line installation, electric meter and electric service installation, cable service and telephone service installation with the local utility companies.
- Arrange for and secure suitable disposal areas off-site. Dispose of all excess soil, waste materials, debris, and all materials not designated for reuse. Waste material and debris include trees, stumps, pine, concrete, asphaltic concrete, or other waste material from the construction operation. Obtain the rights to any waste area for disposal of unsuitable or surplus material either shown or not shown on the plans. All work in disposing of such material shall be considered incidental to the work. All disposal must conform to applicable state waste disposal permit requirements. Obtain all necessary permits at no cost to the Owner.
- Straight line saw-cut existing bituminous or concrete surfacing at the perimeter of pavement removal areas. Use saws that provide water to the blade. Tack and match all connections to existing bituminous pavement.

- Relocate overhead power, telephone, and cable lines as required.
- All materials required for this work shall be new material conforming to the requirements for size, kind, grade, size, quality, and other details specified herein or as shown on the Plans. Do not use recycled or salvaged aggregate, asphaltic pavement, crushed concrete, or scrap shingles. Unless otherwise indicated, the Contractor shall furnish all required materials.
- Restore the public right-of-way. Replace any concrete curb and gutter, bituminous pavement, sidewalk, or vegetative cover damaged by the construction activity. Restore damaged turf with sod within the public right-of-way. The work area shown is general and may need to be adjusted in the field.
- When sawing or drilling concrete or masonry, use saws that provide water to the blade. Do not allow the slurry produced by this process to be soaked outside of the immediate work area or discharged into the sewer system.
- Adjust all curb stops, valve boxes, manhole box castings, catch basin castings, cleanout covers, and similar items to finished grade.
- Install all pipe with the ASTM identification numbers on the top for inspection. Commence pipe laying at the lowest point in the proposed sewer line. Lay the pipe with the bell end or receiving groove end of the pipe pointing upstream. When connecting to an existing pipe, uncover the existing pipe in order to allow any adjustments in the proposed line and grade before laying any pipe.
- Obtain and pay for all permits, tests, inspections, etc. required by agencies that have jurisdiction over the project. The Contractor is responsible for all bonds, letters of credit, or cash reserves related to the work. Excavate and inspect work in accordance with all local and state codes, rules, ordinances, and regulations pertaining to the particular type of work involved.



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Signature: [Signature]
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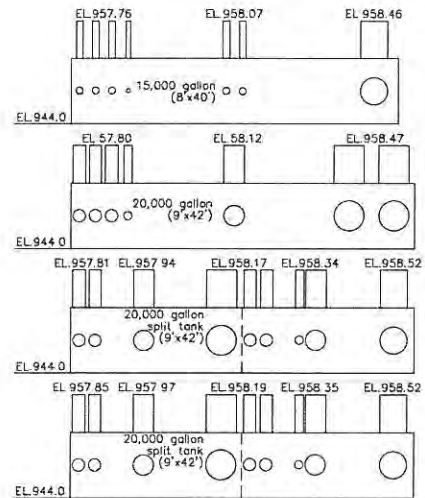
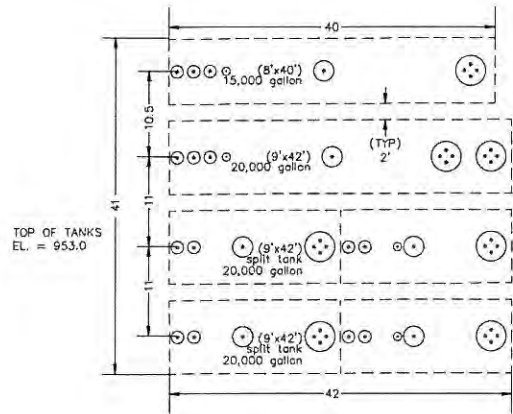
UTILITY PLAN

CONVENIENCE STORE #248
WITH DOUBLE BAY CARWASH

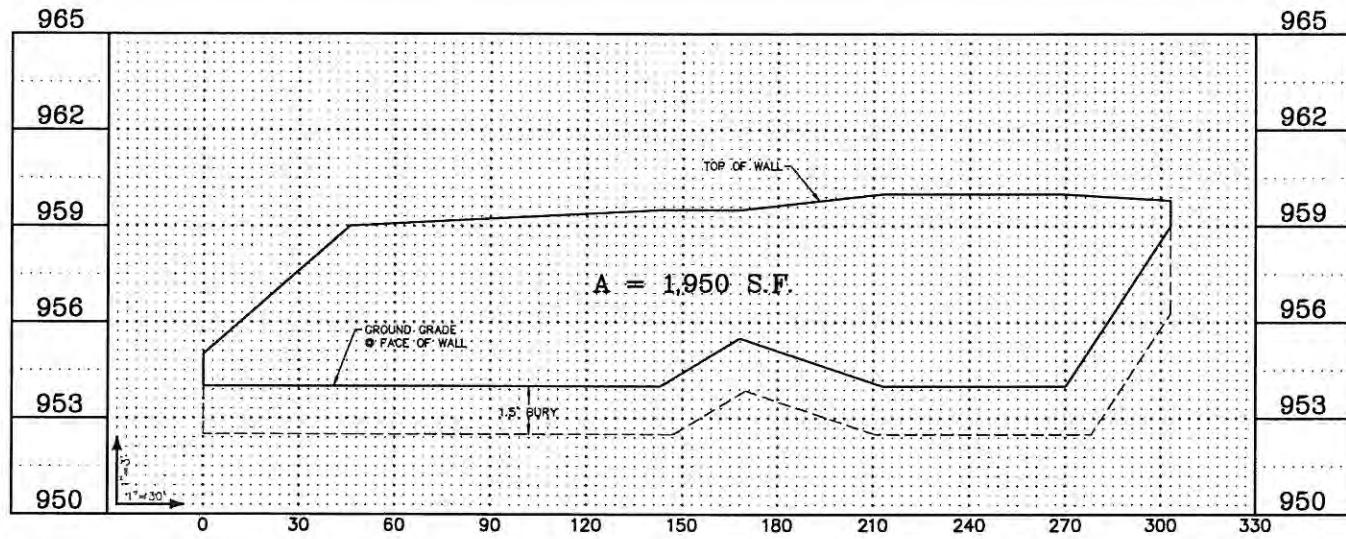
HUDSON BLVD & KEATS AVENUE NORTH
LAKE ELMO, MINNESOTA

#	DATE	DESCRIPTION
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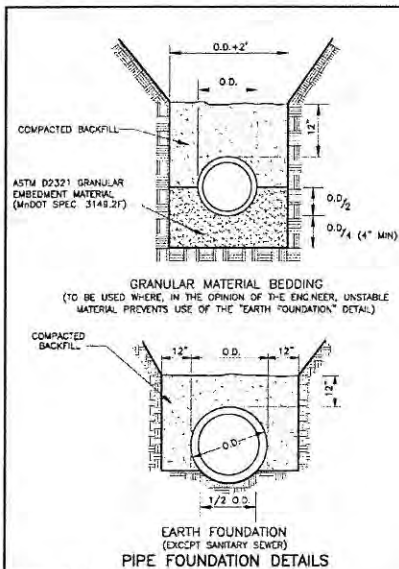
DRAWN BY: JTR/KMM
SCALE: GRAPHIC
PROJECT NO.: 4982-00
DATE: 2014-05-09
SHEET: SP4



1 TANK COMPLEX DIAGRAM AND ELEVATIONS
SP8 NO SCALE



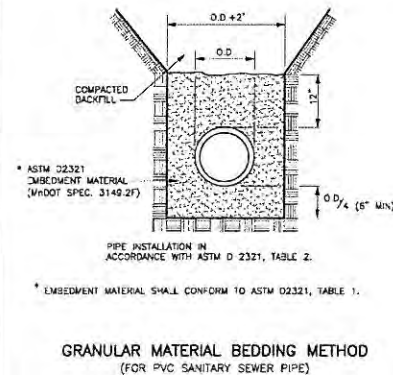
9 RETAINING WALL
SP8 HORIZ: 1"=30'; VERT: 1"=3'



FEBRUARY 2013

CITY OF LAKE ELMO

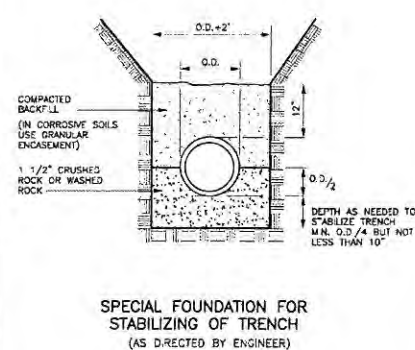
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FEBRUARY 2013

CITY OF LAKE ELMO

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FEBRUARY 2013

CITY OF LAKE ELMO

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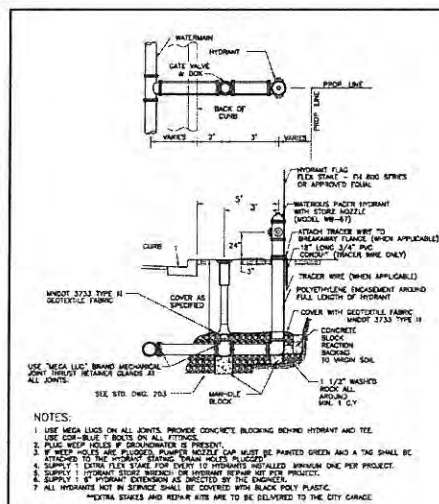
1. ALL WATERMAIN AND ACCESSORIES MUST BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF LAKE ELMO STANDARD SPECIFICATIONS AND DETAILS.
2. MANIPULATION OF EXISTING VALVES SHALL BE PERFORMED ONLY BY CITY PERSONNEL.
3. WATERMAIN SHALL BE DUCTILE IRON PIPE, CLASS-52.
4. ALL FITTINGS SHALL COMPLY WITH CEAM SPEC. 2611.2.1.1. ALL FITTINGS SHALL BE DUCTILE IRON PIPE WITH POLYETHYLENE ENCASEMENT. ALL CONNECTIONS SHALL BE INSTALLED UTILIZING COR-BLUE NUTS & BOLTS.
5. USE GATE VALVES FOR ALL APPLICATIONS UP THROUGH 12 INCHES.
6. GATE VALVES SHALL BE RESILIENT WEDGE AMERICAN FLOW CONTROL SERIES 2500 OR APPROVED EQUAL. GATE VALVES MUST COMPLY WITH CEAM SPEC. 2611.2, C.2.
7. USE BUTTERFLY VALVES FOR ALL APPLICATIONS GREATER THAN 12 INCHES.
8. BUTTERFLY VALVES SHALL BE WUHLER LINESCAL H, OR APPROVED EQUAL. BUTTERFLY VALVES SHALL COMPLY WITH CEAM SPEC. 2611.2, C.3.
9. BOLTS AND NUTS ON ALL VALVES AND HYDRANTS SHALL BE STAINLESS STEEL.
10. ALL HYDRANTS SHALL BE INSTALLED 5.0 FEET BACK OF CURB.
11. HYDRANTS SHALL BE WATEROUS "PACER" MODEL W3 67 OR APPROVED EQUAL, FITTED WITH FH 800 SERIES (LEX STAKE AND PAINTED RED).
12. HYDRANTS SHALL HAVE TWO OUTLET NOZZLES FOR 2-1/2" (O.D.) HOSE CONNECTIONS AND ONE 4" STORZ NOZZLE.
13. THE CURB STOP SERVICE ASSEMBLY SHALL HAVE A MINIMUM 1-FIT ADJUSTMENT RANGE AND SHALL EXTEND 4 INCHES ABOVE FINISHED GRADE FULLY EXTENDED.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING WATER TO HOMES AND BUSINESSES WHOSE WATER SUPPLY IS DISRUPTED DURING THE COURSE OF THE PROJECT.

STANDARD PLAN NOTES WATERMAIN PLANS

FEBRUARY 2013

CITY OF LAKE ELMO

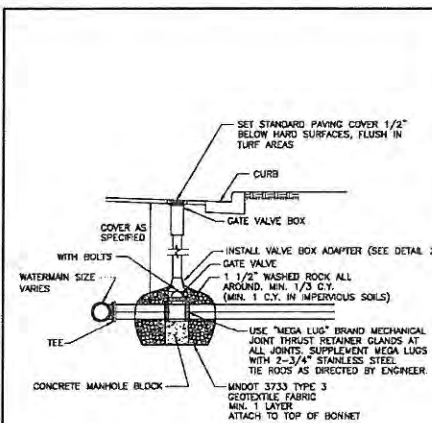
200A
LAKE ELMO



FEBRUARY 2013

CITY OF LAKE ELMO

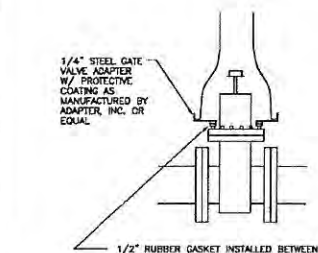
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LAKE ELMO



FEBRUARY 2013

CITY OF LAKE ELMO

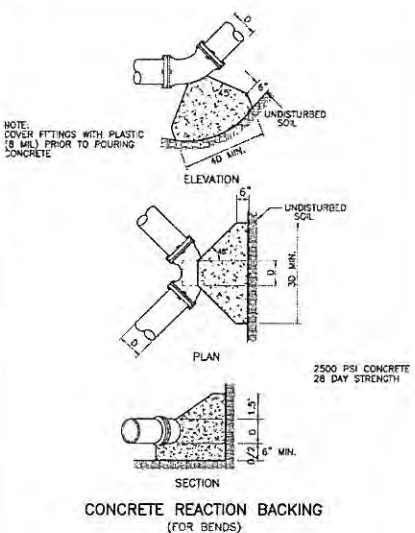
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FEBRUARY 2013

CITY OF LAKE ELMO

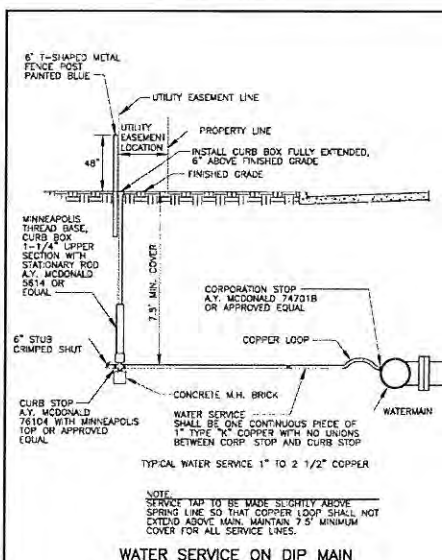
204
LAKE ELMO



FEBRUARY 2013

CITY OF LAKE ELMO

206
LAKE ELMO



FEBRUARY 2013

CITY OF LAKE ELMO

207A
LAKE ELMO

1. ALL SANITARY SEWER AND ACCESSORIES MUST BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF LAKE ELMO STANDARD SPECIFICATIONS AND DETAILS.
2. ALL SANITARY SEWER PVC PIPE SHALL BE INSTALLED ACCORDING TO CITY STANDARD DRAWING 103 "GRANULAR MATERIAL BEDDING METHOD (FOR PVC SANITARY SEWER PIPE)".
3. UNLESS NOTED OTHERWISE, ALL SMOOTH WALLED SANITARY SEWER PVC PIPE AND FITTINGS SHALL BE SDR 35 WITH ELASTOMERIC GASKETED JOINTS.
4. ALL SANITARY SEWER SERVICES SHALL BE 4-INCH PVC, SCH. 40 OR SDR 26 PIPE.
5. SMOOTH WALLED PVC PIPE AND FITTINGS SHALL CONFORM WITH THE REQUIREMENTS OF ASTM D-3034 FOR THE SIZE, STANDARD DIMENSION RATIO (SDR), AND STRENGTH REQUIREMENTS INDICATED ON THE PLANS, SPECIFICATIONS, AND SPECIAL PROVISIONS.
6. REINFORCED CONCRETE PIPE AND FITTINGS SHALL CONFORM WITH THE REQUIREMENTS OF MDDOT SPEC. 3226 (REINFORCED CONCRETE PIPE) FOR THE TYPE, SIZE, AND STRENGTH CLASS SPECIFIED HEREIN.
7. JOINTS OF MANHOLE RISER SECTIONS SHALL BE TONGUE AND GROOVE WITH RUBBER "O" RING JOINTS PROVIDED ON ALL SANITARY SEWER MANHOLES.
8. SANITARY SEWER INLET AND OUTLET PIPES SHALL BE JOINED TO THE MANHOLE WITH A GASKETED, FLEXIBLE, WATER-TIGHT CONNECTION TO ALLOW DIRECTIONAL SETTLEMENT OF THE PIPE AND MANHOLE TO TAKE PLACE.
9. A 1'-0" TO 1'-4" MANHOLE SECTION SHALL BE INSTALLED UNDER THE CONE SECTION TO ALLOW FOR HEIGHT ADJUSTMENT WHENEVER POSSIBLE.
10. ALL SERVICE LINE STUBS MUST HAVE A 2'-0" HARDWOOD MARKER WITH METAL SPIKE RUNNING FROM THE END OF PIPE TO FINISHED GRADE ELEVATION.
11. UPON MAKING A CONNECTION TO AN EXISTING SANITARY SEWER STUB OR MANHOLE, DIRT AND DEBRIS SHALL BE PREVENTED FROM ENTERING THE EXISTING SEWER BY IMMEDIATELY INSTALLING WATER-TIGHT PLUGS AS NEEDED IN THE EXISTING MANHOLE.

STANDARD PLAN NOTES SANITARY SEWER PLANS

FEBRUARY 2013

CITY OF LAKE ELMO

300A
LAKE ELMO

**KWIK
TRIP**

STORES

**KWIK
STAR**

STORES

KWIK TRIP, Inc.
P.O. BOX 2107
1626 OAK STREET
LA CROSSE, WI 54602-2107
PH. (608) 781-8988
FAX (608) 781-8960

**Carlson
McCain**
ENVIRONMENTAL • ENGINEERING • SURVEYING
248 Apollo Dr, Suite 100, Uno Lakes, MN 55014
Phone: 763-489-7900 Fax: 763-489-7959

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota

Name: Joseph T. Radach, P.E.

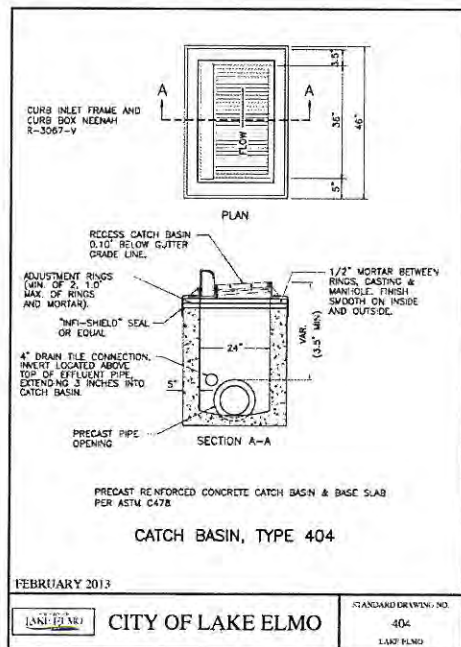
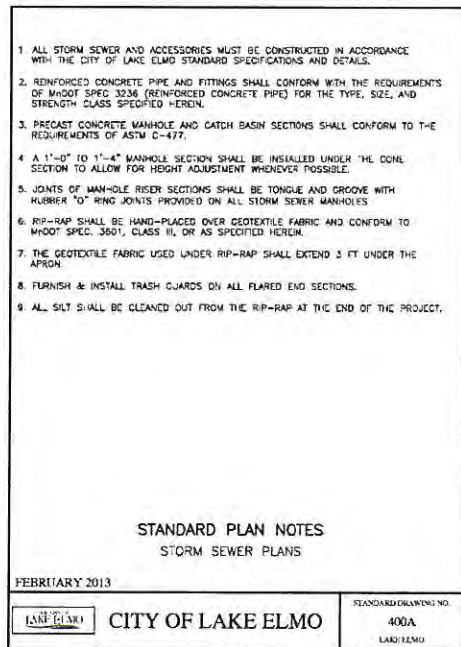
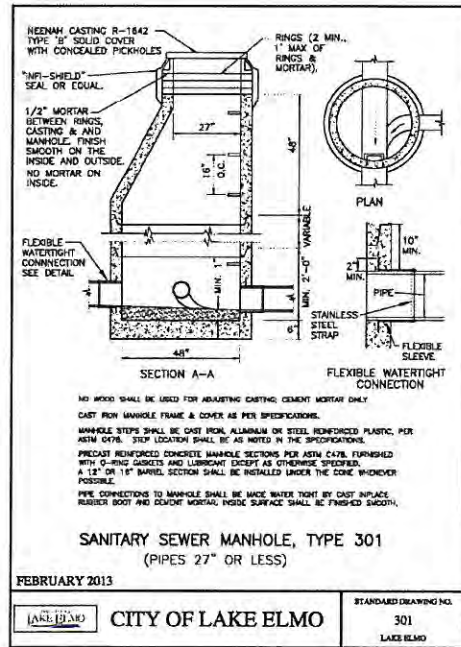
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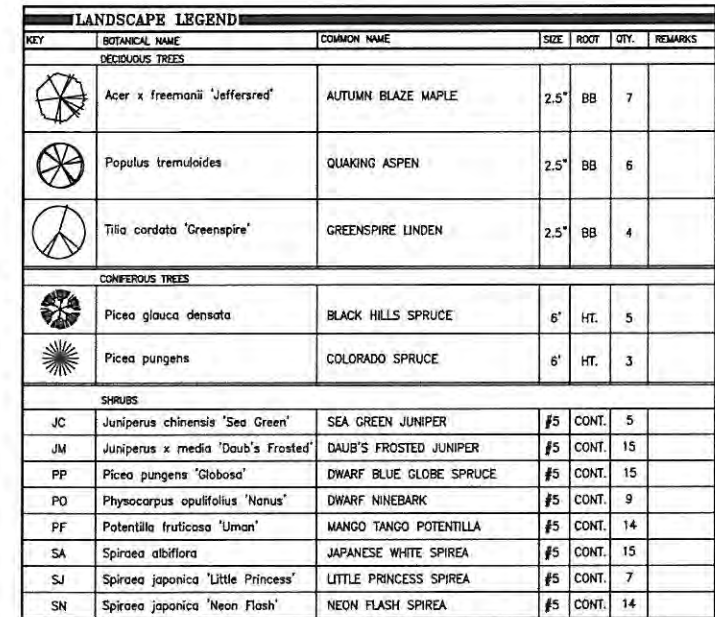
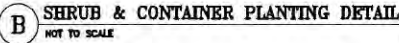
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DETAILS
CONVENIENCE STORE #248
WITH DOUBLE BAY CARWASH
HUDSON BLVD & KEATS AVENUE NORTH
LAKE ELMO, MINNESOTA

#	DATE	DESCRIPTION
1	08/27/14	Per City comments
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DRAWN BY: JTR/KMM
SCALE: GRAPHIC
PROJ. NO.: 4982-00
DATE: 2014-05-09
SHEET: SP7





LANDSCAPE REQUIREMENTS:

154.258 C.1: A MIN. OF ONE (1) TREE SHALL BE PLANTED FOR EVERY FIFTY (50) FT. OF STREET FRONTAGE.

340 FT. STREET FRONTAGE ALONG HUDSON BLVD. / 50 = 7 TREES REQ.

154.258 C.2: IN ADDITION TO THE REQUIREMENTS OF C.1 ABOVE, A MINIMUM OF FIVE (5) TREES SHALL BE PLANTED FOR EVERY ONE (1) ACRE OF LAND THAT IS DEVELOPED OR DISTURBED BY DEVELOPMENT ACTIVITY.

3.5 ACRES DISTURBED = 3.5 X 5 = MIN. 18 TREES REQUIRED



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Trip**™

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Star**™

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FWA
FRIZZELL WINTER ASSOCIATES
LANDSCAPE ARCHITECTS
Prepared by
Frizzell Winter Associates, Landscape Architects

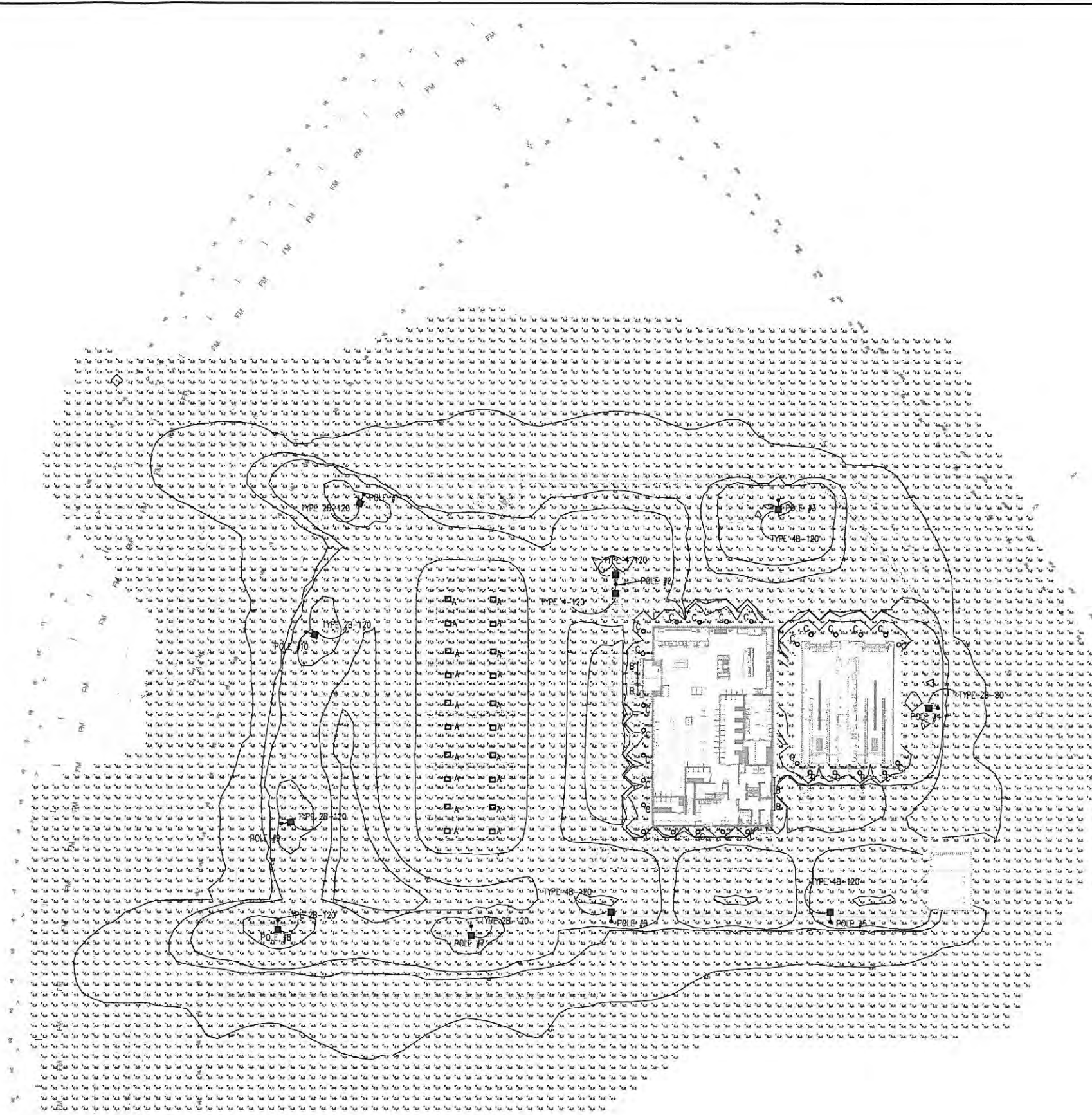
LANDSCAPE PLAN


CONVENIENCE STORE #248
WITH DOUBLE BAY CARWASH

HUDSON BLVD & KEATS AVENUE NORTH
LAKE ELMO, MINNESOTA

#	DATE	DESCRIPTION
1	06/27/14	Per City Comments

DRAWN BY	ELF
SCALE	GRAPHIC
PROJ. NO	4982-00
DATE	2014-05-05
SHEET	L1







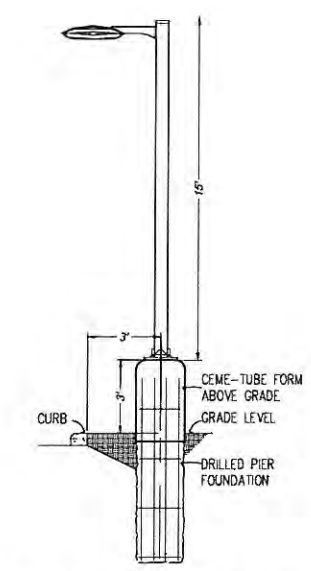
 **PHOTOMETRIC SITE PLAN**
SCALE: 1" = 30'-0"

NOTE:
FOOTCANDLE LEVELS ARE MEASURED AT GROUND LEVEL WITHIN THE PROPERTY BOUNDARY AND AT 6' ABOVE GROUND AT THE PROPERTY LINE AND BEYOND.

- FIXTURE TYPES:**
- A - CREE LIGHTING: CAN-304-SL-RS-06-E-UL-WH-700
MOUNTING HEIGHT - 16'-0"
 - B - LED STRIPLIGHT
LITHONIA -ZL1-L96-LB840
MOUNTING HEIGHT: SEE ARCHITECTURAL ELEVATIONS
 - C - RECESSED LED DOWNLIGHT
GOTHAM EVO-41/29-BAR-120-TRW

- TYPE 2B-80 - CREE LIGHTING: ARE-EDG-2MB-DA-08-E-UL-WH-350
WITH BACKLIGHT SHIELD
POLE MOUNTED AT A TOTAL HEIGHT OF 18'-0"
- TYPE 2B-120 - CREE LIGHTING: ARE-EDG-2MB-DA-12-E-UL-WH-350
WITH BACKLIGHT SHIELD
POLE MOUNTED AT A TOTAL HEIGHT OF 18'-0"
- TYPE 4B-120 - CREE LIGHTING: ARE-EDG-4MB-DA-12-E-UL-WH-350
WITH BACKLIGHT SHIELD
POLE MOUNTED AT A TOTAL HEIGHT OF 18'-0"
- TYPE 4-120 - CREE LIGHTING: ARE-EDG-4M-DA-12-E-UL-WH-350
POLE MOUNTED AT A TOTAL HEIGHT OF 18'-0"

- FIXTURE SYMBOLS:**
- A  RECESSED LED LIGHT MOUNTED UNDER CANOPY
 - B  LED STRIP LIGHT
 - C  RECESSED LED DOWNLIGHT
 -  POLE MOUNTED LED FIXTURE
- TYPES 2 & 4



LOT LIGHT ELEVATION DETAIL
NOT TO SCALE



STORES



STORES

KWIK TRIP, Inc.
P.O. BOX 3127
P.O. BOX 3127
MUSKOGEE, AL 36562-0312
(205) 261-1400
(205) 261-1400

PHOTOMETRIC SITE PLAN

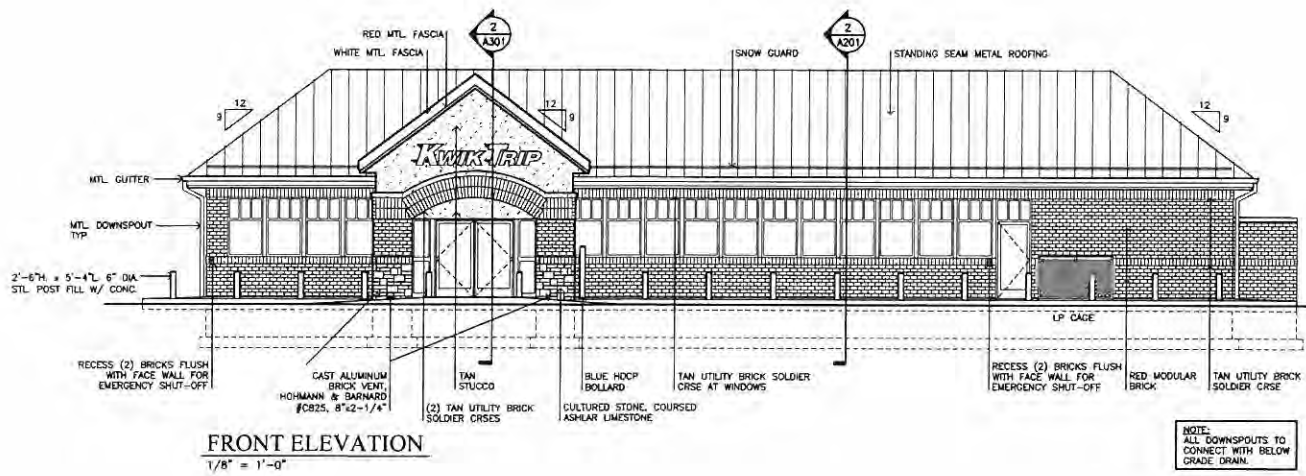
CONVENIENCE STORE #248

WITH DOUBLE BAY CARWASH

314

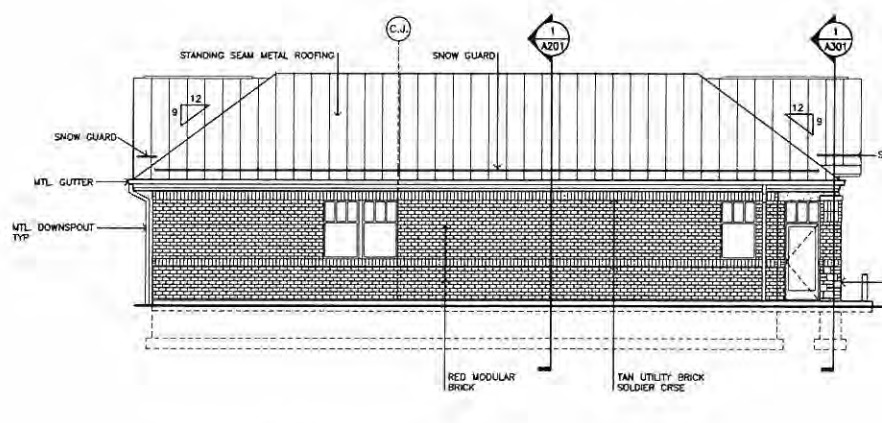
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E1-CITY

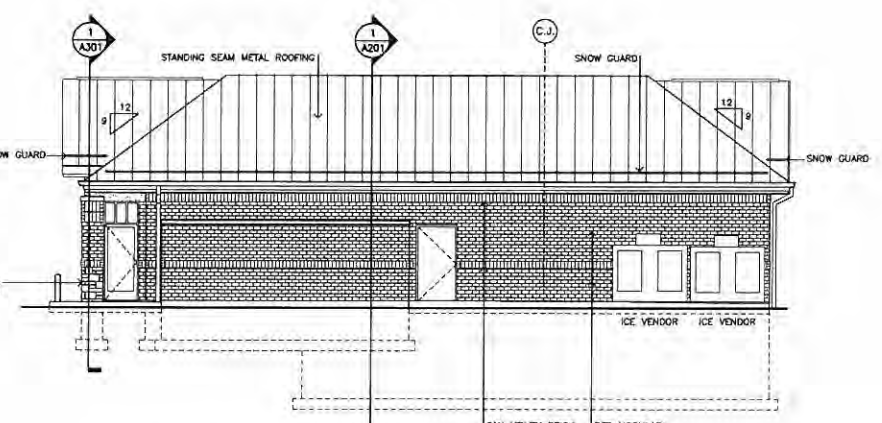


FRONT ELEVATION
1/8" = 1'-0"

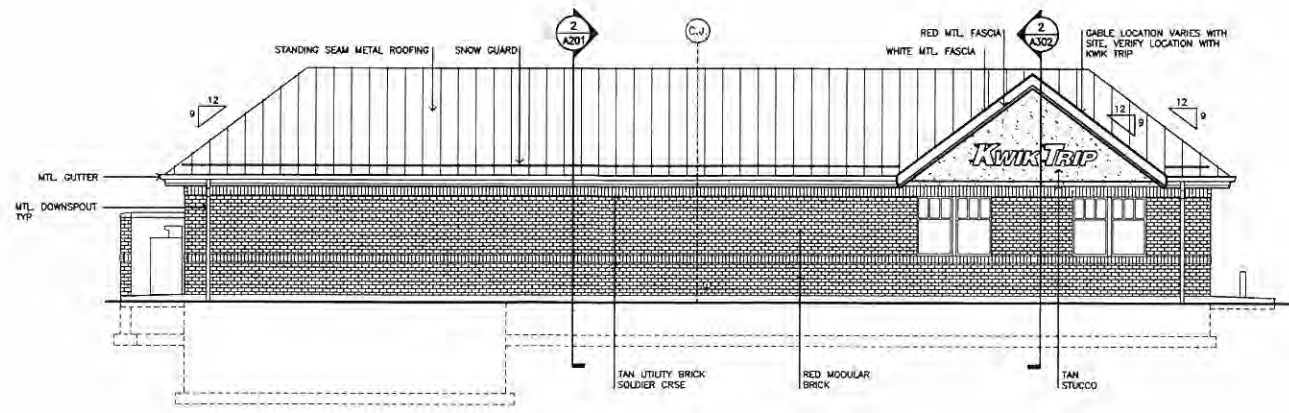
NOTE:
ALL DOWNSPOUTS TO
CONNECT WITH BELOW
GRADE DRAIN.



LEFT SIDE ELEVATION
1/8" = 1'-0"



RIGHT SIDE ELEVATION
1/8" = 1'-0"



BACK ELEVATION
1/8" = 1'-0"

EXT. COLOR SCHED.		
MATERIAL	MANUF.	COLOR
BOLLARDS		BLACK, GLOSS
FASCIA	UNI-CLAD 24 GA	RETAL RED STONE WHITE
METAL ROOF	UNI-CLAD	HEDLOCK GREEN
MORTAR		GREY
RED BRICK	SHOUX CITY	CABERNET BURGUNDY
ROOF CUP DOW WASH	UNI-CLAD	SIERRA TAN
STUCCO	TOTAL WALL	MORNING MIST SWIRL TEXTURE
TAN BRICK	SHOUX CITY	CLEAR BURY
TRASH ENCLOSURE	UNI-CLAD	BURGUNDY

LARSON

ARCHITECT

200 Mason Street #3

Onalaska, WI 54650

(608) 784-6808

info@larsonarchitect.com

Kwik Trip

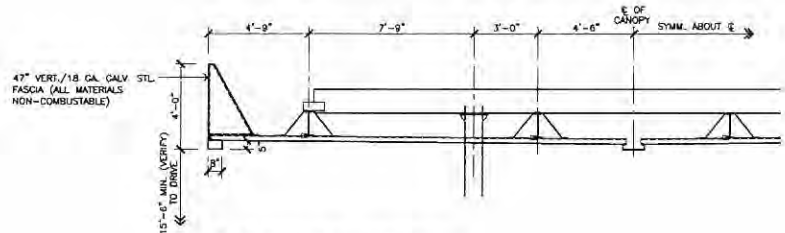
STORES

Kwik Star

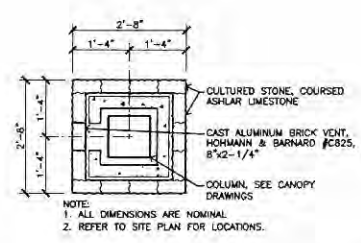
STORES

STORE #248
XXX
LAKE ELMO, MN
96' x 58' LH
STANDARD

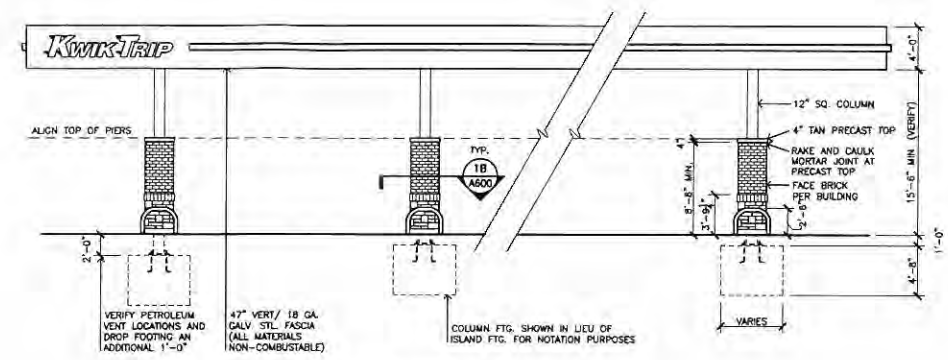
I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly registered Architect under the laws of the state of Wisconsin.
 T. J. Larson
 T. J. Larson, Inc.
 2000 W. Main St.
 Onalaska, WI 54650
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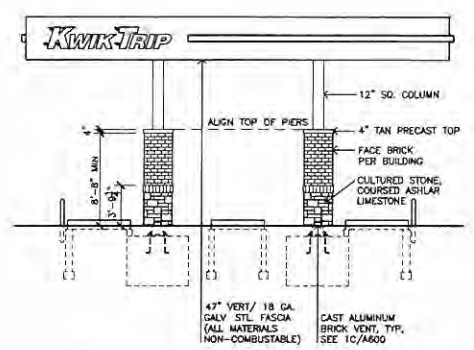
1A CANOPY SECTION
A600 1/4" = 1'-0"



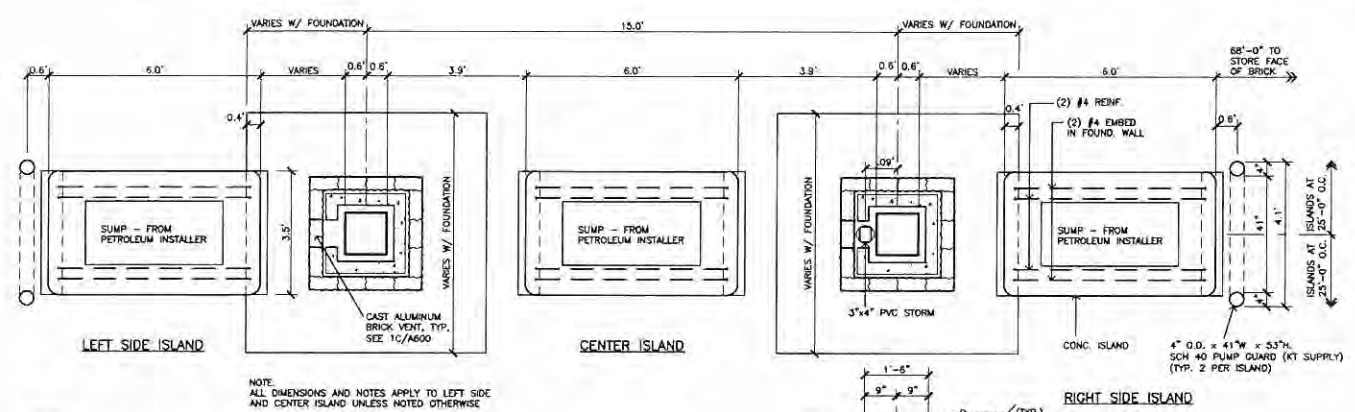
1B CANOPY COLUMN DETAIL
A600 1/2" = 1'-0"



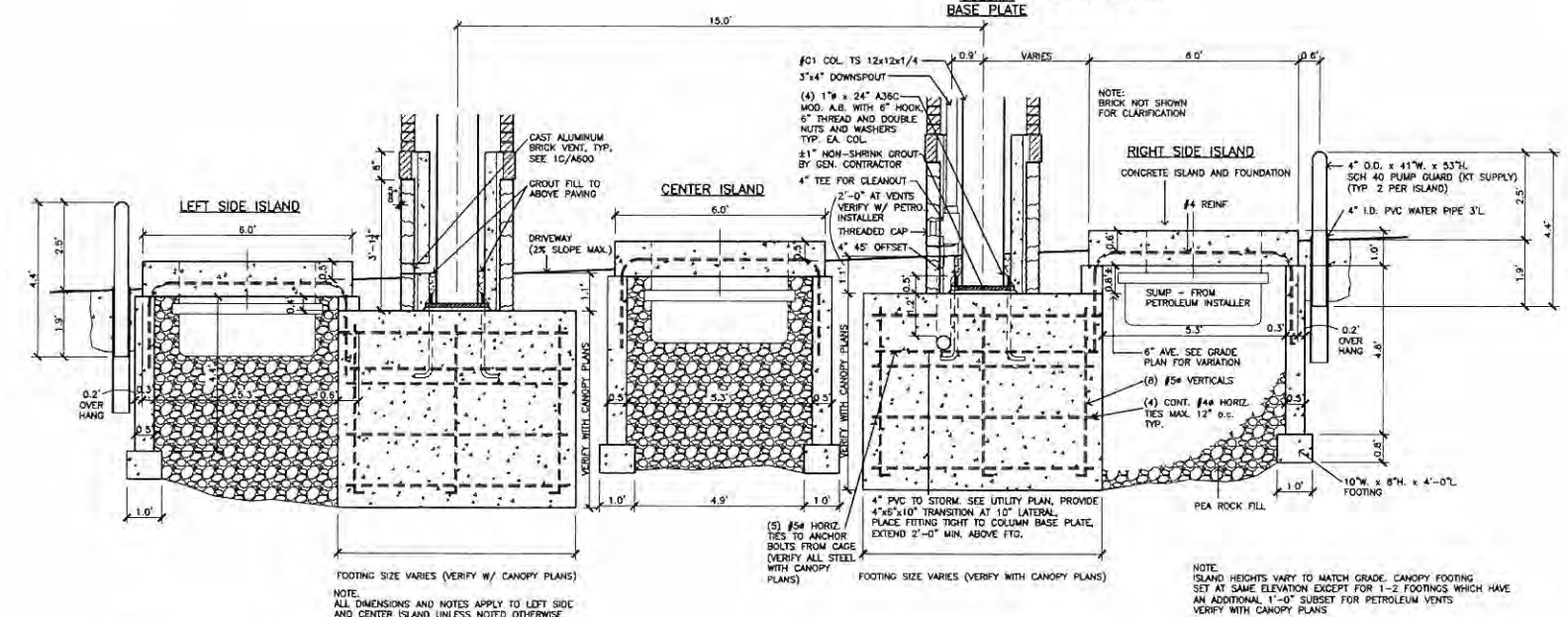
2 END CANOPY ELEVATION
A600 1/8" = 1'-0" (OTHER END SIMILAR)



3 SIDE CANOPY ELEVATION
A600 1/8" = 1'-0" (OTHER SIDE SIMILAR)



4 GAS CANOPY COLUMN PLAN
A600 1:10



5 GAS CANOPY COLUMN SECTION
A600 1:10

LARSON
ARCHITECT
200 Mason Street #3
Onalaska, WI 54650
(608) 784 - 6808
info@larsonarchitect.com

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STORE #248
XXX
LAKE ELMO, MN
96' x 58' LH
STANDARD

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A600

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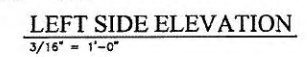
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STORE #248
XXX
LAKE ELMO, MN
96' x 58' LH
STANDARD

Revised :
Issued for : PERMIT
Date : APR 30, 14
Checked : TPL
Drawn :

A703

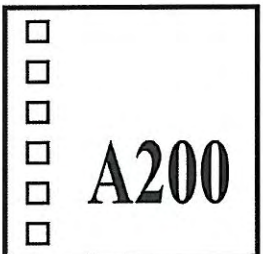


96'x58' L





DB CW



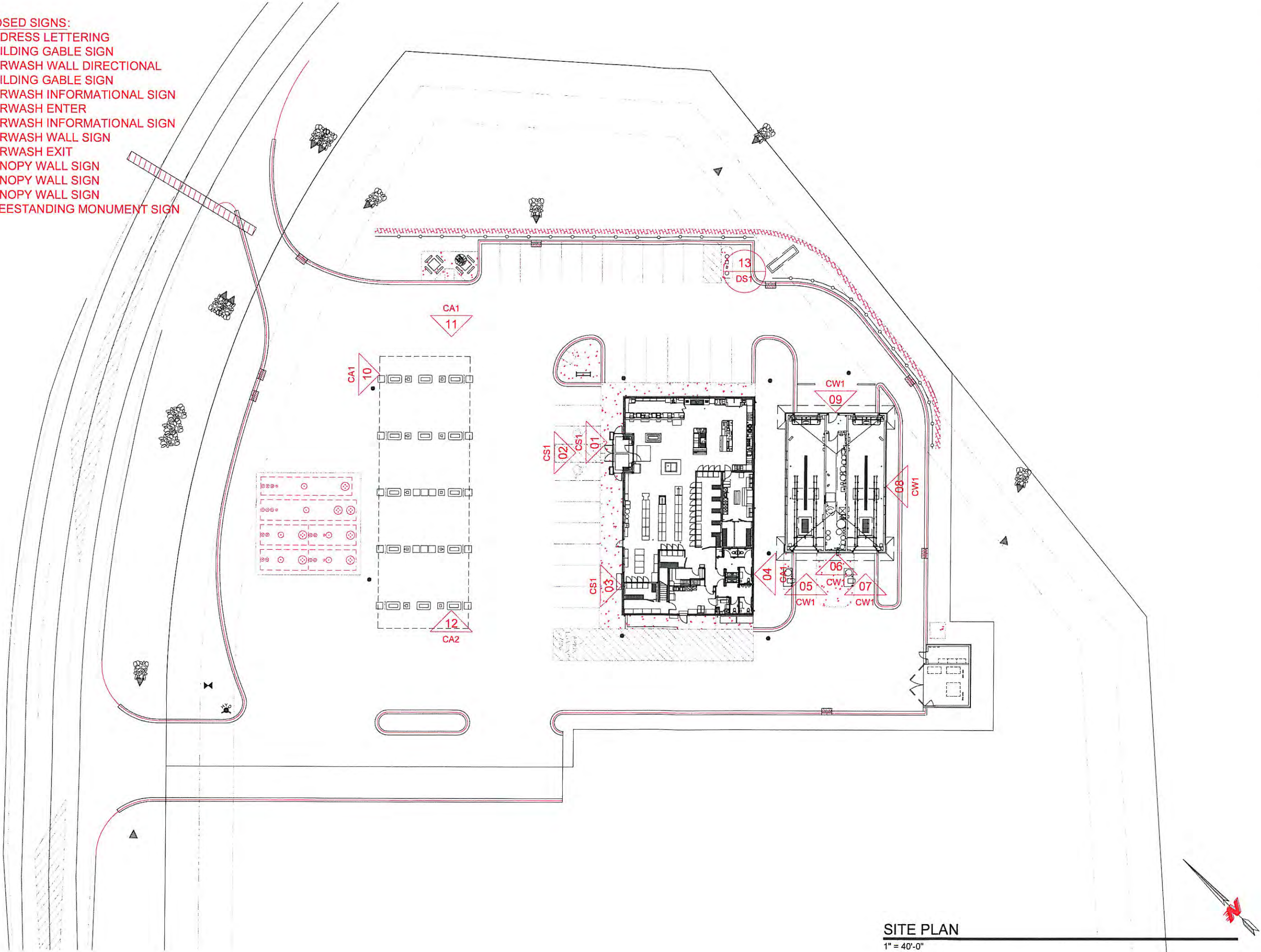


Flat Canopy
w/ Brick

10 MPD



- PROPOSED SIGNS:
#01 ADDRESS LETTERING
#02 BUILDING GABLE SIGN
#03 CARWASH WALL DIRECTIONAL
#04 BUILDING GABLE SIGN
#05 CARWASH INFORMATIONAL SIGN
#06 CARWASH ENTER
#07 CARWASH INFORMATIONAL SIGN
#08 CARWASH WALL SIGN
#09 CARWASH EXIT
#10 CANOPY WALL SIGN
#11 CANOPY WALL SIGN
#12 CANOPY WALL SIGN
#13 FREESTANDING MONUMENT SIGN



SITE PLAN
1" = 40'-0"



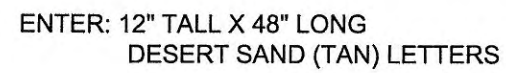
STORES



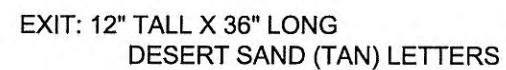
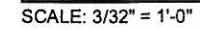
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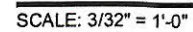
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	HUDSON BLVD LAKE ELMO, MN	
	#	DATE DESCRIPTION
DRAWN BY		B. DUSEK
SCALE		MULTIPLE
PROJ. NO.		0001
DATE		2014-05-08
SHEET		SP1



SCALE: 3/32" = 1'-0"



SCALE: 3/32" = 1'-0"



SCALE: 1/2" = 1'-0"

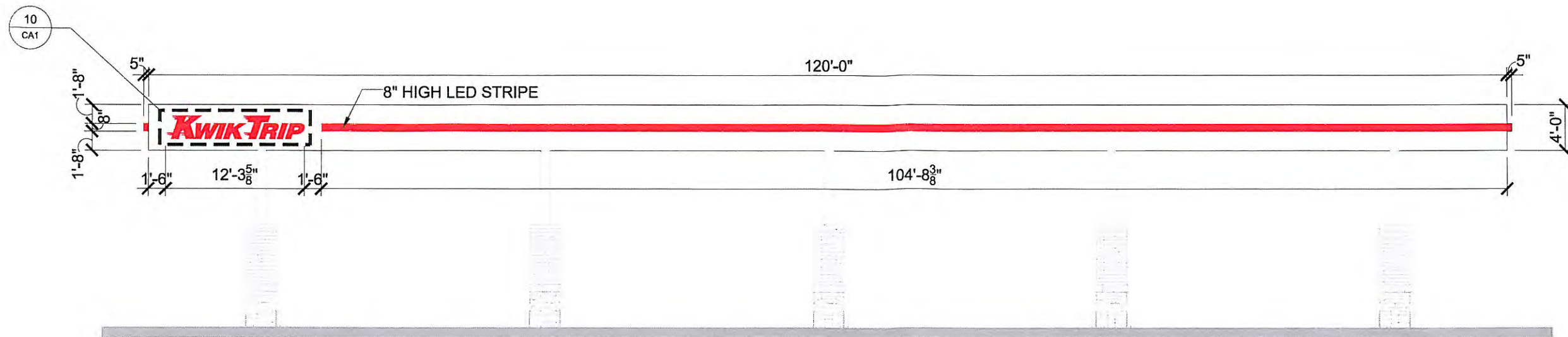


CARWASH SIGNAGE

CONVENIENCE STORE #248
WITH SINGLE CW

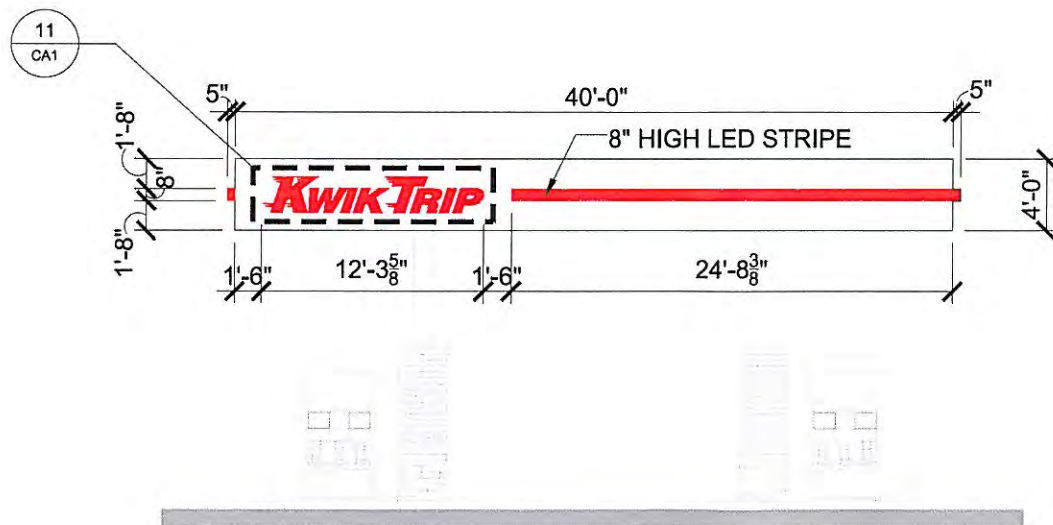
HUDSON BLVD
LAKE ELMO, MN

[illegible]



CANOPY ELEVATION

SCALE: 3/32" = 1'-0"



CANOPY ELEVATION

SCALE: 3/32" = 1'-0"



LOGO DETAIL - SIGNS #10 & #11

SCALE: 1/2" = 1'-0"

Kwik
TRIP

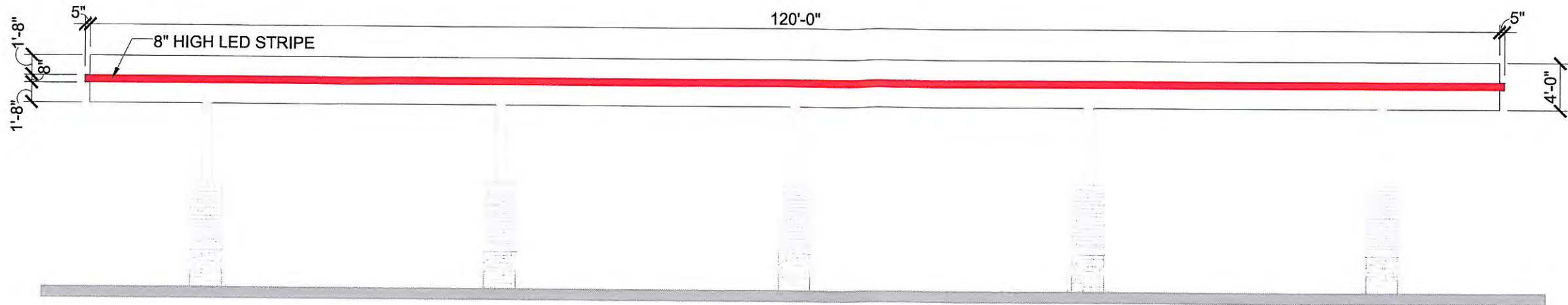
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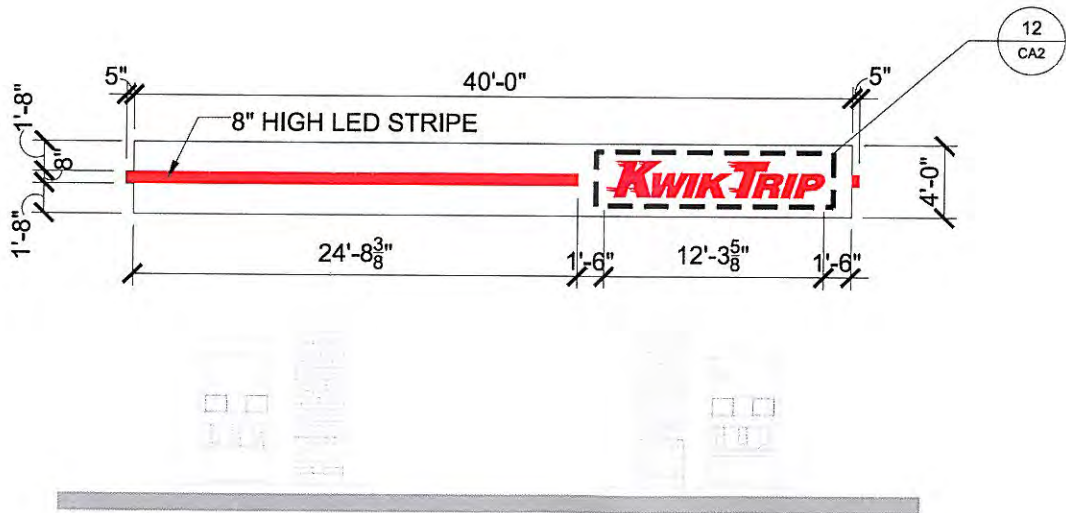
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CANOPY SIGNAGE		CONVENIENCE STORE #248 WITH SINGLE CW		HUDSON BLVD LAKE ELMO, MN	
#	DATE	DESCRIPTION			
DRAWN BY		B. DUSEK			
SCALE		MULTIPLE			
PROJ. NO.		0001			
DATE		2014-05-08			
SHEET		CA1			



CANOPY ELEVATION

SCALE: 3/32" = 1'-0"




CANOPY ELEVATION

SCALE: 3/32" = 1'-0"




LOGO DETAIL - SIGNS #12

SCALE: 1/2" = 1'-0"



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CANOPY SIGNAGE

CONVENIENCE STORE #248
WITH SINGLE CW

HUDSON BLVD
LAKE ELMO, MN

#	DATE	DESCRIPTION

DRAWN BYB. DUSEK

SCALEMULTIPLE

PROJ. NO.0001

DATE2014-05-08

SHEETCA2

Your Choice
TOUCH FREE OR SOFT TOUCH

NON-LIT INFORMATIONAL SIGN
RED VINYL ON WHITE ALUMINUM
1'-0"H X 10'-0"W = 10.0 SQ FT

CARWASH INFORMATION SIGNS #05 & #07

SCALE: 3/4" = 1'-0"

#13 KWIK TRIP FREESTANDING MONUMENT SIGN

SEE ATTACHMENT FROM LA CROSSE SIGN CO

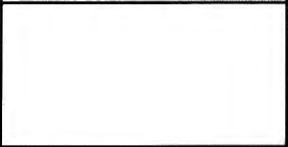


STORES



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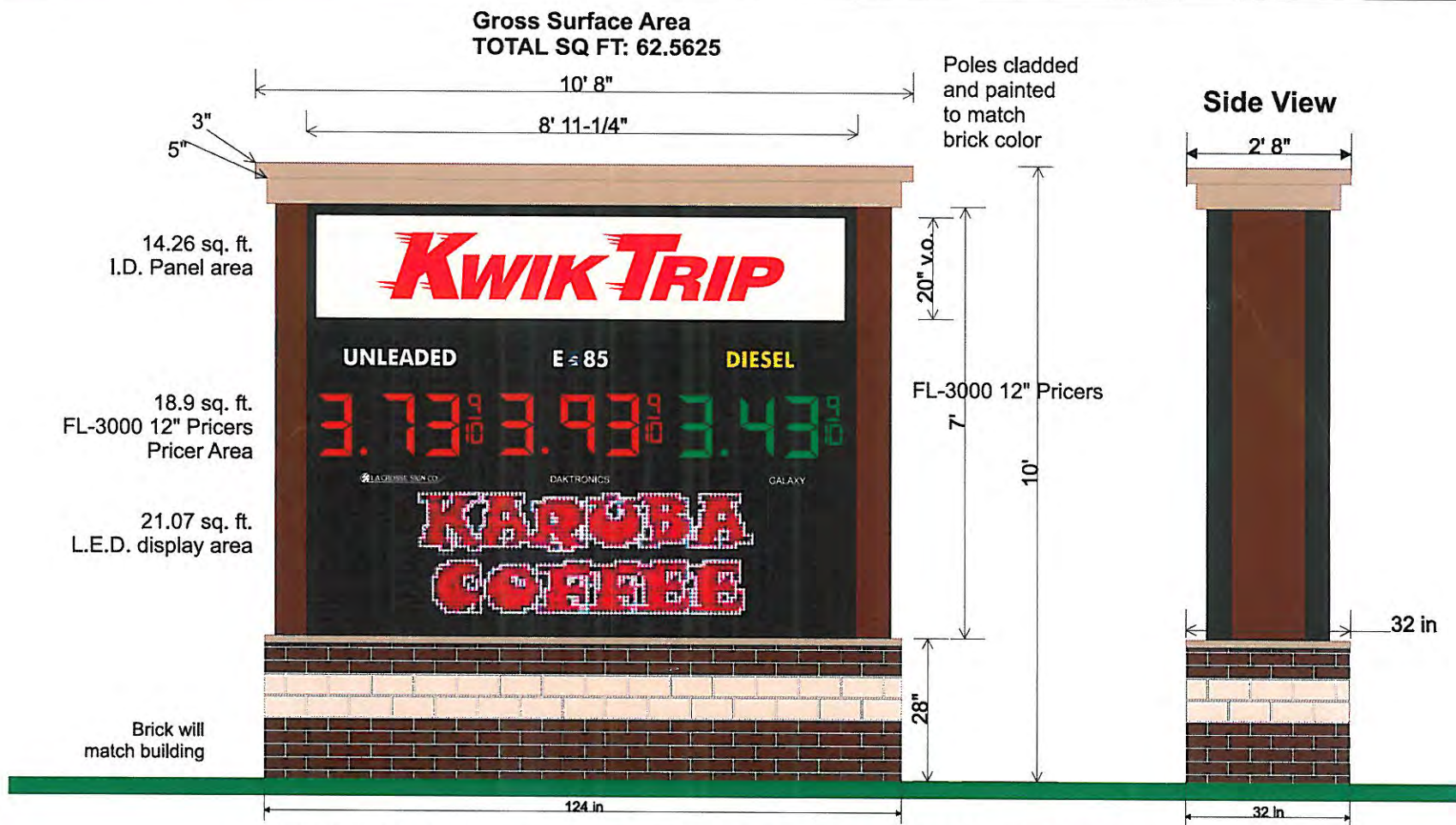
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DIRECTIONAL SIGNAGE	CONVENIENCE STORE #248 WITH SINGLE CW		HUDSON BLVD LAKE ELMO, MN
	#	DATE	
DRAWN BY		B. DUSEK	
SCALE		MULTIPLE	
PROJ. NO.		0001	
DATE		2014-05-08	
SHEET		DS1	

Kwik Trip

#248 Lake Elmo, MN



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Artist: Danielle Waas
Sales: Cindy Bluske
Scale: 3/8"=1'

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APPROVED BY: _____

MEMORANDUM

FOCUS ENGINEERING, inc.

Cara Geheren, P.E. 651.300.4261
Jack Griffin, P.E. 651.300.4264
Ryan Stempski, P.E. 651.300.4267
Chad Isakson, P.E. 651.300.4285

Date: July 9, 2014

To: Nick Johnson, City Planner
Cc: Kyle Klatt, Planning Director
From: Jack Griffin, P.E., City Engineer

Re: Lakewood Crossing – Kwik Trip
Preliminary/Final Plat Review

An engineering review has been completed for the Lakewood Crossings – Kwik Trip. Site Plans were received on June 30, 2014. The submittal consisted of the following documentation prepared by Carlson McCain:

- Lakewood Crossing Preliminary Plat dated June 27, 2014.
- Kwik Trip Store #248 Site Plans dated June 27, 2014.
- Kwik Trip Store #248 Storm Water Management Plan dated June 27, 2014.

Engineering review comments are as follows:

Preliminary Plat / General Comments:

1. The drainage and utility easements must be placed on both the utility plan, Sheet SP4, and the grading plan, Sheet SP3 to demonstrate sufficient easement is being provided. A minimum 15 feet of utility easement is required from the centerline of the proposed watermain utility. It appears additional easement is required.
2. Governing Specifications must be in accordance with the City of Lake Elmo Standard Specifications for General Requirements, including summary of work, project meeting requirements, submittals, regulatory requirements, testing, traffic control, erosion and sedimentation and pollution control, and close-out requirements; and for utility technical specifications. A specification booklet must be created for the project, approved by the City and provided to the contractor for use on the project.
3. Engineering has not reviewed the proposed lighting and landscape improvements.

Site Plans, Traffic and Access Management Requirements:

1. Hudson Boulevard is a local collector roadway and Municipal State Aid route. Hudson Boulevard is expected to receive significant growth in traffic volume as the I94 corridor develops. The road is considered to be a major collector for serving the area but it is the goal of the City to maintain the road as 2-lanes. In order to achieve that goal left and right turn lanes will need to be implemented throughout the corridor to facilitate the turning movements for the developing areas while maintaining the mobility of the through traffic.
2. The Access Management Guidelines per the City's Comprehensive Transportation Plan requires a minimum spacing of 1/8 mile (660 feet) for commercial driveway access along Hudson Boulevard. The proposed westerly access for this site plan appears to sufficiently meet this spacing at ~550 feet. The westerly driveway can be permitted full access if the access is developed as a shared access driveway with Outlot A.
3. The property to the north will also be required to access Hudson Boulevard at this location.
4. The shared access driveway width should be at least 40 feet to accommodate one inbound lane (16 feet wide) and two 12 feet wide outbound lanes (left and right turn lanes).
5. The easterly driveway should be permitted as a right-out-only exit. Future improvements to Hudson Boulevard will likely include the placement of a center raised median to prohibit left turning movements from this site.

6. The right-out-only exit should be revised to increase the site lines for traffic existing the facility. This can be achieved by increasing the intersection angle. The construction of a dedicated acceleration lane could also be considered to address safe egress from the site.
7. The site plan proposes a temporary left turn lane for the westerly driveway access as requested by staff. The pavement markings shall be revised using larger tapers more appropriate for the posted speed limit (see attached TKDA review memorandum and sketch dated June 26, 2014).
8. The plans must be revised to provide greater plan and specification detail for the improvements to Hudson Boulevard. Detailed dimensions shall be placed on the plan sheets indicating start and stop stationing, lane widths, shoulder widths, etc.
9. Shoulder widening details must be dimensioned indicating start and stop stationing, road width increases, pavement section details, and material details. Pavement widening must be completed using minimum practical widths and avoiding tapering at the end points.
10. Specifications and plan notes must be added to further detail the improvements to Hudson Boulevard.
11. All pavement markings shall be shown as epoxy resin with drop-on glass beads per the City's standard specifications.

Grading, Drainage & Erosion Control Plan:

1. City Standard Plan Notes for Grading and Erosion Control, Details 600A-D, must be placed on the Grading and Erosion Control Plan, Sheet SP3. All other conflicting information must be removed. Supplemental notes may be provided if not already addressed by the City standards.
2. Governing Specifications for grading and erosion control must be in accordance with the City of Lake Elmo standard specifications.
3. Retaining walls that exceed 4 feet in height must have a design submitted and certified by an engineer licensed in the state of Minnesota.

Utility Plans:

1. City Standard Plan Notes for Watermain, Detail 200A, for Sanitary Sewer, Detail 300A, and for Storm Sewer, Detail 400A, must be placed on the Utility Plan, Sheet SP4. All other conflicting information must be removed. Supplemental notes may be provided if not already addressed by the City standards.
2. Governing Specifications for utilities must be in accordance with the City of Lake Elmo standard specifications.
3. Add City Standard Detail Plate 210 – Watermain offset to plan sheet SP7. All watermain must be installed at a depth of 7.5 feet or deeper. Shallow watermain with insulation will not be allowed. Watermain offsets will be required when conflicts occur. This appears to be needed at the sanitary sewer crossing point.
4. The water and sewer laterals extending into Outlot A should be realigned to remain parallel to Hudson Boulevard to allow more flexibility to the future extension for this property.
5. The proposed storm sewer system is being constructed entirely on privately owned property and does not conform to the City engineering design standards for storm sewer systems. The storm sewer system should therefore remain privately owned and maintained by the applicant, including the three driveway culverts.
6. Additional pipe cover appears to be needed over the proposed driveway culverts.

Stormwater Management:

1. The Storm Water Management Plan must meet VBWD rules and requirements. The applicant must provide the City written approval from the VBWD.
2. The proposed storm water facilities will receive storm water from a privately owned and maintained storm sewer system that will not be constructed to City engineering design standards. It is therefore recommended that the storm water facilities be privately owned and maintained. A maintenance agreement in a form acceptable to the City should be executed and recorded with the County for all permanent storm water facilities to be located on private property. The agreement shall provide a maintenance plan defining the maintenance responsibilities for the private owner, the type of maintenance and the maintenance intervals.



444 Cedar Street, Suite 1500
Saint Paul, MN 55101
651.292.4400
tkda.com

Memorandum

To:	<u>Ryan Stempski</u>	Reference:	<u>Lakewood Crossing</u>
Copies To:	<u></u>		<u>Traffic Review</u>
	<u></u>		<u>City of Lake Elmo</u>
	<u></u>		<u></u>
From:	<u>Bryant Ficek</u>	Project No.:	<u>15545.000</u>
Date:	<u>June 26, 2014</u>	Routing:	<u></u>

Per your request, we have reviewed the Turn Lane Exhibit for the Lakewood Crossing, dated June 11, 2014, and sent for our review on June 18, 2014. The exhibit shows two points of access for the site, both off Hudson Boulevard. A westbound left-turn lane from Hudson Boulevard into the site is established through restriping and limited pavement widening. The exhibit is attached to this memorandum for reference.

Our review focused on the ingress and egress of the site, particularly the proposed design of the turn lane on Hudson Boulevard. Based on this review, our comments are:

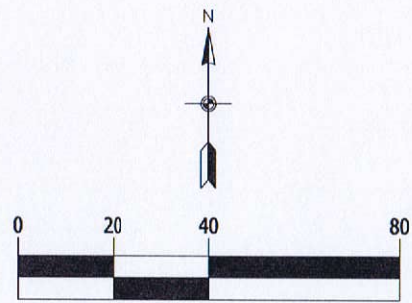
- Keats Avenue is a County State Aid Highway and an A Minor Arterial. The City's 2030 Comprehensive Plan lists an existing average daily traffic of 6,100 vehicles per day on this roadway. Hudson Boulevard is a local frontage road, proposed to be a Major Collector in the future. It has an average daily traffic volume of 1,300 as listed in the City's Comprehensive Plan. The exhibit shows that the site, appropriately, will be accessed from Hudson Boulevard, which is both lower on the hierarchy of roads and carries less traffic than Keats Avenue.
- The west access should be planned for full access into and out of the site. It is proposed to be located farthest from the Hudson Boulevard/Keats Avenue intersection (~550 feet), which will limit the interaction and influence of operations between these two intersections. The width of this driveway should be at least 40 feet to accommodate one inbound lane (16 feet wide) and two outbound lanes (left-turn lane 12 feet wide and right-turn lane 12 feet wide).
- The proposed east access should be planned as a right-out-only exit. Unlike other potential movements to and from this access, right turns from the site will have limited impact to traffic operations on Hudson Boulevard and its intersection with Keats Avenue. Providing a right-out-only exit will also improve site circulation, particularly for truck movements. Proper signing and striping should be used to adequately notify drivers of the restrictions on the access, such as Do Not Enter signs facing traffic on Hudson Boulevard.

- To provide the necessary width for the proposed left-turn lane, the through lanes in each direction on Hudson Boulevard are proposed to be shifted outward using 1:20 tapers. Given that Hudson Boulevard has a posted speed limit of 50 mph, this taper is too abrupt to shift through traffic. A taper rate of 1:50 is more appropriate and satisfies MnDOT State Aid Standards. This will increase the taper distance from 120 feet to 325 feet. There appears to be sufficient space available on each side of the proposed west access to accommodate this larger taper rate. A painted median on Hudson Boulevard would also help to reinforce the right-turn-only exit from the east access.
- Ideally, the combined turn lane length and taper length from a through lane provides for deceleration from the through lane's travel speed and sufficient space for the expected vehicle stacking. If this guideline were followed in this case, the posted speed limit of 50 mph would translate into a distance of 440 feet and a minimum of two cars for stacking would translate into a minimum of 60 feet. The minimum turn lane and taper length would then be 500 feet. However, other circumstances are frequently taken into account, such as accounting for deceleration in the through lane before the turn lane and consideration of the land use or expected traffic operations associated with the turn lane. Based on the characteristics of this site and anticipated traffic operations, a minimum total distance of 280 feet is recommended to provide for a comfortable transition from the through lane (1:10 taper rate or 130 feet) and stacking for several cars and a truck (150 feet).
- As an alternative to the tapers on Hudson Boulevard described in the two bullets above, back-to-back left-turn lanes between the west access and the intersection with Keats Avenue could be considered. The exhibit does not show improvements to the Hudson Boulevard intersection with Keats Avenue, and no documentation was provided that suggests poor traffic operations are expected. However, if the City anticipates issues, this would be an opportunity to improve traffic operations.
- The proposed width of the shifted through lanes matches the existing lane widths on Hudson Boulevard. The proposed left-turn lane on Hudson Boulevard would provide acceptable width for the traffic operations. If the overall width of the road is a concern, each lane width could be reduced to 11 feet. Although not typically desired, the reduced-width lanes would provide a measure of traffic calming and still allow for sufficient traffic operations.
- Hudson Road is posted as a bike route, and bicyclists are generally be expected to use the existing 6-foot-wide shoulder when traveling on this road. While bicycle travel in a vehicle lane of traffic is legal, maintaining a shoulder for bicyclists is a safer option. A minimum 4-foot-wide shoulder would maintain the safety of the roadway shoulders for bicyclists. The roadway shoulders could also be used by pedestrians.

Two sketches are attached to this memorandum that show potential redesign of Hudson Boulevard. One shows the recommended turn lane length and appropriate tapers. The other shows the potential back-to-back left-turn lanes on Hudson Road, assuming an improvement at its intersection with Keats Avenue. It should be noted that any widening of Hudson Road should sawcut and replace some or all of the existing shoulder. Without sufficient width, the newly constructed roadway could fail to properly bond with the existing roadway and lead to future maintenance issues.

If you have any questions or comments regarding the information presented in this memorandum, please contact me at 651.726.7944 or bryant.ficek@tkda.com.





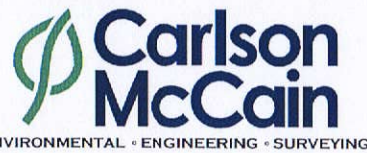
PROPOSED STRIPING
PROPOSED EDGE OF BITUMINOUS

HUDSON BLVD.

KEATS AVE. N.

EXISTING EDGE OF BITUMINOUS
EXISTING FOG LINE STRIPE
EXISTING CENTER LINE STRIPE

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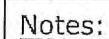
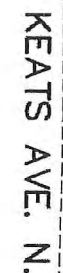
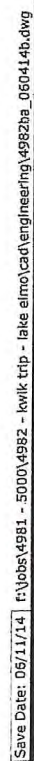


Notes:

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3470 Washington Drive, Suite 102
Eagan, MN 55122

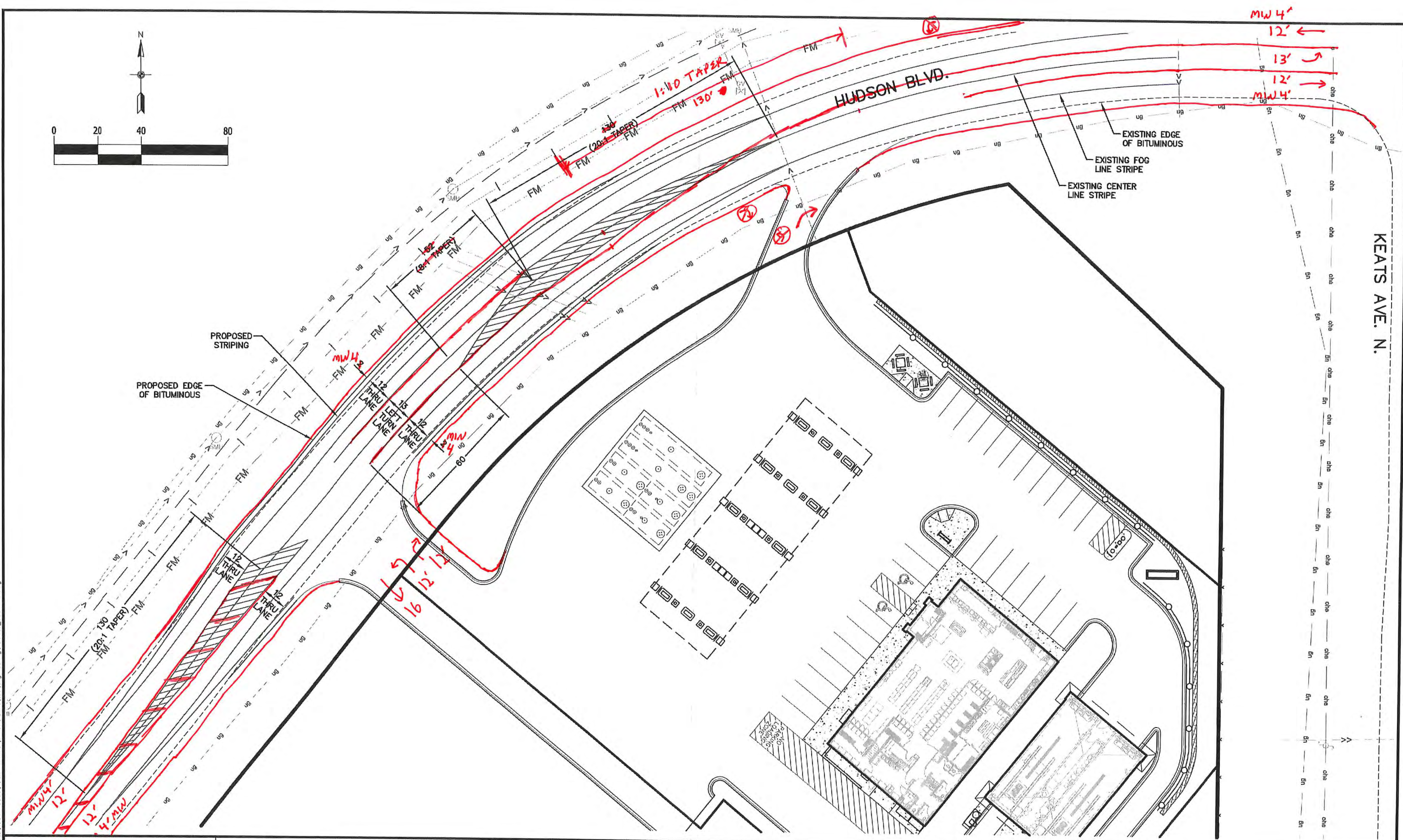
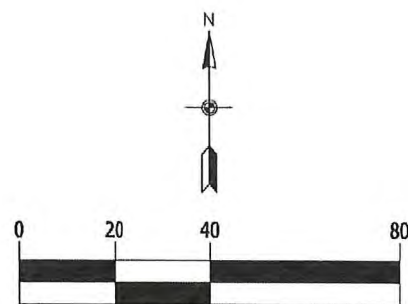
LAKEWOOD CROSSING
Lake Elmo, Minnesota

TURN LANE EXHIBIT



LAKEWOOD CROSSING
Lake Elmo, Minnesota

TURN LANE EXHIBIT



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Notes:

CM PROPERTIES 94
3470 Washington Drive, Suite 102
Eagan, MN 55122

LAKEWOOD CROSSING
Lake Elmo, Minnesota

TURN LANE EXHIBIT



Public Works Department

Donald J. Theisen, P.E.
Director

Wayne H. Sandberg, P.E.
Deputy Director/County Engineer

July 9, 2014,

Nick Johnson
City Planner
City of Lake Elmo
3600 Laverne Avenue North
Lake Elmo, MN 55042

RE: Washington County comments on Kwik Trip, City of Lake Elmo

Dear Nick,

Thank you for providing Washington County with the plans for Kwik Trip adjacent to County State Aid Highway (CSAH 19) Keats Avenue in Section 34 in the City of Lake Elmo. The project consists of a 5,570 square foot convenience/ gas station store, gas pumping islands, car wash and support parking. Based on the plan provided, we have the following comments:

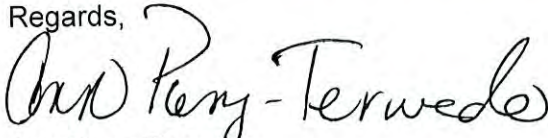
- We reviewed the public right-of-way adjacent the County State Aid (CSAH)19/Keats Avenue and determined that MNDOT has jurisdiction/ownership of a section of property between CSAH 19 and the subject property. This is referenced as Minnesota Department of Transportation Right-of-Way Plat No. 82-43
- Since there is approximately 265 feet of County and Mn Dot public right-of-way along CSAH 19/Keats Avenue no additional right-of way is required.
- The city may wish to consider a modification to the geometrics of the east access drive to Hudson Boulevard. We have provided the attached drawing for reference.
- As development increases along the I-94 Corridor in this section of Lake Elmo, traffic volumes will increase at Hudson Boulevard and CSAH 19 which will cause more increases in delays and congestion. Since the east access drive is less than the required 660 feet (1/8 mile) from CSAH 19/Keats Avenue, those vehicles exiting the site will have difficulty finding a queue in traffic in order to enter the Hudson Boulevard roadway or there may be impacts to the mobility of the roadway. The city should continue to evaluate the situation as development occurs with the possibility that drive be removed in the future.
- All utility connections that require and work in County right-of-way for the development will require Washington County Right of Way permits. Typically, these are the responsibility of the utility companies.
- Please be aware that the D2 alignment of the Gateway Corridor Transitway project runs parallel to Hudson Road in the vicinity of the proposed Kwik Trip. While the exact details of the alignment are to be determined, the County will continue to

July 9, 2014
Kwik Trip

work with the City of Lake Elmo to ensure that the transitway alignments along Gateway Corridor will be coordinated with existing land uses and future land use changes.

Thank you for the opportunity to comment on this project. If you have any questions, please contact me at 651-430-4362 or ann.pung-terwedo@co.washington.mn.us. For any permit applications, please contact Carol Hanson at Carol.hanson@co.washington.mn.us.

Regards,

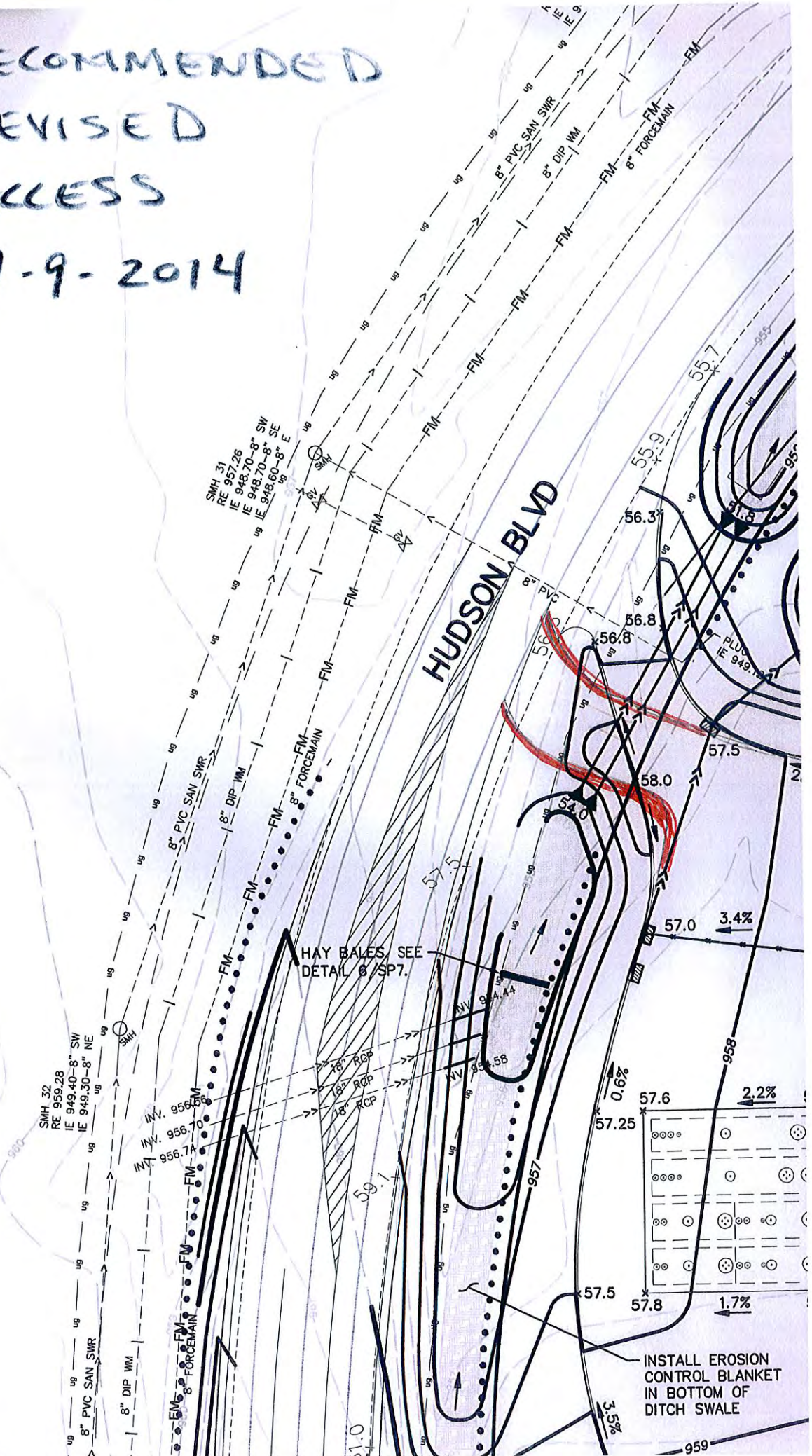
A handwritten signature in black ink that reads "Ann Pung-Terwedo". The signature is written in a cursive, flowing style.

Ann Pung-Terwedo
Senior Planner

C: Carol Hanson, Office Specialist

R/Plat Reviews/City of Lake Elmo/ Kwik Trip

RECOMMENDED
REVISED
ACCESS
7-9-2014



Nick Johnson

From: Sherman, Tod (DOT) <Tod.Sherman@state.mn.us>
Sent: Thursday, July 10, 2014 8:26 AM
To: Nick Johnson
Subject: RE: Lakewood Crossing - Kwik Trip

Nick:

Sorry I can't get a complete review letter out to you by today. Our comments so far are:

Ponding will not be allowed on MnDOT right-of-way. Any use of or work within or affecting MnDOT right of way requires a permit. Permit forms are available from MnDOT's utility website at <http://www.dot.state.mn.us/utility/>. Please include one 11 x 17 plan set and one full size plan set with each permit application. Please direct any questions regarding permit requirements to Buck Craig (651-234-7911) of MnDOT's Metro Permits Section.

A determination as to whether a MnDOT drainage permit is needed will be included in our full letter which we will send you early next week.

Please let me know if you have any questions.

Tod Sherman, Planning Supervisor
Mn/DOT Metro District
1500 W. County Road B-2
Roseville, MN 55113
(651) 234-7794
tod.sherman@state.mn.us

-----Original Message-----

From: Nick Johnson [<mailto:NJohnson@lakeelmo.org>]
Sent: Tuesday, July 08, 2014 10:32 AM
To: 'Ann Pung-Terwedo'; Sherman, Tod (DOT)
Subject: RE: Lakewood Crossing - Kwik Trip

Tod,

I should have sent this one along earlier. My apologies.

Thanks,

Nick M. Johnson | City Planner
City of Lake Elmo, Minnesota
njohnson@lakeelmo.org
(w) 651-747-3912 | (f) 651-747-3901
www.lakeelmo.org

3 Commercial Development

Applicable Zoning Districts:

- Commercial (C)
- Convenience Commercial (CC)

The future commercial areas within the I-94 Corridor and Old Village will include a variety of service, office, retail and other uses that will serve existing and future Lake Elmo residents and beyond. While the differences in character and geography of these two growth areas may attract different types of commercial uses, it is important to establish standards that will ensure quality development outcomes regardless of location and use classification. In the commercial districts, quality development outcomes consist of buildings of high architectural quality and sites that function well for all users, both drivers and pedestrians.

A. Site Design

Building Placement

Goal: To ensure access and circulation for all users in a manner that minimizes traffic disruption and safety concerns, as well as maintains good sightlines from the public street.

- Buildings must be setback at least 30 feet from the public right of way. Buildings are encouraged to be located as close to the public street as possible while still meeting the setback requirement.
- The orientation of multiple buildings on one site must be clearly coordinated.
- Buildings should be oriented parallel or perpendicular to the street they front, promoting continuity of design.
- Buildings should be arranged to provide convenient access to entrances and efficient on-site circulation for vehicles and pedestrians.
- Shared access points from the public ROW are encouraged. Vehicular access points should be limited to minimize traffic disruption.
- For master planned development, the provision of landscaped open or gathering spaces is encouraged within commercial developments.

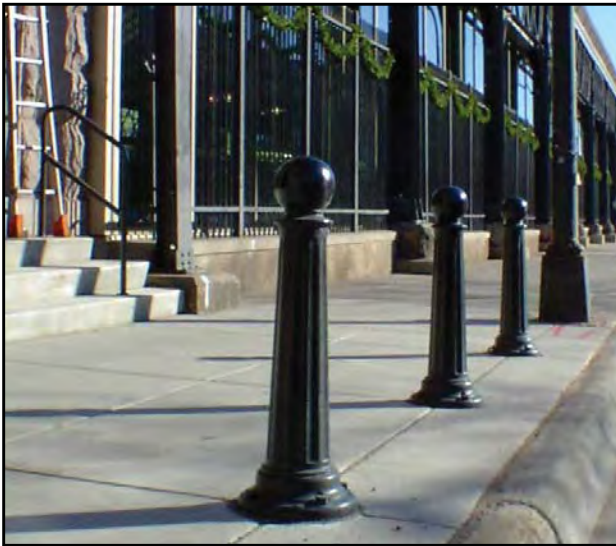


Landscaped gathering spaces provide a distinctive and welcoming space for visitors.

COMMERCIAL DEVELOPMENT



Fencing around outdoor dining areas helps delineate pedestrian walkways and patio space.



Bollards are an effective tool in delineating the pedestrian space. This style of bollard is drawn from the Lake Elmo Branding and Theming Study.

Streetscape

Goal: To create safe, pleasant and functional pedestrian spaces within commercial development, improving access and district identity.



Streetscapes should include plant beds with trees to provide shade and add aesthetic value along sidewalks.

- Sidewalks are required along primary street frontages, unless a suitable alternative that promotes pedestrian access safety is approved. In addition, pedestrian access to the building from the public street shall be provided.
- Street trees shall be installed at regular intervals along the public right of way.
- Ornamental or bollard lighting is encouraged to increase safety, as well as add visual interest.
- Fencing shall be installed around outdoor dining areas that are adjacent to pedestrian areas or streets.
- Site furnishings such as decorative fencing, trash receptacles, planters, bicycle racks, and benches are recommended. Applicants are encouraged to utilize design elements and site amenities from the Lake Elmo Branding & Theming Study.

Landscaping

Goal: To ensure development of a high aesthetic quality, and to reduce the amount of impervious surface at commercial sites.

- Parking, public and streetscape areas should utilize trees, plant beds, and potted plants to add visual interest and break up continuous hardscape.
- Parking, service, storage and utility areas should be buffered by plantings. Near areas of pedestrian circulation, these plantings shall not exceed 4 feet in height for safety purposes.
- Hardy and native plant materials that are resistant to the climate, disease and salt are encouraged.
- Making use of similar plant materials as adjacent properties and public spaces is encouraged to create continuity.
- Mature trees located on building sites should be retained whenever possible.
- Bare soils should be planted or mulched with bark, stone or other suitable material to avoid unnecessary runoff.

Parking

Goal: To provide parking facilities that adequately serve the needs of commercial properties, while ensuring pedestrian safety and maintaining a positive visual aesthetic from the public right of way.



Sidewalks within larger parking lots improve pedestrian circulation and safety.



Landscaping strips along public streets add a visual separation between parking areas and the public right-of-way.

- a. The linear measurement of surface parking areas parallel to the public street are encouraged not to exceed more than 60% of primary street frontages. Sites or projects that are unable to meet this guideline are encouraged to install berms and/or additional landscaping along areas of surface parking adjacent to the primary street frontage.
- b. The entrance to parking facilities should be located on secondary streets when possible.
- c. Shared parking facilities between adjacent uses or businesses are encouraged when possible to avoid excessive amounts of parking.
- d. Structure parking is encouraged, and should be located behind or beneath primary buildings when possible.
- e. Structure parking or parking areas located beneath the primary structure should be screened with architectural elements that match the primary building.

- f. Parking areas should be screened from view of public streets by means of grading and/or landscaping.
- g. Parking areas should be screened from adjacent structures with landscaping strips not exceeding 4 feet in height in order to ensure pedestrian safety.
- h. Landscaped islands should be installed within surface parking areas to break up continuous hardscape and reduce concentration of impervious surface.
- i. Lighting must be provided in parking areas at night for safety purposes. However, direct glare, spillover or other forms of light pollution directed at adjacent properties are prohibited.
- j. Parking facilities must be ADA compliant when deemed necessary.

Delivery, Service, Storage and Utility Areas

Goal: To provide physical and visual separation between delivery, service and storage areas and areas of pedestrian and automobile circulation.

- a. Service, storage, maintenance or trash collection areas should be located out of the view from the public right of way, or significantly screened through landscaping or architectural features.
- b. Service, storage and trash collection areas are not allowed in the setback areas.
- c. The location of delivery, storage and service areas should be clearly marked with signage and should not interfere with other automobile or pedestrian circulation.
- d. Storage and delivery areas should be hard surface, minimizing the dispersal of dust.

B. Building Design

Form and Facade

Goal: To ensure structures of high architectural quality that promote visual interest, thereby supporting district identity.

- a. Blank facades without windows and doors are discouraged. All sides of structures should have architectural treatment. Variety and creativity in building facade is encouraged through changes in building materials, fenestration height, and roof lines, especially on primary facades that face the public right of way.
- b. Window and door styles shall reflect the prevailing architectural style of the structure.
- c. Ground level retail and commercial uses should employ a significant amount of transparent glass in the form of windows and doors, particularly near pedestrian entrances.



Facade articulation and windows with architectural detail add visual interest and break up long expanses of continuous façade.

COMMERCIAL DEVELOPMENT



High quality and durable building materials add aesthetic value and create a more attractive environment.

- d. Minimizing continuous expanses of wall through facade articulation, recession and projection is encouraged.
- e. Structures that are oriented towards the public street are encouraged to provide multiple access points or entrances if the parking area is located to the rear of the structure.
- f. Architects and builders are encouraged to incorporate topographical features into the form of the structure when possible, utilizing natural grades to create unique design.

Building Materials

Goal: To promote quality development through durability and visual aesthetics, thereby supporting district identity.

- a. High-quality and durable materials should be used in street facing facades.
- b. Primary building materials for commercial structures should include brick, finished wood,

stone, cast stone, or pre-cast concrete panels with exposed aggregate, banding, texturing, or other similar decorative finish treatment.

- c. High quality synthetic materials that adequately duplicate natural materials may be acceptable if approved by the City, including, but not limited to, thin brick, hardi plank, decorative concrete masonry, and other materials.
- d. The following building materials are not allowed to be used as the primary facade for commercial development:
 - Unpainted galvanized metal
 - Unfinished “green-treated” lumber
 - Unfinished wood
 - Plain or unpainted concrete
 - Painted concrete block may be used on the rear of the building or sides not visible from the public right of way.
 - Vinyl siding
- e. Facade colors should reflect subtle earth tones. However, other primary facade colors will be considered by the Review Authority. Accent materials should complement the colors of the primary facade.

Scale and Mass

Goal: To establish standards for building with a human scale in mind.

- a. Buildings should be broken down into smaller parts to avoid monotonous or continuous design and the appearance of mass.
- b. Exterior design that provides the appearance of multiple structures is encouraged to reduce scale and minimize mass.
- c. Building mass should be broken up with mul-

tle roof and ridgelines perpendicular with one another.

- d. Structures of two-stories or higher should have articulated facades to minimize the appearance of mass, as well as multiple roof lines with corresponding gables.
- e. Scale should be reduced by utilizing “step-down” methods towards the public street. Entries and other bump out features are effective in this regard.



Parapets of varying heights add architectural interest and accentuate building entries.

Roof Design

Goal: To ensure architectural consideration and consistency in roof design in relation to the structure, and to reduce the visual impact of rooftop equipment.

- a. The design of the roof must be consistent with the overall architecture or design of the structure.
- b. Parapets of varying heights are required for large commercial buildings with flat roofs.
- c. Rooftop equipment, particularly on flat roofs, must be screened by the parapet or other architectural features.

Entries

Goal: To provide identifiable entryways that emphasize access, pedestrian safety, architectural quality and a human scale.

- a. Entryways to commercial structures should be accessible for pedestrians from the public right of way. Large retail sites in particular should consider installing a dedicated pedestrian way.
- b. Architectural features should be incorporated into entryways, such as facade detailing or prominent windows.
- c. The use of canopies, awnings and other sheltering features are encouraged.
- d. Pedestrian amenities such as trash receptacles, benches, or lighted bollards are encouraged near entryways to commercial buildings.

Lighting

Goal: To ensure safety of patrons, employees, pedestrians and automobiles, as well as providing visual interest and aesthetic value to a site, while limiting light pollution of the night sky to the best extent possible.

- a. Lighting must be provided in entryways, parking areas, pedestrian ways, storage and service areas, and other locations that require additional safety lighting.
- b. Lighting height shall be consistent with the City's exterior lighting standards.
- c. Lighting styles should be complementary to the architectural style of the building.
- d. Lighting of architectural features should be used to provide accent and interest, as well as identify the building entryway. Architectural lighting must be downcast and shielded to prevent light pollution.
- e. Bollard lighting is encouraged for pedestrian areas.
- f. Overhead lighting must be shielded to prevent light trespass and spill-over onto adjacent properties.
- g. Commercial uses near residential zones must utilize lighting that minimizes light trespass.
- h. Bare bulb and exposed neon lighting are not allowed.

Signage

Goal: To provide signage that clearly identifies businesses within the Commercial district, while promoting quality and consistency in terms of design and materials.

- a. Building signage should be complementary to the architecture of the structure, as well as consistent with the style of the surrounding buildings or district.
- b. Sign elements that will be evaluated for consistency include scale, color, lighting and materials.
- c. Signs must be constructed of high-quality, durable materials.
- d. Directional signage to delivery, service and storage areas is required.
- e. Two and three-dimensional signs are encouraged to promote creativity and district identity.
- f. All buildings are encouraged to incorporate elements of community theming in appropriate signage, supporting district and city identity.



Signage should complement the architectural style of the building.

Proposed Kwik Trip Location: Site Visit, 7/9/14



Looking west down Hudson Blvd. towards Keats Ave.



Looking east down Hudson Blvd. towards the subject property



The subject property looking southwest from Hudson Blvd.



The subject property looking south towards I-94 and Keats Ave. N.