



March 22, 2019

Mr. Ken Roberts  
Planning Director  
City of Lake Elmo, Minnesota  
3880 Laverne Avenue North  
Lake Elmo, Minnesota 55042

Subject: CUP Application for Metro Transit Park and Ride at  
I-94 and Manning Avenue  
Lot 1, Block 1 of Four Corners 2<sup>nd</sup> Addition to the City of Lake Elmo

Dear Mr. Roberts:

On behalf of the Metropolitan Council, SRF is pleased to provide the following written statement regarding the proposed Conditional Use Permit (CUP) for the proposed I-94 and Manning Avenue park and ride. The following items have been compiled in response to item number 2 (Written Statements) on the Conditional Use Procedure for the City of Lake Elmo document. The items are addressed in the order indicated on the document

- a. *A listing of contact information including name(s), address(es) and phone number(s) of: the owner of record, authorized agents or representatives, engineer, surveyor, and any other relevant associates;*

Current Owner of Record: EN Properties, LLC  
11530 Hudson Boulevard North  
Lake Elmo, MN 55042  
(651) 777-9072  
Terry Emerson

Proposed Owner of Record: Metropolitan Council  
560 Sixth Avenue N  
Minneapolis, MN 55411  
(612) 349-7761  
Mac Sweidan

Engineer: SRF Consulting Group, Inc.  
1 Carlson Parkway North, Suite 150  
Minneapolis, MN 55447  
(763) 249-6756  
Mike Aaron

Surveyor: HTPO  
7510 Market Place Drive  
Eden Prairie, MN 55344  
Charles Howley

- b. *Listing of the following site data: Address, current zoning, parcel size in acres and square feet, property identification number(s) (PID), and current legal description(s);*

Property Address: 11530 Hudson Boulevard North  
Lake Elmo, MN 55042

Current Zoning: Rural Development Transitional (RT)  
Planned Zoning: Commercial (C)

Parcel Size: 5.66 Acres, 246,806 SF

Current Property  
Identification Number: 3602921430005

Legal Description: Lot 1, Block 1 of Four Corners 2<sup>nd</sup> Addition to the City of Lake Elmo, Washington County, Minnesota.

- c. *A narrative regarding the history of the property (current and past uses) and any pre-application discussions with staff;*

The current and past use of the property is for agricultural purposes. The property has been agricultural fields for at least as far back as we have found historical aerial photos, 1947.

Metropolitan Council staff have had many discussions with City staff regarding the proposed park and ride project. The park and ride layout has been included in submittals that the current property owner, EN Properties, LLC (Terry Emerson), has made to the City for the creation of the Four Corners subdivisions. City staff have been assisting the current property owner and Metropolitan Council staff with the project since 2017.

- d. *A specific written description of the proposed use(s);*

i. *Incorporate information describing how the use will work on the proposed site including proposed site changes, existing open spaces, landscaping, traffic circulation, transition areas to adjacent properties, individual uses for existing and proposed structures, and effects on natural areas (wetlands, forests, etc.) both on-site and in the general vicinity of the project.*

The proposed site development will provide a 550 stall park and ride that will increase capacity along the eastern I-94 corridor. The site will also include a bus stop and passenger waiting area/shelter; a driver restroom/maintenance room; passenger drop off/pick up area; lighting; security cameras; bike parking; and electric vehicle charging stations.

Passenger vehicles will enter the site through the southern entrance off of the proposed relocated Hudson Boulevard. This will be a full intersection. The passenger vehicles will park in the spaces east of the passenger waiting and loading area. When passenger vehicles leave the site, they may exit from the southern entry or the northern exit. The northern exit point is a right-out only. It is anticipated that this northern exit will also be used as a joint exit by the property to the north as development occurs. Buses will enter the site through the southern entrance. They will immediately turn north into the buses only loading and unloading drive. This drive is one way north only and is two (2) lanes wide so that buses can pass a bus that is loading or unloading. When buses leave the site, they will exit from the northern right-out only exit.

Landscape screening is provided along Hudson Boulevard. Landscaping of the site is provided to meet City requirements. There are no natural areas (wetlands, forests, etc.) on or in the general vicinity of the site. There are no existing structures on the site. A 10.5' x 12.0' drivers only restroom building will be placed at the north end of the passenger loading and unloading area. In addition to housing a driver's restroom, the building will have a mechanical room that will also house security system equipment for the park and ride lot.

The parcel to the south of the site will be a storm water treatment area for the subdivision. The site storm water treatment will be accomplished through an underground infiltration system on the site. The parcel to the north of the site is anticipated to be developed in the future.

- ii. *Provide specific details about the use including the number of employees, hours of operation, maximum number of animals (if applicable), proposed development schedule, etc.*

The proposed park and ride facility will provide 550 stalls. Thirteen will be accessible stalls, ten will be for drop offs, and two (2) will be for electric vehicle charging. Six (6) additional stalls can be converted to electric vehicle charging stalls in the future if desired. New express bus service to downtown Minneapolis will run from the park and ride facility. The initial startup of the facility will have six (6) trips in the morning and six (6) trips in the evening.

The facility will benefit the City of Lake Elmo by being the impetus for commercial development in the area and providing infrastructure improvements. New transit options are provided to the transit commuter and highway congestion will be reduced for the auto commuter.



The current schedule for the project is to seek City approvals from March through April; request construction bids in April, and begin construction in the fall of 2019. We would expect construction to be completed in the spring of 2020.

e. *Provide justification that the proposed use meets the following findings:*

- i. *The proposed use will not be detrimental to or endanger the public health, safety, comfort, convenience or general welfare of the neighborhood or City.*

The park and ride facility will be of a benefit to the neighborhood and City. Convenient transit operations will provide easy access to downtown Minneapolis for commuters within the City. The site will be well maintained by Metro Transit and security will be a top priority. The site will have surveillance cameras and will follow City requirements for site lighting. Landscaping will be provided to meet the City's requirements.

- ii. *The use or development conforms to the City of Lake Elmo Comprehensive Plan.*

The 2040 Comprehensive Plan designates the proposed area as a park and ride. The park and ride has been in discussion with the City since at least 2011, when the Planning Commission's Work Plan included an item to discuss on potential rezoning on two sites for a park and ride along I-94. The corner of Hudson Boulevard and Manning Avenue was one of these two sites discussed.

- iii. *The use or development is compatible with the existing neighborhood.*

The site is adjacent to I-94 and Manning Avenue and provides easy access for commuters and buses. The proposed commercial development to the north may include a financial building, a restaurant, a gas station/convenience store, and a daycare. The park and ride facility would fit in well with these uses. As residential develops in the area of the park and ride, residents will have easy access to transit options.

- iv. *The proposed use meets all specific development standards for such use listed in Article 7 of the Zoning Code.*

The park and ride facility will comply with the requirements in Article 7 of the Zoning Code.

- v. *If the proposed use is in a flood plain management or shoreland area, the proposed use meets all the specific standards for such.*

The project is not with a flood plain management or shoreland area.

- vi. *The proposed use will be designed, constructed, operated and maintained so as to be compatible in appearance with the existing or intended character of the general vicinity and will not change the essential character of the area.*



The park and ride facility will be compatible with the appearance of existing or intended character of the general vicinity in which it is located. The only building will be a small (10.5' x 12.0') driver restroom facility that will be compatible in appearance with residential and proposed commercial buildings in the area. The remainder of the site will be used for surface parking purposes that are comparable to other surface parking lots in the area.

*vii. The proposed use will not be hazardous or create a nuisance as defined in the zoning code to existing or future neighboring structures.*

The park and ride facility will have only one small (10.5' x 12.0') driver restroom facility that will not be hazardous or create a nuisance as defined in the zoning code to existing or future neighboring structures.

*viii. The proposed use will be served adequately by essential public facilities and services, including streets, police, and fire protection, drainage structures, refuse disposal, water and sewer systems and schools or will be served adequately by such facilities and services provided by the persons or agencies responsible for the establishment of the proposed use.*

The park and ride facility will be served adequately by essential public facilities and services. The proposed relocated Hudson Boulevard will provide street access to the site. Metro Transit police will provide police service to the site. Fire protection is anticipated to be provided by the City of Lake Elmo. Storm water management will be handled on site with storm water structures, storm sewer, and an underground infiltration system. Refuse disposal will be handled by Metro Transit contractors. Water and sewer will be provided with new service lines being installed with the proposed relocation of Hudson Boulevard.

*ix. The proposed use will not create excessive additional requirements at public cost for public facilities and services and will not be detrimental to the economic welfare of the community.*

Public facilities and services are being installed in the proposed relocated Hudson Boulevard by the subdivision developer. Therefore, the park and ride facility will not create excessive additional requirements at public cost for public facilities and services and will not be detrimental to the economic welfare of the community.

*x. The proposed use will not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons, property or the general welfare because of excessive production of traffic, noise, smoke, fumes, glare or odors.*

The park and ride facility will not be detrimental to any persons, property, or the general welfare because of excessive production of traffic, noise, smoke, fumes, glare or odors. Traffic impacts have been studied and found to not be of substantial impact. Being a park and ride facility, the overall reduction in automobile travel times will reduce exhaust emissions, traffic congestion and consumption of fossil fuels.

- xi. Vehicular approaches to the property, where present, will not create traffic congestion or interfere with traffic on surrounding public thoroughfares.*

Traffic impacts for the park and ride facility have been studied and found to not be of substantial impact. As the development around the park and ride facility increases, traffic signal improvements at the Hudson Boulevard and Manning Avenue intersection are anticipated.

- xii. The proposed use will not result in the destruction, loss or damage of a natural or scenic feature of major importance.*

There are no natural or scenic features of major importance on or near the proposed park and ride site.

Sincerely,

SRF CONSULTING GROUP, INC.

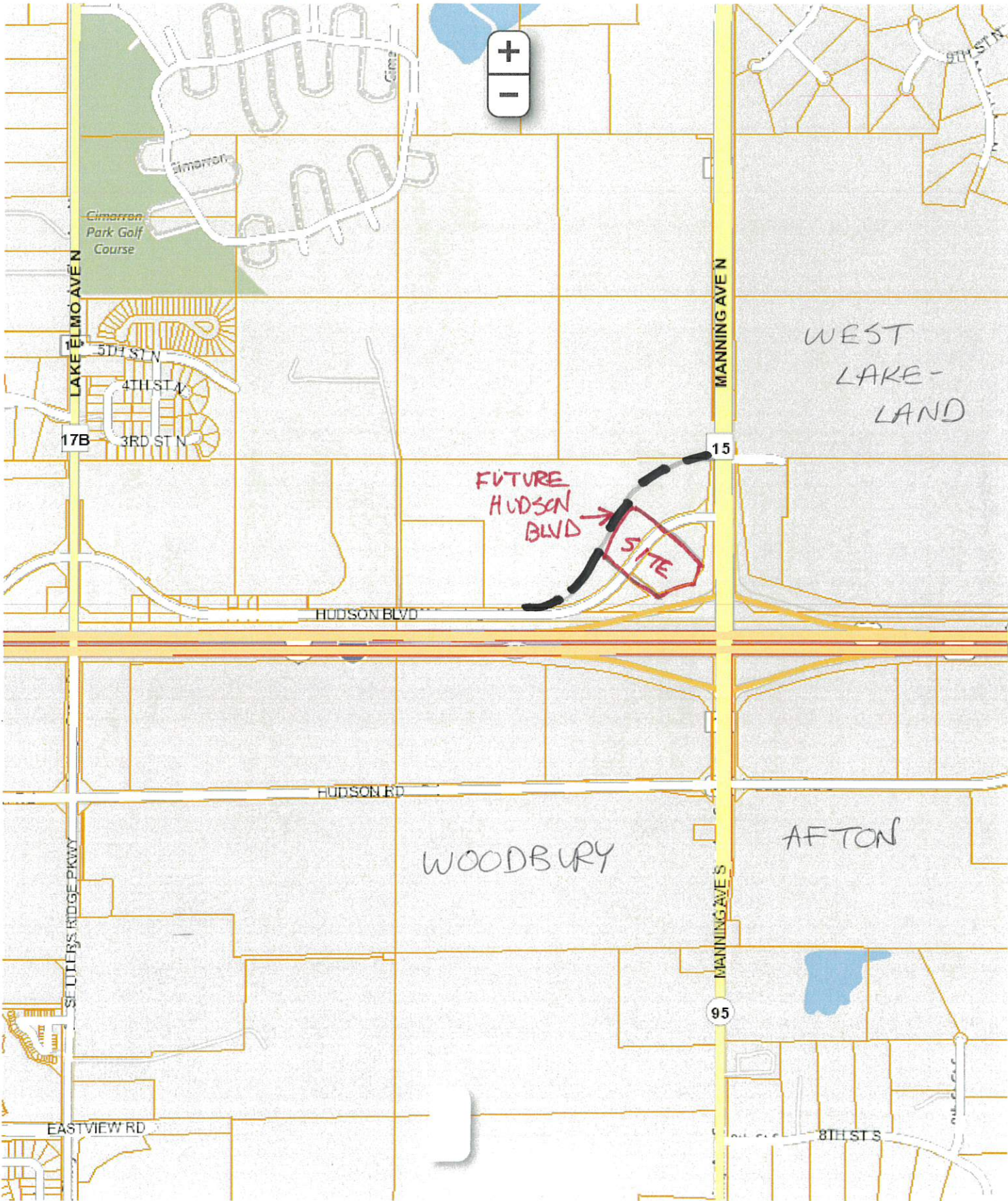


Michael C. Aaron, PE (MN), LEED-AP  
Senior Associate





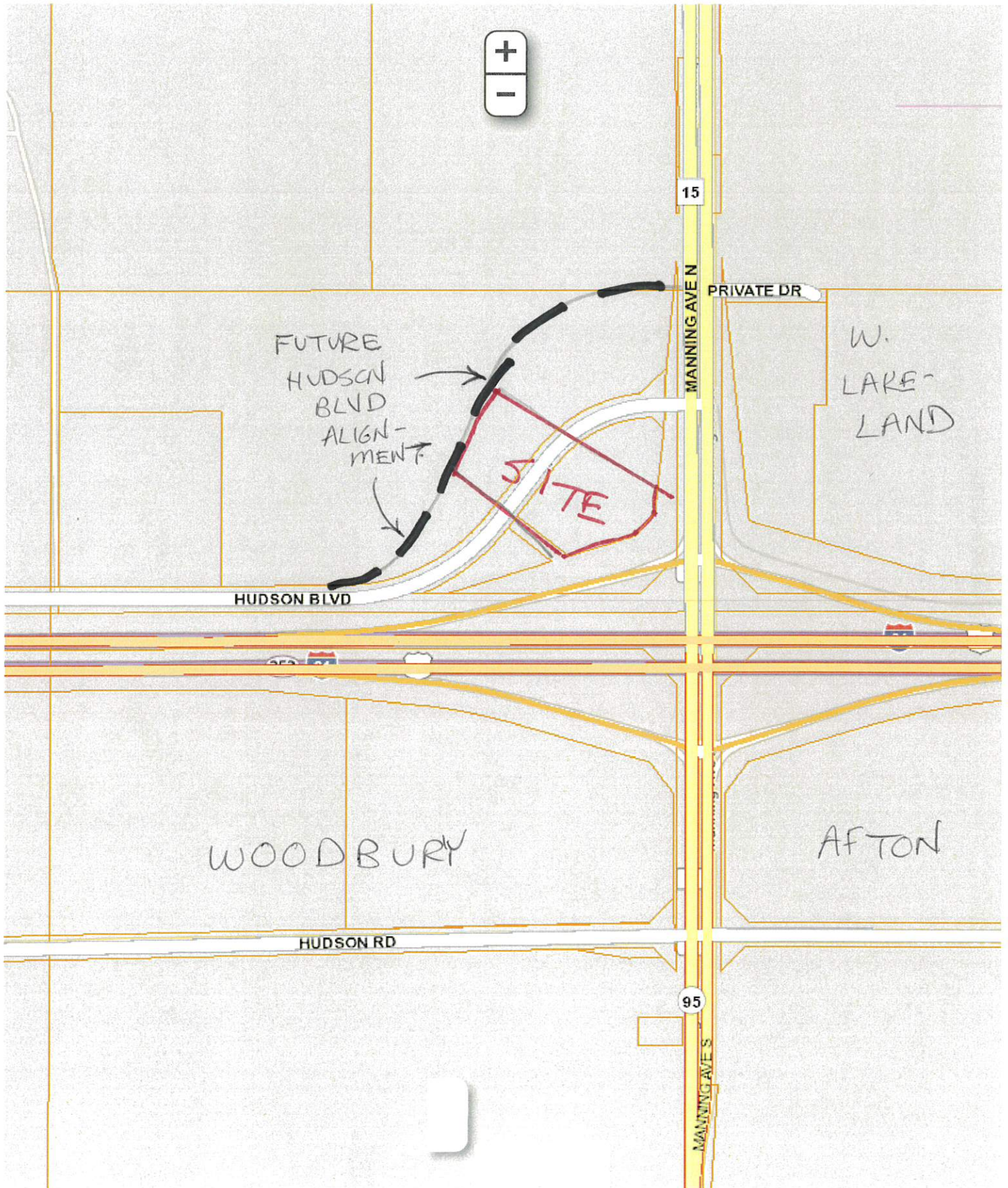
Property Viewer







## Property Viewer



FOUR CORNERS 2ND ADDITION  
PRELIMINARY PLAT

OWNER'S CERTIFICATE

KNOW ALL BY THESE PRESENTS: That E N Properties, LLC, a Minnesota limited liability company owner of the following described property situated in the County of Washington and State of Minnesota, to-wit:

Outlot A, Outlot B, and Outlot C, FOUR CORNERS, according to the plat thereof on file and of record in the office of the County Recorder, Washington County, Minnesota. Also the portions of Parcel 44, Minnesota Department of Transportation Right of Way Plat No. 82-35, State Project No. 8282 (94-392) 904, recorded as Document No. 424557 in said office of the County Recorder, Washington County Minnesota that will accrue to said Outlots by reason of the vacation thereof by the City of Lake Elmo and the Minnesota Department of Transportation.

Has caused the same to be surveyed and platted as FOUR CORNERS 2ND ADDITION and do hereby donate and dedicate to the public the easements created by this plat for drainage and utility purposes only.

In witness whereof, said E N Properties, LLC, a Minnesota limited liability company, has caused these presents to be signed by its proper officer this \_\_\_\_ day of \_\_\_\_\_, 20\_\_.

SIGNED: E N Properties, LLC

By \_\_\_\_\_  
Terry Emerson, Chief Manager

STATE OF MINNESOTA  
COUNTY OF WASHINGTON

The foregoing instrument was acknowledged before me this \_\_\_\_ day of \_\_\_\_\_, 20\_\_ by Terry Emerson, Chief Manager of E N Properties, LLC, a Minnesota limited liability company, on behalf of the company.

\_\_\_\_\_  
(Signature of Notary Public)

\_\_\_\_\_  
(Printed Name of Notary Public)  
My commission expires \_\_\_\_\_

SURVEYOR'S CERTIFICATE

I, Timothy J. Freeman, hereby certify that I have surveyed and platted or directly supervised the survey and platting of the property described on this plat as FOUR CORNERS 2ND ADDITION; that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota; that this plat is a correct representation of the boundary survey; that all mathematical data and labels are correctly designated on the plat; that all monuments depicted on the plat have been correctly set as indicated on the plat; that all water boundaries and wet lands as defined in MS 505.01, Subd. 3 existing as of the date of this certification are shown and labeled on the plat; and that all public ways are shown and labeled on the plat.  
Dated this \_\_\_\_ day of \_\_\_\_\_, 20\_\_.

\_\_\_\_\_  
Timothy J. Freeman, Licensed Land Surveyor  
Minnesota License No. 16989

STATE OF MINNESOTA  
COUNTY OF WASHINGTON

The foregoing instrument was acknowledged before me on this \_\_\_\_ day of \_\_\_\_\_, 20\_\_ by Timothy J. Freeman, Licensed Land Surveyor.

\_\_\_\_\_  
(Signature of Notary Public)

\_\_\_\_\_  
(Printed Name of Notary Public)  
My commission expires \_\_\_\_\_

CITY OF LAKE ELMO

This plat was approved by the City Council of Lake Elmo, Minnesota, this \_\_\_\_ day of \_\_\_\_\_, 20\_\_, and hereby certifies compliance with all requirements as set forth in Minnesota Statutes, Section 505.03, Subd. 2.

By \_\_\_\_\_  
Mayor

By \_\_\_\_\_  
City Clerk

WASHINGTON COUNTY SURVEYOR

Pursuant to Chapter 820, Laws of Minnesota, 1971, and in accordance with Minnesota Statutes, Section 505.021, Subd. 11, this plat has been reviewed and approved this \_\_\_\_ day of \_\_\_\_\_, 20\_\_.

By \_\_\_\_\_  
Washington County Surveyor

By \_\_\_\_\_

WASHINGTON COUNTY AUDITOR/TREASURER

Pursuant to Minnesota Statutes, Section 505.021, Subd.9 and Section 272.12, taxes payable in the year 20\_\_, on real estate hereinbefore described, have been paid; and there are no delinquent taxes, and transfer has been entered on this \_\_\_\_ day of \_\_\_\_\_, 20\_\_.

By \_\_\_\_\_  
Washington County Auditor/Treasurer

By \_\_\_\_\_  
Deputy

WASHINGTON COUNTY RECORDER

Document Number \_\_\_\_\_  
I hereby certify that this instrument was filed in the Office of the County Recorder for record on this \_\_\_\_ day of \_\_\_\_\_, 20\_\_, at \_\_\_\_\_ o'clock \_\_.M., and was duly recorded in Washington County Records.

By \_\_\_\_\_  
Washington County Recorder

By \_\_\_\_\_  
Deputy



FOUR CORNERS 2ND ADDITION  
PRELIMINARY PLAT

Folz Freeman Surveying LLC  
LAND PLANNING and SURVEYING

FFS

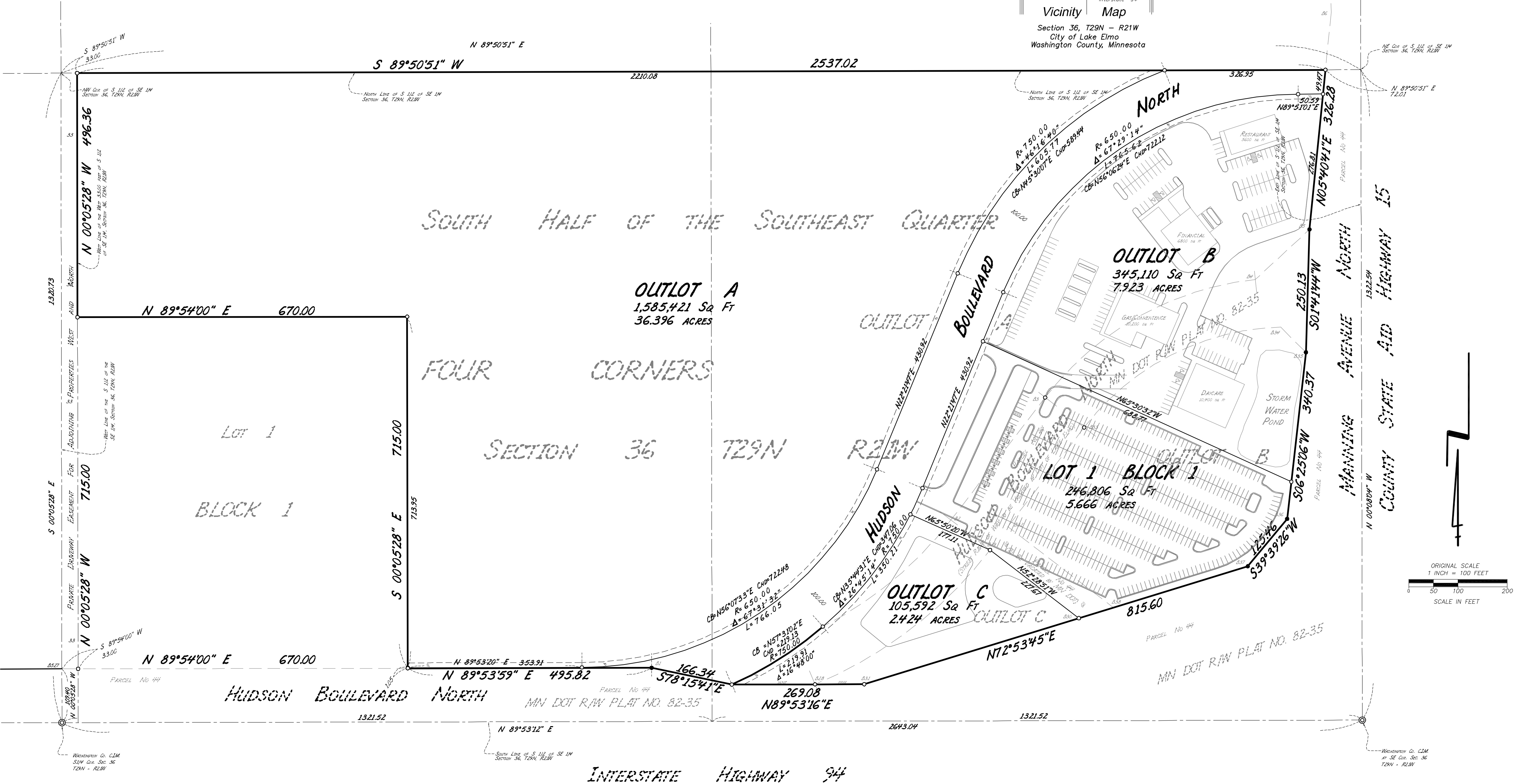
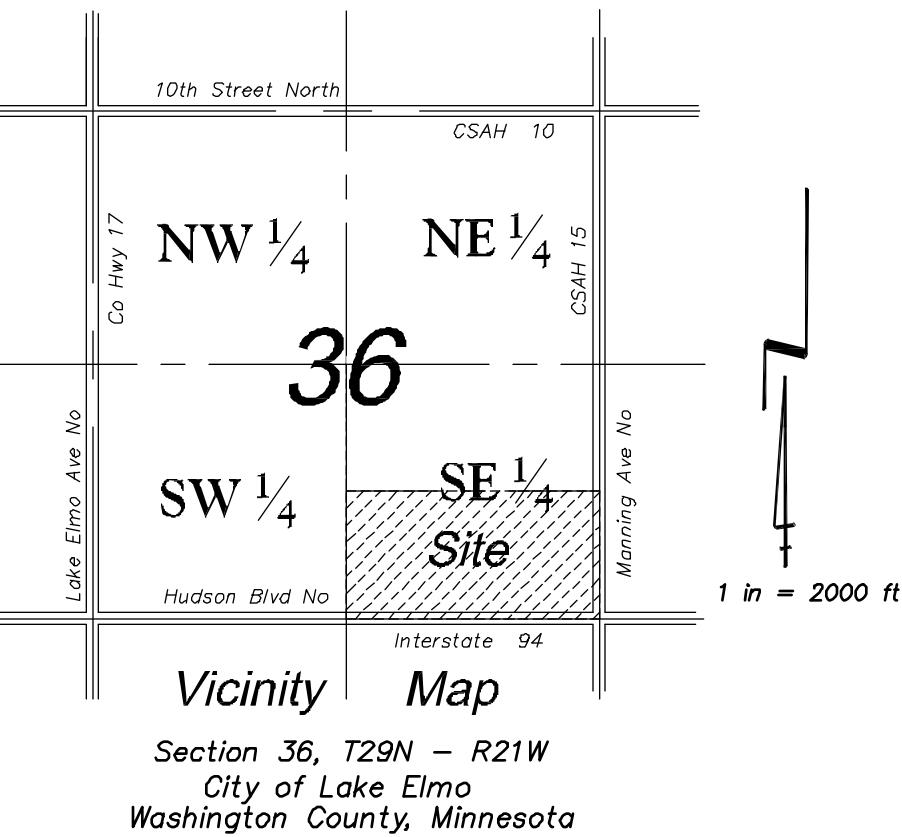
12445 55TH STREET NORTH  
LAKE ELMO, MINNESOTA 55042  
(651) 439-8833 www.FFSurveying.com

LEGEND

- DENOTES SET 1/2 INCH DIA. BY 18 INCH IRON PIPE MONUMENT MARKED WITH A PLASTIC CAP INSCRIBED "FREEMAN LS 16989", UNLESS SHOWN OTHERWISE
- DENOTES FOUND MN DOT 3/4 INCH IRON PIN MONUMENT

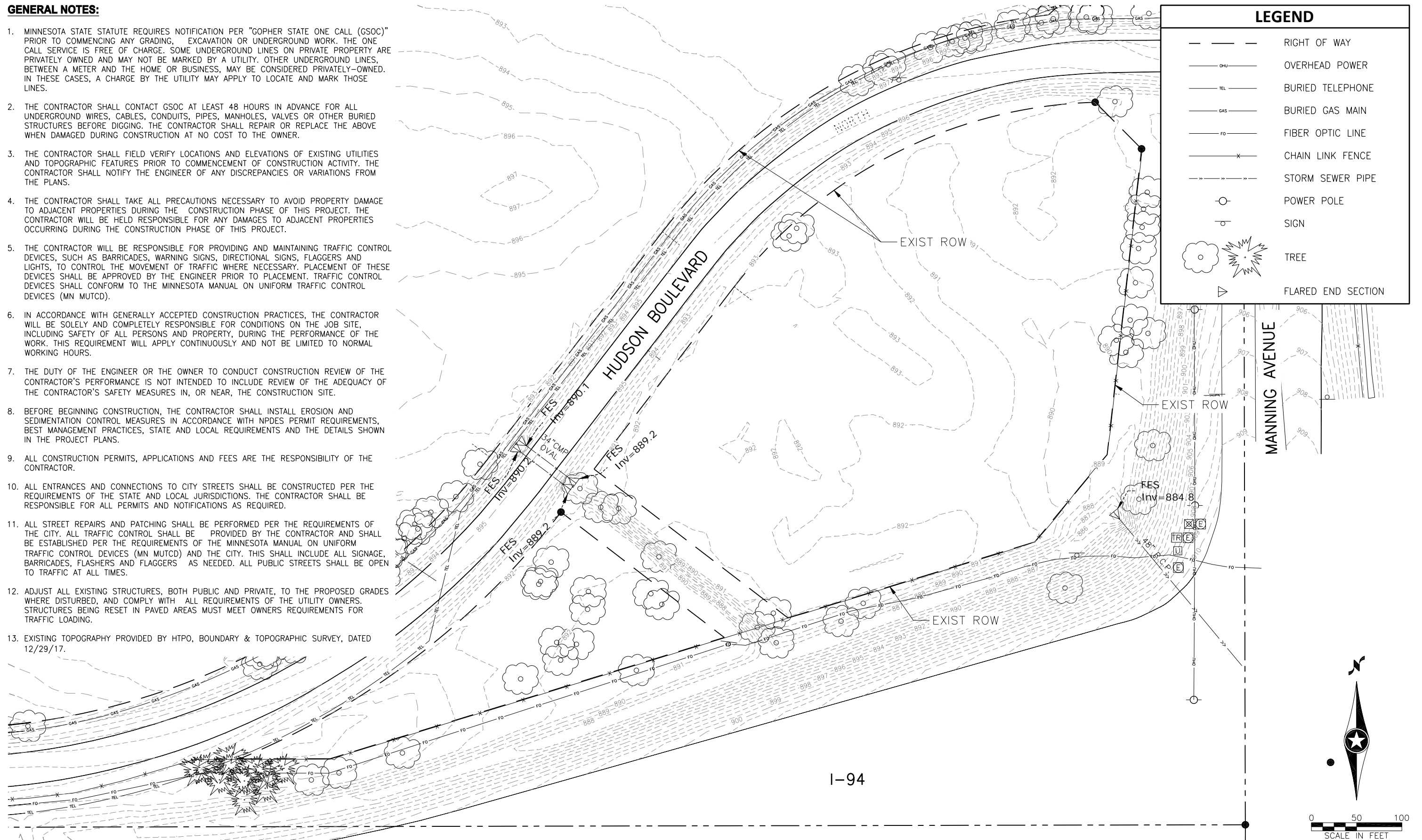
NOTES

ORIENTATION OF THIS BEARING SYSTEM IS BASED ON THE WASHINGTON COUNTY COORDINATE SYSTEM, NAD83, 1986 ADJUSTMENT






1. MINNESOTA STATE STATUTE REQUIRES NOTIFICATION PER "GOPHER STATE ONE CALL (GSOC)" PRIOR TO COMMENCING ANY GRADING, EXCAVATION OR UNDERGROUND WORK. THE ONE CALL SERVICE IS FREE OF CHARGE. SOME UNDERGROUND LINES ON PRIVATE PROPERTY ARE PRIVATELY OWNED AND MAY NOT BE MARKED BY A UTILITY. OTHER UNDERGROUND LINES, BETWEEN A METER AND THE HOME OR BUSINESS, MAY BE CONSIDERED PRIVATELY-OWNED. IN THESE CASES, A CHARGE BY THE UTILITY MAY APPLY TO LOCATE AND MARK THOSE LINES.
2. THE CONTRACTOR SHALL CONTACT GSOC AT LEAST 48 HOURS IN ADVANCE FOR ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
3. THE CONTRACTOR SHALL FIELD VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITY. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES OR VARIATIONS FROM THE PLANS.
4. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASE OF THIS PROJECT. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ANY DAMAGES TO ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASE OF THIS PROJECT.
5. THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES, SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGGERS AND LIGHTS, TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. PLACEMENT OF THESE DEVICES SHALL BE APPROVED BY THE ENGINEER PRIOR TO PLACEMENT. TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MN MUTCD).
6. IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY, DURING THE PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.
7. THE DUTY OF THE ENGINEER OR THE OWNER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES IN, OR NEAR, THE CONSTRUCTION SITE.
8. BEFORE BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL INSTALL EROSION AND SEDIMENTATION CONTROL MEASURES IN ACCORDANCE WITH NPDES PERMIT REQUIREMENTS, BEST MANAGEMENT PRACTICES, STATE AND LOCAL REQUIREMENTS AND THE DETAILS SHOWN IN THE PROJECT PLANS.
9. ALL CONSTRUCTION PERMITS, APPLICATIONS AND FEES ARE THE RESPONSIBILITY OF THE CONTRACTOR.
10. ALL ENTRANCES AND CONNECTIONS TO CITY STREETS SHALL BE CONSTRUCTED PER THE REQUIREMENTS OF THE STATE AND LOCAL JURISDICTIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PERMITS AND NOTIFICATIONS AS REQUIRED.
11. ALL STREET REPAIRS AND PATCHING SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. ALL TRAFFIC CONTROL SHALL BE PROVIDED BY THE CONTRACTOR AND SHALL BE ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MN MUTCD) AND THE CITY. THIS SHALL INCLUDE ALL SIGNAGE, BARRICADES, FLASHERS AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL BE OPEN TO TRAFFIC AT ALL TIMES.
12. ADJUST ALL EXISTING STRUCTURES, BOTH PUBLIC AND PRIVATE, TO THE PROPOSED GRADES WHERE DISTURBED, AND COMPLY WITH ALL REQUIREMENTS OF THE UTILITY OWNERS. STRUCTURES BEING RESET IN PAVED AREAS MUST MEET OWNERS REQUIREMENTS FOR TRAFFIC LOADING.
13. EXISTING TOPOGRAPHY PROVIDED BY HTPQ, BOUNDARY & TOPOGRAPHIC SURVEY, DATED 12/29/17.



1	03/29/19	WCG	PSG	MCA	BID SET
0	02/22/19	WCG	PSG	MCA	CUP APPLICATION SUBMITTAL
NO	DATE	BY	CKD	APPR	
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I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Print Name: MICHAEL C. AARON



Date 03/19/19 License # 25721

DRAWN BY W. GRAY
DESIGNED BY P. GALZKI
CHECKED BY M. AARON
COMM. NO. 11089.00



I-94 AND MANNING AVENUE PARK AND RIDE STATION  
11530 HUDSON BOULEVARD NORTH  
EXISTING CONDITIONS PLAN  
LAKE ELMO, MINNESOTA 55042

SHEET  
C1-0

PETER GALZKI | 3/19/2019 9:27:15 AM  
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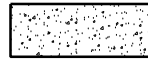

SITE DEVELOPMENT SUMMARY

PID: 3602921430001  
LEGAL DESCRIPTION: FOUR CORNERS  
2ND ADDITION  
LOT 1, BLOCK 1  
PROPERTY ADDRESS: 11530 HUDSON BLVD. N.  
LAKE ELMO, MN 55042  
PROPERTY AREA: 246,806 S.F. (5.67 AC)  
CURRENT PROPERTY OWNER: EN PROPERTIES  
EXISTING ZONING: AGRICULTURAL  
PROPOSED ZONING: COMMERCIAL  
RESTROOM BUILDING TOTAL: 126 S.F.  
BUILDING COVERAGE PROVIDED: ~0.0% (126 / 246,806)  
BUILDING HEIGHT PROVIDED: 10'-2"  
BUILDING SETBACKS:  
FRONT/ROW: 30'  
SIDE: 10'  
REAR: 30'  
IMPERVIOUS SURFACE - PROPOSED: 194,432 S.F. (78.8%)  
LANDSCAPED AREA - PROPOSED: 52,374 S.F. (21.2%)  
PARKING PROVIDED: 550 SPACES  
(INCLUDING 13 ACCESSIBLE)  
PARKING SIZE PROVIDED: 9' WIDE X 18' DEEP  
24' DRIVE AISLE

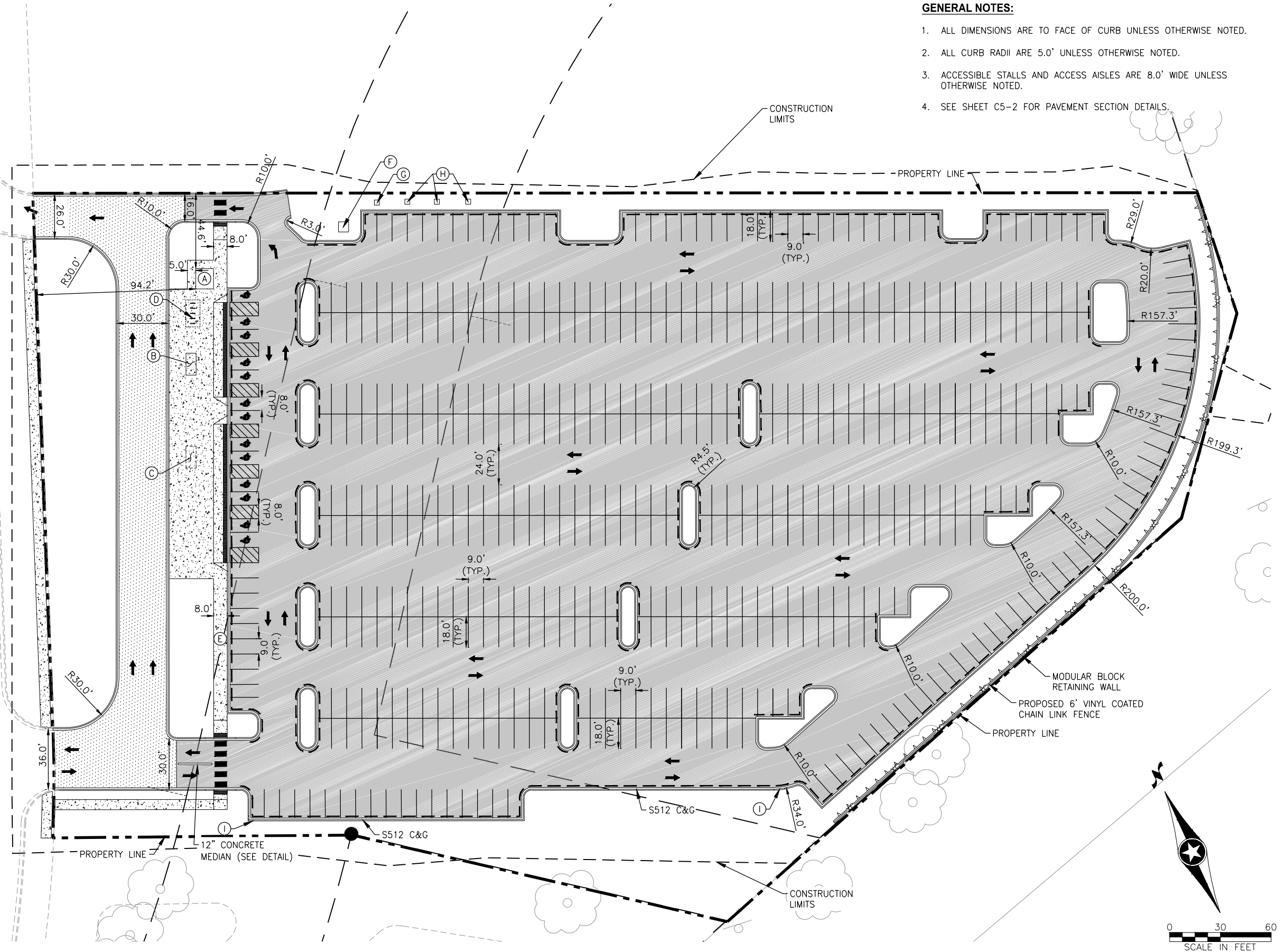
SITE NOTES:

- (A) PROPOSED 10.5'x12.0' RESTROOM BUILDING  
(B) PROPOSED 6.0'x13.0' BUS SHELTER  
(C) FUTURE 6.0'x13.0' BUS SHELTER  
(D) 5 BIKE RACKS W/ OVERHEAD CANOPY  
(E) PASSENGER DROP-OFF AREA  
(F) ELECTRIC SERVICE CABINET  
(G) ELECTRIC CHARGING STATION  
(H) FUTURE ELECTRIC CHARGING STATION  
(I) TRANSITION CURB TYPE IN 5.0'

LEGEND

-  BITUMINOUS PAVEMENT  
(SEE DETAIL)  
 CONCRETE PAVEMENT  
(SEE DETAIL)  
 CONCRETE WALK  
(SEE DETAIL)  
 TIP-OUT GUTTER

HUDSON BOULEVARD (BY OTHERS)



GENERAL NOTES:

1. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
2. ALL CURB RADII ARE 5.0' UNLESS OTHERWISE NOTED.
3. ACCESSIBLE STALLS AND ACCESS AISLES ARE 8.0' WIDE UNLESS OTHERWISE NOTED.
4. SEE SHEET C5-2 FOR PAVEMENT SECTION DETAILS.

0 30 60  
SCALE IN FEET

PETER GALZKI | 2/22/2019 1:14:37 PM  
X:\S (MS-TCU) | \p\galzki\Projects\11089-CP01\11089-CP01.dwg SITE PLAN  
N:\BMS\SRF-PW\PGALZKI\11089-CP01.DWG: SITEPLAN

NO	DATE	BY	CHK	APPR
1	03/29/19	WCG	PSG	MCA
0	02/22/19	WCG	PSG	MCA
BID SET				
CUP APPLICATION SUBMITTAL				

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
Print Name: MICHAEL C. AARON  
Date 03/19/19 License # 25721

DRAWN BY  
W. GRAY  
DESIGNED BY  
P. GALZKI  
CHECKED BY  
M. AARON  
COMM. NO. 11089.00



I-94 AND MANNING AVENUE PARK AND RIDE STATION  
11530 HUDSON BOULEVARD NORTH  
SITE PLAN  
LAKE ELMO, MINNESOTA 55042

SHEET  
C2-0

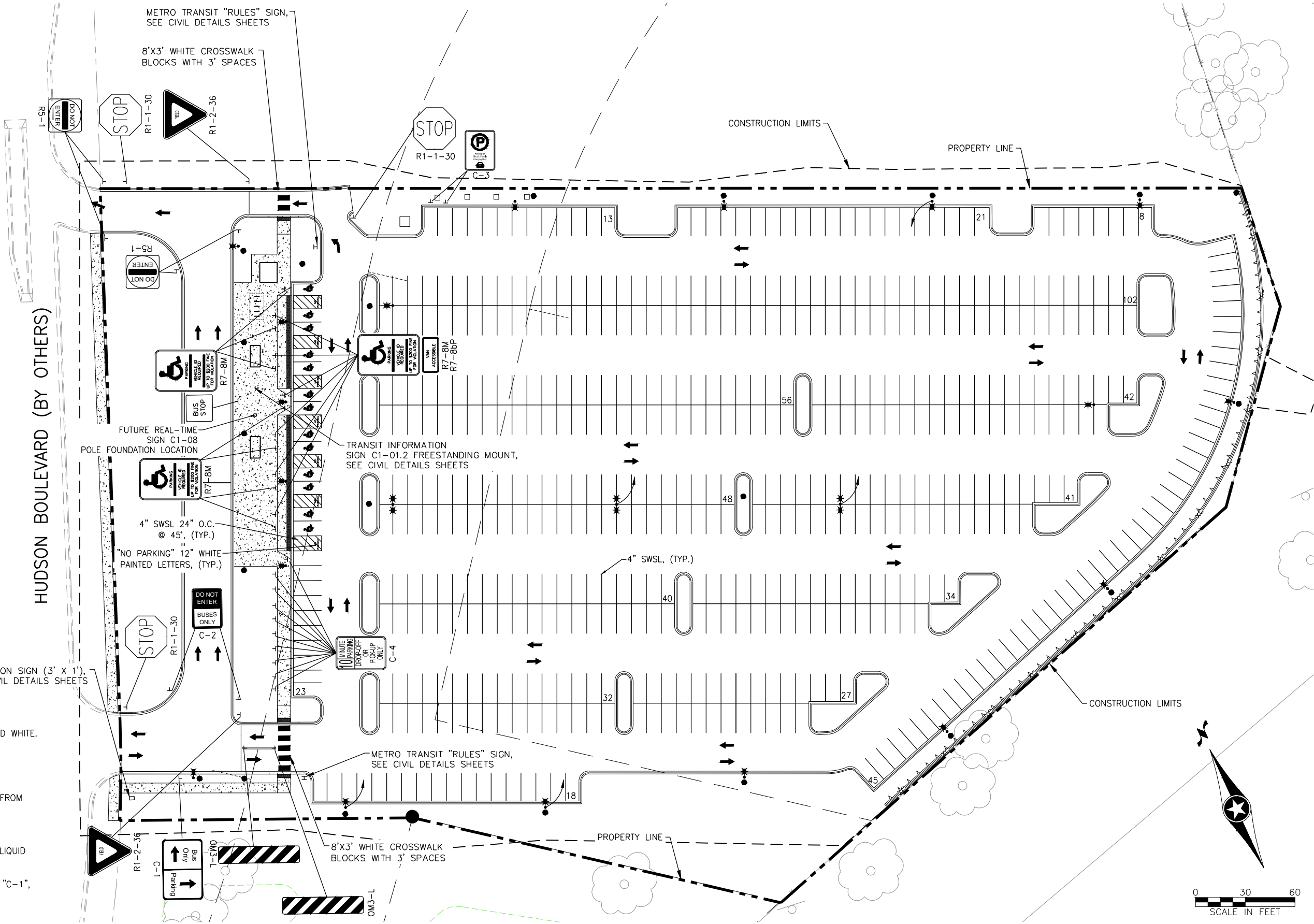
LEGEND

SWSL

SINGLE WHITE SOLID LINE

←

TRAFFIC DIRECTION  
(INFORMATIONAL ONLY)



GENERAL NOTES:

1. STANDARD PARKING STALL STRIPING TO BE 4" SOLID WHITE.
2. ADA PARKING AND SYMBOL TO BE 4" SOLID WHITE.
3. ALL SIGNS CLASSIFIED AS MnDOT TYPE C.
4. ALL SIGNS TO BE INSTALLED A MIN. OF 2' CLEAR FROM VEHICULAR ACCESS (FACE OF CURB).
5. ALL SIGNS, POSTS AND HARDWARE TO BE NEW.
6. ALL PAINT STRIPING SHALL BE MULTI-COMPONENT LIQUID PAVEMENT MARKINGS (MULTI-COMP).
7. SEE SHEET C2-3 FOR SPECIAL SIGNS DESIGNATED "C-1", "C-2", "C-3" AND "C-4".

1	03/29/19	JWA	SEH	AEM	BID SET
0	02/22/19	JWA	AEM	JWA	CUP APPLICATION SUBMITTAL
NO	DATE	BY	CKD	APPR	
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I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
Print Name: **ANDREW E. MCCALLUM**  
*Andrew McCallum*  
Date **3/18/19** License # **54861**

HT  
PO

Engineering • Surveying  
Landscape Architecture  
HANSEN THORP PELLINEN OLSON, Inc.  
2510 Market Place, Suite 400 • Eden Prairie, MN 55344  
952.420.0700 • 952.420.7000 FAX

DRAWN BY  
**A. MCCALLUM**

DESIGNED BY  
**A. MCCALLUM**

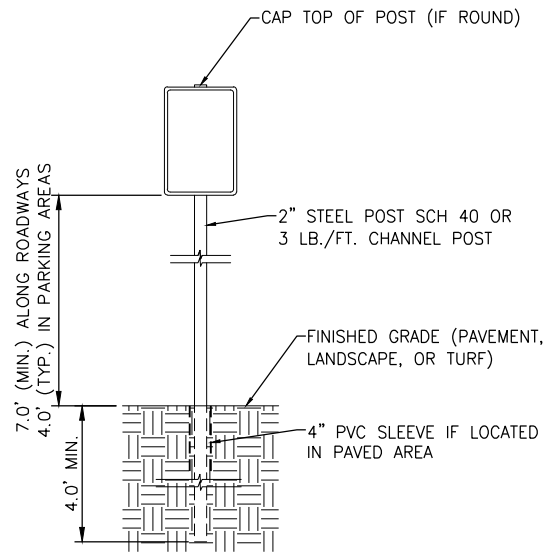
CHECKED BY  
**A. CARRELL**

COMM. NO. 11089.00

I-94 AND MANNING AVENUE PARK AND RIDE STATION  
11530 HUDSON BOULEVARD NORTH  
SIGNING AND PAVEMENT MARKING PLAN  
LAKE ELMO, MINNESOTA 55042

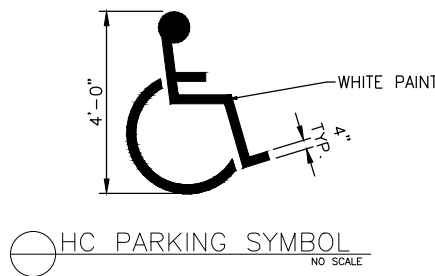
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C2-2



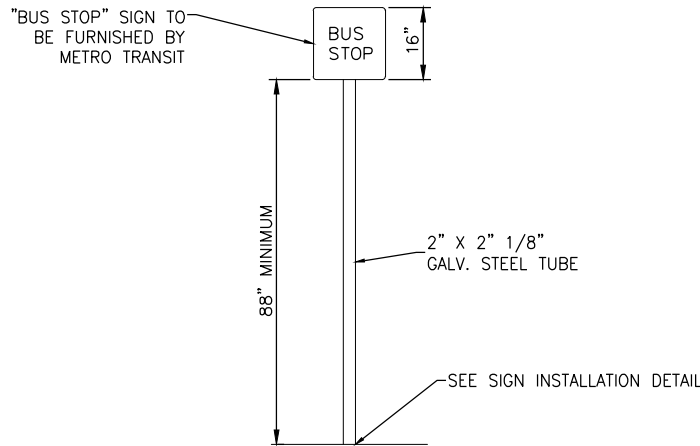


- NOTE:
- ALL STEEL SHALL BE GALVANIZED.
  - SIGNAGE INSTALLATION SHALL CONFORM TO MnDOT SPEC. 2564

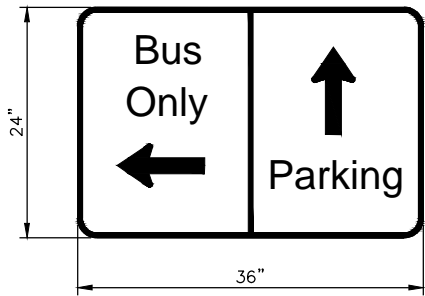
SIGN INSTALLATION DETAIL  
NO SCALE



HC PARKING SYMBOL  
NO SCALE

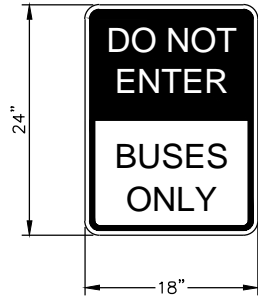


BUS STOP  
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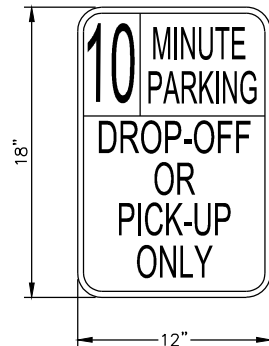
C-1

COLOR: BLACK LETTERING AND ARROWS  
WITH WHITE BACKGROUND  
LETTER HEIGHT: 3"



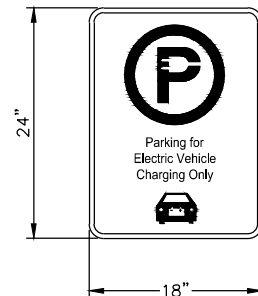
C-2

COLOR: WHITE LETTERING WITH BLACK BACKGROUND AND  
BLACK LETTERING WITH WHITE BACKGROUND  
LETTER HEIGHT: 3"



C-4

NOTE: R7-108 SIGN LAYOUT  
COLOR: GREEN LETTERING  
WITH WHITE BACKGROUND  
LETTER HEIGHT: 3"



C-3

NOTE: ARTWORK IS PROVIDED BY METRO TRANSIT

SIGN DIMENSIONS  
NO SCALE


GENERAL NOTES:

- SIGNS SHALL CONFORM TO MUTCD STANDARD LETTER HEIGHTS.
- THE CONTRACTOR SHALL REFERENCE MNDOT SIGN DETAILS AS THE DESIGN STANDARD FOR INSTALLATION OF "SPECIAL SIGNS".
- MOUNT SIGNS IN ACCORDANCE WITH MANUFACTURE'S INSTRUCTIONS.

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NO	DATE	BY	CHKD	APPR	
1	03/29/19	JWA	SEH	AEM	BID SET
0	02/22/19	JWA	AEM	JWA	CUP APPLICATION SUBMITTAL

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
Print Name: ANDREW E. MCCALLUM  
*Andrew McCallum*  
Date: 3/18/19 License #: 54861

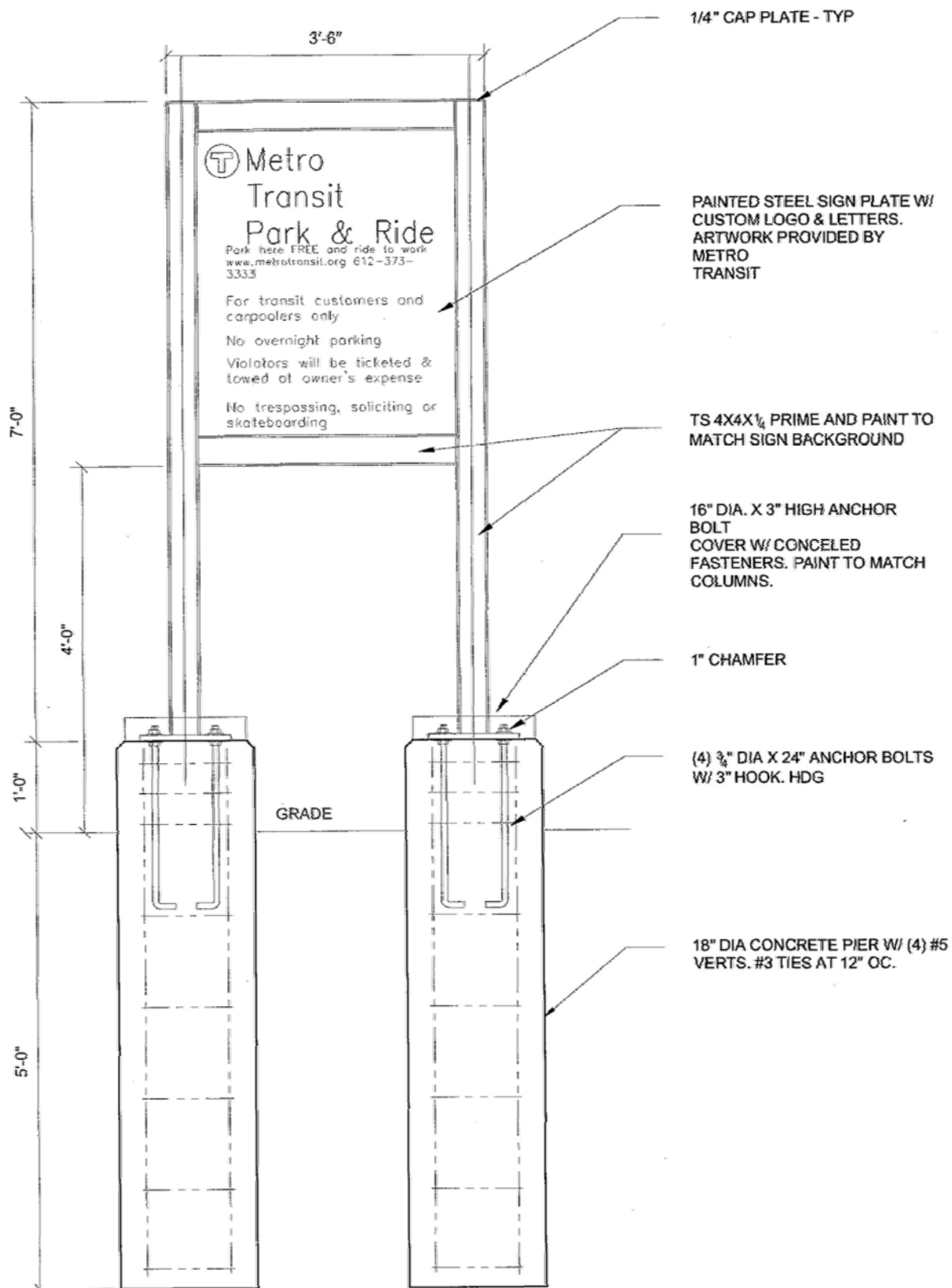


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952-833-0700 • 952-833-7000 FAX

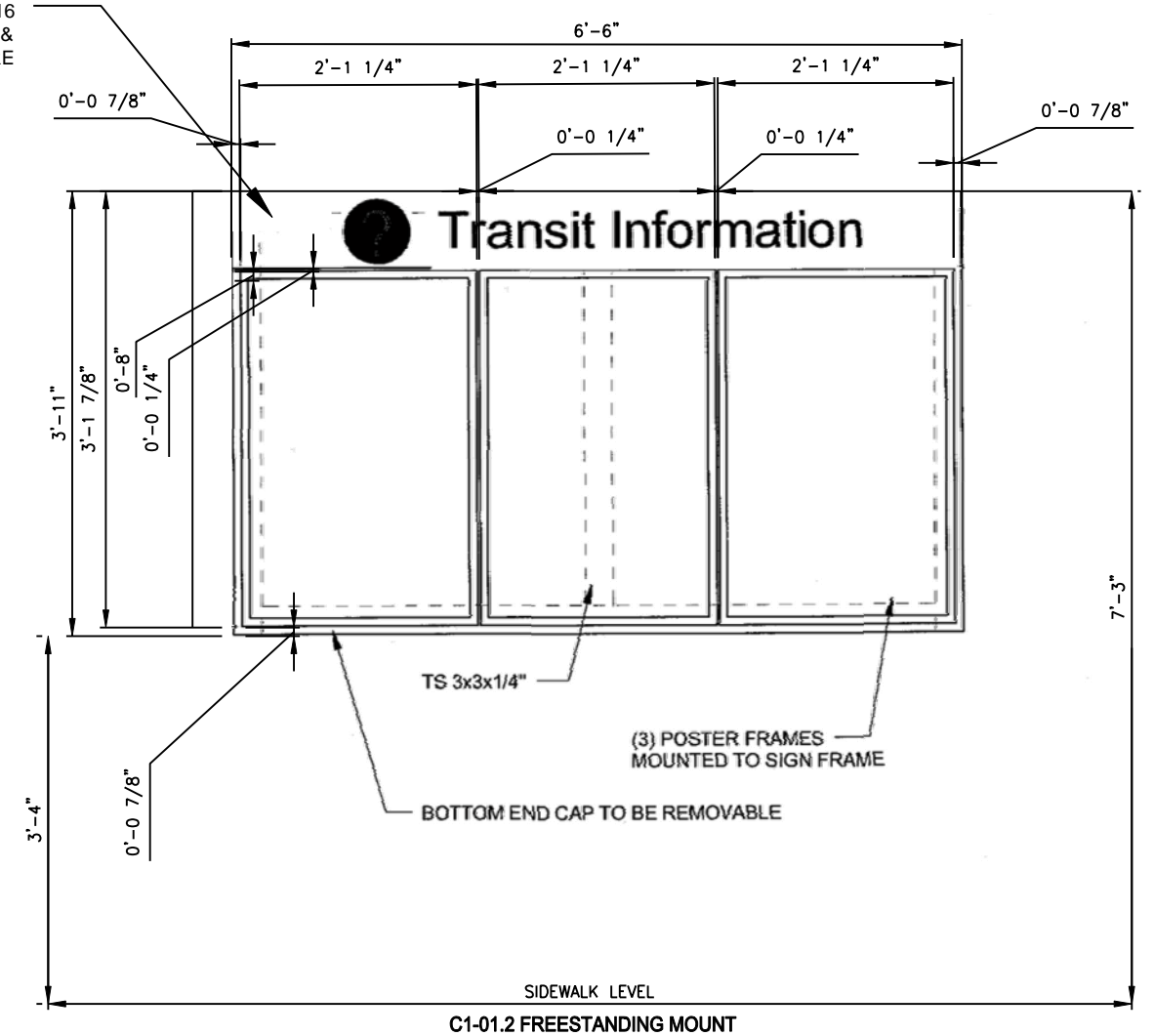
DRAWN BY  
A. MCCALLUM  
DESIGNED BY  
A. MCCALLUM  
CHECKED BY  
A. CARRELL  
COMM. NO. 11089.00



I-94 AND MANNING AVENUE PARK AND RIDE STATION 11530 HUDSON BOULEVARD NORTH SIGNING AND PAVEMENT MARKING DETAILS LAKE ELMO, MINNESOTA 55042	SHEET C2-3
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76"x 8" 1/8" FLAT SIGN S-16  
PAINTED BACKGROUND &  
GRAPHICS. ADHERE



#### MONUMENT / PILLAR SIGN

DESCRIPTION: 2 SIDED INTERNALLY ACRYLIC FACE .090  
ALUMINUM SURROUND FRAME.

ALUMINUM FRAMES SIGN. PAINTED IN 2 PART CATALYZED PPG DURANAR OR  
EQUIL. ALL CORNER JOINTS ARE WELDED AND GROUND SMOOTH.  
FASTENERS SHALL MATCH FRAME COLOR.

TYPOGRAPHY: EPS LAYOUT TO BE SUPPLIED BY OWNER. VACUUM FORMED  
FACE LETTERING SHALL BE UP TO 1/4" DEPTH.

COLOR: SIGN FRAME COLORS TO BE DETERMINED BY OWNER. SEE  
SPECIFICATIONS FOR SIGN FACE COLORS.

SHOP DRAWINGS: SHOP DRAWINGS AND COLOR SAMPLES SHALL BE  
APPROVED BY OWNERS REPRESENTATIVE PRIOR TO ANY PRODUCTION.

QUANTITY: 1 EACH

#### COLOR SPECIFICATIONS

COLORS: SIGNAGE COLORS ARE RED, BLUE, YELLOW, AND WHITE. THE  
APPROPRIATE PAINT USED ON ANY SIGN MUST EXACTLY MATCH THE  
FOLLOWING 3M VINYL COLORS.

RED	- 3M SCOTCHCAL 220 FILM, GERANIUM
BLUE	- 3M SCOTCHCAL 220 FILM, SAPPHIRE BLUE
YELLOW	- 3M SCOTCHCAL 220 FILM, SUNFLOWER
WHITE	- 3M SCOTCHCAL 220 FILM, GLOSS WHITE

GRAPHICS MATERIAL AND SIGN FABRICATION: BACKGROUND COLOR MUST  
BE POLYURETHANE ACRYLIC PAINT, WITH A NON-GLARE FINISH. GRAPHICS  
AND TEXT MUST BE 3M SCOTCHCAL 220 FILM.

ALL SIGN EDGES MUST BE DEBURRED AND SMOOTHED.

FINAL COLORS AND ARTWORK TO BE PROVIDED BY CAR.

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0	02/22/19	WCG	PSG	MCA	CUP APPLICATION SUBMITTAL
NO	DATE	BY	CKD	APPR	
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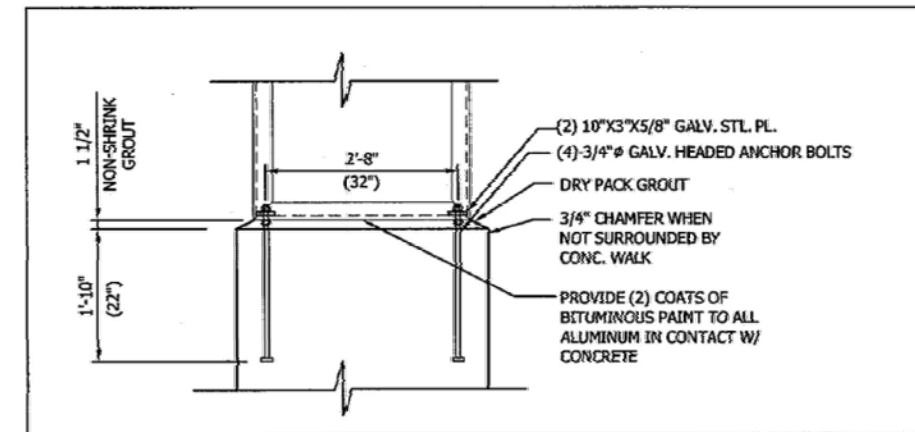
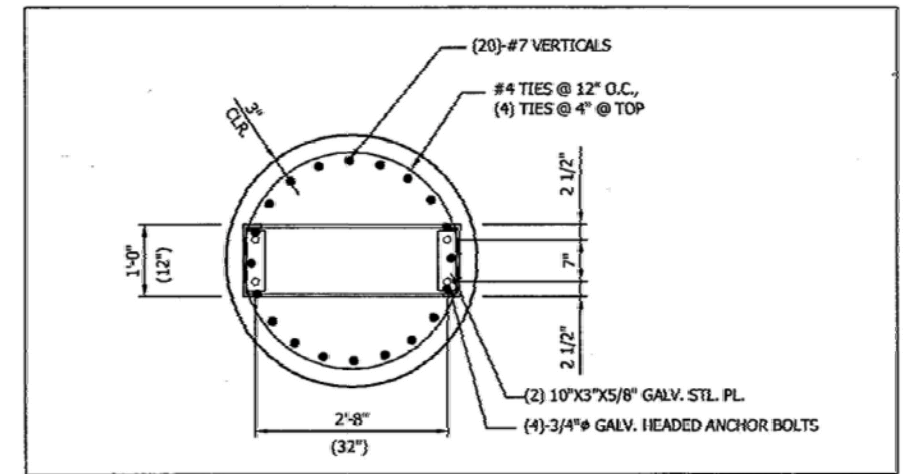
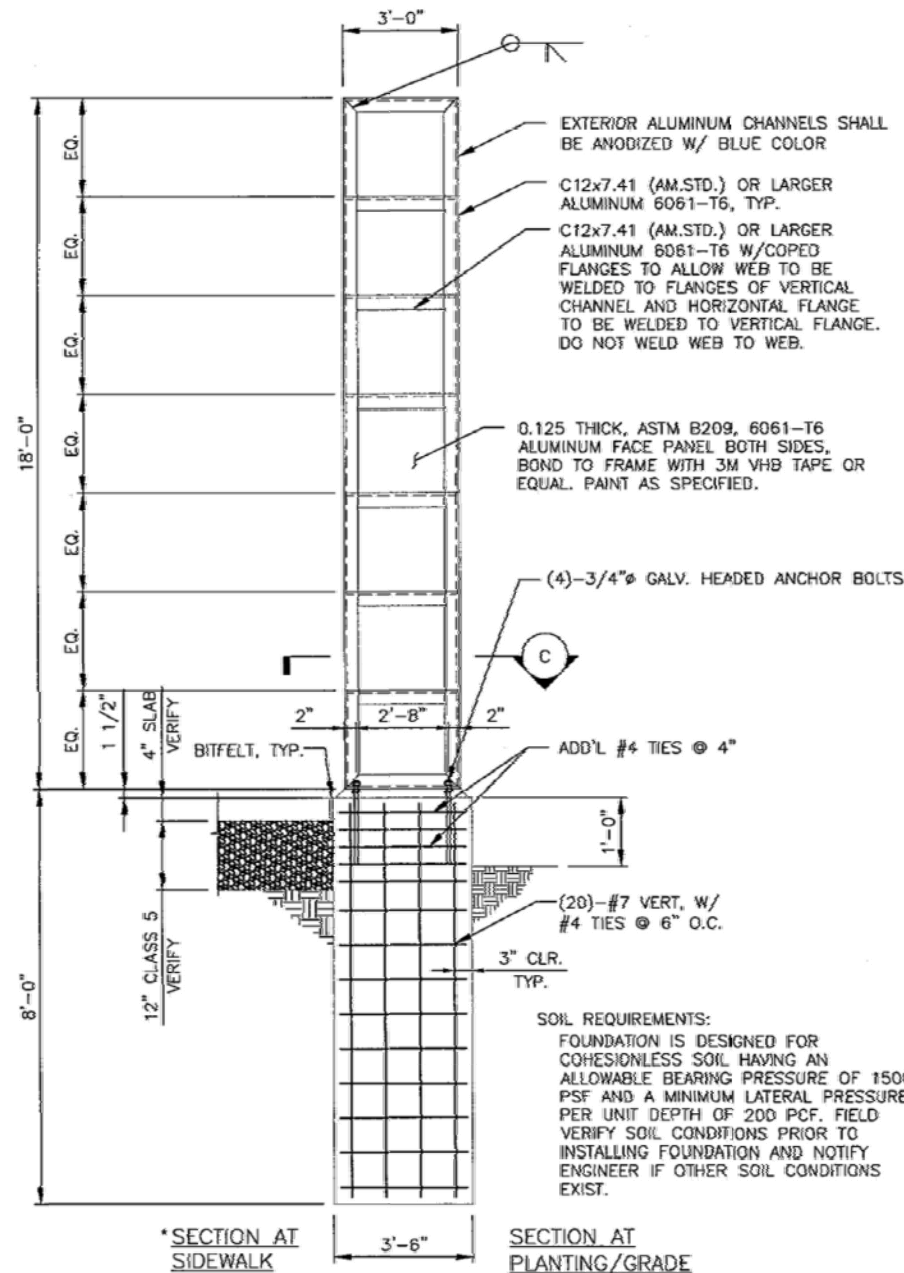
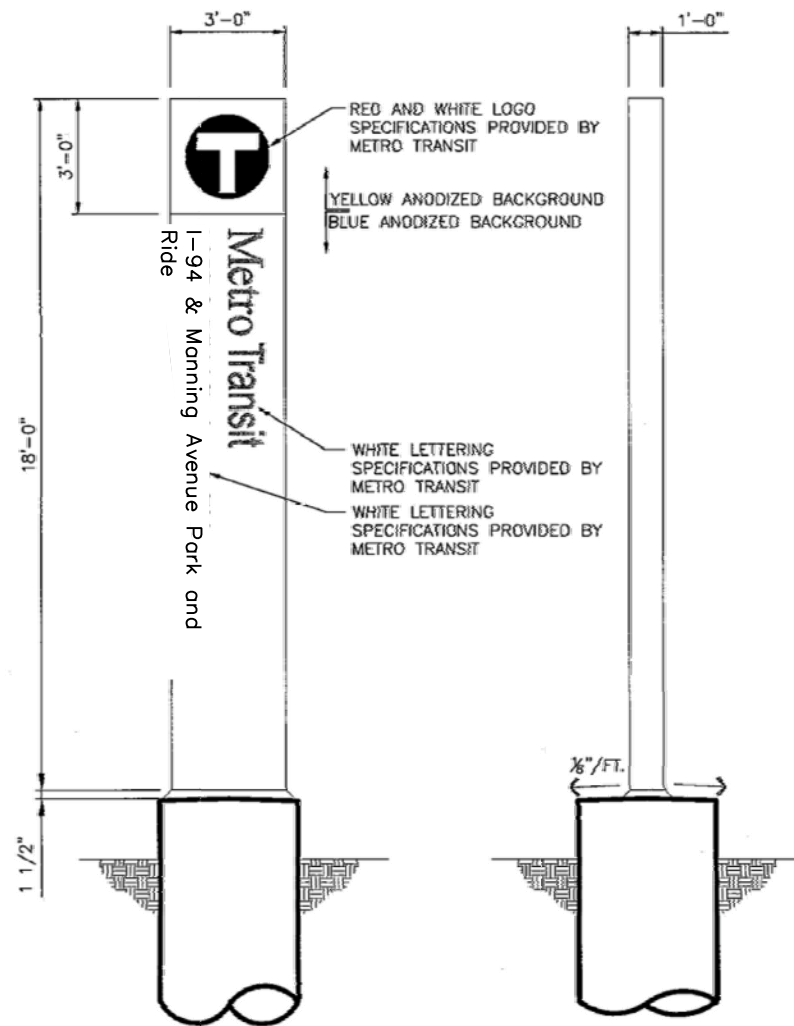
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I-94 AND MANNING AVENUE PARK AND RIDE STATION  
11530 HUDSON BOULEVARD NORTH  
CIVIL DETAILS  
LAKE ELMO, MINNESOTA 55042

SHEET

C5-28



\*SECTION AT SIDEWALK  
TO BE USED IF METRO TRANSIT CHOOSES SIGN LOCATION ALONG 21ST AVENUE BITUMINOUS PATH

SECTION AT PLANTING/GRADE

SOIL REQUIREMENTS:  
FOUNDATION IS DESIGNED FOR COHESIONLESS SOIL HAVING AN ALLOWABLE BEARING PRESSURE OF 1500 PSF AND A MINIMUM LATERAL PRESSURE PER UNIT DEPTH OF 200 PCF. FIELD VERIFY SOIL CONDITIONS PRIOR TO INSTALLING FOUNDATION AND NOTIFY ENGINEER IF OTHER SOIL CONDITIONS EXIST.

#### MONUMENT / PILLAR SIGN

DESCRIPTION: 2-SIDED INTERNALLY ACRYLIC FACE. 0.90 ALUMINUM SURROUND FRAME.

ALUMINUM FRAMES SIGN. PAINTED IN 2 PART CATALYZED PPG DURANAR OR EQUAL. ALL CORNER JOINTS ARE WELDED AND GROUND SMOOTH. FASTENERS SHALL MATCH FRAME COLOR.

TYPOGRAPHY: EPS LAYOUT TO BE SUPPLIED BY OWNER. VACUUM FORMED FACE LETTERING SHALL BE UP TO 1/4" DEPTH.

COLOR: SIGN FRAME COLORS TO BE DETERMINED BY OWNER. SEE SPECIFICATIONS FOR SIGN FACE COLORS.

SHOP DRAWINGS: SHOP DRAWINGS AND COLOR SAMPLES SHALL BE APPROVED BY OWNERS REPRESENTATIVE PRIOR TO ANY PRODUCTION.

QUANTITY: 1 EACH

#### COLOR SPECIFICATIONS

COLORS: SIGNAGE COLORS ARE RED, BLUE, YELLOW, AND WHITE. THE APPROPRIATE PAINT USED ON ANY SIGN MUST EXACTLY MATCH THE FOLLOWING 3M VINYL COLORS.

RED - 3M SCOTCHCAL 220 FILM, GERANIUM  
BLUE - 3M SCOTCHCAL 220 FILM, SAPPHIRE BLUE  
YELLOW - 3M SCOTCHCAL 220 FILM, SUNFLOWER  
WHITE - 3M SCOTCHCAL 220 FILM, GLOSS WHITE

GRAPHICS MATERIAL AND SIGN FABRICATION: BACKGROUND COLOR MUST BE POLYURETHANE ACRYLIC PAINT, WITH A NON-GLARE FINISH. GRAPHICS AND TEXT MUST BE 3M SCOTCHCAL 220 FILM.

ALL SIGN EDGES MUST BE DEBURRED AND SMOOTHED.

FINAL COLORS AND ARTWORK TO BE PROVIDED BY CAR.

#### DESIGN NOTES:

WIND SPEED = 90 MPH  
DESIGN WIND PRESSURE = 30 PSF WITH 1/2" RADIAL ICE.

CAST-IN PLACE CONCRETE  
f'c = 4000 PSI W/ 6.0% AIR ± 1.0%

REINFORCEMENT:  
fy = 60,000 PSI

STRUCTURAL STEEL SHALL BE GRADE 36.

ALUMINUM CHANNELS AND TUBES SHALL BE 6061-T6.

#### NOTES:

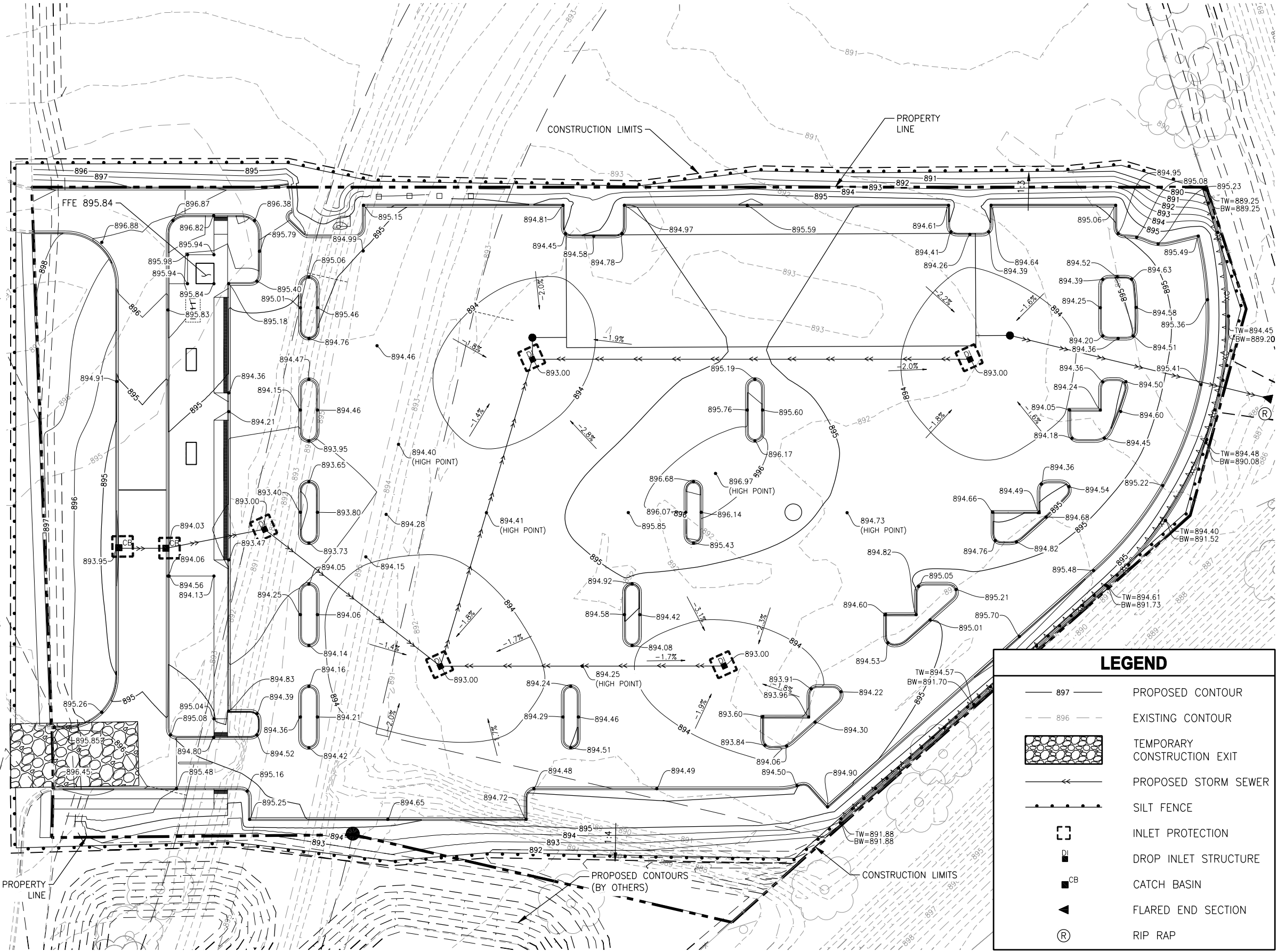
- SIGN PANELS ARE DOUBLE FACED. (2 FACES TOTAL PER SIGN).
- ONLY ONE METHOD OF LETTERING (PAINTED OR APPLIED VINYL) WILL BE ALLOWED PER SIGN TYPE, EXCEPT FOR ROUTE NUMBERS AND GATE LETTERS, WHICH SHALL BE APPLIED VINYL IN ALL APPLICATIONS. EXCEPT FOR THE VINYL ROUTE NUMBERS AND GATE LETTERS, ALL LETTERING LOWER THAN 8" ABOVE FINISHED GRADE SHALL BE PAINTED. EXCEPT FOR THE VINYL ROUTE NUMBERS AND GATE LETTERS, LETTERING HIGHER THAN 8" ABOVE FINISHED GRADE SHALL BE EITHER APPLIED VINYL OR PAINTED ONTO THE ALUMINUM PANELS AT THE CONTRACTOR'S OPTION.
- ATTACH FACILITY SIGN TO SIGN FRAME USING A DOUBLE BEAD OF "3M VHB TAPE" OR EQUAL TO ISOLATE ALUMINUM PLATE FROM STEEL HSS MEMBER.
- PAINT TOP COAT SHALL BE ACRYLIC POLYURETHANE AS SPECIFIED, COLORS PER METRO TRANSIT STANDARDS.
- FOR INSTALLATION IN EXISTING CONCRETE PAVEMENT, CUT & REMOVE CONCRETE SIDEWALK TO THE NEAREST CONSTRUCTION JOINT AND A MINIMUM OF 4'-0"x4'-0".
- CONSTRUCT SONOTUBE FOUNDATION AS SHOWN. REMOVE EXPOSED SONOTUBE AFTER CONCRETE IS CURED.
- RECOMPACT CLASS 5 TO 98% STD. PROCTOR EFFECT.
- PROVIDE GRAFFITI COATING AS SPECIFIED.

1	03/29/19	WCG	PSG	MCA	BID SET
0	02/22/19	WCG	PSG	MCA	CUP APPLICATION SUBMITTAL
NO	DATE	BY	CKD	APPR	



GENERAL NOTES:

1. SPOT ELEVATIONS REPRESENT FINISHED SURFACE GRADES, GUTTER/FLOW LINE, FACE OF BUILDING, OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
2. CATCH BASINS AND MANHOLES IN PAVED AREAS SHALL BE SUMPED 0.04 FEET. ALL CATCH BASINS IN GUTTERS SHALL BE SUMPED 0.16 FEET. RIM ELEVATIONS SHOWN ON PLANS DO NOT REFLECT SUMPED ELEVATIONS.
3. GRADING OF THE INFILTRATION AREAS SHALL BE ACCOMPLISHED USING LOW-IMPACT EARTH-MOVING EQUIPMENT TO PREVENT COMPACTION OF THE UNDERLYING SOILS. SMALL TRACKED DOZERS AND BOBCATS WITH RUNNER TRACKS ARE RECOMMENDED.
4. ALL DISTURBED UNPAVED AREAS ARE TO RECEIVE MINIMUM OF 4 INCHES OF TOP SOIL AND SEED/MULCH OR SOD. THESE AREAS SHALL BE WATERED/MAINTAINED BY THE CONTRACTOR UNTIL VEGETATION IS ESTABLISHED.
5. REFER TO THE REPORT OF GEOTECHNICAL EXPLORATION AND ENGINEERING REVIEW, DATED APRIL 20, 2017 AS PREPARED BY BRAUN INTERTEC FOR AN EXISTING SUBSURFACE SITE CONDITION ANALYSIS AND CONSTRUCTION RECOMMENDATIONS.
6. STREETS MUST BE CLEANED AND SWEEPED WHENEVER TRACKING OF SEDIMENTS OCCURS AND BEFORE SITES ARE LEFT IDLE FOR WEEKENDS AND HOLIDAYS. A REGULAR SWEEPING SCHEDULE MUST BE ESTABLISHED.
7. DUST MUST BE ADEQUATELY CONTROLLED.
4. ON SLOPES 3:1 OR GREATER MAINTAIN SHEET FLOW AND MINIMIZE RILLS AND/OR GULLIES.
5. ALL STORM DRAINS AND INLETS MUST BE PROTECTED UNTIL ALL SOURCES OF POTENTIAL DISCHARGE ARE STABILIZED.
6. TEMPORARY SOIL STOCKPILES MUST HAVE EFFECTIVE SEDIMENT CONTROL AND CAN NOT BE PLACED IN SURFACE WATERS OR STORM WATER CONVEYANCE SYSTEMS. TEMPORARY STOCKPILES WITHOUT SIGNIFICANT AMOUNT OF SILT, CLAY, OR ORGANIC COMPOUNDS ARE EXEPTM EX: CLEAN AGGREGATE STOCK PILES, DEMOLITION CONCRETE STOCKPILES, SAND STOCKPILES.
7. FINAL STABILIZATION REQUIRES THAT ALL SOIL DISTURBING ACTIVITIES HAVE BEEN COMPLETED AND THAT DISTURBED AREAS ARE STABILIZED BY A UNIFORM PERENNIAL VEGETATIVE COVER WITH 70% OF THE EXPECTED FINAL DENSITY, AND THAT ALL PERMANENT PAVEMENTS HAVE BEEN INSTALLED. ALL TEMPORARY BMP'S SHALL BE REMOVED, DITCHES STABILIZED, AND SEDIMENT SHALL BE REMOVED FROM PERMANENT CONVEYANCES AND SEDIMENTATION BASINS IN ORDER TO RETURN THE POND TO DESIGN CAPACITY.
8. THE WATERSHED DISTRICT OR THE CITY MAY HAVE REQUIREMENTS FOR INSPECTIONS VERIFYING PROPER CONSTRUCTION OF THE BMPs.
9. SEE SWPPP FOR ADDITIONAL EROSION CONTROL NOTES AND REQUIREMENTS.
10. SEE UTILITY PLAN FOR WATER AND SANITARY SEWER INFORMATION.
11. SEE SITE PLAN FOR CURB AND BITUMINOUS TAPER LOCATIONS.



LEGEND

— 897 —

PROPOSED CONTOUR

- - - 896 - - -

EXISTING CONTOUR

[Pattern]

TEMPORARY CONSTRUCTION EXIT

—<—<

PROPOSED STORM SEWER

—•—•—•—

SILT FENCE

[Box]

INLET PROTECTION

[Symbol]

DROP INLET STRUCTURE

[Symbol]

CATCH BASIN

[Symbol]

FLARED END SECTION

[Symbol]

RIP RAP

1	03/29/19	WCG	PSG	MCA	BID SET
0	02/22/19	WCG	PSG	MCA	CUP APPLICATION SUBMITTAL
NO	DATE	BY	CKD	APPR	

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
Print Name: MICHAEL C. AARON  
Date 03/19/19 License # 25721

DRAWN BY W. GRAY  
DESIGNED BY P. GALZKI  
CHECKED BY M. AARON  
COMM. NO. 11089.00



I-94 AND MANNING AVENUE PARK AND RIDE STATION  
11530 HUDSON BOULEVARD NORTH  
GRADING, DRAINAGE & EROSION CONTROL PLAN  
LAKE ELMO, MINNESOTA 55042

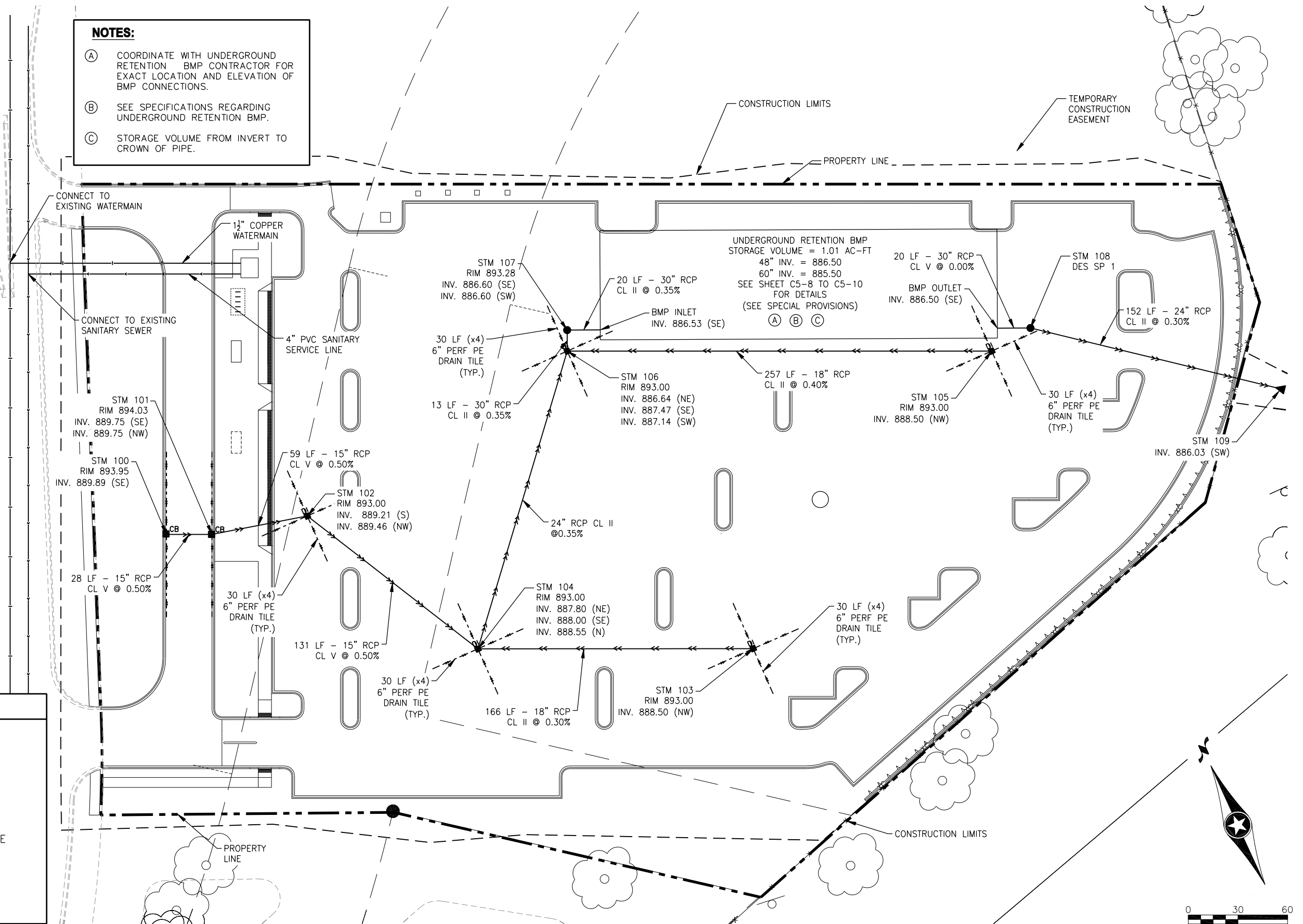
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1. ALL SANITARY SEWER AND WATERMAIN UTILITIES SHALL BE FURNISHED AND INSTALLED PER THE REQUIREMENTS OF THE SPECIFICATIONS/THE MINNESOTA PLUMBING CODE, THE LOCAL GOVERNING UNIT, AND THE STANDARD UTILITIES SPECIFICATION OF THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), 2013 EDITION.
2. ALL UTILITY PIPE BEDDING SHALL BE COMPACTED SAND OR FINE GRANULAR MATERIAL. ALL COMPACTION SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CEAM SPECIFICATION AND THE GEOTECHNICAL REPORT.
3. ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE PERFORMED PER THE REQUIREMENTS OF THE STATE AND LOCAL JURISDICTIONS. THE CITY DEPARTMENT OF ENGINEERING AND BUILDING INSPECTIONS DEPARTMENT AND THE CONSTRUCTION ENGINEER MUST BE NOTIFIED AT LEAST 48 HOURS PRIOR TO ANY WORK WITHIN THE PUBLIC RIGHT OF WAY, OR WORK IMPACTING PUBLIC UTILITIES.
4. ALL SANITARY SEWER AND WATER SERVICES SHALL TERMINATE 5.0' FROM FACE OF BUILDING UNLESS OTHERWISE NOTED.
5. A MINIMUM OF 18 INCHES OF VERTICAL SEPARATION AND 10 FEET OF HORIZONTAL SEPARATION IS REQUIRED FOR ALL UTILITIES UNLESS OTHERWISE NOTED.
6. ALL NEW WATERMAIN AND SERVICES MUST HAVE A MINIMUM OF 8.0' FEET OF COVER. EXTRA DEPTH MAY BE REQUIRED TO MAINTAIN A MINIMUM 18" VERTICAL SEPARATION TO SANITARY OR STORM SEWER LINES. THE CONTRACTOR SHALL FIELD ADJUST WATERMAIN TO AVOID CONFLICTS WITH SANITARY SEWER, STORM SEWER, AND OTHER SERVICES AS REQUIRED. INSULATION OF WATER AND SANITARY SEWER LINES SHALL BE PROVIDED WHERE 8.0' MINIMUM DEPTH CANNOT BE ATTAINED.
7. ALL FIRE HYDRANTS SHALL BE LOCATED 5.0' BEHIND BACK OF CURB OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
8. PROPOSED PIPE MATERIALS:

## LEGEND

SANITARY SEWER  
WATERMAIN  
STORM SEWER  
DRAIN TILE  
DROP INLET STRUCTURE  
CATCH BASIN  
FLARED END SECTION

- (A) COORDINATE WITH UNDERGROUND RETENTION BMP CONTRACTOR FOR EXACT LOCATION AND ELEVATION OF BMP CONNECTIONS.
- (B) SEE SPECIFICATIONS REGARDING UNDERGROUND RETENTION BMP.
- (C) STORAGE VOLUME FROM INVERT TO CROWN OF PIPE.



A scale bar labeled "SCALE IN FEET" with markings at 0, 30, and 60 feet. The bar is divided into segments of alternating black and white colors.

1	03/29/19	WCG	PSG	MCA	BID SET
0	02/22/19	WCG	PSG	MCA	CUP APPLICATION SUBMITTAL
NO	DATE	BY	CKD	APPR	
N:\BMS\SRF-PW\GALZKI\DMS13368\11089_SW01.DWG: UTILITYPLAN					

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Print Name: MICHAEL C. AARON

*Michael C. Aaron*

Date 03/19/19 License # 25721

DRAWN BY W. GRAY
DESIGNED BY P. GALZKI
CHECKED BY M. AARON
COMM. NO. 11089.00



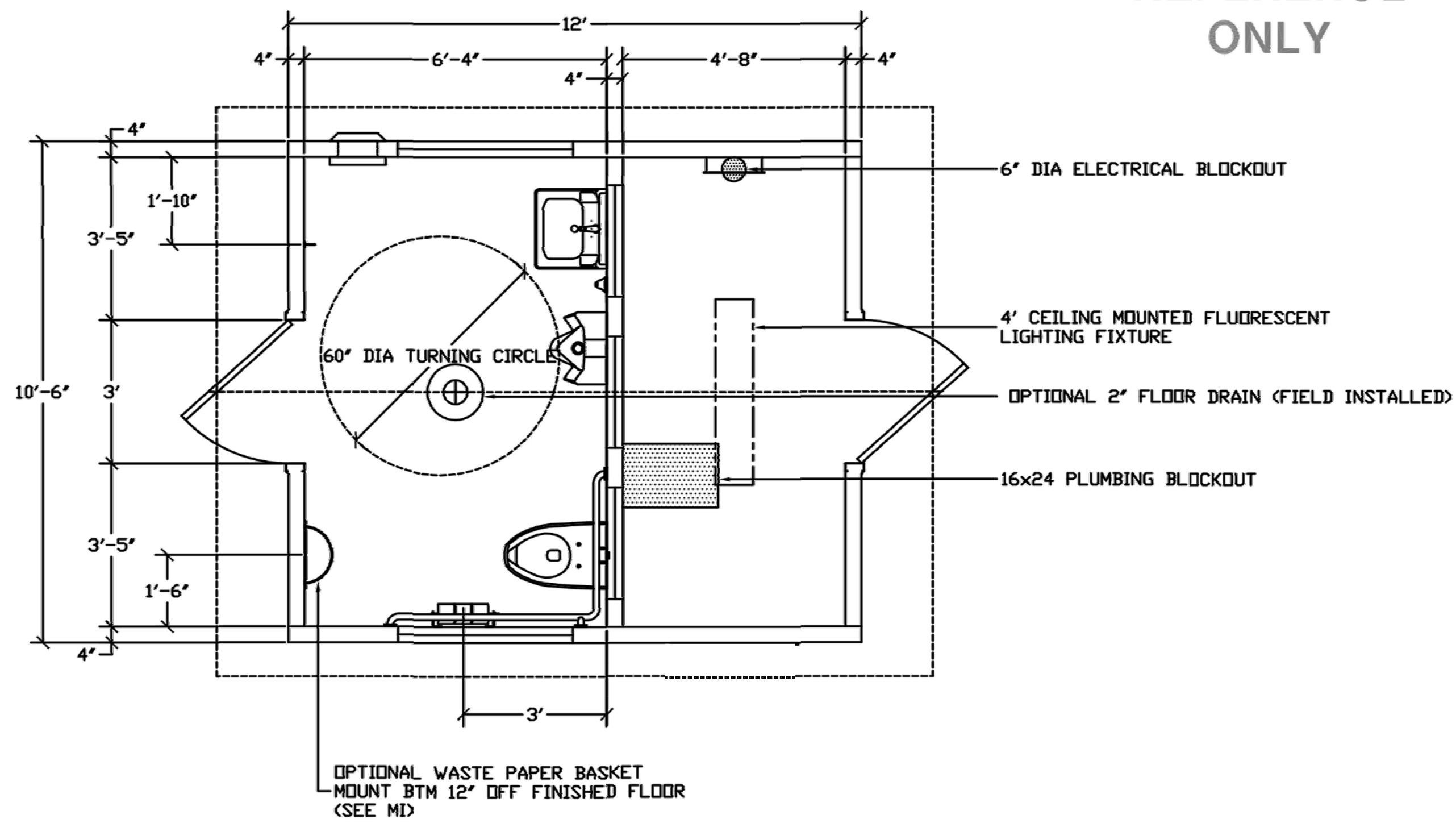
I-94 AND MANNING AVENUE PARK AND RIDE STATION  
11530 HUDSON BOULEVARD NORTH  
UTILITY PLAN  
LAKE ELMO, MINNESOTA 55042

HEET  
C4-0

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FOR  
REFERENCE  
ONLY



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0	02/22/19	WCG	PSG	MCA	CUP APPLICATION SUBMITTAL
NO	DATE	BY	CKD	APPR	






I-94 AND MANNING AVENUE PARK AND RIDE STATION	SHEET
11530 HUDSON BOULEVARD NORTH	
CIVIL DETAILS	
LAKE ELMO, MINNESOTA 55042	C5-27








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LEGEND


DECIDUOUS TREE

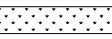
ORNAMENTAL TREE

EVERGREEN TREE

SHRUB

PERENNIAL

EXISTING TREE

NO MOW SEED AREA

SEED MIX:

NO MOW SEED MIX:

APPLY AT A RATE OF 6 LBS PER 1,000SF

30% CARDINAL CREEPING RED FESCUE

30% CREEPING RED FESCUE

15% PENNLAWN CREEPING RED FESCUE

10% HARD FESCUE

10% CHEWING FESCUE

5% SHEEP FESCUE

- LANDSCAPE NOTES:
- IRRIGATION WILL BE A DESIGN BUILD ABOVE GROUND TEMPORARY SYSTEM, TO BE REMOVED AT FINAL ACCEPTANCE.
  - PLACE GATOR BAGS AT EACH TREE IN PARKING ISLANDS.
  - PROVIDE SLEEVES FOR IRRIGATION UNDER ROAD AND WALKS WHERE NEEDED.
  - MULCH ALL PARKING ISLANDS AND PLANTING BEDS WITH A MIN. 3" OF HARDWOOD SHREDDED MULCH.

CITY REQUIREMENTS:

550 PARKING SPACES = 37 TREES REQUIRED

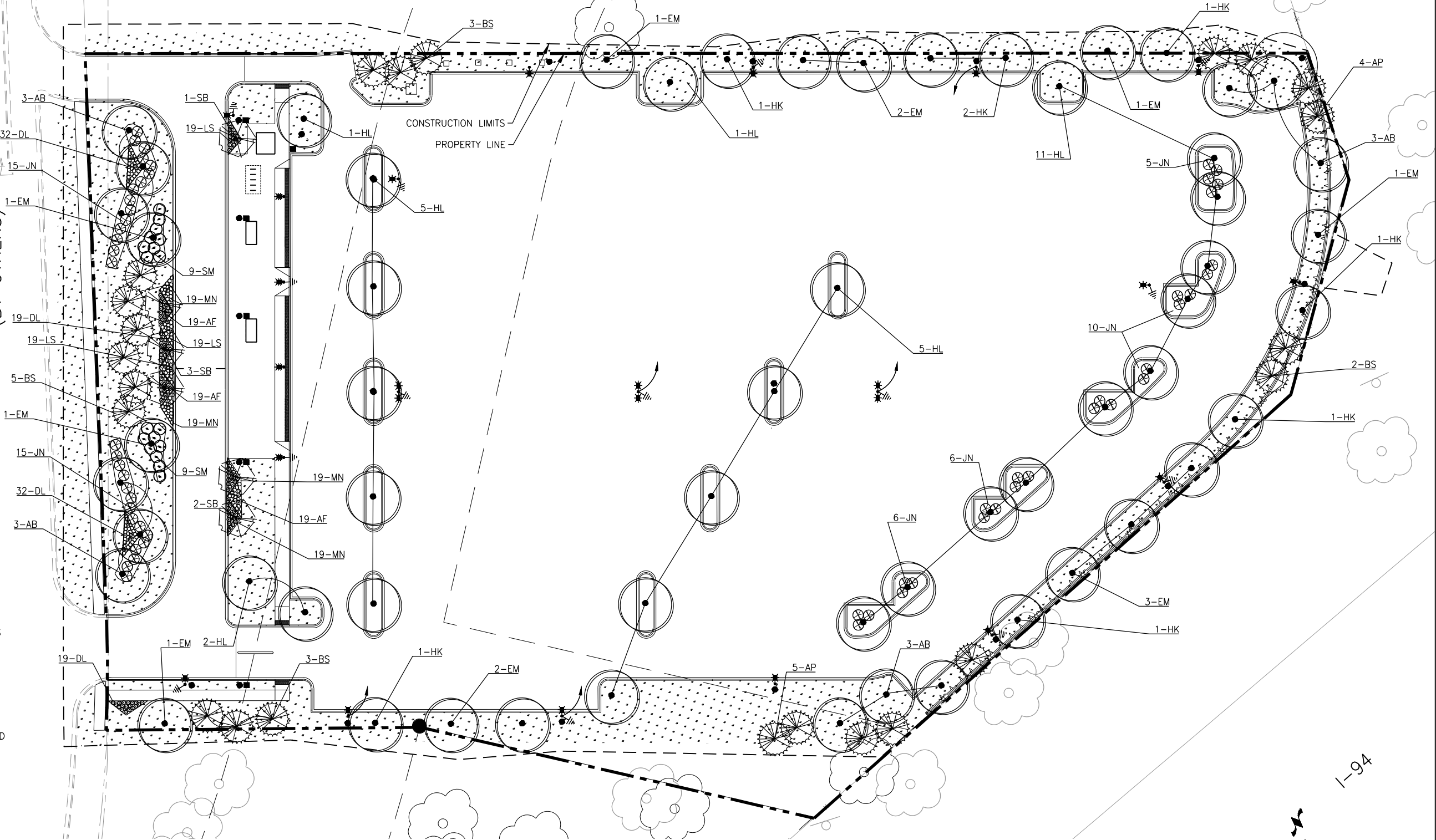
5 TREES PER ACRE DISTURBED = 4.7 Ac X 5 = 24 TREES REQUIRED

1 TREE PER 50'-FT OF FRONTAGE= 8 TREES REQUIRED

8 TREES REQUIRED @ MIN. 1 CAL. INCH TREE REPLACEMENT

77 TOTAL TREE REQUIRED

HUDSON BOULEVARD  
(BY OTHERS)



PLANT SCHEDULE – TREES					
SYM	QTY	SIZE	COMMON NAME	LATIN NAME	NOTES
AP	9	6' B&B	AUSTRIAN PINE	PINUS NIGRA	
BS	13	6' B&B	BLACK HILLS SPRUCE	PICEA GLAUCA DENSATA	
AB	12	2.5" B&B	AUTUMN BLAZE MAPLE	ACER X FREEMANII "JEFFERSRED"	
HL	23	2.5" B&B	IMPERIAL HONEYLOCUST	GLEDITISA TRIACANTHOS VAR. INERMIS "IMPCOLE"	
HK	8	2.5" B&B	HACKBERRY	CELTIS OCCIDENTALIS	
EM	13	2.5" B&B	ACCOLADE ELM	ULMUS JAPONICA X WILSONIANA "MORTON"	
SB	6	2.0" B&B	SERVICEBERRY	AMELANCHER X GRANDIFLORA "AUTUMN BRILLIANCE"	

PLANT SCHEDULE – SHRUBS AND PERENNIALS					
SYM	QTY	SIZE	COMMON NAME	LATIN NAME	NOTES
SM	18	#5 CONT	STAGHORN SUMAC	RHUS TYPHINA	
JN	57	#5 CONT	BUFFALO JUNIPER	JUNIPEROUS SABINA "BUFFALO"	
DL	102	#1 CONT	DAYLILY	HEMEROCALLIS "HAPPY RETURNS"	
MN	76	#1 CONT	MAYNIGHT SALVIA	SALVIA X SYLVESTRIS "MAINACHT"	
AF	57	#1 CONT	AUTUMN FIRE SEDUM	SEDUM "AUTUMN FIRE"	
LS	57	#1 CONT	BLACK-EYE SUSAN	RUDBECKIA "LITTLE GOLD STAR"	

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0	02/22/19	JWA	AEM	JWA	CUP APPLICATION SUBMITTAL
NO	DATE	BY	CKD	APPR	
C:\BMS\SRF-PW\SHARDING\HTPO.COM\DMS13368\11089_LN01.DWG: LN01					

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the State of Minnesota.

Print Name: SARAH HARDING

Date: 3/18/19 License #: 45592

HTPO

Engineering • Surveying  
Landscape Architecture

HANSEN THORP PELLINEN OLSON, Inc.


7510 Market Place, Suite 400 • Eden Prairie, MN 55344  
952.420.0700 • 952.420.7000

DRAWN BY  
J. ANDERSON

DESIGNED BY  
J. ANDERSON

CHECKED BY  
A. MCCALLUM

COMM. NO. 11089.00



I-94 AND MANNING AVENUE PARK AND RIDE STATION

11530 HUDSON BOULEVARD NORTH


LANDSCAPE PLAN

LAKE ELMO, MINNESOTA 55042


SHEET

L1-0

LEGEND



EXISTING TREES



EXISTING TREE TO BE REMOVED

HUDSON BOULEVARD (BY OTHERS)

TREE PRESERVATION NOTES:

1. SIGNIFICANT TREES ONSITE TO BE REMOVED =  
#1 - 12" BOXELDER (COMMON TREE)  
#2 - 18" BOXELDER (COMMON TREE)  
#3 - 15" ASH (COMMON TREE)  
TOTAL 45 CAL INCHES
2. TOTAL CAL INCHES OF SIGNIFICANT TREES ONSITE = 45 CAL INCHES.
3. 30% OF 45 CAL INCHES = 13.5 CAL INCHES ALLOWABLE TREE REMOVAL
4. CAL INCHES OF REMOVED TREES ABOVE THE ALLOWABLE TREE REMOVAL LIMIT EQUALS 31.5 CAL INCHES.
5. TREE REPLACEMENT EQUALS 1/4 OF THE 31.5 CAL INCHES REMOVED, WHICH EQUALS 8 CAL INCHES.
6. REFER TO THE LANDSCAPE PLAN FOR REPLACEMENT TREE SPECIES AND LOCATIONS.

#1 12" BOX

10" TRIP BOX

#2 18" BOX

10" BOX

7" BOX

#3 15" ASH

CONSTRUCTION LIMITS

PROPERTY LINE

CONSTRUCTION LIMITS, TYP

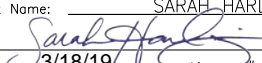
PROPERTY LINE

8" WIL



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NO	DATE	BY	CKD	APPR	
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I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the State of Minnesota.  
Print Name: SARAH HARDING  
  
Date 3/18/19 License # 45592

HTPO

Engineering • Surveying  
Landscape Architecture

HANSEN THORP PELLINEN OLSON, Inc.


7510 Market Plaza Drive • Eden Prairie, MN 55344  
952-820-0700 • 952-820-7000 FAX

DRAWN BY  
J. ANDERSON

DESIGNED BY  
J. ANDERSON

CHECKED BY  
A. MCCALLUM

COMM. NO. 11089.00



I-94 AND MANNING AVENUE PARK AND RIDE STATION  
11530 HUDSON BOULEVARD NORTH  
TREE PRESERVATION PLAN  
LAKE ELMO, MINNESOTA 55042

SHEET  
L1-1



STANDARD PLAN NOTES  
LANDSCAPE PLANS

- Contractor must contact Gopher State One Call 811 (www.gopherstateonecall.org) prior to tree staking and planting operations to verify underground utilities. Where private utilities exist on-site the Contractor is required to have those located as well.
- Plant materials shall meet American Standard for Nursery Stock: ANSI Z60.1, latest edition.
- No plant substitutions shall be made without the prior written authorization from the City.
- All tree locations to be field staked prior to installation. Contractor to coordinate field review of proposed tree locations with the City and Project Landscape Architect prior to any tree installation.
- All plants shall be planted immediately upon arrival to project site. No plant material is to be left overnight on the project site without being installed unless written approval by City.
- All trees, shrubs, perennials, turf lawn and native seeding to have a two-year warranty beginning upon written acceptance by the City. Defective plants as determined by the City shall be replaced within 30 days of notice during the growing season, and replacement materials shall receive the same two year warranty until plants are successfully established.
- Contractor to protect and maintain all plantings and plant beds, including protection from wildlife, weeding, re-mulching, fertilization, irrigation and all other typical forms of horticultural care until the end of the warranty period as determined and approved by City.
- All plants installed and maintained on City property shall be in accordance with City of Lake Elmo Bee-Safe Resolution No. 2015-13.
- An irrigation system or other water supply adequate to support the specified plant materials shall be provided.

APRIL 2017

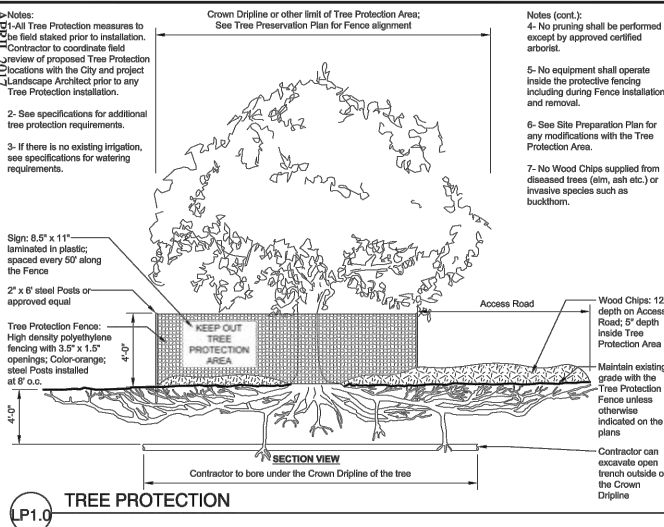


CITY OF LAKE ELMO

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LAKE ELMO

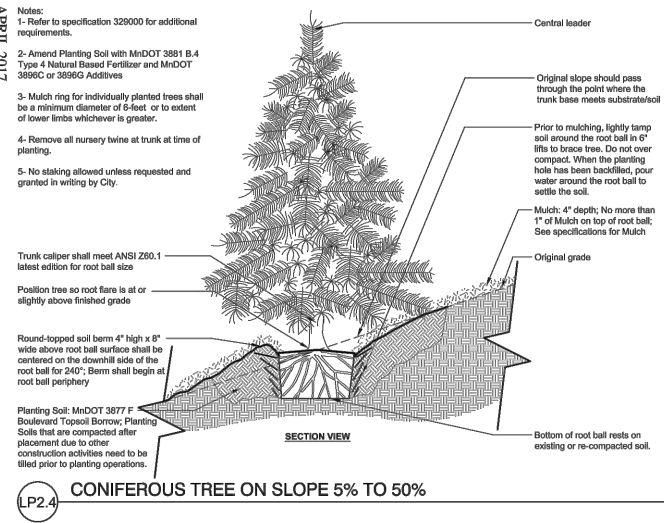
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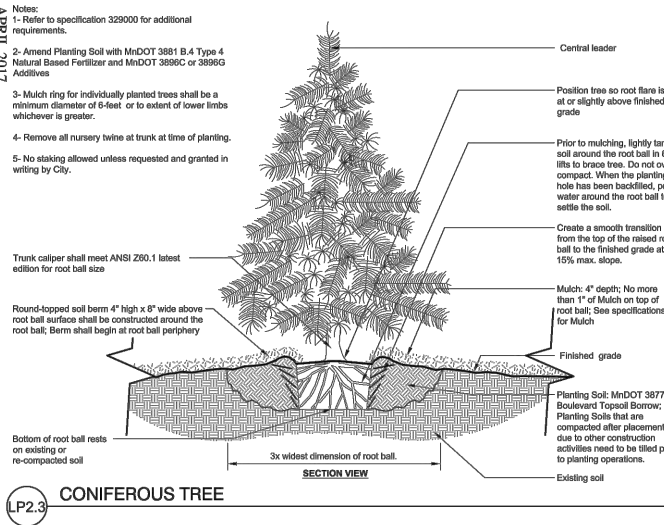
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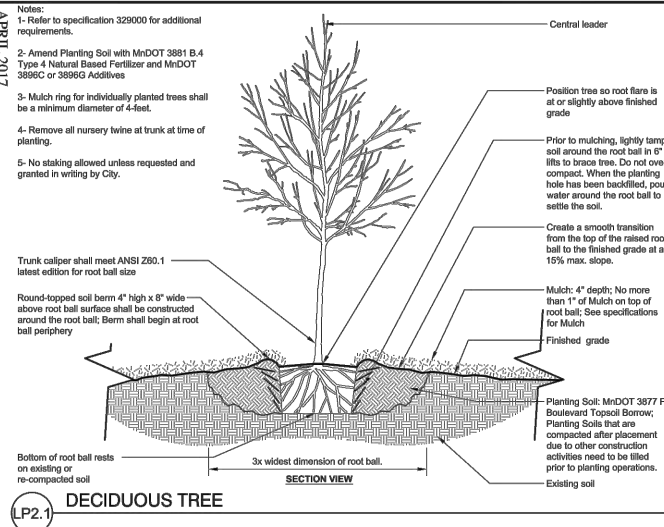
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CITY OF LAKE ELMO

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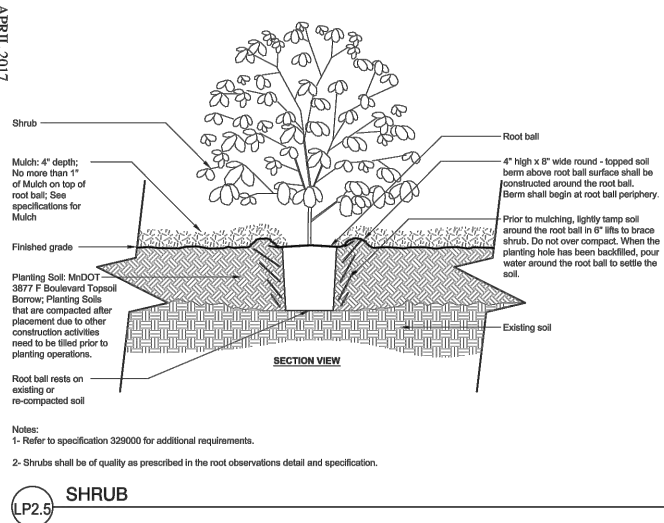
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CITY OF LAKE ELMO

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LAKE ELMO



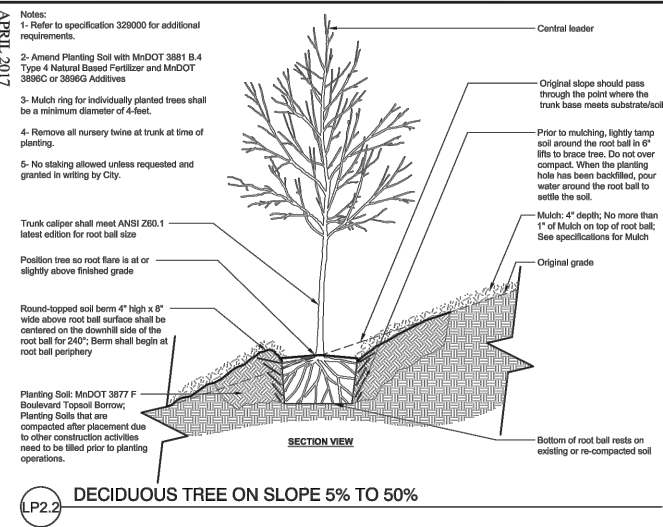
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CITY OF LAKE ELMO

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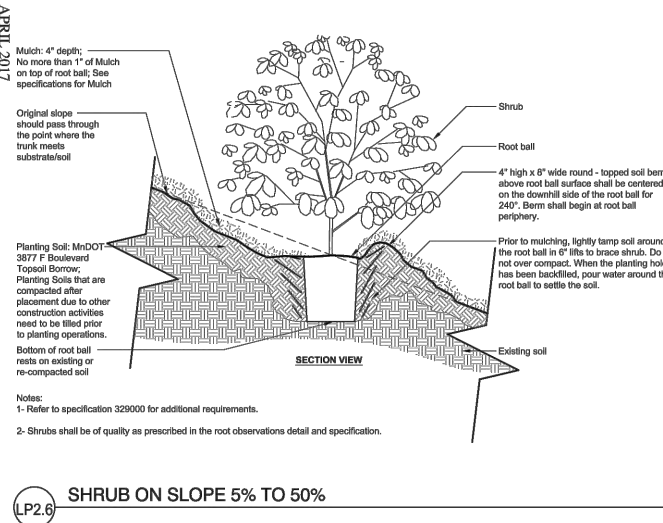
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CITY OF LAKE ELMO

STANDARD DRAWING NO.  
LP2.2  
LAKE ELMO



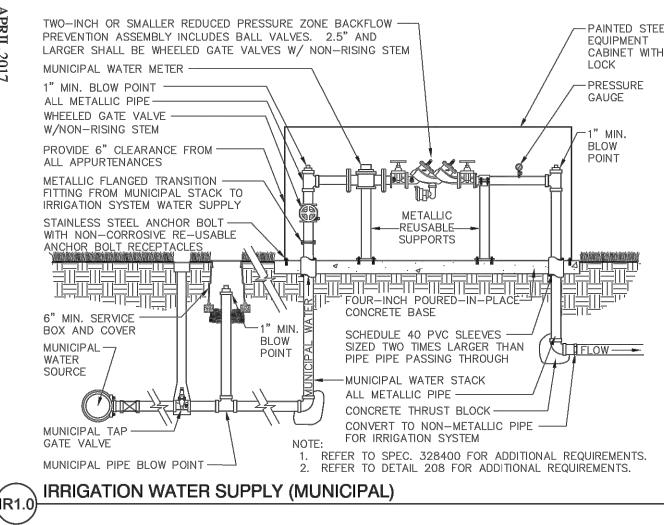
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CITY OF LAKE ELMO

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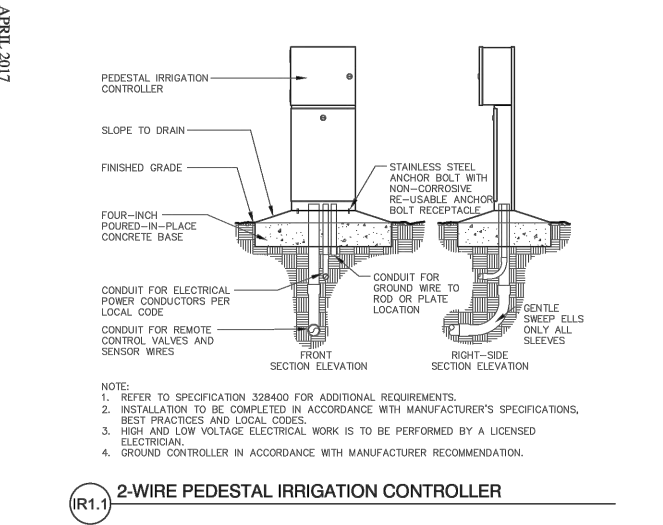
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CITY OF LAKE ELMO

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LAKE ELMO  
CITY OF LAKE ELMO

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LAKE ELMO



1	03/29/19	JWA	SEH	AEM	BID SET	
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NO	DATE	BY	CKD	APPR		

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I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the State of Minnesota.  
Print Name: SARAH HARDING  
Date: 3/18/19 License #: 45592

Engineering • Surveying  
Landscape Architecture  
HANSEN THORP PELLIN OLSON, INC.  
DESIGNED BY  
J. ANDERSON  
CHECKED BY  
A. MCCALLUM  
COMM. NO. 11089.00



I-94 AND MANNING AVENUE PARK AND RIDE STATION  
11530 HUDSON BOULEVARD NORTH  
LANDSCAPE DETAILS  
LAKE ELMO, MINNESOTA 55042

SHEET  
L1-2



# MEMORANDUM

## FOCUS ENGINEERING, inc.

Cara Geheren, P.E.	651.300.4261
Jack Griffin, P.E.	651.300.4264
Ryan Stempski, P.E.	651.300.4267
Chad Isakson, P.E.	651.300.4285

Date: April 18, 2019

To: Ken Roberts, Planner Director  
Cc: Chad Isakson, Assistant City Engineer  
From: Jack Griffin, P.E., City Engineer

Re: Metro Transit Park & Ride at I94 and CSAH-15  
Engineering Plan Review

An engineering review has been completed for the Metro Transit Park & Ride Station Engineering Plans at I94 and Manning Avenue (CSAH-15) received on March 25, 2019. The submittal consisted of the following documentation:

- I-94 and Manning Avenue Park & Ride Station Plans, dated 03.29.2019, prepared by SRF.
- Stormwater Management, dated November 2018, prepared by SRF. Not Certified.

### **STATUS/FINDINGS: Engineering has prepared the following review comments:**

#### PRELIMINARY PLAT AND EASEMENTS

- No lot easements have been provided as required.
- A retaining wall and chain link fence are proposed along the length of the rear lot line. The retaining wall and chain link fence encroach the required rear lot line easement. Temporary construction easements or permits will be required from adjacent properties to construct the proposed retaining wall. Temporary construction easements should be shown on the plans and provided to the City.
- The plans show a future charging station encroaching the required side lot easement on the north side. The plans should be revised to relocate the future charging station outside of the easement.
- Drainage and utility easements must be added to the plans over all watermain and hydrants. Easements must extend minimum 15-feet from pipe and hydrants (see Utility Plan comments). Drainage and utility easements must be provided in the City's standard form of drainage and utility easement agreement.

#### STORMWATER MANAGEMENT

- The site plan is subject to a storm water management plan meeting State, VBWD and City rules and regulations. A storm water management plan has been submitted but has not yet been reviewed by the VBWD. The stormwater management plan must be prepared and certified by an engineer licensed in the State of Minnesota. The plan must be resubmitted with professional certification. Any plan revisions made to obtain the VBWD approval must be resubmitted to the City for review and approval.
- The site plan shows storm water management being addressed using on-site underground storage located beneath the proposed parking spaces. The storm water facilities constructed for this development will remain privately owned and maintained. The applicant will be required to execute and record a Stormwater Maintenance and Easement Agreement in the City's standard form of agreement.
- The 100-year high water flood level (HWL) and 100-year HWL contour must be calculated and shown on the grading plans, must be fully contained within private property, and must be protected by easement.

## PERMITS/APPROVALS








- A copy of the VBWD permit is required prior to start of construction.
- A copy of the MnDOT drainage permit is required prior to start of construction.
- A copy of the MPCA Stormwater Construction permit is required prior to start of construction.
- Any plan revisions made to obtain the above permits must be resubmitted for City review and approval.
- An executed Stormwater Maintenance and Easement Agreement in the City's standard form is required prior to start of construction.
- Retaining walls that exceed 4 feet in height must have a design submitted and certified by an engineer licensed in the State of Minnesota.
- Written landowner permission must be provided for any off-site grading work, easements and storm water discharges to adjacent properties.
- No construction for this project may begin until the applicant has received City Engineer approval for the Final Construction Plans; the applicant has obtained and submitted to the City all applicable permits, easements and permissions needed for the project; a site improvement agreement has been executed with the City and all required securities have been provided, and a preconstruction meeting has been held by the City's engineering department.

## CONSTRUCTION PLANS

- Construction Phasing. The plans should be updated to better identify the Hudson Boulevard Improvements as "proposed construction by others" and to require coordination between contractors. The plans and specifications should carefully address construction phasing, such as the timing for water and sewer connection and testing, and driveway connections to Hudson Boulevard.
- C3.0. Grading, Drainage and Erosion Control. Revise the erosion control plans to include the City of Lake Elmo standard plan notes for erosion control and City standard details for erosion control. The specifications must be updated to include the City of Lake Elmo standard specifications for erosion control. The City specifications, details and plan notes must be identified to supersede all other plans and specifications in case of conflict.
- C3.0. Grading, Drainage and Erosion Control. Revise plans to identify Hudson Boulevard as "proposed" and to be constructed "by others". Add plan note that grading revisions may be required to match into the final constructed grades for Hudson Boulevard.
- C3.0. Grading, Drainage and Erosion Control. Add erosion control and restoration measures at the storm sewer outfall pipe off-site.
- C4.0. Utility Plan. Revise the utility plans to show the sewer and water service connections to be located at the right-of-way of Hudson Boulevard. The public improvement project must be updated to provide sewer and water service stubs from the mains to the right-of-way consistent with City of Lake Elmo standard service details.
- C4.0. Revise the utilities shown in Hudson Boulevard from "existing" to "installed by others".
- C4.0. Utility Plan. Add hydrants interior to the site as required by the Lake Elmo Fire Department. See attached plans showing required locations.
- C4.0. Utility Plan. Revise plans to extend 8-inch watermain interior to site to each hydrant and create a looped main configuration. Update the plans to show a minimum 30-foot utility easement centered over all watermain and hydrants. Easement must extend minimum 15-feet from all pipe and hydrants.
- C4.0. Utility Plan. Revise the utility plans to include the City of Lake Elmo standard plan notes for watermain and City standard details for watermain. The specifications must be updated to include the City of Lake Elmo standard specifications for watermain. The City specifications, details and plan notes must be identified to supersede all other plans and specifications in case of conflict.
- C5-X. Civil Details. The plans must be revised to replace all civil details, plan notes and specification for erosion and sediment control to meet the standards of the City of Lake Elmo.
- L1.0. Landscape Plans. All proposed utilities must be shown on the landscape plans and all trees relocated to maintain 10 feet of separation from all public utilities.

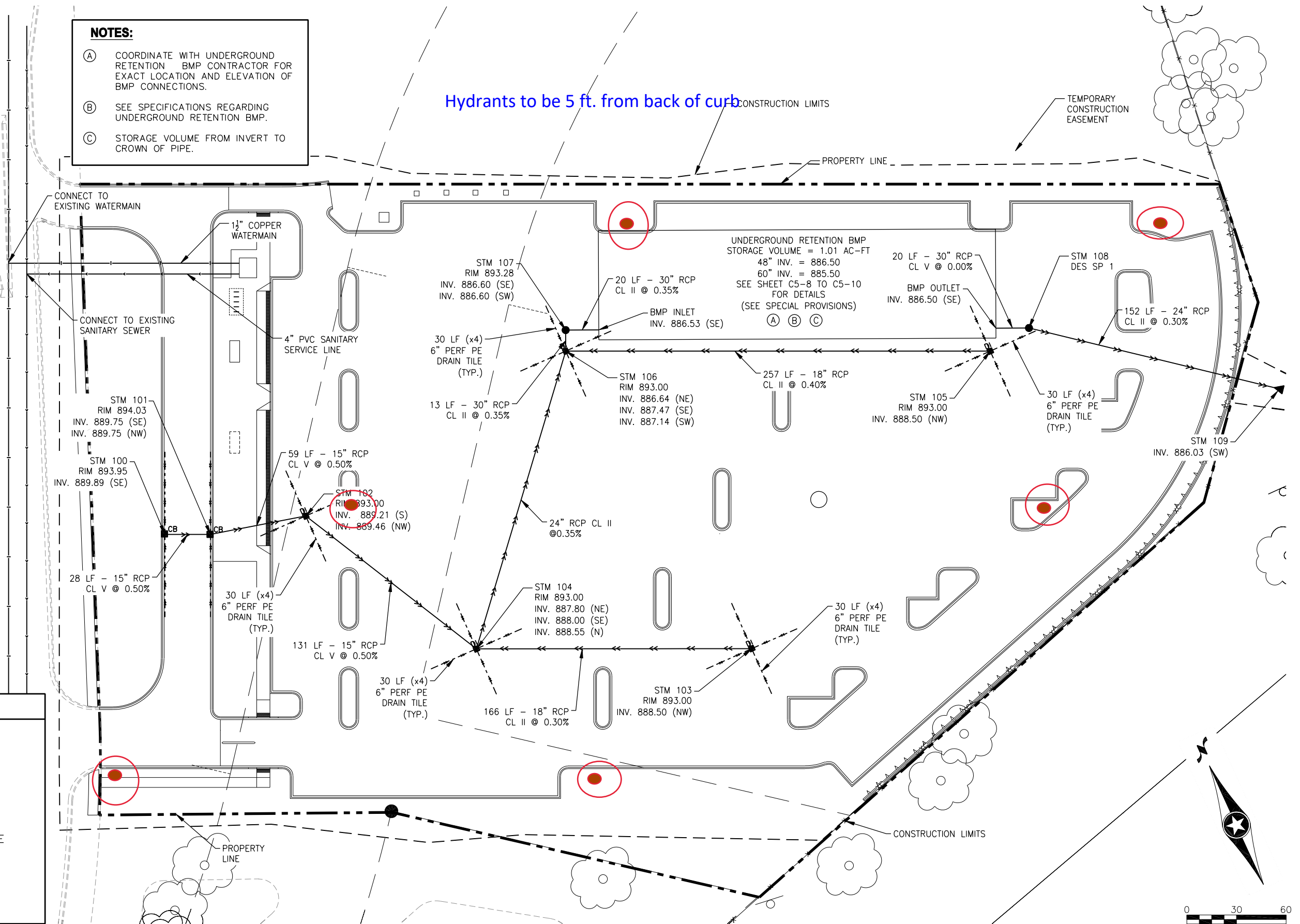
1. ALL SANITARY SEWER AND WATERMAIN UTILITIES SHALL BE FURNISHED AND INSTALLED PER THE REQUIREMENTS OF THE SPECIFICATIONS/THE MINNESOTA PLUMBING CODE, THE LOCAL GOVERNING UNIT, AND THE STANDARD UTILITIES SPECIFICATION OF THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), 2013 EDITION.
2. ALL UTILITY PIPE BEDDING SHALL BE COMPACTED SAND OR FINE GRANULAR MATERIAL. ALL COMPACTION SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CEAM SPECIFICATION AND THE GEOTECHNICAL REPORT.
3. ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE PERFORMED PER THE REQUIREMENTS OF THE STATE AND LOCAL JURISDICTIONS. THE CITY DEPARTMENT OF ENGINEERING AND BUILDING INSPECTIONS DEPARTMENT AND THE CONSTRUCTION ENGINEER MUST BE NOTIFIED AT LEAST 48 HOURS PRIOR TO ANY WORK WITHIN THE PUBLIC RIGHT OF WAY, OR WORK IMPACTING PUBLIC UTILITIES.
4. ALL SANITARY SEWER AND WATER SERVICES SHALL TERMINATE 5.0' FROM FACE OF BUILDING UNLESS OTHERWISE NOTED.
5. A MINIMUM OF 18 INCHES OF VERTICAL SEPARATION AND 10 FEET OF HORIZONTAL SEPARATION IS REQUIRED FOR ALL UTILITIES UNLESS OTHERWISE NOTED.
6. ALL NEW WATERMAIN AND SERVICES MUST HAVE A MINIMUM OF 8.0' FEET OF COVER. EXTRA DEPTH MAY BE REQUIRED TO MAINTAIN A MINIMUM 18" VERTICAL SEPARATION TO SANITARY OR STORM SEWER LINES. THE CONTRACTOR SHALL FIELD ADJUST WATERMAIN TO AVOID CONFLICTS WITH SANITARY SEWER, STORM SEWER, AND OTHER SERVICES AS REQUIRED. INSULATION OF WATER AND SANITARY SEWER LINES SHALL BE PROVIDED WHERE 8.0' MINIMUM DEPTH CANNOT BE ATTAINED.
7. ALL FIRE HYDRANTS SHALL BE LOCATED 5.0' BEHIND BACK OF CURB OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
8. PROPOSED PIPE MATERIALS:

## LEGEND

- |   |                      |
|---|----------------------|
|  | SANITARY SEWER       |
|  | WATERMAIN            |
|  | STORM SEWER          |
|  | DRAIN TILE           |
|  | DROP INLET STRUCTURE |
|  | CATCH BASIN          |
|  | FLARED END SECTION   |

- (A) COORDINATE WITH UNDERGROUND RETENTION BMP CONTRACTOR FOR EXACT LOCATION AND ELEVATION OF BMP CONNECTIONS.
- (B) SEE SPECIFICATIONS REGARDING UNDERGROUND RETENTION BMP.
- (C) STORAGE VOLUME FROM INVERT TO CROWN OF PIPE.

Hydrants to be 5 ft. from back of curb



A scale bar labeled "SCALE IN FEET" with markings for 30 and 60 feet. The bar is divided into segments, with the first segment being black and the others white.

1	03/29/19	WCG	PSG	MCA	BID SET
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NO	DATE	BY	CKD	APPR	
N:\BMS\SRF-PW\PGALZKI\DMS13368\11089_SW01.DWG: UTILITYPLAN					

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Print Name: MICHAEL C. AARON

*Michael C. Aaron*

Date 03/19/19 License # 25721

DRAWN BY W. GRAY
DESIGNED BY P. GALZKI
CHECKED BY M. AARON
COMM. NO. 11089.00



I-94 AND MANNING AVENUE PARK AND RIDE STATION  
11530 HUDSON BOULEVARD NORTH  
UTILITY PLAN  
LAKE ELMO, MINNESOTA 55042

HEET  
4-0

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**To:** Ken Roberts, City of Lake Elmo Planning Director  
**From:** Lucius Jonett, Wenck Landscape Architect  
**Date:** March 29, 2019  
**Subject:** City of Lake Elmo Landscape Plan Review  
Metro Transit Park and Ride, Review #1

**Submittals**

- Tree Preservation Plan, dated March 18, 2019, received March 26, 2019.
- Landscape Plans, dated March 18, 2019, received March 26, 2019.

**Location:** Northwest quadrant of the intersection of I-94 and Manning Avenue. Lot 1, Block 1 of the Four Corners 2<sup>nd</sup> Addition development.

**Land Use Category:** C - Commercial

**Surrounding Land Use Concerns:** None

**Special landscape provisions in addition to the zoning code:** None

### **Tree Preservation:**

- A. A tree preservation plan has been submitted that **does not** meet all requirements.
1. Significant trees are not appropriately designated:
    - i. The nuisance tree category includes noxious or invasive species such as buckthorn, boxelder and cottonwood.

Entire Site		
Total Caliper Inches of Significant Trees On-Site:	45.0	Cal Inches
Common Trees	15	
Conifer/Evergreen Trees	0	
Hardwood Trees	0	
Nuisance Trees	30	
Significant Inches Removed On-Site	45	Cal Inches
Common Trees	15	
Conifer/Evergreen Trees	0	
Hardwood Trees	0	
Nuisance Trees	30	
30% Tree Removal Limits (Cal. Inches)	Allowed	Proposed
Subtract Common Tree Removals	4.5	15
Subtract Conifer/Evergreen Tree Removals	0	0
Subtract Hardwood Tree Removals	0	0
Removals in excess of 30% allowances		
Removals in excess of 30% allowances	10.5	Cal Inches
Common Removals in Excess of 30% Allowance	10.5	
Conifer Removals in Excess of 30% Allowance	0.0	
Hardwood Removals in Excess of 30% Allowance	0.0	
Common Tree Replacement Needed (1/4 the dia inches removed)	2.6	Cal Inches
Conifer Tree Replacement Needed (1/2 the dia inches removed)	0.0	Cal Inches
Hardwood Tree Replacement Needed (1/2 the dia inches removed)	0.0	Cal Inches
<b>Common Tree Replacement Required @ 2.5" per Tree</b>	1.1	<b># Trees</b>
<b>Conifer Tree Replacement Required @ 3" per 6' Tall Tree</b>	0	<b># Trees</b>
<b>Hardwood Tree Replacement Required @ 2.5" per Tree</b>	0	<b># Trees</b>

- B. Tree replacement is required because more than thirty (30) percent of the diameter inches of significant trees surveyed will be removed.
- C. Tree replacement calculations **do not** follow the current required procedure as of the ordinance updates approved on 9-19-2018:
1. A calculation must be provided which breaks out the number of inches removed for hardwood, evergreen/coniferous, and common trees. The 30% removal figure applies to each category individually and trees over the 30% allowance are to be replaced according to:
    - i. Common tree species shall be replaced with new trees at a rate of one-fourth (1/4) the diameter inches removed.

- ii. Coniferous/evergreen tree species shall be replaced with new coniferous or evergreen trees at a rate of one-half (1/2) the diameter inches removed. Since coniferous species are often sold by height rather than diameter inch, the following conversion formula can be used: Height of Replacement Coniferous Tree/2 = Diameter Inches of Credit
- iii. Hardwood deciduous tree species shall be replaced with new hardwood deciduous trees at a rate of (1/2) the diameter inches removed.

D. This project is commercial development, therefore mitigation replacement trees can be included toward landscape required tree counts.

### **Landscape Requirements:**

The preliminary landscape plans meet the code required number of trees.

	Preliminary Plan (Code Required)	Preliminary Plan Proposed	
Street frontage	400		Lineal Feet
Lake Shore	0		Lineal Feet
Stream Frontage	0		Lineal Feet
Total Linear Feet	400		Lineal Feet
/50 Feet = Required Frontage Trees	8		Trees
Development or Disturbed Area	-		SF
Development or Disturbed Area	4.7		Acres
*5 = Required Development Trees	24		Trees
Interior Parking Lot Spaces*	550		Spaces
/15 = Required Parking Lot Trees	37		Trees
Perimeter Parking Lot Frontage Length*	0		Lineal Feet
/50 = Required Frontage Strip Trees	0		Trees
Required Mitigation Trees	1		
<b>Required Number of Trees**</b>	<b>70</b>		
<b>Total Trees to Date</b>		<b>84</b>	

\* Parking lot landscaping or screening trees are included in landscape required tree counts.

None if 0 - 30 Parking Spaces

1 tree per 10 spaces if 31 - 100 Parking Spaces

1 tree per 15 spaces If >101 Parking Spaces

\*\* Commercial development - mitigation replacement trees are included in landscape required tree counts.

1. A minimum one (1) tree is proposed for every fifty (50) feet of street frontage.
2. A minimum of five (5) trees are proposed to be planted for every one (1) acre of land that is developed or disturbed by development activity.



The landscape plans do meet the minimum compositions of required trees:

Preliminary Plan	Qty	% Composition	
Deciduous Shade Trees	56	67%	>25% required
Coniferous Trees	22	26%	>25% required
Ornamental Trees	6	7%	<15% required
<b>Tree Count</b>		<b>84</b>	

- A. A landscape plan has been submitted that **does not** include all requirements.
1. Utilities are not shown on the landscape plan to review for tree placement conflicts.
- B. The landscape plan does meet the landscape layout requirements.
- C. Interior Parking Lot Landscaping – The development does include an interior parking lot with more than 30 spaces and satisfies the landscaping requirements.
1. At least 5% of the interior area of parking lots is devoted to landscape planting areas. The parking lot is measured at approximately 203,250 square feet and planting areas measuring approximately 15,000 square feet in the parking lot islands and bump outs. The planting area is approximately 7% of the interior of the parking lot area.
  2. The interior parking lot planting area includes approximately 45 shade trees satisfying the minimum required tree planting requirements for interior parking lots.
- D. Perimeter Parking Lot Landscaping – The development does include a perimeter parking lot and satisfies the landscaping requirements.
1. A landscaped frontage strip is provided along Hudson Boulevard for screening from the public street. The width of the strip exceeds the required 8' minimum.
- E. Screening – Screening is not required by City code.

**Findings:**

1. Tree Preservation Plan – Boxelder trees were classified as common trees. According to the ordinance updates approved on 9-19-2018, they are now classified as nuisance trees and not required to be included in the tree inventory and mitigation calculations.
2. Tree Replacement Calculations – Because of the different tree classification, the applicant over estimated the number of replacement trees required. When calculated, 1.1 replacement trees is required. Fraction amounts are typically required to be rounded up. Since only 1 common tree (15" ash) is being removed and the fraction is 0.1; 1 replacement tree will be adequate.
3. Landscape Plan - Utilities are not shown on the landscape plan to review for tree placement conflicts.

**Ken Roberts**  
Planning Director  
City of Lake Elmo  
March 29, 2019



**Recommendation:**

The preliminary tree preservation and landscape plans meet and exceed City requirements. It is recommended that a condition of approval include:

1. Submit a revised landscape plan showing locations of utilities to review that all trees are 10' minimum away from utility lines.

Sincerely,

A handwritten signature in black ink that reads 'Lucius Jonett'. The signature is fluid and cursive, with a long horizontal stroke extending from the end.

Lucius Jonett, PLA (MN)  
Wenck Associates, Inc.  
City of Lake Elmo Municipal Landscape Architect

February 15, 2019

Ken Roberts, Planning Director  
City of Lake Elmo  
3880 Laverne Avenue North  
Lake Elmo, MN 55042

SUBJECT: Four Corners Second Edition Preliminary Plat  
MnDOT Review #P19-002  
NW quad of I-94 and CSAH 15 (Manning Ave N)  
City of Lake Elmo, Dakota County

Dear Mr. Roberts:

MnDOT has reviewed the preliminary plat received 1/31/19 for the Four Corners Second Edition project in compliance with Minnesota Statute 505.03, subdivision 2, Plats. Before further development, please address the following:

***Design***

The preliminary plat shows the relocation of the frontage road (Hudson Blvd) and its intersection with CSAH 15 (Manning Ave N) within the MnDOT right-of-way, which extends several hundred feet north of I-94 along Manning Ave. Roadway configuration changes of this magnitude require a Level 2 layout, as noted in MnDOT's review of the EAW for the Manning Avenue Corridor Management and Safety Improvement Project (see MnDOT comment letter of 12/21/2016 attached).

MnDOT layout design guidance and layout requirements are provided at:  
<http://www.dot.state.mn.us/design/geometric/index.html>. Please direct questions regarding roadway design to Jeff Rones of MnDOT's Metro District Design Section at 651-234-7647 or [Jeff.Rones@state.mn.us](mailto:Jeff.Rones@state.mn.us).

***Traffic***

MnDOT strongly recommends that the city require a traffic impact study (TIS) to be performed, as the traffic volumes associated with the proposed development and uses are significant (likely exceeding 15,000 AADT) and meet MnDOT warrants for a TIS (2,500 or more new daily vehicle trips). See Chapter 5 of MnDOT's Access Management Manual for TIS guidance:  
[www.dot.state.mn.us/accessmanagement/docs/pdf/manualchapters/chapter5.pdf](http://www.dot.state.mn.us/accessmanagement/docs/pdf/manualchapters/chapter5.pdf).

Please direct questions regarding traffic to Kaare Festvog of MnDOT's Metro District Traffic Engineering Section at 651-234-7814 or [Kaare.Festvog@state.mn.us](mailto:Kaare.Festvog@state.mn.us).

***Right-of-Way***

The 60' drainage way shown on the future Lot 1 labeled "PARCEL No. 44 (TO BE VACATED BY MN DOT)" is currently owned in fee by MnDOT. Therefore, MnDOT will need to convey fee ownership of the parcel to the owner. Please contact Jennifer Matti in MnDOT's Metro District Surveys Section at 651-234-3474 or [Jennie.Bailey@state.mn.us](mailto:Jennie.Bailey@state.mn.us) regarding acquisition of the drainage way.



Please direct questions regarding right-of-way to Michael Lynch of MnDOT's Metro District Right of Way Section at 651-234-7558 or [Mike.Lynch@state.mn.us](mailto:Mike.Lynch@state.mn.us).

### ***Drainage***

A MnDOT drainage permit will be required to ensure that current drainage rates to MnDOT right-of-way will not be increased. MnDOT's Drainage Permits Checklist is attached. The drainage permit application is available online at: <https://dotapp7.dot.state.mn.us/OLPA> and requires the following information:

1. A grading plan showing existing and proposed contours.
2. Drainage area maps for the proposed project showing existing and proposed drainage areas. Any off-site areas that drain to the project area should also be included in the drainage area maps. The direction of flow for each drainage area must be indicated by arrows.
3. Drainage computations for pre- and post-construction conditions during the 2-, 10-, 50- and 100-year rain events.
4. Time of concentration calculations.
5. An electronic copy of any computer modeling used for the drainage computations.
6. See also the attached Drainage Permits Checklist for more information.

The application should be submitted to:

MnDOT – Metropolitan District Permit Office  
1500 West County Road B-2  
Roseville, MN 55113

MnDOT will perform a thorough review of the application and request any additional information that may be needed. For questions, please contact Bryce Fossand of MnDOT's Metro District Water Resources Section at 651-234-7529 or [Bryce.Fossand@state.mn.us](mailto:Bryce.Fossand@state.mn.us).

### ***Permits/No Impacts to Interstate Highway***

In addition to the drainage permit, any other work affecting or impacting MnDOT right of way on Manning Ave N will require a permit. No work within or impacts to the I-94 right-of-way will be allowed. Permit forms are available from MnDOT's utility website: <http://www.dot.state.mn.us/utility/forms.html>. Please include one 11"x17" plan set and one full size plan set with each permit application.

Please direct questions regarding these requirements to Buck Craig of MnDOT's Metro District Permits Section at 651-234-7911 or [Buck.Craig@state.mn.us](mailto:Buck.Craig@state.mn.us).

### ***Review Submittal Options***

MnDOT's goal is to review proposed development plans and documents within 30 days of receipt. Electronic file submittals are typically processed more rapidly. There are four submittal options:

1. Email documents and plans in .pdf format to [metrodevreviews.dot@state.mn.us](mailto:metrodevreviews.dot@state.mn.us). Attachments may not exceed 20 megabytes per email. If multiple emails are necessary, number each message.
2. Upload .pdf file(s) to MnDOT's external shared internet workspace site at: <https://mft.dot.state.mn.us>. Contact MnDOT Planning development review staff at [metrodevreviews.dot@state.mn.us](mailto:metrodevreviews.dot@state.mn.us) for access instructions and send an email listing the file name(s) after the documents have been uploaded.

3. Mail, courier, or hand deliver documents and plans in .pdf format on a CD-ROM compact disc to:  
MnDOT – Metro District Planning Section  
Development Reviews Coordinator  
1500 West County Road B-2  
Roseville, MN 55113
4. Submit printed documents via U.S. Mail, courier, or hand delivery to the address above.  
Include one set of full size plans.

You are welcome to contact me with questions at 651-234-7795.

Sincerely,

David Elvin, AICP  
Senior Planner

**Copy via E-Mail:**

Adam Josephson, Area Engineer  
Ryan Coddington, Engineering  
Jeff Rones, Design  
Mike Lynch, Right of Way  
Jennifer Matti, Surveys  
Buck Craig, Permits  
Bryce Fossand, Water Resources  
Kaare Festvog, Traffic Engineering  
Carl Jensen, Transit Advantages  
Mike Nelson, Surveys  
Cameron Muhic, Multi-Modal  
Russell Owen, Metropolitan Council  
Michael Welling, Washington County Survey Division