



STAFF REPORT

DATE: 5/12/2025

ITEM #: 5a

TO: Planning Commission
FROM: Nathan Fuerst, AICP, Consulting Planner
AGENDA ITEM: **Inwood 8th Addition - Preliminary Plat, PUD, and Comprehensive Plan Amendment**
REVIEWED BY: Jason Stopa, Community Development Director
Sophia Jensen, City Planner

BACKGROUND:

This application is returning to the Planning Commission for review after being tabled at the April 14, 2025, allowing for revisions to the project plans. On April 29, 2025 the preliminary plat and PUD plans were resubmitted to the City for review after a redesign of the eastern half of the project was completed. Additional background on prior approvals can be found in the April 14 staff report.

Per the Applicant's resubmitted narrative, the following general revisions were made to their plans:

- Aligned access points east and west of Island Trail.
- New 28' street through the center of the development for improved streetscaping.
- Stub streets removed.
- Landscaping features added to open space, and buffering added to south.
- Increased open space area.

As revised, this phase of the Inwood project would include a 123 unit apartment west of Island Trail, and 148 attached townhomes primarily east of Island Trail for a total of 271 units. This is the same number of apartment units and one fewer attached townhome unit than the version of the plan last reviewed by the planning commission.

The following applications were required for this project:

1. **Preliminary Plat** – This project requires a Preliminary and Final Plat.
2. **PUD** – Any flexibility required to the City's standards must be granted through a new PUD, no flexibilities were granted in the initial Inwood PUD.
3. **Comprehensive Plan Amendment** – This is required to consolidate the future land use guidance on the subject parcels from three separate categories to one, maintaining the site-wide limit of 15 units per acre.

ISSUE BEFORE THE PLANNING COMMISSION:

The Planning Commission is being asked to review the comprehensive plan amendment and revised preliminary plat, and PUD requests to make a recommendation to the City Council.

REQUEST DETAILS:

Applicant: M/I Homes of Minneapolis/St. Paul, LLC.
Property Owner: Vadnais Associates, LLC.
Location: Unaddressed parcel Southeast and Southwest of the intersection of 5th Street North and Island Trail.
PID#: 33.029.21.14.0029, 33.029.21.13.0028, 33.029.21.42.0017, and 33.029.21.42.0018
Current Zoning: High-Density Residential (HDR)/Planned Unit Development (PUD), Shoreland Overlay
Proposed Zoning HDR/PUD, Shoreland Overlay
Future Land Use: Mixed Use Commercial/High-Density Residential/Business Park
Surrounding: Low-Density Residential (LDR) PUD, Public Facility, Business Park PUD, HDR PUD/Commercial
Deadline for Action: Application Complete: 2/14/2025
120 Day Preliminary Plat Deadline: 6/14/2025

PROPOSAL DETAILS/ANALYSIS:**Site Data.**

Total Site Area	19.27 acres
Total Wetland Area	0 acres
Total Units	272 units
Overall Net Density	14.12 units/acres

Comprehensive Plan Conformance.

A comprehensive plan amendment is proposed by the developer to reguide the land west of Island Trail from Mixed Use Commercial to High Density Residential. The primary reason is to allow for the residential net density to be blended across this part of the Inwood development.

The Inwood Townhomes project is currently proposed across two different Future Land Use Categories. East of Island Trail, the future land use is designated as High Density Residential (HDR) which allows residential uses at a density range of between 8.01-15 units per acre. West of Island Trail, the future land use designation is Mixed Use Commercial (MU-C), which allows a density range of 10-15 units per acre. On their own, the two different parts of the development would have a net density of over 23 units per acre on the west side of Island Trail and 11.7 units per acre on the east side of Island Trail. There are two small existing outlots guided Business Park which would also be reguided to HDR. Collectively, the entire project's net density is 14.06 units per acre which is within the allowable density of both future land use categories.

This development had an approved preliminary plat and EAW which anticipated this level of density. Additionally, the entire project is below the maximum density in both existing future land use categories. Staff are therefore supportive of the request to amend the City's comprehensive plan.

Land Use(s).

The City's zoning map indicates that the subject property is zoned High Density Residential (HDR) with a PUD overlay. The proposed land uses, Multifamily Residential and Single Family Attached (townhomes), are permitted with conditions in the underlying HDR zoning district.

After a review of the Inwood PUD ordinance, staff have concluded that no PUD flexibilities or other standards were created for this part of the overall Inwood Development Project. Therefore, the zoning requirements for this property are all determined based on all current applicable standards from Lake Elmo City Code.

Environmental Review. This project is covered under an existing Environmental Assessment Worksheet (EAW) originally prepared for the Inwood Development at the time of the Inwood Preliminary Plat. Staff have reviewed the EAW and determined it accounted for the number of units and intensity of development proposed in this phase of the Inwood development. Furthermore, this project does not trigger a mandatory EAW on its own under Minnesota state rules. No further environmental review is required.

Project Phasing. As proposed, the Townhome portion of the development would be constructed in three separate phases, with the first phase beginning in 2025. The apartment building is not included in the phasing plan or narrative, but is assumed by staff be included as part of the project's first phase as it will be constructed by a separate developer, Rachel Development.

PUD Minimum Requirements.

The Developer is proposing private drives, which are allowed through a Conditional Use Permit by ordinance, but which create a need for other flexibilities based on the lack of street frontage to the residential units. A PUD is a negotiated zoning district, and according to the Lake Elmo Zoning Code Article 18, Planned Unit Development Regulations, zoning flexibility can be granted in order to better utilize site features and to obtain a higher quality of development.

City Code establishes minimum thresholds a project must meet in order to qualify as a PUD. Staff has found that the proposed development does meet the following minimum requirements of a PUD:

- a. **Lot Area:** The site area exceeds the minimum lot area for of five acres for a PUD development.
- b. **Open Space:** The PUD ordinance indicates that at least 20% of the development area not within roadway corridors is to be reserved as open space.
- c. **Street Layout:** In existing developed area, the PUD should maintain the existing street grid, where present, and restore the street grid where it has been disrupted. In newly developing areas, streets shall be designed to maximize connectivity in each cardinal direction, except where environmental or physical constraints make this infeasible. All streets shall terminate at other streets, at public land, or at a park or other community facility, except that local streets may terminate in stub streets when those will be connected to other streets in future phases of the development or adjacent developments.

PUD Identified Objectives.

When evaluating a PUD proposal, the City must find a PUD meets one or more objectives contained in Article 18, which are listed below:

- a. Innovation in land development techniques that may be more suitable for a given parcel than conventional approaches.
- b. Promotion of integrated land uses, allowing for a mixture of residential, commercial, and public facilities.
- c. Establishment of appropriate transitions between differing land uses.
- d. Provision of more adequate, usable, and suitably located open space, recreational amenities, natural resource protection and other public facilities than would otherwise be provided under conventional land development techniques.
- e. Accommodation of housing of all types with convenient access to employment opportunities and/or commercial facilities; and especially to create additional opportunities for lifecycle housing to all income and age groups.
- f. Preservation and enhancement of important environmental features through careful and sensitive placement of buildings and facilities.
- g. Coordination of architectural styles and building forms to achieve greater compatibility within the development and surrounding land uses.
- h. Creation of more efficient provision of public utilities and services, lessened demand on transportation, and the promotion of energy resource conservation.
- i. Establishing measures to protect and preserve groundwater storage.
- j. Allowing the development to operate in concert with a redevelopment plan in certain areas of the City and to ensure the redevelopment goals and objectives will be achieved.

In their narrative, the applicant claims to meet all identified objectives. Staff find that the development may not meet all objectives, but could support findings for meeting b, c, e, and g.

Bulk Standard Review.

Cells in the 'As Proposed' column shaded in **blue** indicate a deviation requested from City Code.

HDR Zoning Standards (105.12.720)	Requirements	Proposed - Single Family Attached	Proposed - Multifamily Dwelling
Single Family Attached - Minimum Lot Area	1,750 sq ft/unit	4,306 sq ft/unit	-
Multifamily Dwelling - Minimum Lot Area	1800 sq ft/unit	-	1608 sq ft/unit
Single Family Attached - Minimum Lot Width	20 ft/unit	21.3 feet	-
Multifamily Dwellings – Minimum Lot Width	60 ft/ bldg	-	> 300 ft
Maximum Impervious Cover	75%	Not Provided (Estimated at 80-83%)	46.4%
Minimum Setback – Front	20 ft	9'	50 ft
Minimum Setback – Side	10 ft	20 ft (between buildings)	30 ft
Minimum Setback – Corner	15 ft	20 ft	-
Minimum Setback – Rear	20 ft	5' to deck, 11' to bldg	30 ft
Minimum Setback – 5 th St. N.	40 ft	26'- 36' ft	> 60 ft
Maximum Height	50 ft	30-40'	38'

Specific standards for Single Family Attached in the HDR. City Code Section 105.12.740 (j) contains standards for attached townhome units in the HDR Zoning District. The Development meets requirements for buildings to be below ten units, and adequate open space is provided. The City's standards require buildings to front a public street with 15' of frontage and with no parking between the building and street. Private drives are permitted by the City's ordinance through a conditional use permit. The private lots are all greater than 15' wide and front a private drive. Because all proposed units are now double fronted, staff find the development is compliant with the intent of the City's requirements, and any necessary flexibilities will be approved through the PUD.

Specific standards for Multifamily Dwellings in the HDR. City Code Section 105.12.740 (k) requires that parking be screened from the public streets and that common open space of a minimum 200 square feet per unit. Both design standards are met by this proposed project.

Design Guideline Conformance. City Code Section 105.12.840 requires design review against standards in the Lake Elmo Design Standards Manual. The proposed housing types are provided in the packet and include elevations and floor plans. Staff review of conformance is below:

Townhome Site Design

- Building placement – Buildings are oriented perpendicular to the street, with entrances oriented towards the street where possible. All townhome units are double fronted, and are oriented to a street where feasible, and public open space with connected sidewalks where street frontage is not possible.
- Streetscape – Concrete sidewalks are provided through the development, and run along the street where feasible. Buildings are connected with sidewalks and fronted towards the street where feasible.
- Landscaping – Overstory trees will not meet the City's boulevard tree spacing standard of one tree every 50' in areas outside of the proposed Street C. Landscaping with shallow root systems which will not impact utility service lines should be planted between driveways to screen mechanical and HVAC equipment. Preservation of existing trees will help the development blend in with the existing development along the 5th Street North Corridor.
- Parking – All parking would be in enclosed spaces or on driveways and would not impede pedestrian movement. Street parking is proposed as 90-degree stalls, however staff would recommend parallel parking, instead, located in bump-outs consistent with engineering standards.

Townhome Building Design

- Form and Façade – Buildings are proposed with architecture consistent throughout the site, and the surrounding areas. Facades are articulated with little or no blank wall space.
- Building Materials – Vinyl siding is prohibited by the City's design standards and approval would be required through the proposed PUD. Otherwise, design and color appear to be generally consistent with standards for townhomes.
- Mass and Scale – Buildings are articulated to create the appearance of multiple structures. Due to the private street design approach, buildings are located more closely together than nearby residential development, creating a denser appearance.
- Roof Design, Entries, Lighting – Features proposed are consistent with City standards for this type of construction.

- Signage – No signage appears to be proposed on the site plan and is thus not reviewed as part of this project.

Apartment Building Site Design

- Building placement – The building is oriented to both 5th Street North and Island Trail.
- Streetscape – Better connectivity has been provided from the north side of the apartment to 5th Street North for people choosing to walk or bike to area commercial amenities.
- Landscaping – Overstory trees will line the edges of this site along the public right of way with additional tree plantings and open space exceeding minimum requirements.
- Parking – The parking is central to the apartment site and is fully screened from public right of way.

Apartment Building Design

- Form and Façade – The apartment building facades are articulated with blank wall space minimized. No parking is proposed between the façade and street, meeting design standards.
- Building Materials – Materials and finishes are generally of quality with colors consistent with a requirement for earth or muted tones.
- Mass and Scale – Given the surrounding land uses of the apartment building site, the scale and mass of the building is an appropriate transition to lower intensity residential land uses east of the site.
- Roof Design, Entries, Lighting – features proposed are consistent with City standards for this type of construction and site layout.
- Signage – No signage appears to be proposed on the site plan and is thus not reviewed as part of this project.

Parks and Trails. The City Code requires park land dedication for all new residential developments. The City may also determine that land is not necessary and accept a cash fee in lieu for the purpose of enhancing the existing park and trail system.

At its meeting on March 17, 2025, the City’s Park Commission discussed the proposed development and made a recommendation to the City Council for acceptance of cash in lieu of land. Any park land or cash dedication would be subject to a credit from dedication of park land (Ivywood Park) in previous additions of the Inwood development. Staff recommend acceptance of cash in lieu of park land given proximity of this project existing parks.

Subdivision Signs. Section 105.12.430 allows 1 subdivision sign per entrance to the residential development. One primary sign is allowed with a maximum sign area of 32 sq. ft. and all other signs must be 24 sq. ft. or less. No signs are currently identified on the development plans. Staff suggest a condition of approval to clarify that no flexibility is granted for signage and that sign permits are required if and when any signage is proposed.

Tree Preservation. There is an existing stand of primarily pine and spruce trees on the east side of the property. The Developer is not proposing to remove any trees and will preserve what is on site. No mitigation is required. It will be the developer’s responsibility to coordinate with city development staff to appropriately install tree protections at the grading limits.

Landscaping. The City's Landscape Architect has reviewed the prior planting plan for conformance with the City's ordinance. The Applicant is currently showing enough trees to meet the City's Code. The development does not meet requirements for 50' spacing of boulevard (street tree) plantings. Flexibility is requested to this standard.

Buffering and Screening. Given the location of this property adjacent to more intense land use, there is not a requirement for the developer to screen their land use under City Code Section 105.12.480 (f). This is because the more intensive land use (the Bremer services lot) is the one required by code to provide screening.

In consideration of the land use to the south, Bremer, the Applicant has revised the project plans to have the garage doors face south along with the addition of a landscaping buffer at the southern property boundary. These changes are expected to aid in compatibility of the land uses.

Irrigation. An irrigation plan was provided with the submittal. This phase of the Inwood development is not proposed at this time to utilize stormwater reuse. The Lake Elmo City Council has required stormwater reuse in recent developments with approved PUDs.

Shoreland Standards. This project is located in the City's Shoreland Overlay District. A buffer of 300' surrounds an unnamed tributary on the southeast corner of the Inwood Development. Some minor overlap exists between the shoreland overlay and improvements (driveway and parking area) on the proposed apartment lot. The City's staff has clarified with the Minnesota DNR that no shoreland variances are needed because improvements within the master planned Inwood Development are well below the 30% impervious surface maximum allowed in sewered districts.

Off-Street Parking, Single-Family Attached. Since all of the attached townhome units will have more than 2 bedrooms, the City's code will require two spaces per unit. City code will also require an additional 30 parking spaces for visitors (10% of total parking required). The spaces required for parking the units will be found in the two-car garages provided with each unit. The driveways for each unit are deep enough to support an additional two parking spaces per unit. Additional street parking is available throughout the townhome areas. The parking proposed for the townhome units will satisfy the amount required by city code.

Off-Street Parking, Multifamily. City code requires one space per one bedroom unit, and two spaces for all units over 2 bedrooms. An additional one space for every four units is required for visitor parking. The Apartment site provides 123 enclosed stalls and 98 surface parking stalls for a total of 221 stalls. There are 55 units with 2 or more bedrooms, and 68 that have one bedroom. Therefore City's code requires a total of 209 stalls, which includes 31 stalls of visitor parking.

Traffic and Access. The proposed land uses are consistent with the anticipated density range of future development and access spacing requirements of the City. The Applicant has provided an assessment of projected traffic volumes for this use. Collectively, the townhome and apartment AM Peak volume would be 120 trips, with a PM Peak of 148 trips. The study concludes this project is within anticipated volumes. The City Engineer has anticipated needs for turn lanes out of the site and along Island Trail, and secondary access to all units. These requirements are reflected in the City Engineering memo.

Lighting. A photometric plan was provided for the apartment building. The plan shows that the building complies with city code. All lighting fixtures proposed in the development will be required to be downcast and compliant with applicable lighting standards.

PROPOSED PUD DEVIATIONS AND REVIEW.

The following list summarizes the proposed deviations from City Code that would be approved with this PUD as presented. ~~Strikethrough~~ text is used where flexibilities are eliminated:

1. Minimum lot size for multifamily apartment building
2. Front and rear yard setbacks for townhome lots
3. Impervious Surface for attached townhome lots (individually)
4. Building setback from 5th Street North
5. Attached Townhomes not fronting a public street (private streets)
6. Boulevard Tree Spacing a minimum of every 50'
7. ~~Parking between front façade of attached townhomes and the street (lots along southern project boundary)~~
8. Lake Elmo Design Standards Manual
 - Vinyl siding proposed on attached townhomes
 - ~~Access to apartment buildings is not located along a public street frontage~~

Staff's concerns are reduced given the redesign of the attached townhome area. ~~Strikethrough~~ text is used where concerns are eliminated or reduced:

- ~~The plans do not exceed standards for landscaping, and in some cases, landscaping is fit into tight spaces where it may not be viable long term.~~
- ~~Sidewalks and trees are located very close to buildings and/or easements.~~
- ~~The streetscape will lack trees/greenspace and be mostly paved driveway areas.~~
- Parallel Street parking is suggested along streets to create a better streetscape and maximize green space. A condition has been added.
- Sidewalk connectivity is lacking in the southeast and southwest corners of the site east of Island Trail. Staff suggest adding connections at those corners to better link this development with the surrounding sidewalks. A condition has been added.
- The City's design standards prohibit vinyl siding as a primary material and favor other, more durable, materials for facades. Staff recommend that the vinyl siding be replaced with an alternative that is consistent with the City's design standard manual. A condition has been added.
- ~~Except for an existing wooded area, the open space serving the attached townhomes east of Island Trail largely consists of stormwater swales or mandatory setbacks between buildings. Although it could technically meet minimum standards, this development is not providing high quality open spaces or streetscapes.~~
- ~~The proposed rear loaded design has created a combination of a street and alley, which does not conform with the design standards in city code or guidelines. The site could be redesigned to include a combination of public streets with private drives/alleys to create truly double-fronted lots with better streetscaping.~~
- ~~Access from the apartment building to the commercial uses west and north is lacking. The site plan should be modified to make better use of the open space to the north of the building to both connect the building to the sidewalk along 5th street north and possibly provide some amenities to activate the open space.~~

- ~~Screening of the parking lot to the south should be considered. Staff anticipate conflicts with the parking area of Bremer financial.~~

PLANNED UNIT DEVELOPMENT AND PRELIMINARY PLAT FINDINGS.

Given the plan revisions put forward by the Applicant, staff are supportive of a conditional approval of the project. The project is closer to conformance with the City's design standards and flexibilities requested through the PUD are therefore more appropriate.

City Code Article XVIII provides for the requirements and process to approve a Planned Unit Development. To approve a PUD, the project will need to meet minimum standards for a PUD found in Section 105.12.1150 and meet at least one identified objective in section 105.12.1130. Both sections are provided above in this report.

The process for the review of preliminary plats is established in City Code Section 103.00.090. A development should be approved only if it does not meet the criteria for denial as follows:

- That the proposed subdivision is in conflict with the city's comprehensive plan, development code, capital improvements program, or other policy or regulation.
- That the physical characteristics of the site, including, but not limited to, topography, vegetation, susceptibility to erosion and siltation, susceptibility to flooding, water storage, and retention, are such that the site is not suitable for the type or intensity of development or use contemplated.
- That the design of the subdivision or the proposed improvements are likely to cause substantial and irreversible environmental damage.
- That the design of the subdivision or the type of improvements will be detrimental to the health, safety, or general welfare of the public.
- That the design of the subdivision or the type of improvement will conflict with easements on record or with easements established by judgment of a court.
- That the subdivision is premature as determined by the standards of this title.

The Planning Commission is asked to create findings of fact in support of recommended action to the City Council. Considerations for findings include the following:

- Is the project consistent with the City's Comprehensive Plan?
- Is the project consistent with the City's subdivision ordinance and criteria for approval or denial?
- Is the project consistent with the City's minimum requirements and identified objectives for Planned Unit Developments?
- Is the project conforming with the City's zoning ordinance and design standards?
- How might the project impact surrounding land uses?

COMPREHENSIVE PLAN AMENDMENT FINDINGS.

The process for the review of Comprehensive Plan Amendments is established in City Code Section 3.12.280 and Minnesota State Statutes. While specific findings are not established in that ordinance, staff recommend the following findings:

1. That the applicant has submitted a request to the City of Lake Elmo to amend the Comprehensive Plan in accordance with the procedures as established by the Lake Elmo Planning Department and the Lake Elmo Planning Commission; and

2. That the request is to amend the Map 3-3, Future Land Use Map to change the future land use category of the parcel addressed 8699 5th St N, with the PID 3302921130028, from Mixed Use Commercial to High Density Residential; and
3. That the proposed amendment is consistent with the overall goals and objectives of the Lake Elmo Comprehensive Plan.
4. That the proposed amendment is consistent with the preliminary plat for the Inwood Development granted by the Lake Elmo City Council on December 2, 2014.

AGENCY REVIEW:

The Applicant's revised plans were circulated to City review staff for review. At this time, no revised comments have yet been received. The following review comments are still noted:

- Fire Department – Memo dated March 4, 2025 provides comments on minimum standards for roads, drives and parking areas, fire suppression requirements, building safety systems, addressing and street naming, and fire department access.
- City Engineer – Memo dated April 7, 2025 provides comments on how the development would be revised to meet minimum City engineering design standards.
- City Landscape Architect – Memo dated March 6, 2025 provides comments on conformance with City tree preservation and landscaping requirements.
- South Washington Watershed District (SWWD) – Watershed District staff indicated there were no comments.
- MN DNR – The MN DNR did not provide any comments.
- Washington County – County Staff indicated there were no comments on the project or Comprehensive Plan Amendment.
- City of Oakdale – City Staff indicated there were no comments on the project or Comprehensive Plan Amendment.
- City of Woodbury – City Staff indicated there were no comments on the project or Comprehensive Plan Amendment.

Minimum conditions of approval have been included to address agency review comments where applicable.

CONDITIONS OF APPROVAL:

Should the City approve the development, City staff suggest the following as minimum conditions for approval:

1. Prior to the City finding any application for a final plat and final PUD complete, the applicant shall fully address all comments in the following review memos, as may be subsequently revised, to the satisfaction of the City:
 - a. City Engineer's memo dated April 7, 2025;
 - b. City Landscape Architect's memo dated March 6, 2025;
 - c. City Fire Chief's memo dated March 4, 2025; and,
 - d. City Planning comments related to addressing.
2. The Applicant shall make the following plan revisions prior to resubmitting for Final Plat:
 - a. Replace 90-degree parking east of Island Trail with parallel parking, where appropriate, or on street parking where street widths permit.

- b. Create sidewalk connections on the southeast and southwest corners of the site east of Island Trail to the proposed internal pedestrian network.
 - c. Replace proposed vinyl siding on the attached townhome facades with an approved alternative.
3. That prior to the City finding any application for final plat and final PUD complete, the applicant shall demonstrate that the plans reflect compliance with South Washington Watershed (SWWD) review requirements and that the applicant provide the City evidence that all conditions attached to a SWWD permit will be met before the starting any grading activity on the site.
 4. That the applicant shall obtain all necessary permits including but not limited to all applicable City permits (building, grading, sign, etc.), NPDES/SWPPP permits and South Washington Watershed District approval before starting any grading or construction activities.
 5. The final plat shall include all necessary and additional public right-of-way and easements.
 6. The applicant/developer is responsible, at their own expense, for installing all required right of way improvements.
 7. A storm water maintenance and easement agreement in a form acceptable to the City shall be executed and recorded with the final plat.
 8. The applicant/developer shall provide the City a fee in lieu of park land dedication as required by Section 103.00.150, less any applicable credits, to be paid prior to recording of the final plat.
 9. If landscaping is proposed in the City's Right of Way, a landscape and irrigation license agreement in a form acceptable to the City shall be executed and recorded with the final plat.
 10. A sign permit will be required for any signage installed on site, and signage must comply with applicable city standards.
 11. The Applicant shall submit information demonstrating compliance with a Plat Opinion by the City Attorney, to the satisfaction of the City, before execution or recording of the Final Plat.
 12. If necessary, the applicant shall provide the City with a copy of written permission for any off-site grading work and storm sewer discharges to adjacent properties before starting any site work, grading and as part of any final plat or final PUD application.
 13. Before the execution and recording of a final plat for the development, the developer or applicant shall enter into a Developer's Agreement or a Site Work Agreement with the City. Such an Agreement must be approved by the City Attorney and by the City Council. The Agreement shall delineate who is responsible for the design, construction and payment for the required improvements with financial guarantees therefore. The Agreement shall outline any approved phasing plan.
 14. The applicant or developer shall enter into a separate grading agreement with the City before starting any grading activity in advance of final plat/ PUD approval. The City Engineer shall review any grading plan that is submitted in advance of a final plat or final PUD, and said plan shall document the extent of any proposed grading on the site.
 15. That the Applicant must submit the final plat within 180 days after City Council approval of the preliminary plat or approval of the preliminary plat shall be considered void unless a request for a time extension is submitted in writing and approved by the City Council.

PUBLIC COMMENT:

A public hearing was sent to surrounding property owners on April 1st, 2025, and published in the Stillwater Gazette on April 4th, 2025. The Planning Commission held a public hearing on this project at its regular meeting on April 14, 2025.

Community members present provided comments on the project which staff have summarized:

- Concerns for increase in vehicle traffic in areas surrounding the development project
- Concern for traffic speeds along 5th Street North
- Desire for safe crossing locations across 5th Street North
- Comments on stormwater reuse system in existing Inwood additions

OPTIONS:

The Planning Commission may:

- Recommend approval of the request;
- Recommend approval of the request with conditions;
- Recommend denial of the request. Citing findings of fact for denial;
- Table the request for consideration at a future meeting.

RECCOMENDATION:

Given the revisions made to the plans, staff are supportive of approval with conditions. A recommendation to the City Council is requested to allow this project to move forward for review.

The Planning Commission should discuss the requests made by the Developer and provide a recommended action with supporting findings of fact. Considerations for possible findings are provided in the above sections of this report.

Example [Approval/Denial] recommendation motion:

“Move to recommend the City Council [approve/deny] the preliminary plat and PUD for the Inwood Townhome Development requested by M/I Homes with the findings of fact proposed by the Commission.”

Example Comprehensive Plan Amendment approval motion:

“Move to recommend City Council approval of the Comprehensive Plan Amendment request by M/I Homes for the subject property with the findings proposed by City Staff.”

ATTACHMENTS:

1. Submittal Package (revised plans received April 29, 2025)
2. City Engineer Memo April 7, 2025
3. City Fire Dept Memo March 4, 2025
4. City Landscape Architect Memo March 6, 2025

INWOOD TOWNHOMES
COMPREHENSIVE PLAN
AMENDMENT,
PRELIMINARY PLAT,
DEVELOPMENT STAGE
PLANNED UNIT
DEVELOPMENT REQUESTS

INTRODUCTION

M/I Homes of Minneapolis/St. Paul, LLC (M/I Homes) is submitting application for a Comprehensive Plan Amendment, Preliminary Plat, Development Stage Planned Unit Development (PUD) Plan for a development containing 148 townhomes and a 123-unit apartment building. M/I Homes will be constructing the townhomes and partnering with Rachel Development, Inc. for the apartment building. Townhomes and an apartment building were shown on the approved Inwood preliminary planned unit development plans, and the Inwood Preliminary Plat and Planned Unit Development (PUD) Resolution 2014-94 approved the PUD which includes 275 single family homes and 264 multifamily units. An Environmental Assessment Worksheet (EAW) and Traffic Study were completed and approved with the approval of this preliminary plat. M/I Homes (dba Hans Hagen Homes) developed Inwood, including the extension of sewer and water to the multi-family property, as well as the construction of 5th Street and Island Trail. M/I Homes also constructed ponding and an infiltration basin to serve the subject property.

We brought forth the application to the April 14, 2025, Planning Commission, and it was communicated that some changes would need to be made to the plan in order for the Planning Commission to recommend approval. Since then, we've made several changes to the site plan, which are outlined below.

SITE PLAN

We are proposing private streets throughout the community with one street connection to 5th Street North and one connection to Island Trail. All townhomes will be our City Collection Townhomes, which are our new rear-loaded three-story townhomes. The proposed apartment is located on the northwest corner of the site, separated from all but one townhome building by Island Trail. M/I Homes and Rachel Development have experience working together on multi-family projects similar to this one. One such project is Marsh View which is located at 137th Avenue North in Rogers, Minnesota. An aerial photo of this development is included in this narrative.

Changes since Sketch Plan. M/I Homes previously submitted a sketch plan review application to City staff and have subsequently made amendments to the proposed plan. We have revised the street layout to eliminate dead-end road and have provided an additional access to the apartment building. We also increased the front yard setbacks from the private street from 25 feet to 30 feet. Additionally, we have combined two access points off Island Trail to one and have rotated the townhome buildings to the west of Island Trail to face the public street and have combined those buildings, creating parking in the rear.

Changes since Planning Commission. As previously mentioned, the Preliminary Plat, PUD and Comprehensive Plan applications were considered at the April 14, 2025 Planning Commission. Since this meeting, the following changes have been made:

- We aligned the access points from the east and west of Island Trail.
- We created a 28-foot-wide street through the center of the development. This will create an aesthetically pleasing streetscape with front entrances of the townhomes facing the entrance street. Alleys will circulate the development to accommodate the rear-loaded garages.
- Stub streets were removed on the east side of the plan.
- Entrance landscaping was added as well as a landscaping feature in the middle of the site and landscape buffering from the bank's parking lot to the south.
- Increased open space from 8.63 acres to 9.21 acres.

Staff Report/Planning Commission Feedback Responses. The following provides response to comments received from both the Staff Report included in the April 14, 2025 Planning Commission meeting packet as well as feedback received from said meeting:

- Traffic Concerns.
 - Included in this resubmittal is a Traffic Review of the Inwood Development executed by SSTs, LLC. The conclusion of this analysis is that the traffic volume shows less intense traffic volumes than those previously studied in the 2014 Traffic Study for the Inwood development. No adverse traffic impacts are anticipated on surrounding roadways and intersections. Traffic from the proposed development is anticipated to be primarily to/from the south (I-94 and retail) and west (I-694 and retail) with very low demand to use Island Trail through the neighborhood to the north. Additionally, Island Trail is designated as a Minor Collector per the City's 2040 Comprehensive Plan and was designed and built accordingly.
- Turn Lanes.
 - The engineering memorandum indicates that turn lanes should be added along Island Trail for both the east and west access locations, each having more than 35 units. The Inwood Preliminary Plat contemplated a very similar site design to what has been submitted with this application with a similar number of units, and Island Trail was designed and built to accommodate this plan. No turn lanes were required at that time. There is not sufficient room to accommodate turn lanes, and this would result in ripping up previously constructed sidewalks. Additionally, the adopted Lake Elmo Engineering Design and Construction Standards Manual do not require a turn lane for developments with 35 units or more.
- Design Comments.
 - The staff report states that vinyl is prohibited, but the Lake Elmo Design Guidelines and Standards Manual state that materials which are prohibited as the **primary** façade material include vinyl. This indicates that vinyl is allowed but must be secondary to the primary building materials outlined in said manual. We would agree to use Hardie siding for the board and batten on the townhomes but would not be amenable to using this material for the entire façade for a number of reasons. First, Hardie requires more maintenance than vinyl including painting and caulking. This would increase Homeowners Association dues and maintenance. Vinyl breathes well, has a higher energy rating, and there is almost no possibility of water intrusion with no requirement to paint or caulk. Second, Hardie would add a significant cost increase, as much as \$7,000 a townhome, where buyers see little to no value in this change in material. We have provided a breakdown of the percentage of vinyl the townhomes would incorporate if we used Hardie for the board and batten, and the total percentage would be 42.6% vinyl, with the front elevation consisting of only 30.7% vinyl. We have also included a rendering of the apartment building from the 5th Street side in response to a comment about the apartment design.
- Landscaping.
 - We added additional entrance landscaping as well as a landscape feature in the middle of the site and buffering from the bank parking lot. While there is not a tree proposed exactly every 50 feet along the private streets, the ordinance reads that a minimum of one tree per lot or one tree for every 50 feet of street frontage, lake shore or stream frontage, or fraction thereof (whichever is greater) shall be planted and specifies that such trees may be clustered or placed at regular intervals to best complement existing landscape design patterns in the area. We also meet the requirement of providing a minimum of five trees per acre of disturbance in addition to the aforementioned requirement.

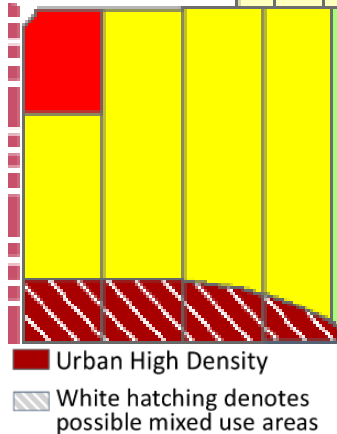
COMPREHENSIVE PLAN AMENDMENT

With the 2040 Comprehensive Update (well after the entire Inwood site had an approved preliminary plat), a portion of the site to the west of Island Trail was re-guided from High Density Residential to Mixed Use Commercial, and the east side of Island Trail remained High Density Residential. This Land Use Guide Plan change was inconsistent with the approved preliminary plat for Inwood. At the time of the Inwood PUD approval the City of Lake Elmo allowed the blending of densities across parcels. Since, then, however,

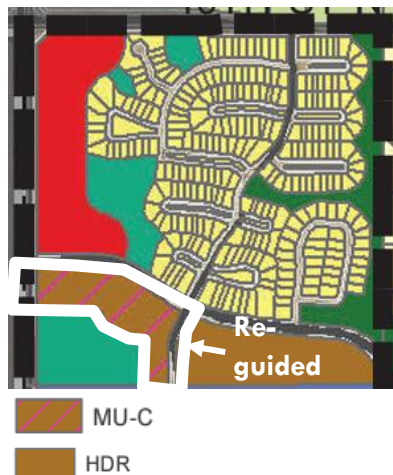
Inwood Townhomes

standards have changed, and a Comprehensive Plan amendment is necessary to allow the proposed neighborhood as originally approved by the City. The parcel to the west of Island Trail has a proposed density of 21.13 units per acre, which is above the allowed maximum density of the Mixed-Use Commercial land use guidance of 15 units per acre. The east side of Island Trail proposes 141 townhomes, and with a site area of 13.1 acres, has a net density of 10.76 units per acre, which is well below the allowed density of 15 units per acre. As such, we are requesting a Comprehensive Plan Amendment to guide the parcel on the west side of Island Trail back to High Density Residential and blend the density across the entire site as originally approved in the Inwood PUD. The City's land use plan has long designed the subject property as High Density, which occurred before any of the existing homes were built in the single-family portion of the Inwood PUD

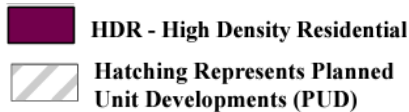
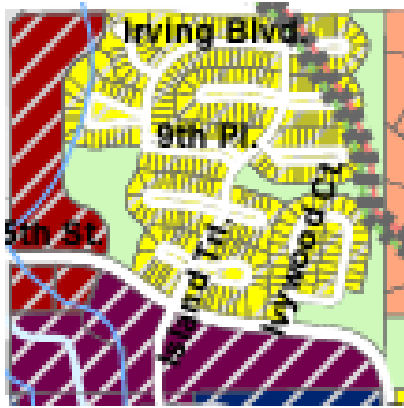
Existing Land Use at time of Adoption of 2040 Comprehensive Plan



Future Land Use at time of Adoption of 2040 Comprehensive Plan



The proposed development is zoned High Density Residential (HDR)



The proposed development includes the following parcels:

PID# 3302921140029 (12.7 acres)

PID# 3302921130028 (5.7 acres)

PID# 3302921420018 (0.4 acres)

PID# 3302921420017 (0.5 acres)

Total site acreage: 19.3 acres

With 271 units, this results in a density of 14.06 units per acre, which is within the allowable density.

GENERAL DEVELOPMENT STAGE PUD/PRELIMINARY PLAT

The proposed development is part of the approved Inwood PUD. The development meets the City's minimum PUD requirements in that it is larger than five acres, at least 20% of the site is open space which will be protected by a drainage and utility easement, and streets are designed to maximize connectivity in each cardinal direction.

We believe the following PUD objectives are met with this site plan:

- 1) Innovation in land development techniques that may be more suitable for a given parcel than conventional approaches.

Response: The original Inwood PUD included the use of private streets. Private streets are necessary to implement the original Inwood PUD approval. We believe the site design provides sufficient circulation and an eye-pleasing streetscape.

- 2) Promotion of integrated land uses, allowing for a mixture of residential, commercial and public features.

Response: The Inwood PUD provides a mixture of commercial and a wide variety of housing options including single-family homes, villa homes (which are targeted towards an aging population), townhomes, and an apartment.

- 3) Establishment of appropriate transitions between differing land uses.

Response: 5th Street North buffers the apartment site and townhomes. The townhomes are mostly buffered from the apartment by Island Trail. This is consistent with the City's Comprehensive Plan.

- 4) Provision of more adequate, usable, and suitably located open space, recreational amenities, natural resource protection and other public facilities than would otherwise be provided under conventional land development techniques.

Response: The Inwood PUD includes a public park and a linear trail corridor than extends from 5th Street to 10th Street. Other trails and opens space areas are incorporated into the overall Inwood PUD. Private open space areas are also incorporated into the townhome portion of the Inwood PUD

- 5) Accommodation of housing of all types with convenient access to employment opportunities and/or commercial facilities; and emphasized to create additional opportunities for lifecycle housing to all income and age groups.

Response: See response to 2.

- 6) Preservation and enhancement of important environmental features through careful and sensitive placement of buildings and facilities.

Response: An Environmental Assessment Worksheet, which included this site and contemplated the multifamily and townhome portion of the site, was submitted and accepted by the City as part of the approved original PUD. There are no significant environmental features on this site.

- 7) Coordination of architectural styles and building forms to achieve greater compatibility within the development and surrounding land uses.

Response: As previously mentioned, our rear-loaded City Collection townhomes will front all public streets, providing visual interest from the single-family homes across from 5th Street North. The apartment will include landscaping to provide a buffer from 5th Street North, and the parking lot is screened from 5th Street North by the apartment building itself.

- 8) Creation of more efficient provision of public utilities and services, lessened demand on transportation, and/or the promotion of energy resource conservation.

Response: Private streets will be owned and maintained by the HOA, which will reduce demand on City infrastructure and snow removal services.

- 9) Establishing measures to protect and preserve groundwater storage.

Response: Ponding and an infiltration basin were constructed with the development of the single-family home portion of the Inwood development. Stormwater reuse for irrigation is being utilized in the single-family portion of the site.

- 10) Allowing the development to operate in concert with a redevelopment plan in certain areas of the city and to ensure the redevelopment goals and objectives will be achieved.

Response: This application will allow the completion of the Inwood PUD as originally approved.

The following table demonstrates requested zoning flexibility.

Standard	HDR Zoning Standard (Single Family Attached)	Proposed
Minimum Lot Size	1,750 square feet (acknowledges that common lot areas will add a higher total than 1,750 square feet)	19.3 acres=840,708 square feet This divided by the number of units totals 3,102.24 square feet per unit
Minimum Lot Width	20 feet	21.33 feet
Minimum Front Setback	20 feet	20 feet from Island Trail and 5 th Street North with stoop (15 feet) abutting 5 th Street North, Island Trail and Street A. 30 feet to back of curb on private streets.
Minimum Side Setback	10 feet	20 feet between buildings ¹
Minimum Corner Side Setback	15 feet	20 feet ¹
Rear Setback	20 feet	30 feet to boundary ¹
Street Frontage	15 feet of street frontage	This is a requested flexibility because the majority of townhome units are on private streets.

Inwood Townhomes

Impervious Surface	75%	60% (the existing plan shows 52%)
Maximum Number of Units within a Single Building	8	8
Minimum Street Frontage	15 feet	We are proposing private streets, which are a conditional use per Section 105.12.740 (g)(2)(a).
Common or Private Open Space	300 square feet per unit for single family attached (300 X 148 = 44,400 square feet or 1.03 acres) and 200 square feet per unit for multifamily (200 X 123 = 24,600 square feet or 0.56 acres) or 1.59 acres total	8.63 acres of open space, which excludes the public park and open space areas already dedicated to the City as part of the Inwood PUD.
Private Streets	Private Streets are an allowed conditional use in the HDR district.	We are proposing 24-foot-wide private streets with 30-foot front yard setbacks.
Driveway setbacks	No driveway or curb cut shall be less than 50 feet from any right-of-way line of any street intersections. A driveway must be at least five feet from any side lot line.	We are requesting flexibility from these standards, as these are private streets that will only be used by this development's residents and their visitors.
Garage Design Guidelines	Attached garages are encouraged to be side or rear loaded	All townhomes will be our City Collection townhomes, which have rear-loaded garages.
Parking	Multifamily: One space per one bedroom unit and two spaces per two-bedroom unit or larger (66 studio and one-bedroom units and 57 two-bedroom units or larger=180 required) and one space per four units for visitor parking (123X0.25=31, 211 total) Single-family attached: Ten percent of parking spaces for visitor parking (148X0.1=15 visitor spaces	Multi Family: 99 above ground and 123 underground parking spaces (222 total) Single-family attached: Two garage and two driveway parking spaces plus staff has stated that street parking will be provided on both sides of Street A, which will accommodate approximately 50 parking spaces as well as 18 off-street parking spaces

1. City standards do not contemplate individually platted units on private streets with surrounding commons area, which leads to noncompliance with strict interpretation of the code.

DESIGN STANDARDS

The following demonstrates how the proposed plan meets the City's design standards:

- Buildings are located as close to and easily accessible by the street as possible while still meeting setback requirement with allowed flexibility.
- Common open spaces are located at the interior and rear of the site.
- Sidewalks are provided parallel to the street and provide interconnectivity within the development.

- Parking is screened from public streets.
- Townhomes that front a public street are rear loaded.
- Buildings provide multiple roof lines and visual interest.

PHASING

We are proposing to begin construction on the first phase in spring of 2025, the second phase in the spring of 2026, and the third phase in spring of 2027.

LANDSCAPING/TREE PRESERVATION

The landscaping plan is included with the submittal. M/I Homes has also previously completed all of the landscaping and berming along 5th Street as part of the Inwood PUD. This landscaping includes the boulevards adjacent to the subject property. There are only six boulevard trees proposed to be removed along 5th Street North to facilitate construction, which will be replaced in similar locations. We have provided sufficient boulevard trees, meeting the 50-foot spacing requirement for both the townhome and apartment site and have provided the sufficient number of trees required per area of disturbance (this includes the trees that will remain on site). Parking lot landscaping meets landscaping area and shade tree requirements.

TOWNHOME INFORMATION

The townhomes in the proposed development will consist of M/I's Carriage and City Collection townhomes. The different townhome designs provide for added character and visual interest within the neighborhood as well as varying prices. The two-story Carriage Collection is planned to be positioned to the south and east of the site, adjacent to the existing single-family homes, with the three-story City Collection along public street frontages. A variety of exterior colors will be used throughout the neighborhood to provide additional visual interest while avoiding a monotony of colors.

The Carriage Collection offers a range of design features and options and ranges in size from 1,667 square feet to 1,772 square feet with 3 bedrooms and 2.5 bathrooms. The City Collection plans will provide up to 4 bedrooms and 3.5 bathrooms ranging from 1,898 square feet to 2,080 square feet.

RESTRICTIVE COVENANTS

Inwood Townhomes will have a Homeowners Association (HOA) that will provide for the maintenance of the overall common elements in the community, including landscaping and irrigation of the common areas, snow removal from sidewalks and private streets, and exterior maintenance. In the unlikely event that the site runs out of snow storage, the HOA service will haul out snow from the site. The HOA will also provide for restrictions on outdoor storage, parking (no boats, campers, or trailers) in order to keep the community looking orderly and well maintained.

FEES

With the single-family portion of the development, the Developers Agreements for each addition detailed the following:

- **Parkland.** The Development Contract for Inwood dated June 9, 2015 detailed that park dedication fees were satisfied for the single-family portion of the Inwood Development with the dedication of Outlot L (net 10.73 acres) and that the 1.16 acres of excess park dedication credit was to be applied for the future multi-family residential area as conceptually shown on the Inwood PUD concept plan.
- **Sewer and Water Availability Charges.** M/I Homes was responsible for paying \$3,000 for both Sewer Availability Charge (SAC) and Water Availability Charge (WAC) per residential equivalency

unit (REU) prior to recording the final plat. A \$1,000 Sewer Connection Charge, the Metropolitan Council's Sewer Availability Charge (\$2,435 at the time) and a Water Connection Charge of \$1,000 per REU was due at the time of building permit.

- **County Right-of-Way (ROW) Improvements.** M/I Homes was responsible for improvements in the Washington County ROW along Inwood Avenue (CSAH 13) including construction of a new median crossing, turn lanes, and other improvements as were required by the County as well as its proportionate share of the future traffic signal at the intersection of Inwood and 5th Street. There were to be no further obligations to the City or County for the construction or payment of a future traffic signal for any future phases of the Inwood PUD.

ELEVATION EXAMPLES

City Collection*



*Colors will vary, and slight design modifications are being made.

Apartment Building

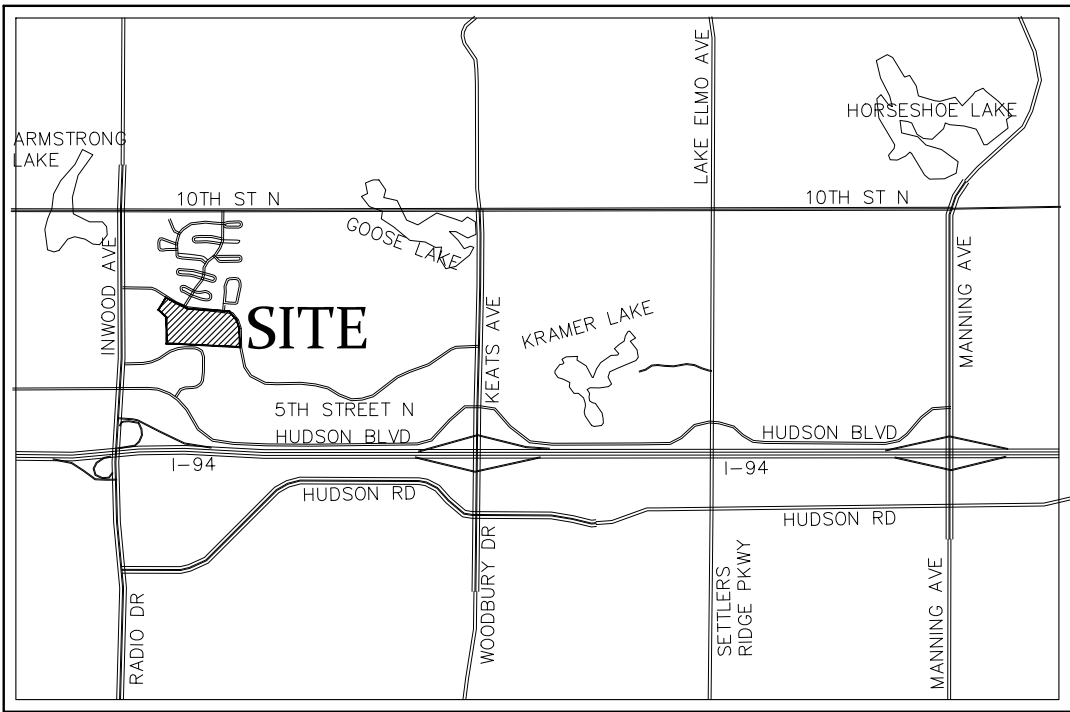


Marsh View Townhome/ Development



INWOOD TOWNHOMES

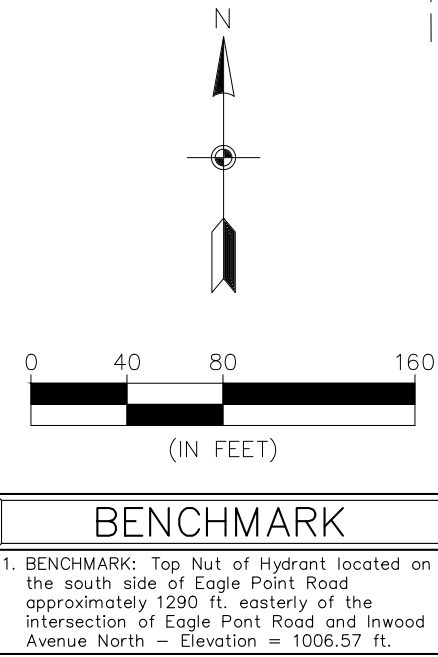
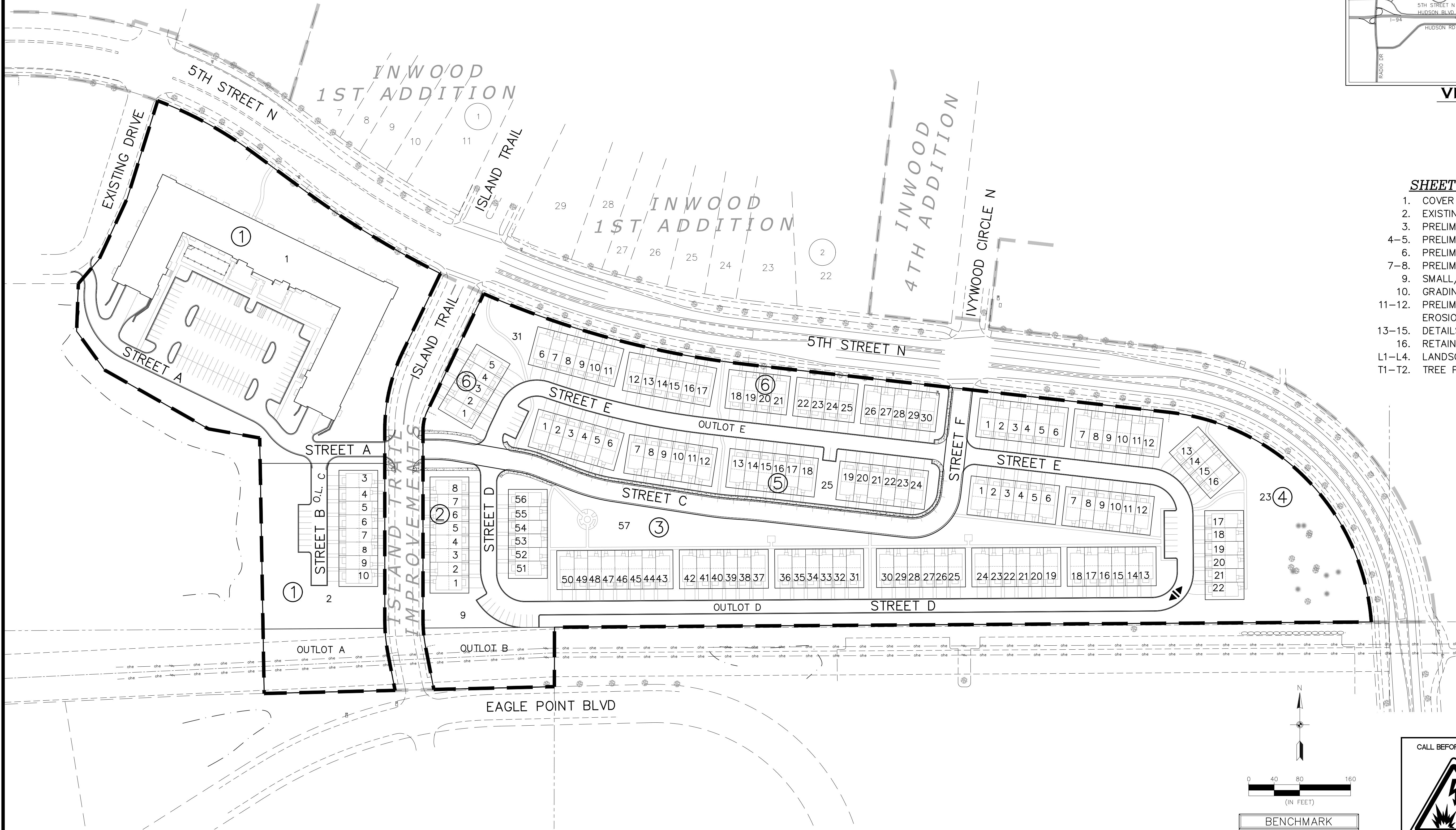
LAKE ELMO, MINNESOTA



VICINITY MAP
NOT TO SCALE

SHEET INDEX

1. COVER
2. EXISTING CONDITIONS
3. PRELIMINARY PLAT INDEX
- 4-5. PRELIMINARY PLAT
6. PRELIMINARY SITE & UTILITY PLAN INDEX
- 7-8. PRELIMINARY SITE & UTILITY PLAN
9. SMALL/DRY UTILITY CORRIDOR LAYOUT
10. GRADING INDEX
- 11-12. PRELIMINARY GRADING & EROSION CONTROL PLAN
- 13-15. DETAILS
16. RETAINING WALL PROFILES
- L1-L4. LANDSCAPE PLANS
- T1-T2. TREE PRESERVATION PLANS



CALL BEFORE YOU DIG



The subsurface utility information shown on this plan is utility Quality Level D. This quality level was determined according to the guidelines of CI/ASCE 38-02, entitled "Standard Guideline for the Collection and Depiction of Existing Subsurface Utility Data."



3890 PHEASANT RIDGE DR NE
SUITE 100
BLAINE, MN 55449
TEL 763.489.7900
FAX 763.489.7959
CARLSON-ENGINEERING.COM

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota

Print Name: Brian J. Krystofski, P.E.
Signature: *Brian J. Krystofski*
Date: 12/20/24 License #: 25063

Drawn: KRO
Designed: BJK
Date: 12/20/24

Revisions:
1. 1/23/25 per City Comments.
2. 2/11/25 per City Comments.
3. 4/28/25 per City Comments.

M/I HOMES OF MINNEAPOLIS, LLC
5354 Parkdale Drive, #100
St. Louis Park, MN 55416

INWOOD TOWNHOMES
Lake Elmo, Minnesota

COVER

1
of
16

LEGEND

- - Denotes Washington County Section Monument, as noted

⊙

- Denotes Set PK Nail

●

- Denotes Found Iron Monument, as noted

○

- Denotes 5/8 inch by 14 inch rebar, marked with RLS 40361

⊙

- Denotes Light Pole

⊙

- Denotes Miscellaneous Sign

○

- Denotes Sanitary Manhole

○

- Denotes Storm Manhole

○

- Denotes Catch Basin

○

- Denotes Flared End Section

○

- Denotes Fire Hydrant

○

- Denotes Gate Valve

○

- Denotes Cleanout

○

- Denotes Utility Pole
- ET

- Denotes Electric Transformer

EB

- Denotes Electric Box

EH

- Denotes Hand Hole

ut

- Denotes Underground Telephone

ue

- Denotes Underground Electric

ug

- Denotes Underground Gas

ufo

- Denotes Underground Fiber Optic

ohs

- Denotes Overhead Utility Line(s)

- Denotes Watermain

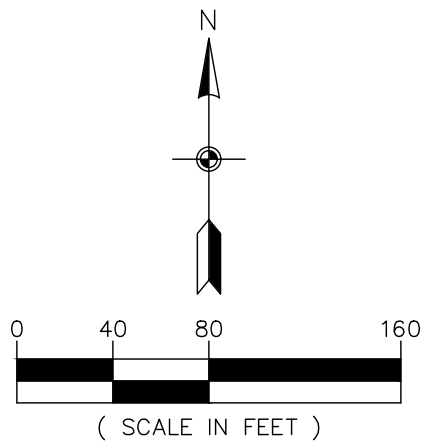
- Denotes Sanitary Sewer

- Denotes Storm Sewer

- Denotes Concrete Surface

- Denotes Bituminous Surface

- Denotes Existing Contour



BENCHMARK
1. BENCHMARK: Top Nut of Hydrant located on the south side of Eagle Point Road approximately 1280 ft. easterly of the intersection of Eagle Point Road and Inwood Avenue North - Elevation = 1006.57 ft.

PROPERTY DESCRIPTION: (Per Schedule A of Title Commitment No. 696840, with a commitment date of May 23, 2024 at 7:00 A.M., prepared by Land Title, Inc. as issuing agent for Stewart Title Guaranty Company)

Abstract Property

Outlot A, Inwood, Washington County, Minnesota.

AND

Outlot B, Inwood Sixth Addition, Washington County, Minnesota.

Torrens Certificate No. 81980

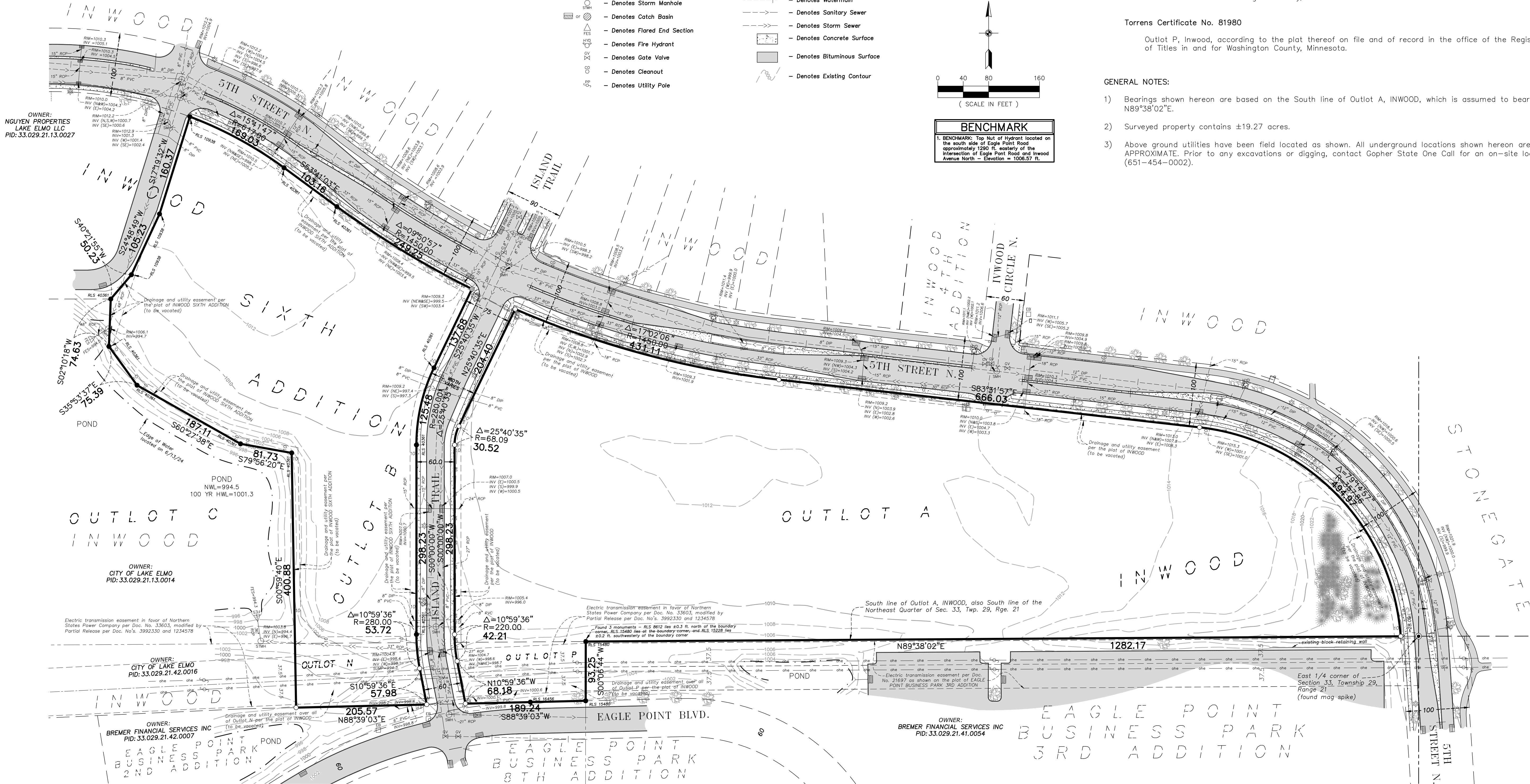
Outlot N, Inwood, according to the plat thereof on file and of record in the office of the Registrar of Titles in and for Washington County, Minnesota.

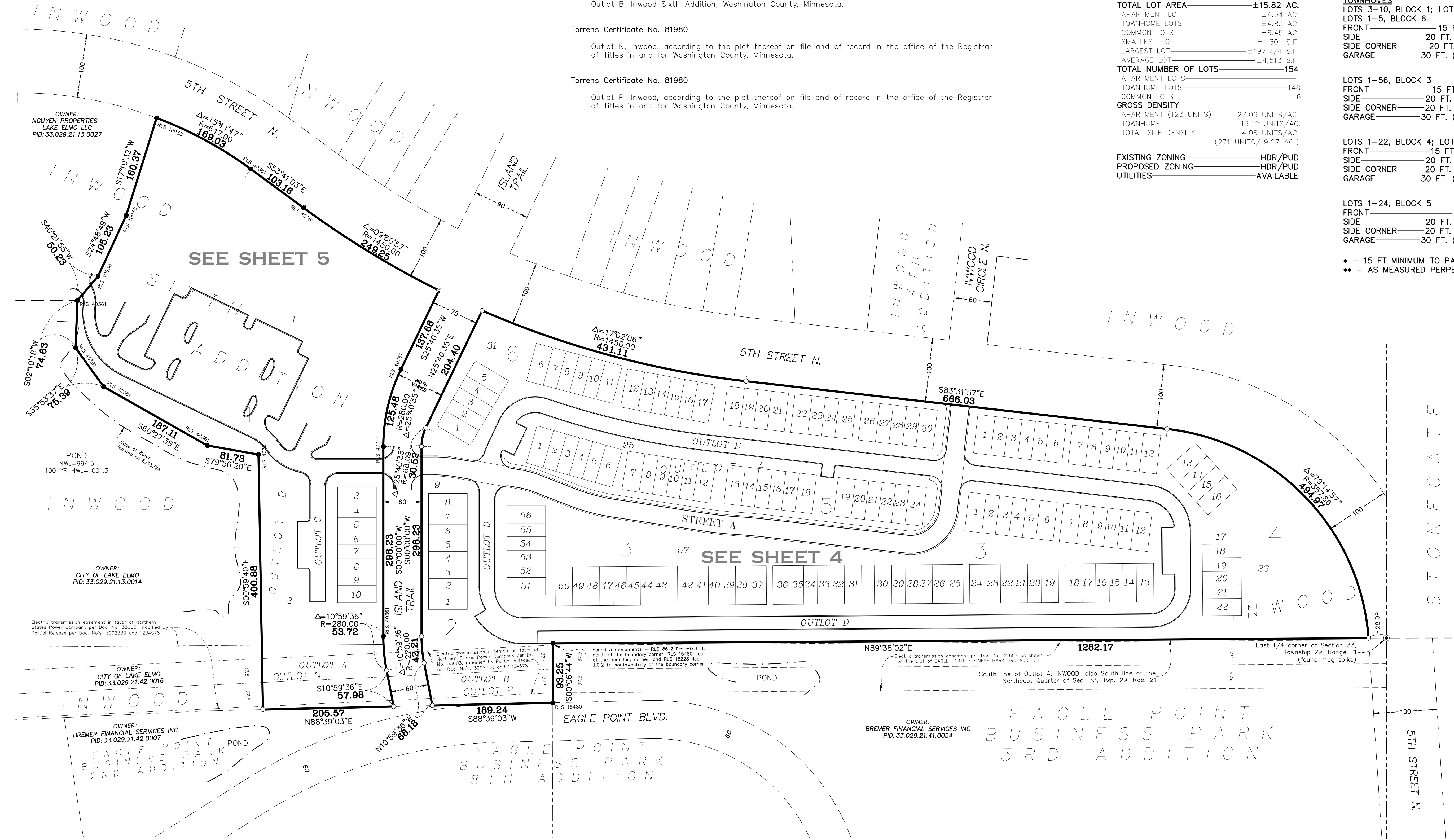
Torrens Certificate No. 81980

Outlot P, Inwood, according to the plat thereof on file and of record in the office of the Registrar of Titles in and for Washington County, Minnesota.

GENERAL NOTES:

- Bearings shown hereon are based on the South line of Outlot A, INWOOD, which is assumed to bear N89°38'02"E.
- Surveyed property contains ±19.27 acres.
- Above ground utilities have been field located as shown. All underground locations shown hereon are APPROXIMATE. Prior to any excavations or digging, contact Gopher State One Call for an on-site location (651-454-0002).





PROPERTY DESCRIPTION: (Per Schedule A of Title Commitment No. 696840, with a commitment date of May 23, 2024 at 7:00 A.M., prepared by Land Title, Inc. as Issuing agent for Stewart Title Guaranty Company)

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Torrens Certificate No. 81980

Outlot P, Inwood, according to the plat thereof on file and of record in the office of the Registrar of Titles in and for Washington County, Minnesota.

SITE DATA

TOTAL SITE AREA	±19.27 AC.
TOTAL ROW AREA	±0.76 AC.
TOTAL OUTLOT AREA	±2.69 AC.
OUTLOT A	±0.45 AC.
OUTLOT B	±0.43 AC.
OUTLOT C	±0.15 AC.
OUTLOT D	±1.06 AC.
OUTLOT E	±0.60 AC.
TOTAL LOT AREA	±15.82 AC.
APARTMENT LOT	±4.54 AC.
TOWNHOME LOTS	±4.83 AC.
COMMON LOTS	±6.45 AC.
SMALLEST LOT	±1,301 S.F.
LARGEST LOT	±197,774 S.F.
AVERAGE LOT	±4,513 S.F.
TOTAL NUMBER OF LOTS	154
APARTMENT LOTS	1
TOWNHOME LOTS	148
COMMON LOTS	6
GROSS DENSITY	
APARTMENT (123 UNITS)	27.09 UNITS/AC.
TOWNHOME	13.12 UNITS/AC.
TOTAL SITE DENSITY	14.06 UNITS/AC. (271 UNITS/19.27 AC.)
EXISTING ZONING	HDR/PUD
PROPOSED ZONING	HDR/PUD
UTILITIES	AVAILABLE

SETBACK DATA

APARTMENT	
LOT 1, BLOCK 1	
FRONT	50 FT. (TO 5TH STREET N.)
	50 FT. (TO ISLAND TRAIL)
SIDE	30 FT (TO LOT LINE)
REAR	30 FT (TO LOT LINE)
TOWNHOMES	
LOTS 3-10, BLOCK 1; LOTS 1-8, BLOCK 2	
LOTS 1-5, BLOCK 6	
FRONT	15 FT. (TO ISLAND TRAIL)
SIDE	20 FT. (BETWEEN BUILDINGS)
SIDE CORNER	20 FT. (TO BACK OF CURB)
GARAGE	30 FT. (TO BACK OF CURB**)

LOTS 1-56, BLOCK 3	
FRONT	15 FT (TO BACK OF CURB)
SIDE	20 FT. (BETWEEN BUILDINGS)
SIDE CORNER	20 FT. (TO BACK OF CURB*)
GARAGE	30 FT. (TO BACK OF CURB**)

LOTS 1-22, BLOCK 4; LOTS 6-30, BLOCK 6	
FRONT	15 FT. (TO 5TH STREET N.)
SIDE	20 FT. (BETWEEN BUILDINGS)
SIDE CORNER	20 FT. (TO BACK OF CURB*)
GARAGE	30 FT. (TO BACK OF CURB**)

LOTS 1-24, BLOCK 5	
FRONT	15 FT. (TO STREET A)
SIDE	20 FT. (BETWEEN BUILDINGS)
SIDE CORNER	20 FT. (TO BACK OF CURB*)
GARAGE	30 FT. (TO BACK OF CURB**)

* - 15 FT MINIMUM TO PARKING BAY
** - AS MEASURED PERPENDICULAR TO FACE OF GARAGE

LEGEND

- - Denotes Washington County Section Monument, as noted
- ⊙ - Denotes Set PK Nail
- - Denotes Found Iron Monument, as noted
- - Denotes 5/8 inch by 14 inch rebar, marked with RLS 40361



3890 PHEASANT RIDGE DR NE
SUITE 100
BLAINE, MN 55449
TEL 763.489.7900
FAX 763.489.7959
CARLSON-ENGINEERING.COM

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota

Print Name: Thomas R. Balluff, L.S.
Signature: *Thomas R. Balluff*
Date: 12/20/24 License #: 40361

DRAWN BY: NPC
ISSUE DATE: 12/20/24
FILE NO: 1870

Revisions:
1. 1/23/25 per City Comments
2. 2/11/25 per City Comments
3. 4/28/25 per City Comments

M/I HOMES OF MINNEAPOLIS/ST. PAUL, LLC

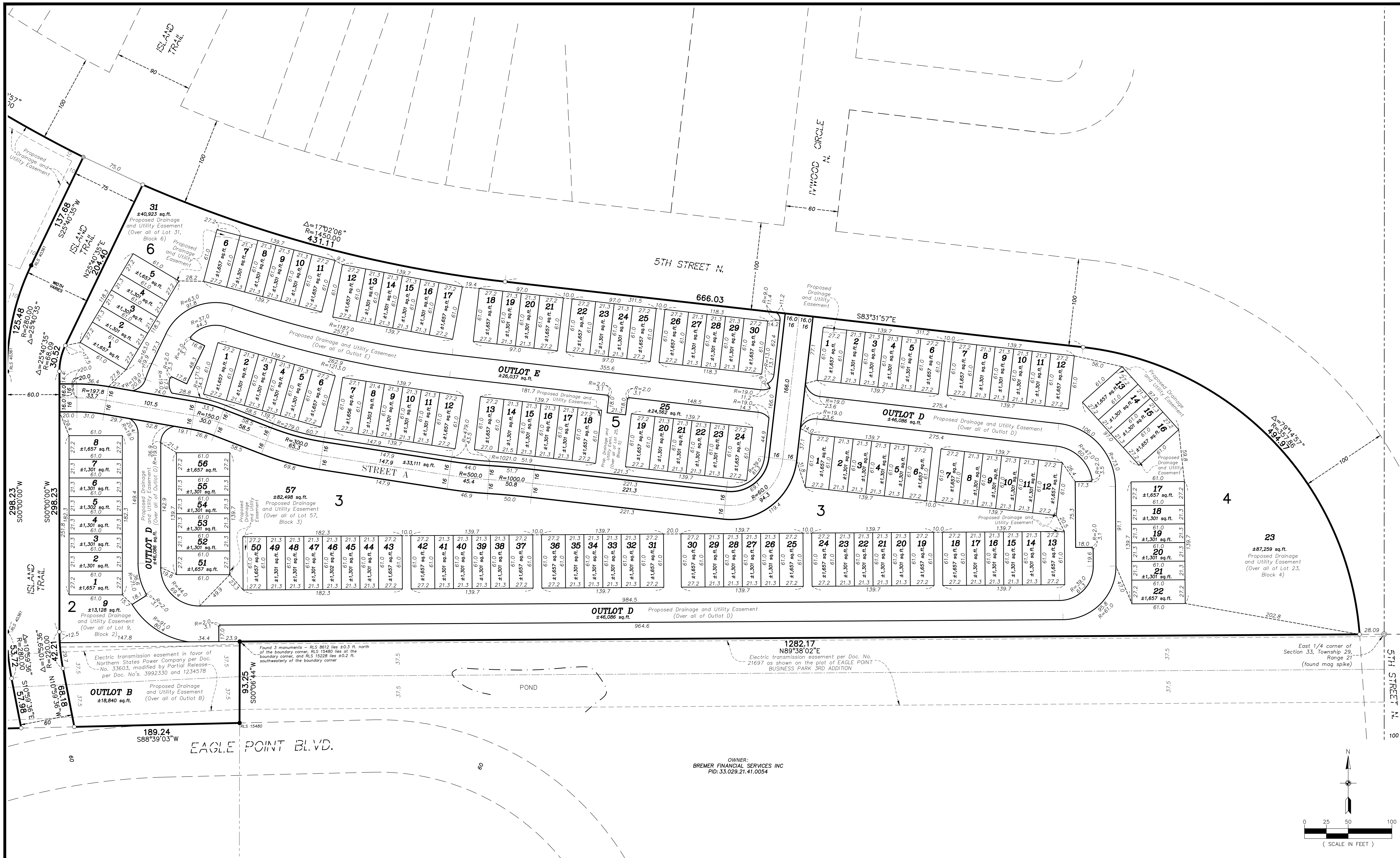
5354 Parkdale Drive, #100
St. Louis Park, MN 55416

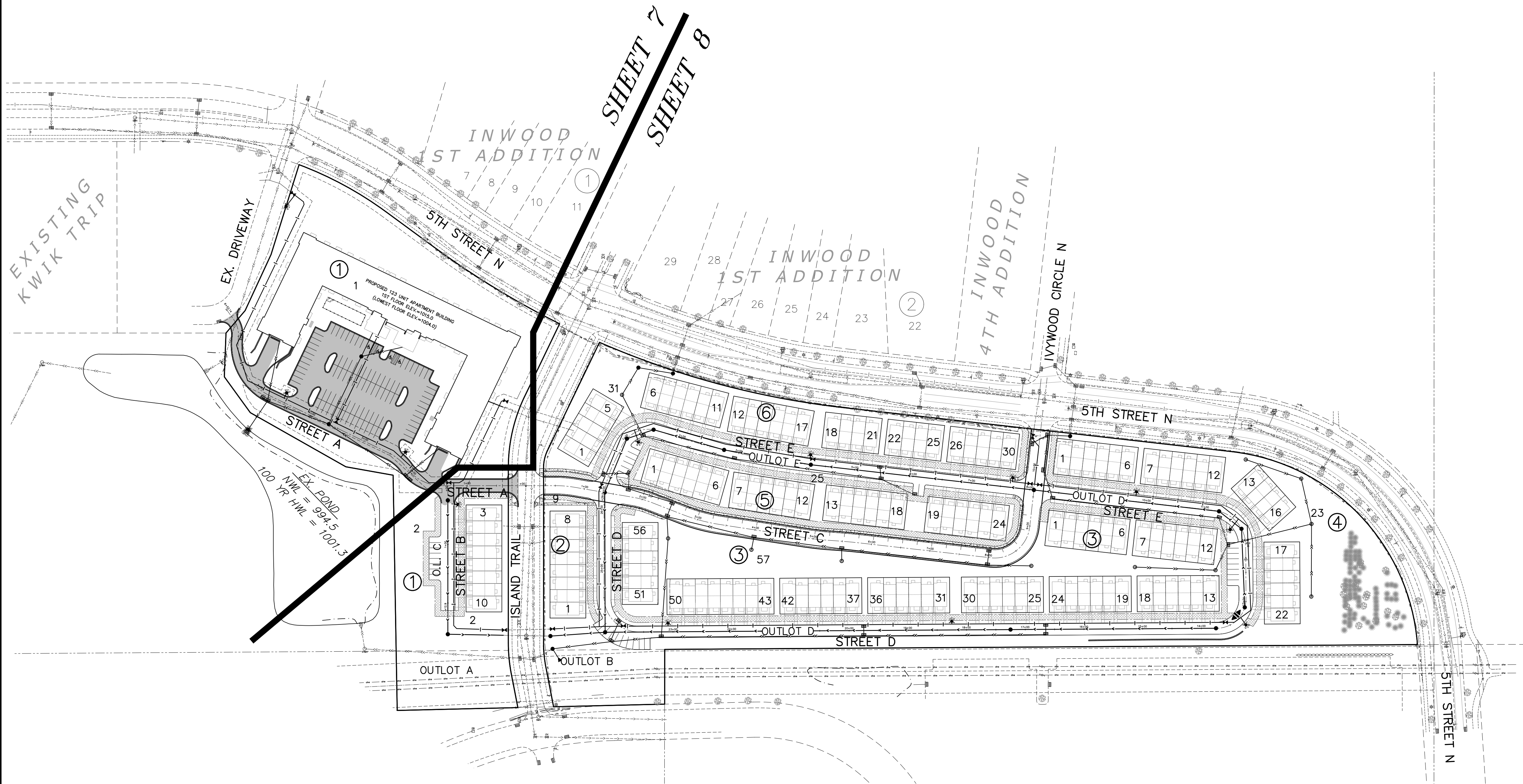
INWOOD TOWNHOMES

Lake Elmo, Minnesota

PRELIMINARY PLAT INDEX

3 of 16





SITE PLAN LEGEND

	EXISTING	PROPOSED
LIGHT POLE		
SANITARY MANHOLE		
MISCELLANEOUS SIGN		
EXISTING SPOT ELEVATION		
EXISTING TREE		
TELEVISION BOX		
FIRE HYDRANT		
GATE VALVE		
CLEANOUT		
ELECTRIC BOX		
GAS METER		
CATCH BASIN		
RAIN GUARDIAN		
ELECTRIC METER		
TELEPHONE BOX		
UTILITY POLE		
FLARED END SECTION		
STORM MANHOLE		
SERVICE		
SOIL BORING/TEST HOLE		
RETAINING WALL		
WATERMAIN		
SANITARY SEWER		
STORM SEWER		
PROPERTY LINE		
SETBACK LINE		
WETLAND		
UNDERGROUND TELEPHONE		
UNDERGROUND ELECTRIC		
UNDERGROUND GAS		
UNDERGROUND FIBEROPTIC		
FENCE		
OVERHEAD ELECTRIC		
CONCRETE SURFACE		
BITUMINOUS SURFACE		
SMALL/DRY UTILITY CORRIDOR		

APARTMENT PARKING SUMMARY

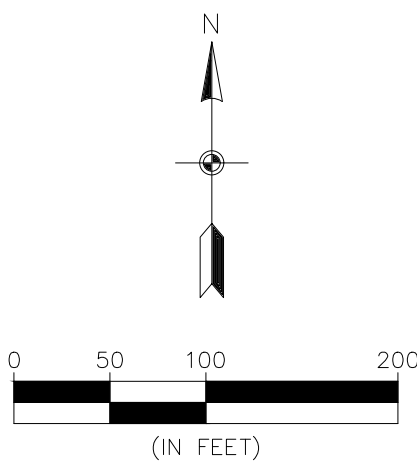
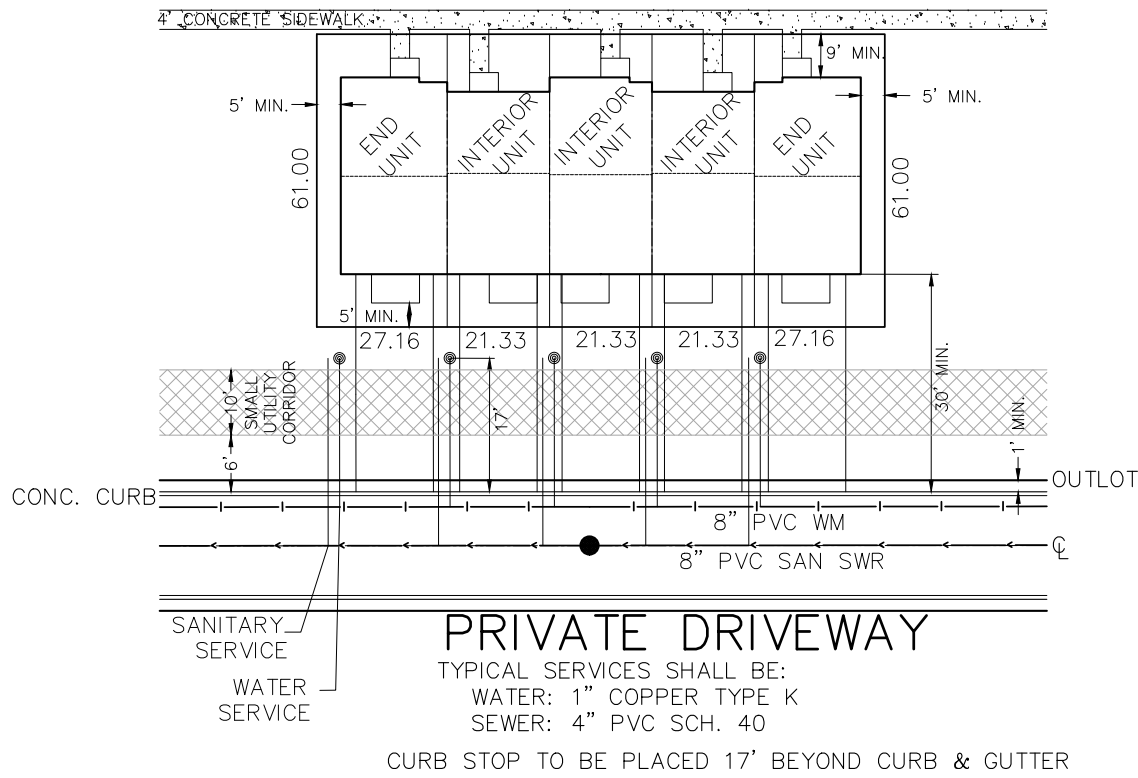
PARKING PROVIDED IN APARTMENT PARKING LOT		
STANDARD STALLS	=	94 STALLS
HANDICAP STALLS	=	4 STALLS
TOTAL	=	98 STALLS
PARKING PROVIDED IN 123 UNIT APARTMENT BUILDING		
STANDARD STALLS	=	123 STALLS
TOTAL	=	123 STALLS
TOTAL APARTMENT PARKING		
STANDARD STALLS	=	217 STALLS
HANDICAP STALLS	=	4 STALLS
TOTAL	=	221 STALLS

TOWNHOME PARKING SUMMARY

ROW HOMES (148 UNITS)		
STANDARD STALLS	=	33 STALLS
ON STREET PARKING (STREET C)	=	28 STALLS
GARAGE STALLS (2 PER UNIT)	=	296 STALLS
TOTAL	=	357 STALLS

CITY COLLECTION MULTI-FAMILY TOWNHOME LOT

(LOTS 3-10, BLOCK 1, 1-8, BLOCK 2, 1-56, BLOCK 3, 1-22, BLOCK 4, 1-24, BLOCK 5, & LOTS 1-30, BLOCK 6)



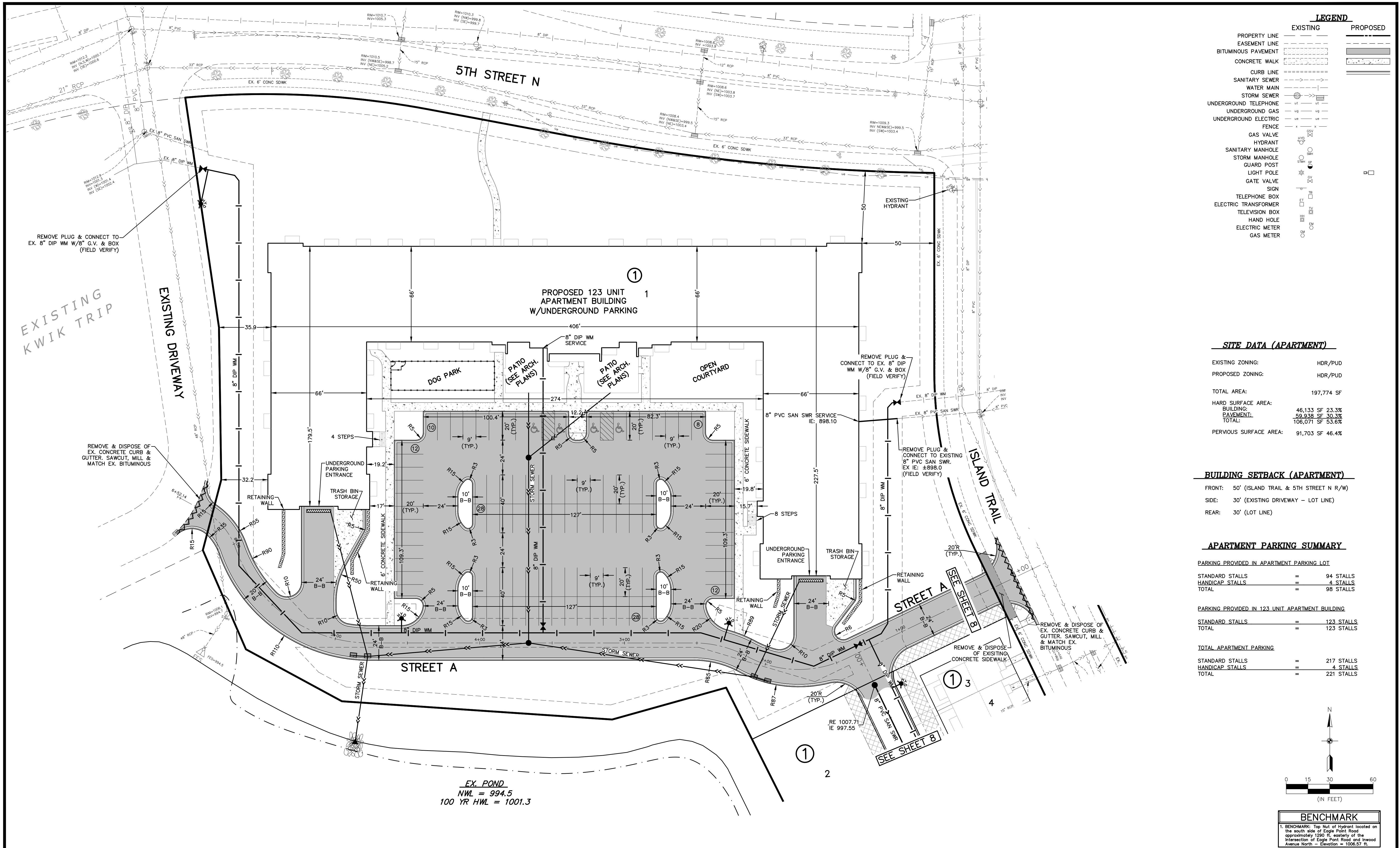
BENCHMARK
1. BENCHMARK: Top Nut of Hydrant located on the south side of Eagle Point Road approximately 1290 ft. easterly of the intersection of Eagle Point Road and Inwood Avenue North - Elevation = 1006.57 ft.

CALL BEFORE YOU DIG

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Know what's below.
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The subsurface utility information shown on this plan is utility Quality Level D. This quality level was determined according to the guidelines of CI/ASCE 38-02, entitled "Standard Guideline for the Collection and Depiction of Existing Subsurface Utility Data."





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PLANNING

3890 PHEASANT RIDGE DR NE
SUITE 100
BLAINE, MN 55449
TEL 763.489.7900
FAX 763.489.7959
CARLSON-ENGINEERING.COM

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota

Print Name: Brian J. Krystofiak, P.E.
Signature: *Brian J. Krystofiak*
Date: 12/20/24 License #: 25063

Drawn: KRO
Designed: BJK
Date: 12/20/24

Revisions:
1. 1/23/25 per City Comments.
2. 2/11/25 per City Comments.
3. 4/28/25 per City Comments.

M/I HOMES OF MINNEAPOLIS, LLC
5354 Parkdale Drive, #100
St. Louis Park, MN 55416

INWOOD TOWNHOMES
Lake Elmo, Minnesota

**PRELIMINARY SITE &
UTILITY PLAN**

TOWNHOME PARKING SUMMARY

ROW HOMES (148 UNITS)		
STANDARD STALLS	=	33 STALLS
ON STREET PARKING (STREET C)	=	28 STALLS
GARAGE STALLS (2 PER UNIT)	=	296 STALLS
TOTAL	=	357 STALLS

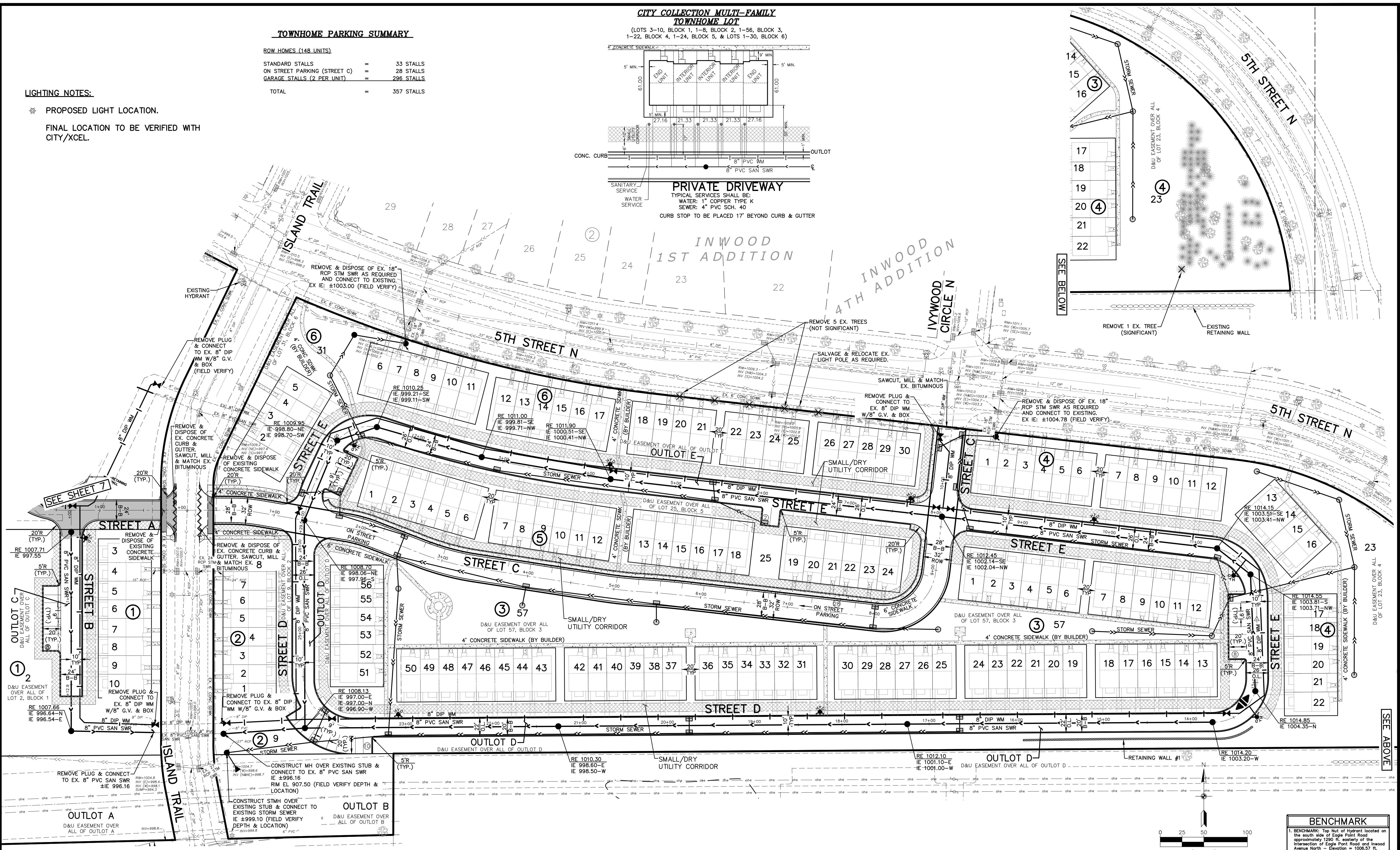
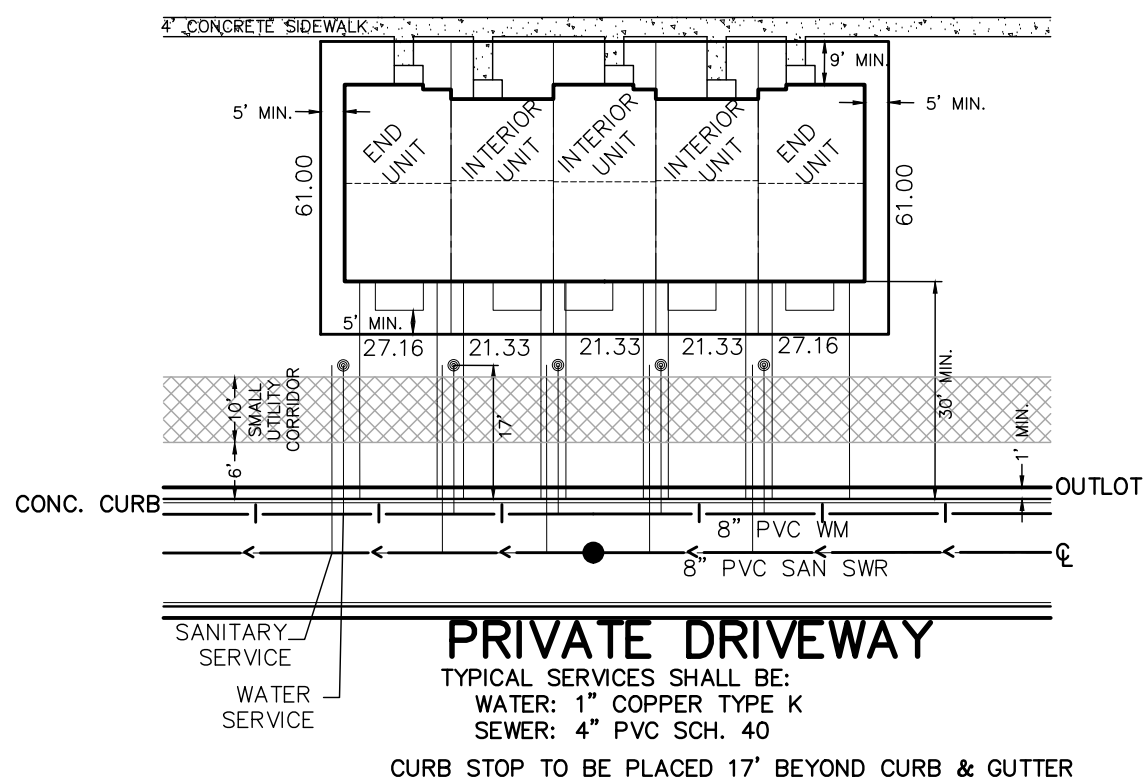
LIGHTING NOTES:

☼ PROPOSED LIGHT LOCATION.

FINAL LOCATION TO BE VERIFIED WITH CITY/XCEL.

CITY COLLECTION MULTI-FAMILY TOWNHOME LOT

(LOTS 3-10, BLOCK 1, 1-8, BLOCK 2, 1-56, BLOCK 3, 1-22, BLOCK 4, 1-24, BLOCK 5, & LOTS 1-30, BLOCK 6)



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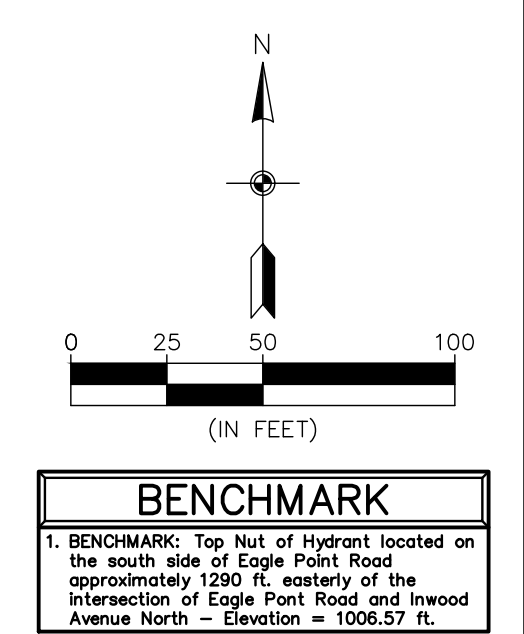
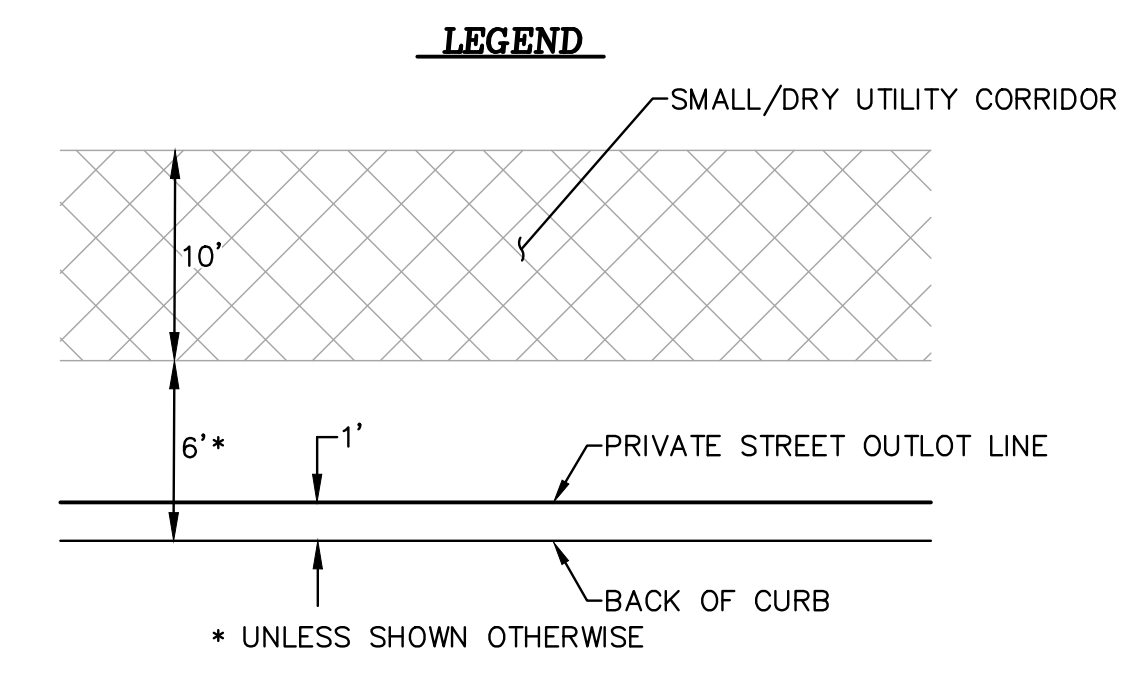
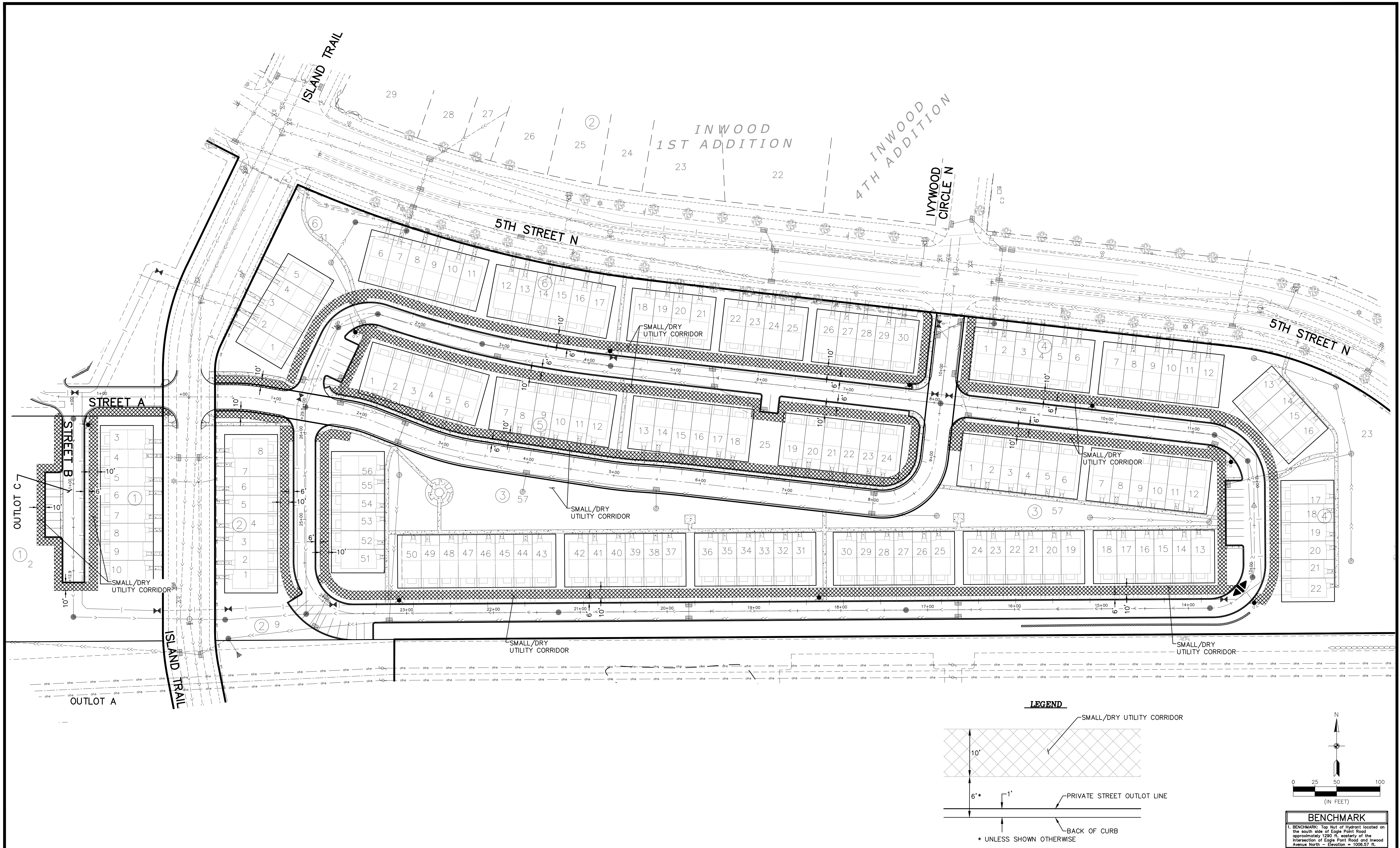
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INWOOD TOWNHOMES
Lake Elmo, Minnesota

PRELIMINARY SITE & UTILITY PLAN

8 of 16





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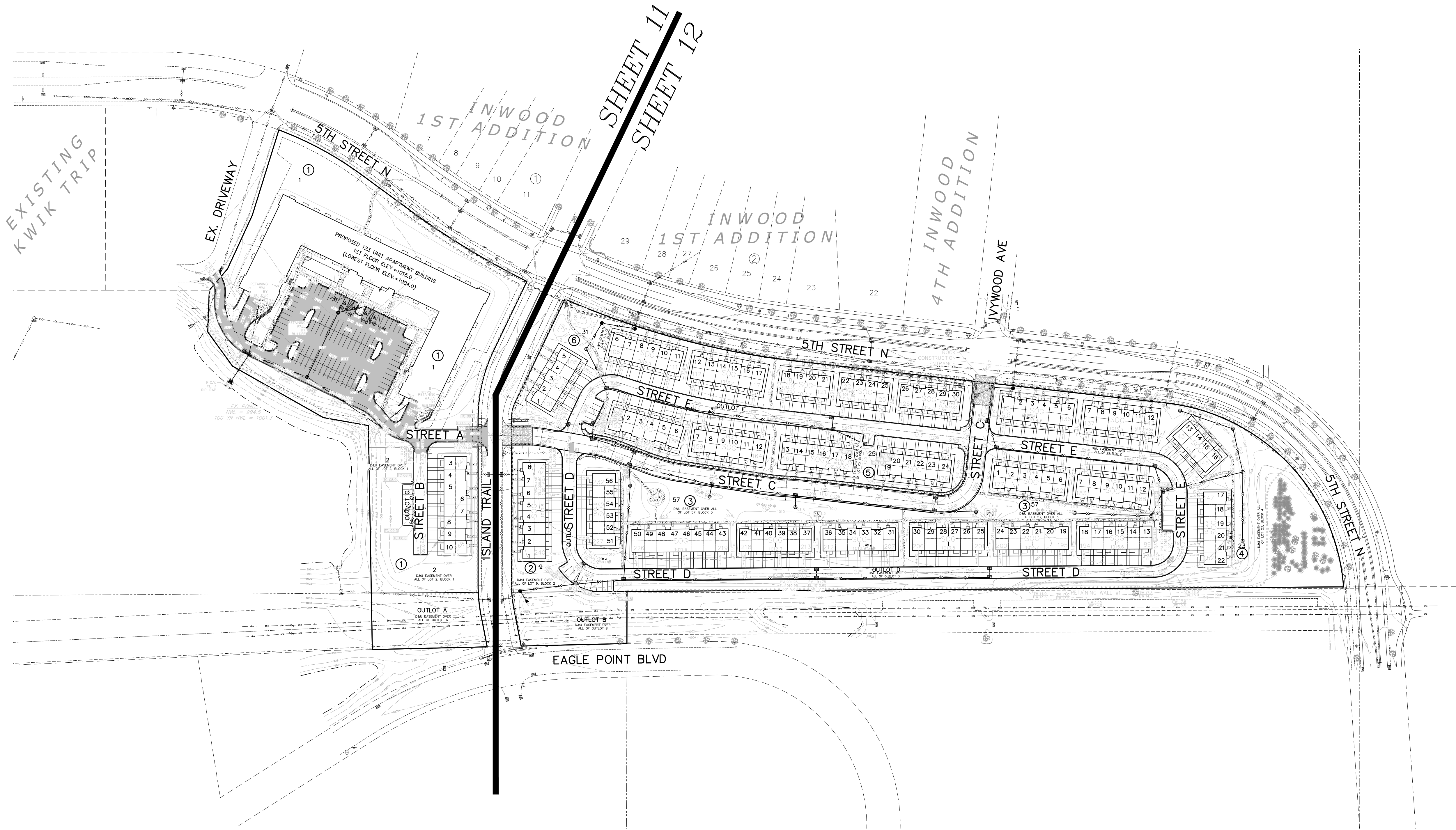
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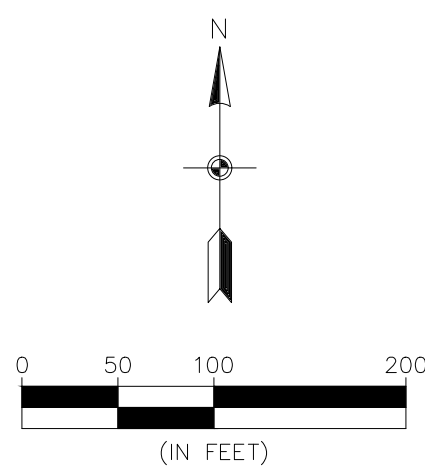
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5354 Parkdale Drive, #100
St. Louis Park, MN 55416

INWOOD TOWNHOMES
Lake Elmo, Minnesota

**SMALL/DRY UTILITY
CORRIDOR LAYOUT**



LEGEND		
	EXISTING	PROPOSED
PROPERTY LINE	---	---
EASEMENT LINE	---	---
CURB LINE	---	---
BITUMINOUS	---	---
CONCRETE	---	---
SANITARY SEWER	---	---
STORM SEWER	---	---
DRAIN TILE	---	DT
WATER MAIN	---	---
OVERHEAD UTILITY	---	---
STORM CATCH BASIN	---	---
STORM MANHOLE	---	---
MANHOLE	---	---
HYDRANT	---	---
GATE VALVE	---	---
TELEVISION BOX	---	---
TELEPHONE BOX	---	---
UTILITY POLE	---	---
RETAINING WALL	---	---
FENCE	---	---
10' CONTOUR	---	---
2' CONTOUR	---	---
FLOOD WAY	---	---
FLOOD PLAIN	---	---
WETLAND LINE	---	---
SPOT ELEVATION	---	---
EMERGENCY OVERFLOW	---	---
SURFACE DRAINAGE ARROW	---	---
STREET GRADE ARROW	---	---
INFILTRATION AREA	---	---
MAINTENANCE ACCESS	---	---
WETLAND SIGN	---	---
SILT FENCE	---	---
TREE FENCE	---	---
GRADING LIMITS	---	---
PHASE LINE	---	---
TREELINE	---	---
RIP-RAP	---	---
SOIL BORING	---	---
WELL	---	---
LIGHT POLE	---	---
FLAG POLE	---	---
ELECTRIC BOX	---	---
TELEPHONE BOX	---	---
TELEVISION BOX	---	---
SIGN	---	---



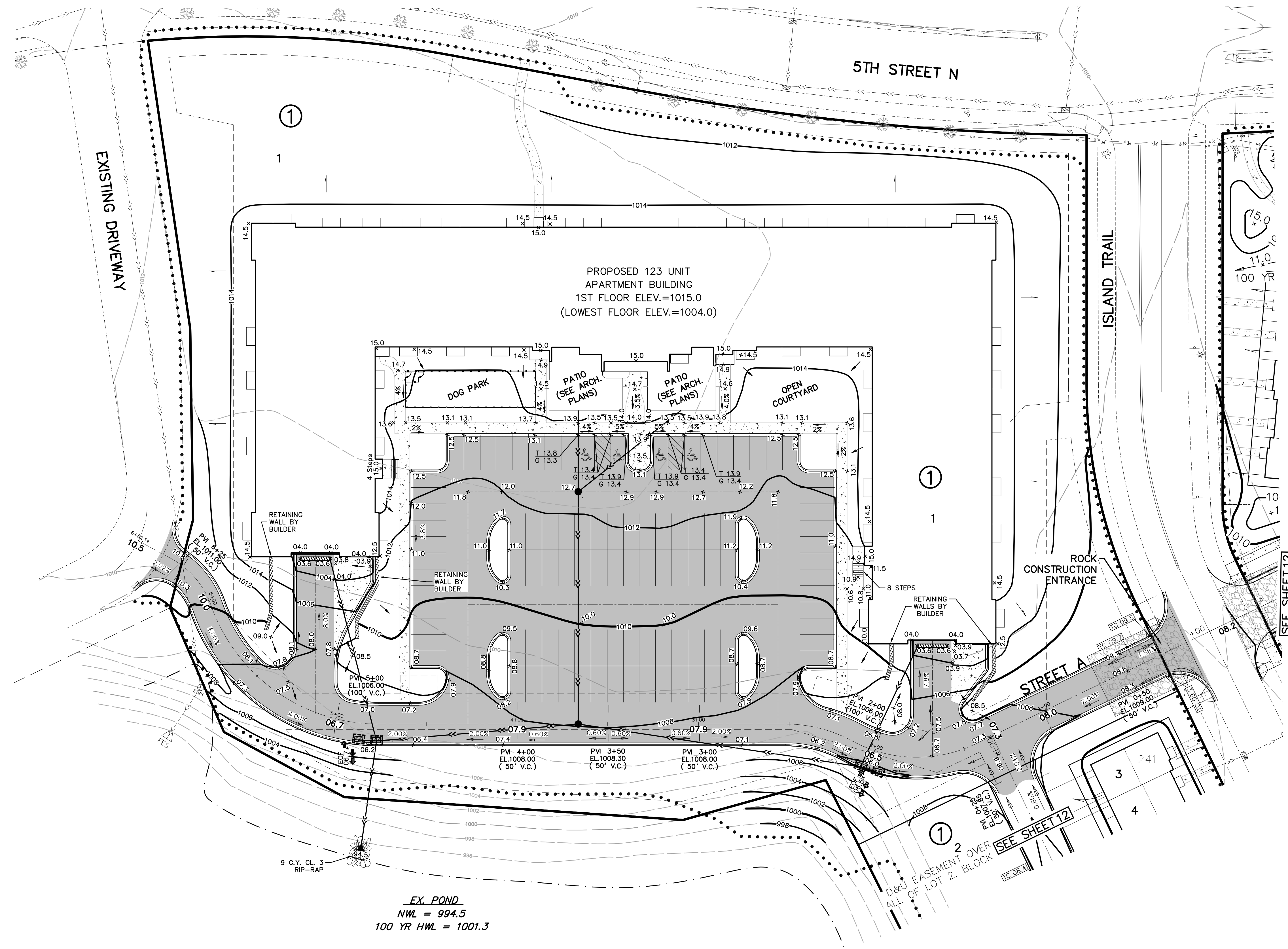
BENCHMARK
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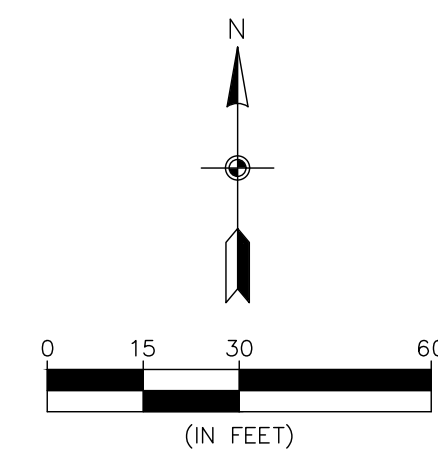
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LEGEND		
	EXISTING	PROPOSED
PROPERTY LINE	---	---
EASEMENT LINE	---	---
CURB LINE	---	---
BITUMINOUS	---	---
CONCRETE	---	---
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EX. POND
NWL = 994.5
100 YR HWL = 1001.3



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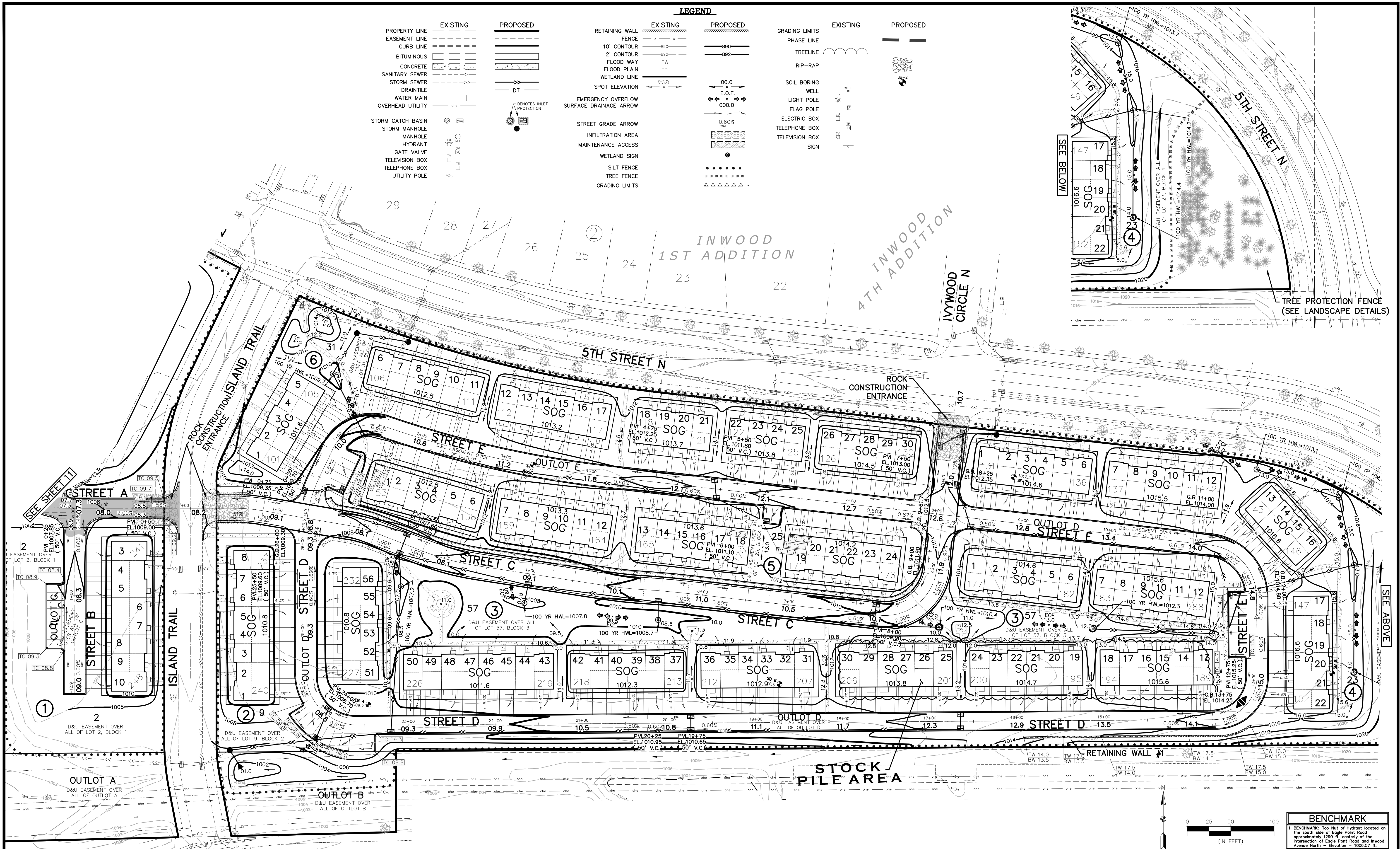
M/I HOMES OF MINNEAPOLIS, LLC
5354 Parkdale Drive, #100
St. Louis Park, MN 55416

INWOOD TOWNHOMES
Lake Elmo, Minnesota

**PRELIMINARY GRADING
& EROSION CONTROL PLAN**

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of
16

#5172-01



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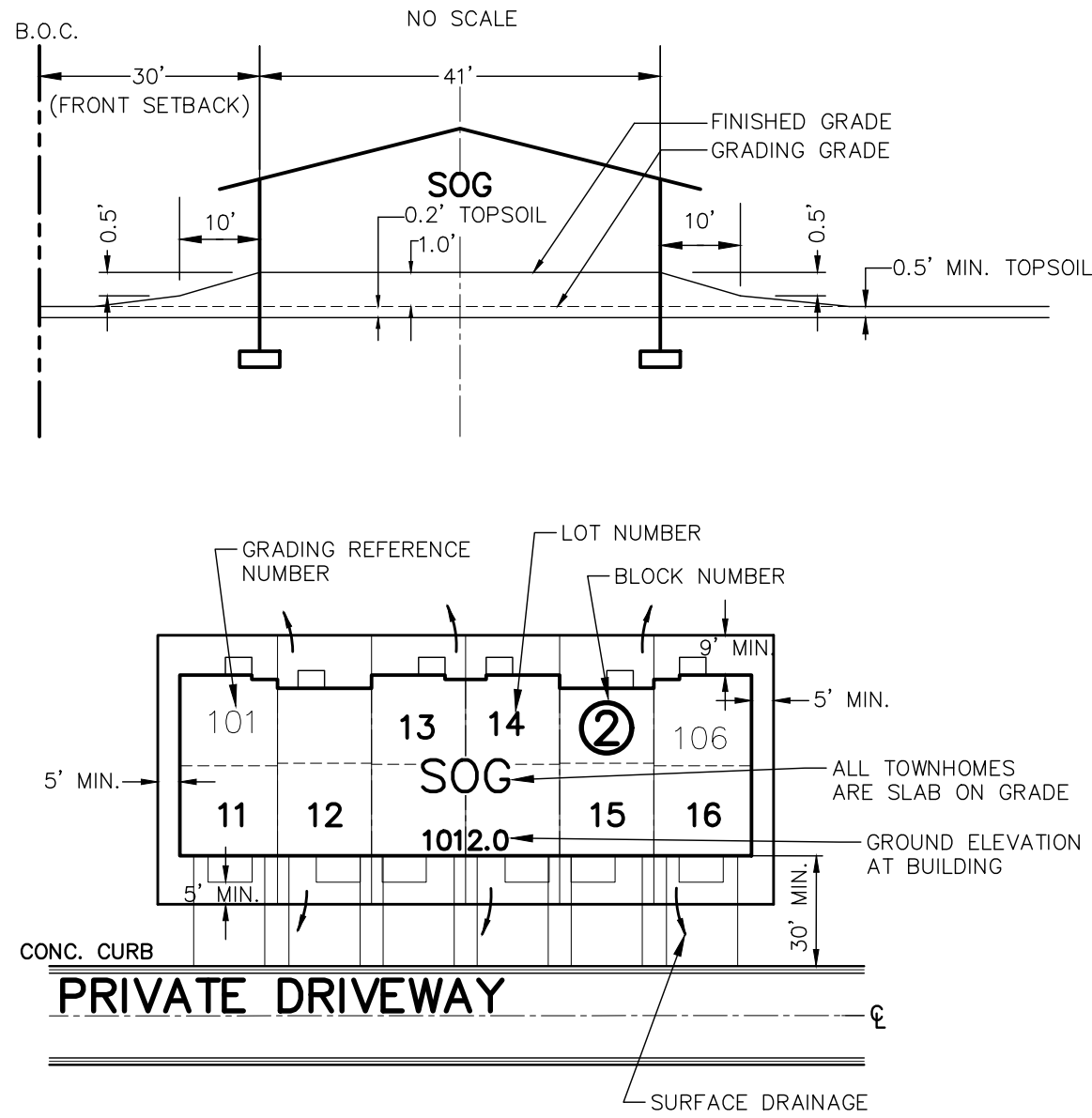
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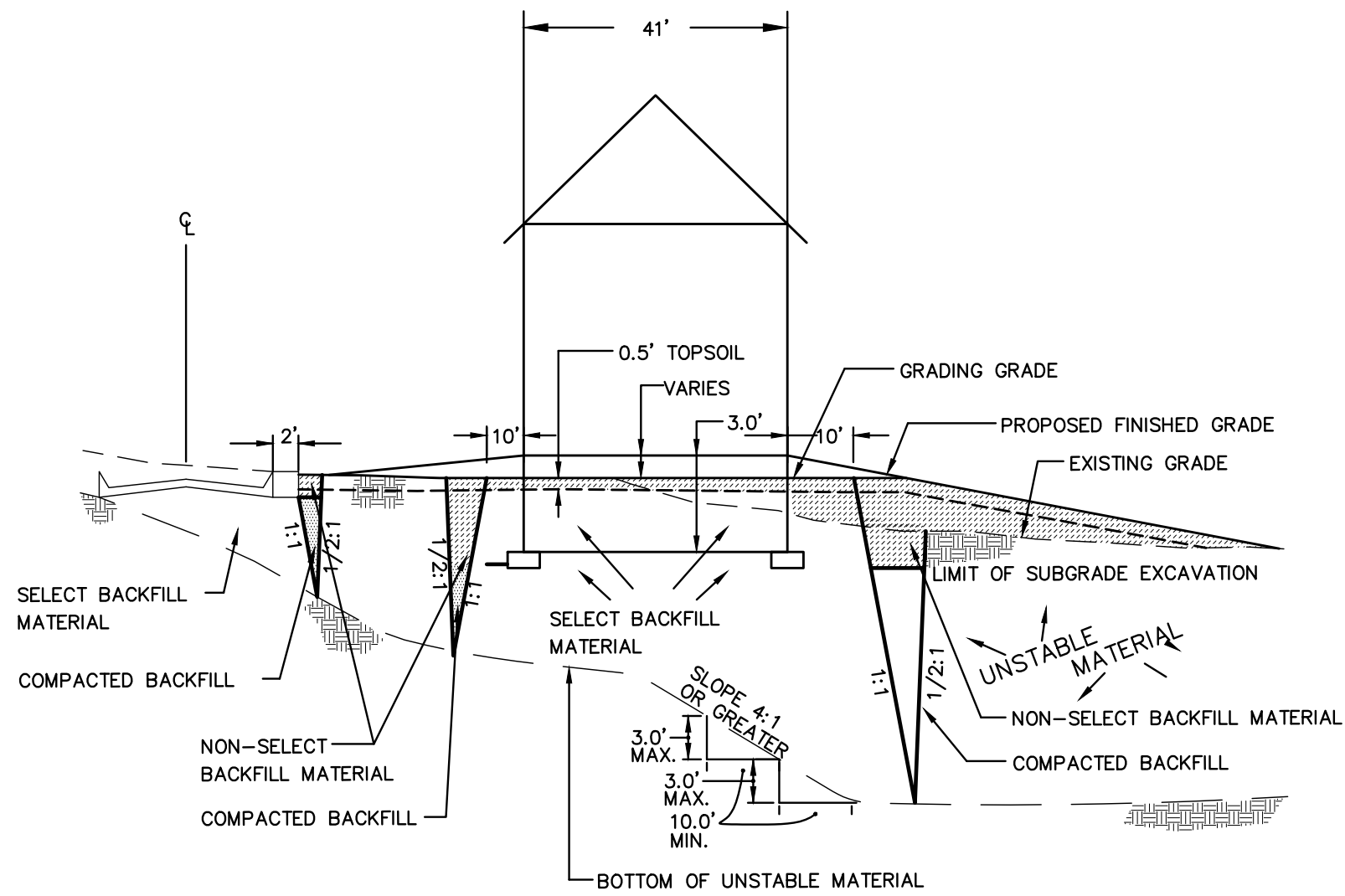
INWOOD TOWNHOMES
Lake Elmo, Minnesota

**PRELIMINARY GRADING
& EROSION CONTROL PLAN**

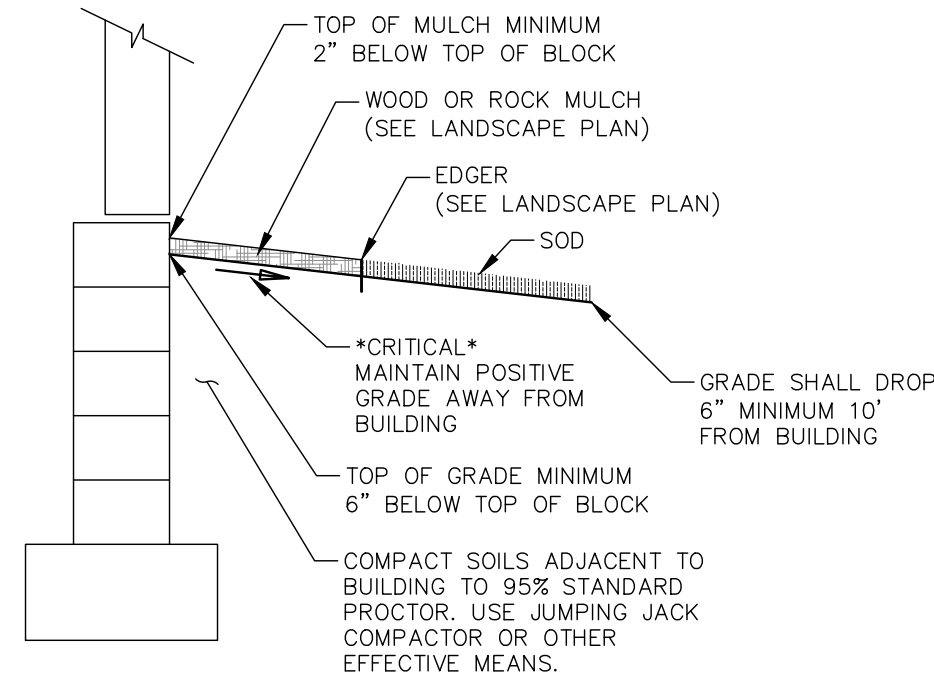
ROW TOWNHOME HOLDDOWN
SLAB ON GRADE



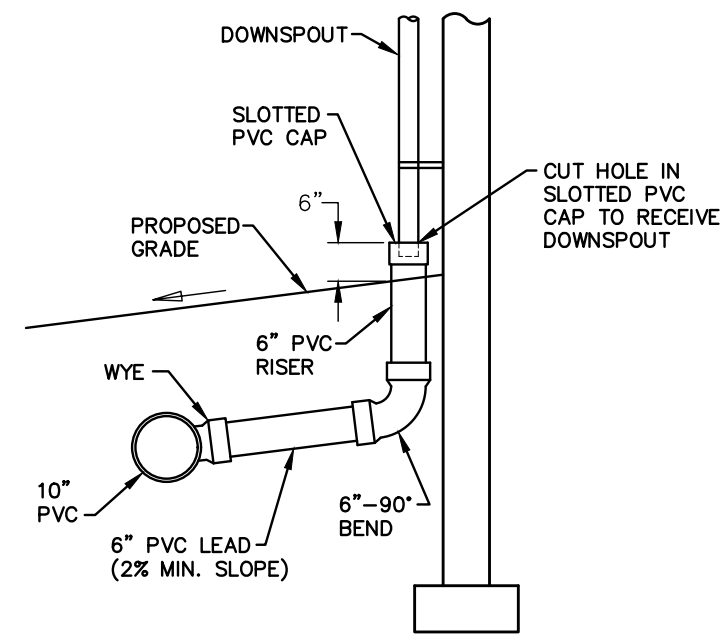
SUBGRADE CORRECTION



TYPICAL GRADING AT APARTMENT BUILDING

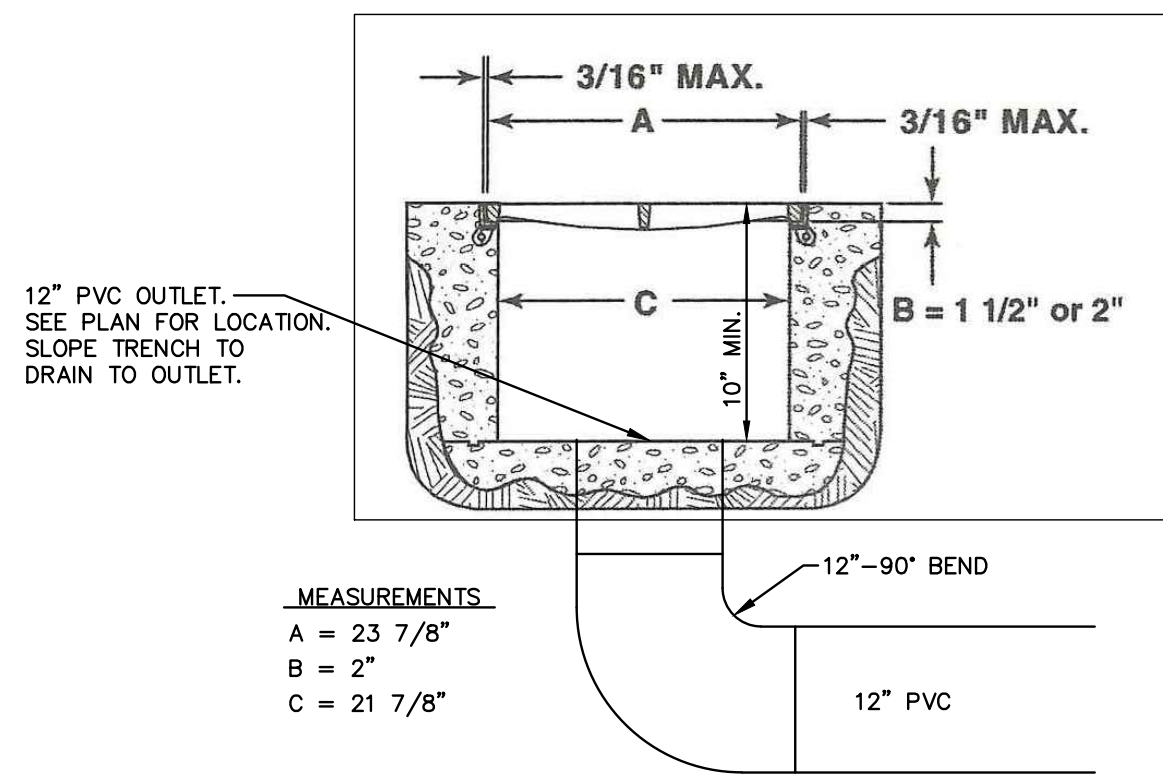


DOWNSPOUT CONNECTION

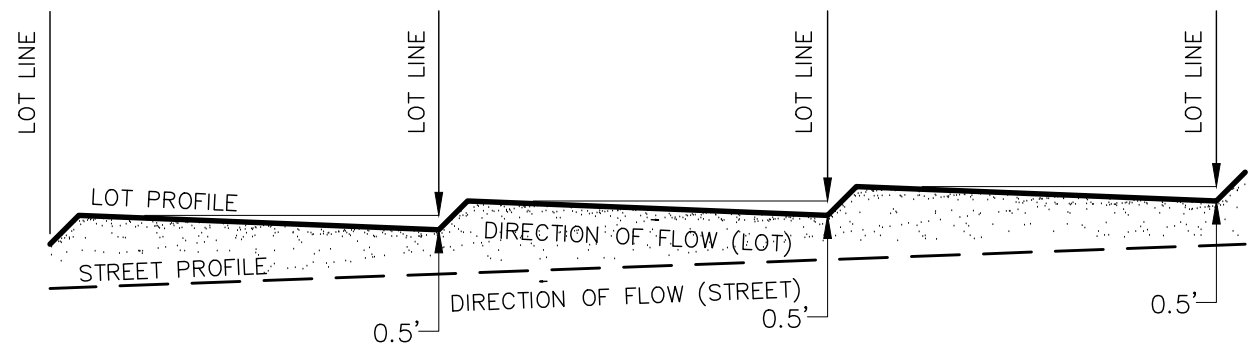


TRENCH DRAIN DETAIL

R-4999-L6 DETAIL (PER MANUFACTURER'S SPEC.)



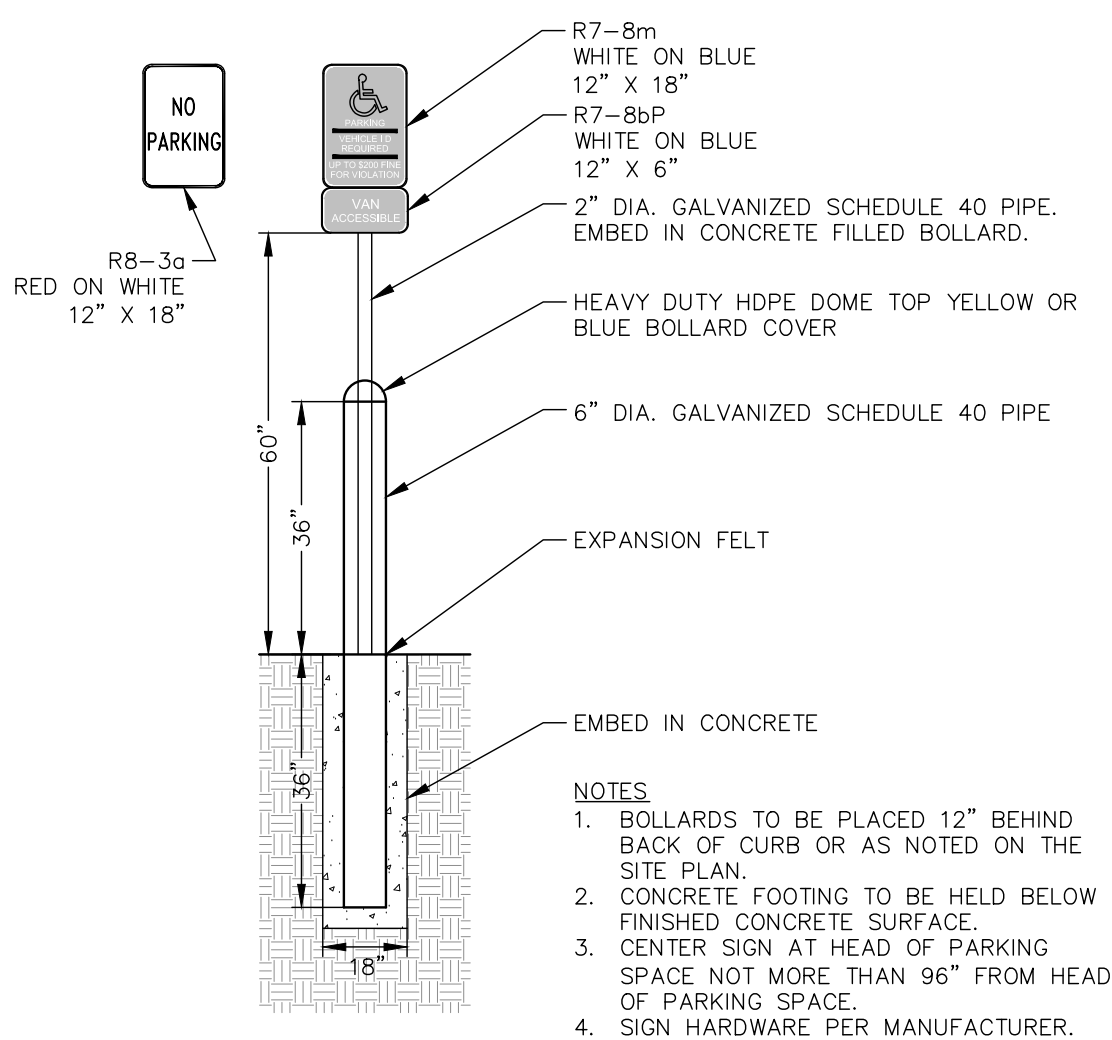
LOT BENCHING DETAIL



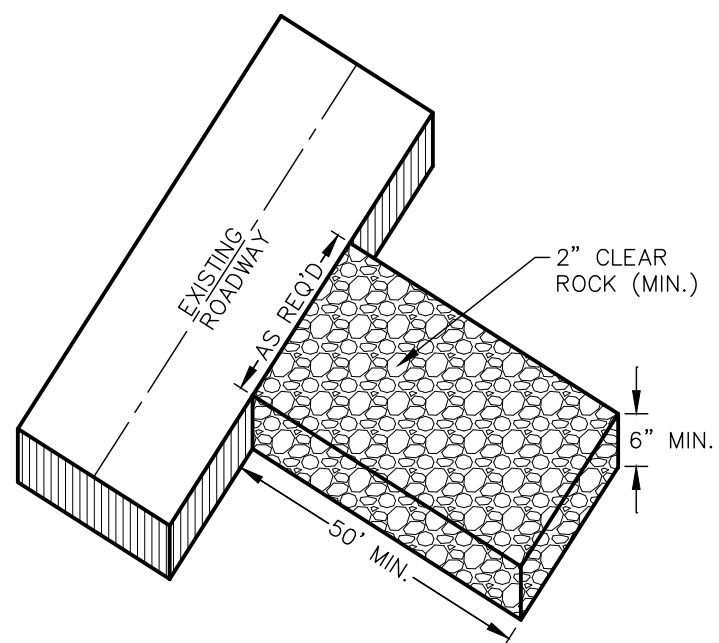
TURF ESTABLISHMENT

1. TURF ESTABLISHMENT SHALL APPLY TO ALL DISTURBED AREAS AND SHALL BE ACCORDING TO MNDOT STANDARD SPECIFICATIONS FOR CONSTRUCTION (LATEST EDITION) EXCEPT AS MODIFIED BELOW.
2. TURF ESTABLISHMENT SHALL OCCUR AS SOON AS POSSIBLE BUT IN NO CASE MORE THAN 7 DAYS.
3. SEED: MNDOT MIXTURE 25-141 AT 60 POUNDS PER ACRE.
4. DORMANT SEED: SHALL BE APPLIED AT TWICE THE NORMAL RATE AFTER NOVEMBER 1ST.
5. MULCH: TYPE 1 AT 2 TONS PER ACRE (DISK ANCHORED).
6. FERTILIZER: TYPE 10-10-10 AT 200 POUNDS PER ACRE.

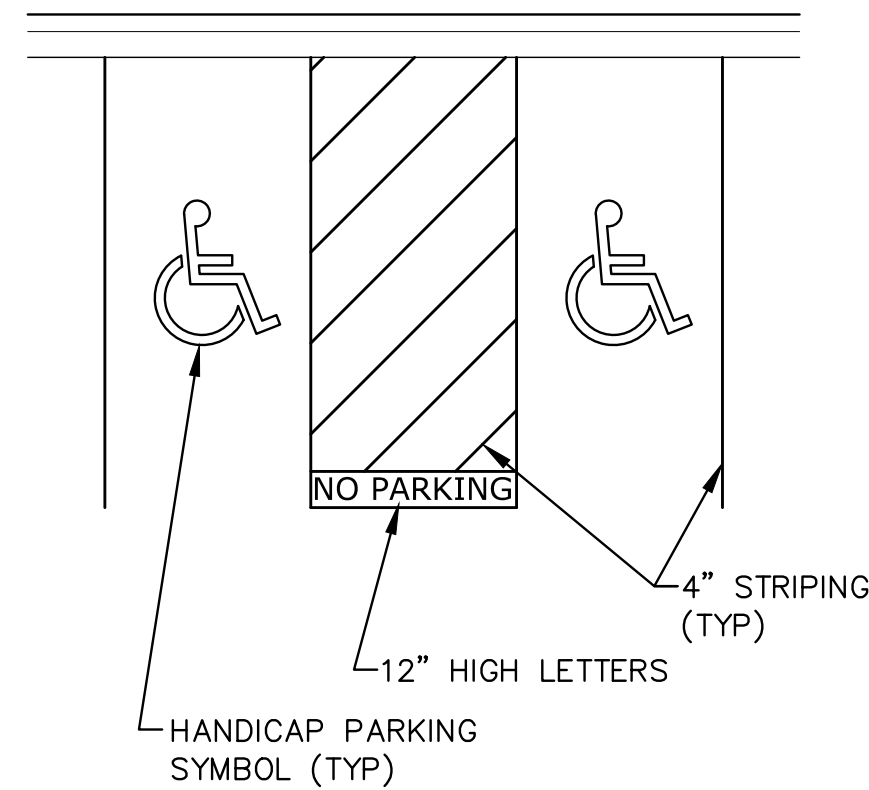
SIGN ON BOLLARD



ROCK CONSTRUCTION ENTRANCE



HANDICAP PARKING SPACE STRIPING



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INWOOD TOWNHOMES
Lake Elmo, Minnesota

DETAILS

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of
16

1. THE CONTRACTOR SHALL CONDUCT OPERATIONS AND IMPLEMENT MINNESOTA POLLUTION CONTROL AGENCY (MPCA) BEST MANAGEMENT PRACTICES (BMP) TO CONTROL SITE SILTATION AND EROSION INTO DRAINAGE WAYS. THE CONTRACTOR SHALL COMPLY WITH ALL CONDITIONS AND COMPLETION DATES RELATIVE TO ALL PERMITS ISSUED FOR THE WORK TO BE COMPLETED. THE ENGINEER MAY ISSUE A STOP WORK ORDER FOR ALL DEVELOPMENT WORK AND BUILDING CONSTRUCTION FOR NONCOMPLIANCE WITH THESE MEASURES.
2. SEQUENCING. ALL SILT FENCE AND OTHER EROSION CONTROL MEASURES SHALL BE IN PLACE AND APPROVED BY ENGINEER PRIOR TO ANY REMOVALS. EXCAVATION OR CONSTRUCTION AND SHALL BE MAINTAINED UNTIL VIABLE TURF OR GROUND COVER HAS BEEN ESTABLISHED AND APPROVED BY THE ENGINEER.
3. SILT FENCE. THE CONTRACTOR SHALL INSTALL SILT FENCE AT THE LOCATIONS SHOWN ON THE PLANS AND IN ACCORDANCE WITH THE CITY STANDARD DETAILS. SILT FENCE DAMS AND INTERIM SUMPS SHALL BE PLACED TO INTERCEPT SILT FROM CONCENTRATED RUNOFF FROM OPEN GRADED AREAS. ADDITIONAL SILT FENCE SHALL BE REQUIRED AS DIRECTED BY THE ENGINEER.
4. STOCKPILES. ALL STOCKPILE AREAS SHALL HAVE SILT FENCE OR SEDIMENT TRAPPING SYSTEMS PLACED AROUND THE ENTIRE PERIMETER.
5. INLET PROTECTION. THE CONTRACTOR SHALL INSTALL INLET PROTECTION ON ALL EXISTING STORM SEWER INLETS IN ACCORDANCE WITH THE CITY STANDARD DETAILS. INLET PROTECTION SHALL ALSO BE PROVIDED ON ALL PROPOSED STORM SEWER INLETS IMMEDIATELY FOLLOWING CONSTRUCTION OF THE INLET. INLET PROTECTION MUST BE INSTALLED IN A MANNER THAT WILL NOT IMPOUND WATER FOR EXTENDED PERIODS OF TIME OR IN A MANNER THAT PRESENTS A HAZARD TO VEHICULAR OR PEDESTRIAN TRAFFIC.
6. TEMPORARY SEDIMENT BASINS. THE CONTRACTOR SHALL INCORPORATE TEMPORARY SEDIMENT BASINS THROUGHOUT THE CONSTRUCTION SITE TO CAPTURE RUNOFF AND SLOW THE FLOW OF WATER AND ALLOW SEDIMENT TO SETTLE OUT. TEMPORARY SEDIMENT BASINS SHALL BE INSTALLED AS DIRECTED BY THE CITY ENGINEER.
7. ROCK CONSTRUCTION ENTRANCE. A ROCK ENTRANCE SHALL BE CONSTRUCTED AND MAINTAINED AS SHOWN ON THE PLAN TO REDUCE TRACKING OF SILT AND DIRT ONTO THE PUBLIC STREETS. A GEOTEXTILE FABRIC SHALL BE PLACED UNDERNEATH THE ROCK. THE ROCK SHALL BE PERIODICALLY REPLENISHED TO MAINTAIN THE INTENDED PERFORMANCE. MUD AND DEBRIS SHALL BE REMOVED OR SCRAPPED FROM TIRES AND VEHICLE UNDERCARRIAGE PRIOR TO LEAVING THE SITE.
8. STREET SWEEPING. ALL STREETS USED FOR ACCESS TO THE SITE AND HAUL ROUTES USED FOR CONSTRUCTION EQUIPMENT AND MATERIAL SUPPLIES SHALL BE CLEANED AT THE END OF EACH WORKING DAY. THE CITY OR ENGINEER MAY ORDER ADDITIONAL SWEEPING OF THE STREETS AS DEEMED REQUIRED AT DEVELOPER/CONTRACTOR EXPENSE.

STANDARD PLAN NOTES
GRADING AND EROSION CONTROL PLANS

FEBRUARY 2015



CITY OF LAKE ELMO

STANDARD DRAWING NO.
600A
LAKE ELMO

9. DEWATERING. EACH EXCAVATION SHALL BE KEPT DRY DURING THE COURSE OF ALL WORK HEREIN, INCLUDING SUBGRADE CORRECTION, PIPE INSTALLATION, STRUCTURE CONSTRUCTION AND BACKFILLING, TO THE EXTENT THAT NO DAMAGE FROM HYDROSTATIC PRESSURE, FLOTATION OR OTHER DAMAGE RESULTS. ALL EXCAVATIONS SHALL BE DEWATERED TO A DEPTH OF AT LEAST 3 INCHES BELOW THE BOTTOM OF THE CONCRETE SLAB OR PIPE TO BE INSTALLED THEREIN. THE CONTRACTOR MAY USE ANY METHOD OR COMBINATION OF METHODS FOR DEWATERING HE CHOOSES; HOWEVER, ALL DEWATERING METHODS AND EQUIPMENT WHICH IN THE OPINION OF THE ENGINEER, ARE INEFFECTIVE SHALL BE ABANDONED, IMPROVED, REPLACED OR OTHERWISE ALTERED TO OBTAIN EFFECTIVE DEWATERING. THE CONTRACTOR SHALL PROVIDE ALL POWER, PUMPS, MATERIALS AND APPARATUS NECESSARY, AND SHALL BE RESPONSIBLE FOR DISPOSING OF THE WATER PUMPED FROM THE EXCAVATION IN A MANNER WHICH WILL NOT INTERFERE WITH OTHER WORK WITHIN THE AREA AND NOT TO DAMAGE PUBLIC OR PRIVATE PROPERTY. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR THE CONDITION OF ANY PIPE, CONDUIT, DITCH, CHANNEL OR NATURAL WATERCOURSE UTILIZED FOR DRAINAGE PURPOSES, AND ALL EROSION, SEDIMENT OR OTHER ADVERSE RESULTS OF THEIR USE SHALL BE REPAIRED.
10. POSITIVE DRAINAGE AND PROTECTION. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE THROUGHOUT THE SITE AT ALL TIMES. LOW POINTS WITHIN AND ALONG ROADWAYS ARE EXPRESSLY PROHIBITED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TEMPORARY DITCHES, PIPING OR OTHER MEANS TO FACILITATE PROPER DRAINAGE DURING CONSTRUCTION. TO PROTECT PREVIOUSLY GRADED AREAS FROM EROSION, WOOD FIBER BLANKET SHALL BE PLACED IMMEDIATELY ON STEEP SLOPES (1:3 OR GREATER) AND EMBANKMENTS, PERMANENT AND TEMPORARY PONDS, AND OUTLETS AND OVERFLOWS TO PROTECT THE COMPLETED GRADE AND MINIMIZE SILT IN THE RUNOFF.
11. DRAINAGE DITCHES. THE NORMAL WETTED PERIMETER OF ANY TEMPORARY OR PERMANENT DRAINAGE DITCH OR SWALE THAT DRAINS WATER FROM ANY PORTION OF THE CONSTRUCTION SITE, OR DIVERTS WATER AROUND THE SITE, MUST BE STABILIZED WITHIN 200 LINEAL FEET FROM THE PROPERTY EDGE, OR FROM THE POINT OF DISCHARGE INTO ANY SURFACE WATER. STABILIZATION OF THE LAST 200 LINEAL FEET MUST BE COMPLETED WITHIN 24 HOURS AFTER CONNECTING TO A SURFACE WATER. STABILIZATION OF THE REMAINING PORTIONS OF ANY TEMPORARY OR PERMANENT DITCHES OR SWALES MUST BE COMPLETE WITHIN 14 DAYS AFTER CONNECTING TO A SURFACE WATER AND CONSTRUCTION IN THAT PORTION OF THE DITCH HAS TEMPORARILY OR PERMANENTLY CEASED. TEMPORARY OR PERMANENT DITCHES OR SWALES THAT ARE BEING USED AS A SEDIMENT CONTAINMENT SYSTEM (WITH PROPERLY DESIGNED ROCK DITCH CHECKS, BIO ROLLS, SILT DIKES, ETC.) DO NOT NEED TO BE STABILIZED. THESE AREAS MUST BE STABILIZED WITHIN 24 HOURS AFTER NO LONGER BEING USED AS A SEDIMENT CONTAINMENT SYSTEM.
12. TURF ESTABLISHMENT. ALL EXPOSED SOIL AREAS MUST BE STABILIZED AS SOON AS POSSIBLE TO LIMIT SOIL EROSION BUT IN NO CASE LATER THAN 14 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

STANDARD PLAN NOTES
GRADING AND EROSION CONTROL PLANS

FEBRUARY 2015



CITY OF LAKE ELMO

STANDARD DRAWING NO.
600B
LAKE ELMO

13. MAINTENANCE AND INSPECTION. EROSION CONTROL MEASURES SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION AND UNTIL SATISFACTORY ESTABLISHMENT OF PERMANENT GROUND COVER IS OBTAINED. ALL EROSION AND SEDIMENTATION CONTROL MEASURES AND STORMWATER OUTFALLS MUST BE INSPECTED WEEKLY, AND WITHIN 24 HOURS OF THE SITE RECEIVING 0.5 INCHES OF RAIN. REPAIRS MUST BE MADE ON THE SAME DAY OR FOLLOWING DAY OF THE INSPECTION. UNSATISFACTORY CONDITIONS NOT REPAIRED OR CLEANED UP WITHIN 48-HOURS OF NOTIFICATION SHALL RESULT IN A STOP WORK ORDER, AND/OR SAID WORK SHALL BE COMPLETED AT CONTRACTOR'S EXPENSE.
14. REMOVAL. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL TEMPORARY EROSION CONTROL MEASURES, STRUCTURES AND DEVICES ONLY AFTER RECEIVING ENGINEER APPROVAL. ALL DEBRIS, STAKES, AND SILTS ALONG SILT FENCES SHALL BE REMOVED AND DISPOSED OFF SITE. THE CONTRACTOR SHALL HAND RAKE Silted AREAS ALONG THE FENCE LOCATIONS TO PROVIDE A SMOOTH FINAL GRADE AND SHALL RESTORE THE GROUND SURFACE WITH SEED OR SOD, AS REQUIRED, TO MATCH THE FINISHED GRADE TO THE ADJACENT AREA.
15. FINAL STORM SEWER SYSTEM. AT THE COMPLETION OF THE WORK AND BEFORE THE FINAL WALK THROUGH, THE CONTRACTOR SHALL REMOVE STORM SEWER INLET PROTECTION MEASURES AND THOROUGHLY FLUSH THE STORM SEWER SYSTEM. SEDIMENT AND DEBRIS SHALL BE COMPLETELY REMOVED AND CLEANED AT THE INLETS, OUTLETS, AND DOWNSTREAM OF EACH OUTLET. RIPRAP AND GEOTEXTILE FABRIC MAY REQUIRE REPLACEMENT AS DIRECTED BY THE ENGINEER TO OBTAIN A LIKE NEW INSTALLATION ACCEPTABLE TO THE CITY.
16. DITCH CHECK (BIOROLL BLANKET SYSTEM). BIOROLL AND BLANKET SYSTEMS SHALL BE INSTALLED AS DITCH CHECKS ONLY IN SPECIFIED LOCATIONS AS APPROVED BY THE CITY ENGINEER. BIOROLLS ARE NOT TO BE UTILIZED IN AREAS WHERE VEHICLE AND CONSTRUCTION TRAFFIC OCCUR.
17. FLOTATION SILT CURTAIN. FLOTATION SILT CURTAIN SHALL BE UTILIZED WHEN CONSTRUCTION ACTIVITIES OCCUR DIRECTLY ADJACENT TO LAKES, STREAMS OR WETLANDS IN ORDER TO CONTAIN SEDIMENTS NEAR THE BANKS OF WORKING AREAS. THE INSTALLATION OF FLOTATION SILT CURTAINS WILL BE REQUIRED AS DIRECTED BY THE CITY ENGINEER.
18. CONCRETE WASHOUT ONSITE. ALL LIQUID AND SOLID WASTES GENERATED BY CONCRETE WASHOUT OPERATIONS MUST BE CONTAINED IN A LEAK-PROOF CONTAINMENT FACILITY OR IMPERMEABLE LINER. A COMPACTED CLAY LINER THAT DOES NOT ALLOW WASHOUT LIQUIDS TO ENTER GROUND WATER IS CONSIDERED AN IMPERMEABLE LINER. THE LIQUID AND SOLID WASTES MUST NOT CONTACT THE GROUND, AND THERE MUST NOT BE RUNOFF FROM THE CONCRETE WASHOUT OPERATIONS OR AREAS. LIQUID AND SOLID WASTES MUST BE DISPOSED OF PROPERLY AND IN COMPLIANCE WITH MPCA REGULATIONS. A SIGN MUST BE INSTALLED ADJACENT TO EACH WASHOUT FACILITY TO INFORM CONCRETE EQUIPMENT OPERATORS TO UTILIZE THE PROPER FACILITIES.

STANDARD PLAN NOTES
GRADING AND EROSION CONTROL PLANS

FEBRUARY 2015



CITY OF LAKE ELMO

STANDARD DRAWING NO.
600C
LAKE ELMO

1. RESTORE ALL DISTURBED AREAS WITH 6 INCHES OF TOPSOIL CONFORMING TO MNDOT 3877.
2. PROTECT ALL STORM SEWER INLETS AS SPECIFIED HEREIN AND MAINTAIN UNTIL STREET CONSTRUCTION IS COMPLETED.
3. MAINTAIN ALL SILT FENCE AND REPAIR OR REPLACE AS NEEDED OR REQUIRED UNTIL TURF HAS BEEN ESTABLISHED.
4. RESTORATION WORK SHALL BEGIN WITHIN 7 DAYS OF FINAL GRADING.
5. A MINIMUM OF 2 ROWS OF SOD SHALL BE PLACED ADJACENT TO THE BACK OF CURBS ALONG ALL BOULEVARDS. SILT FENCE SHALL BE PLACED DIRECTLY BEHIND THE SOD IN ACCORDANCE WITH THE CITY STANDARD DETAILS.
6. BOULEVARD AND DITCH RESTORATION INCLUDES FINE GRADING, WHICH INCLUDES THE REMOVAL OF ROCKS, DEBRIS AND SOIL CHUNKS, WHILE MAINTAINING POSITIVE DRAINAGE.

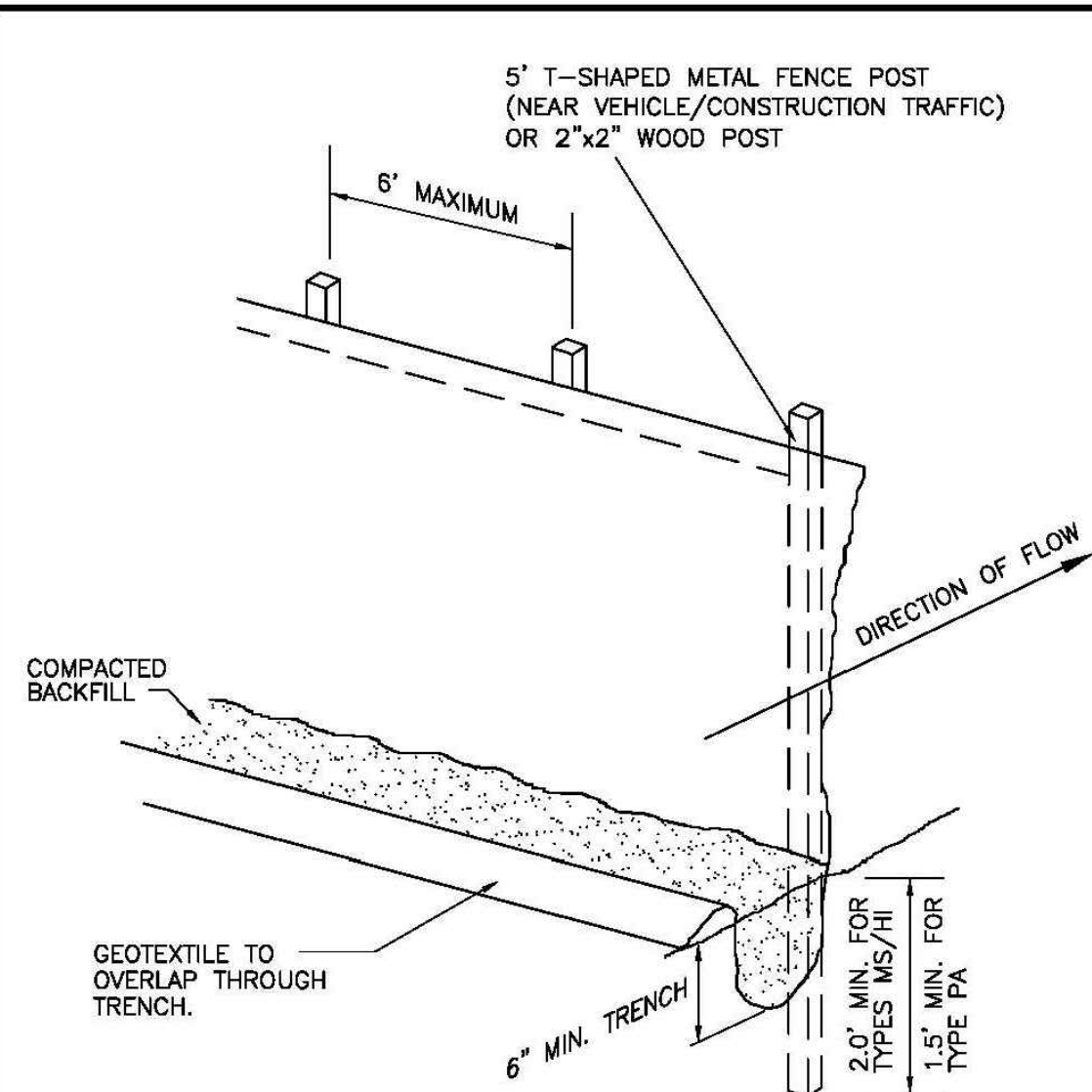
STANDARD PLAN NOTES
SITE RESTORATION PLANS

FEBRUARY 2015



CITY OF LAKE ELMO

STANDARD DRAWING NO.
600D
LAKE ELMO



NOTE : -SILT FENCE INSTALLATION SHALL CONFORM TO MNDOT2573.3, TYPE MS/Hi NEAR VEHICLE/CONSTRUCTION TRAFFIC, TYPE PA AT ALL OTHER LOCATIONS.
-MATERIALS SHALL CONFORM TO MNDOT 3886.

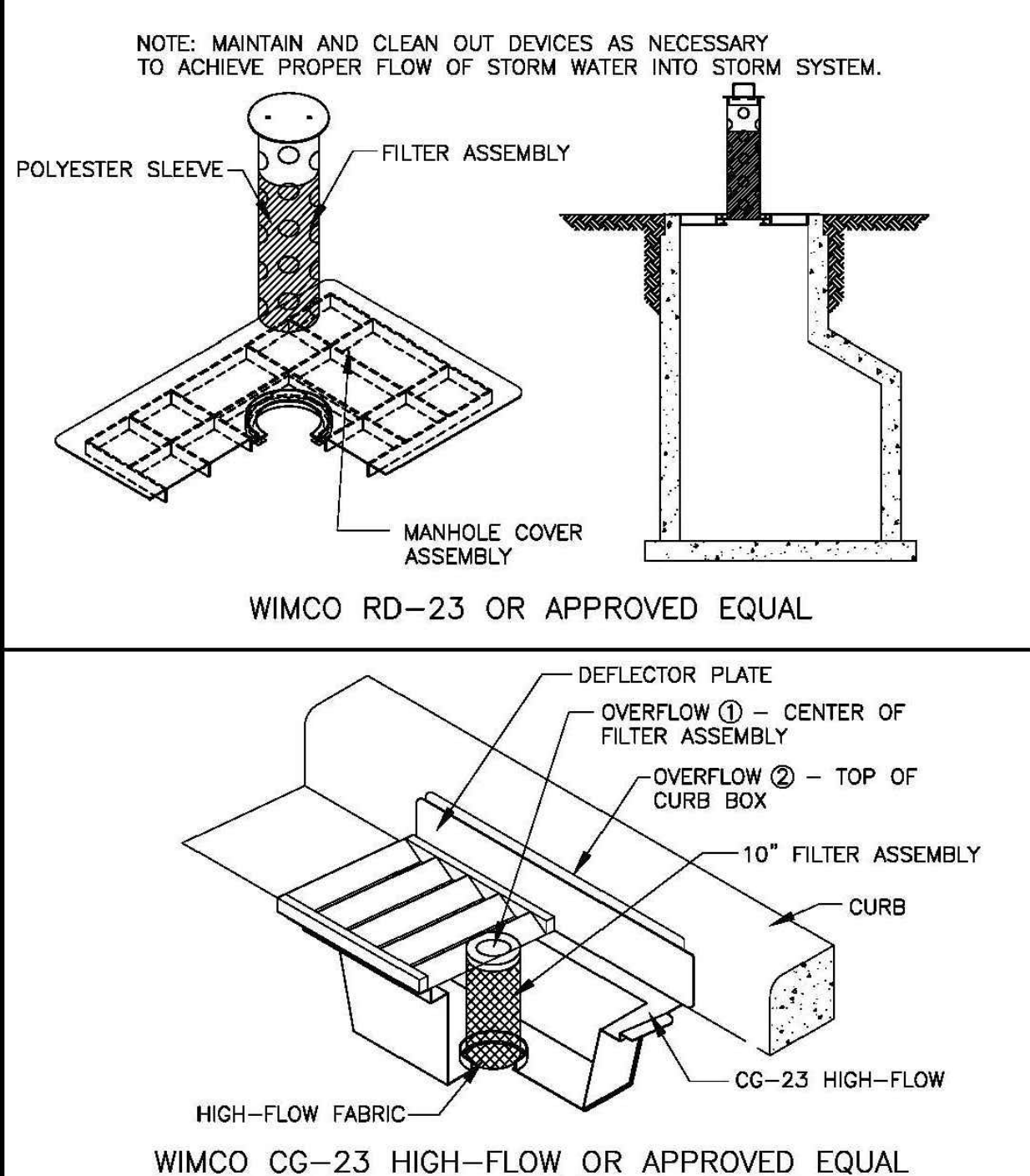
SILT FENCE

FEBRUARY 2015



CITY OF LAKE ELMO

STANDARD DRAWING NO.
601
LAKE ELMO



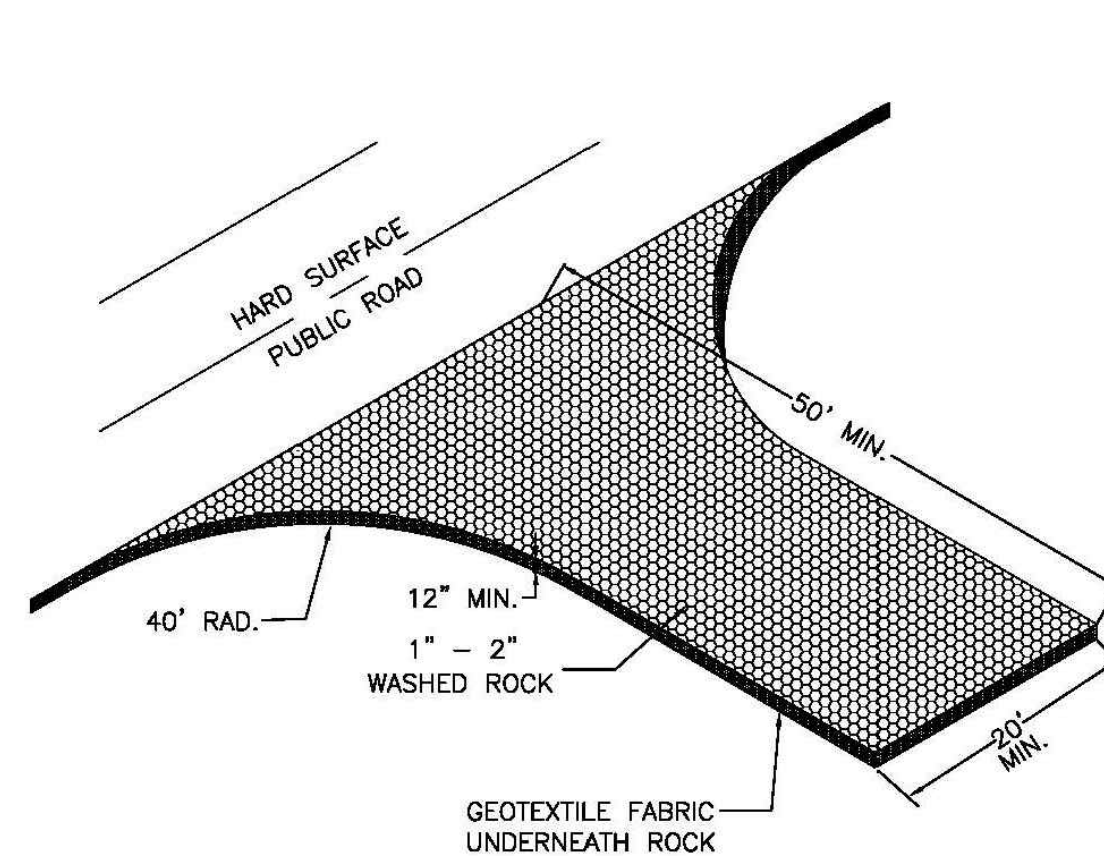
SEDIMENT CONTROL AROUND STORM SEWER INLET

FEBRUARY 2015



CITY OF LAKE ELMO

STANDARD DRAWING NO.
604
LAKE ELMO



NOTES:

- ① MAXIMUM WIDTH OF CONSTRUCTION ENTRANCE IS 24 FEET.
- ② A MNDOT 3733 TYPE V GEOTEXTILE FABRIC SHALL BE USED UNDER THE ROCK TO PREVENT MIGRATION OF THE UNDERLYING SOIL INTO THE STONE.
- ③ CONSTRUCTION ENTRANCE IS REQUIRED FOR ALL NEW HOME CONSTRUCTION AND NEW STREET CONSTRUCTION.
- ④ CONSTRUCTION ENTRANCE SHALL BE MAINTAINED TO PREVENT TRACKING OF MUD ONTO ROADWAYS THAT ADJOIN THE PROJECT. THIS WILL REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL ROCK OR REMOVAL AND REINSTALLATION OF THE ROCK ENTRANCE.
- ⑤ REMOVE MUD AND DEBRIS FROM TIRES AND VEHICLE UNDERCARRIAGE PRIOR TO LEAVING THE SITE.

ROCK CONSTRUCTION ENTRANCE

FEBRUARY 2015



CITY OF LAKE ELMO

STANDARD DRAWING NO.
605
LAKE ELMO



ENGINEERING
SURVEYING
PLANNING

3890 PHEASANT RIDGE DR NE
SUITE 100
BLAINE, MN 55449
TEL 763.489.7900
FAX 763.489.7959
CARLSON-ENGINEERING.COM

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota

Print Name: Brian J. Krystofiak, P.E.
Signature: *Brian J. Krystofiak*
Date: 12/20/24 License #: 25063

Drawn: KRO
Designed: BJK
Date: 12/20/24

Revisions:
1. 1/23/25 per City Comments.
2. 2/11/25 per City Comments.
3. 4/28/25 per City Comments.

M/I HOMES OF MINNEAPOLIS, LLC

5354 Parkdale Drive, #100
St. Louis Park, MN 55416

INWOOD TOWNHOMES

Lake Elmo, Minnesota

DETAILS

14
of
16

#5172-01

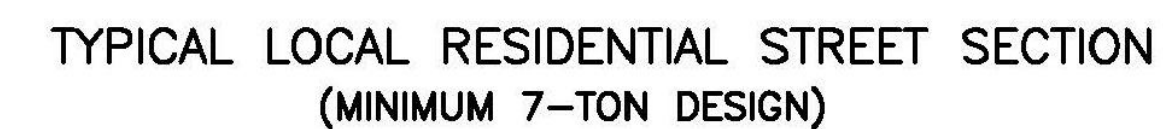


1. SAW AND SEAL CUT LOCATIONS SHALL BE AT 40' +/- SPACING AND AT MANHOLES, OR AS DIRECTED BY THE ENGINEER.
2. SAW AND SEAL CUT SHALL BE STRAIGHT AND MATCH JOINTS IN CURB AND CUTTER.
3. JOINTS TO BE BLOWN CLEAN AND DRIED WITH A HEAT LANCE IMMEDIATELY BEFORE SEALING.
4. CONTRACTOR TO SETUP AND MAINTAIN TRAFFIC CONTROL, INCLUDING FLAGGERS, IN ACCORDANCE WITH MMUTCD, APPENDIX B.
5. BOND BREAKER TAPE IS OPTIONAL.



CITY OF LAKE ELMO

LAKE ELMO



CITY OF LAKE ELMO

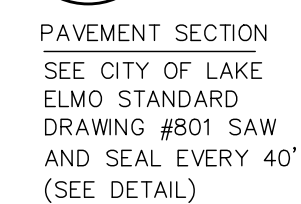
LAKE ELMO



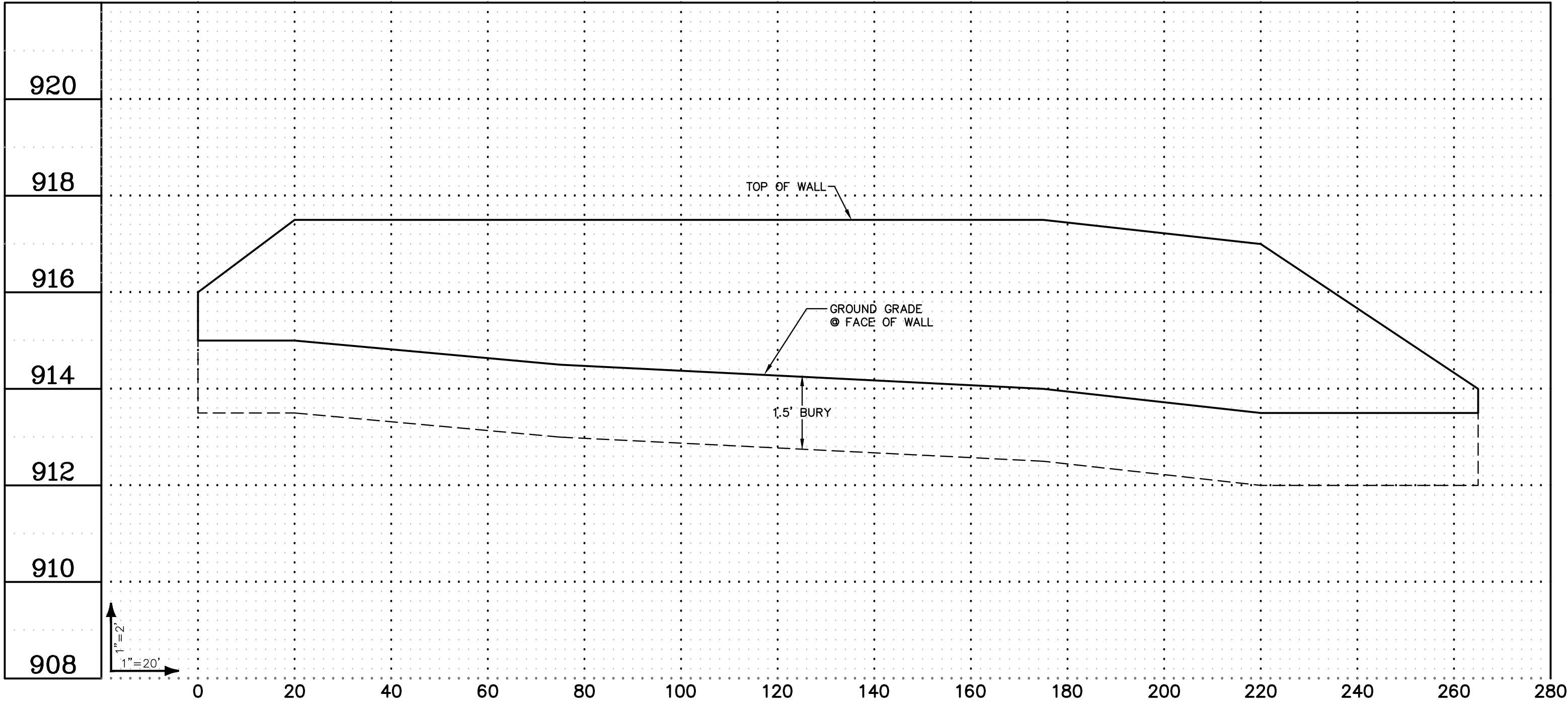
B618 CURB & GUTTER
TC = $\frac{1}{2}$ + 0.09'



PAVEMENT SECTION
SEE CITY OF LAKE
ELMO STANDARD
DRAWING #801 SAW
AND SEAL EVERY 40'
(SEE DETAIL)



MODULAR RETAINING WALL #1
(1156 SF)



LANDSCAPE PLAN NOTES

- EXISTING CONDITIONS. CONTRACTOR SHALL VERIFY EXISTING CONDITIONS PRIOR TO BIDDING AND CONSTRUCTION START. ANY DISCREPANCIES FOUND THAT AFFECT THE WORK SHALL BE REPORTED TO THE OWNER/LANDSCAPE ARCHITECT FOR CLARIFICATION PRIOR TO BIDDING OR APPROVING ANY ADDITIONAL WORK REQUIRED.
- UTILITY LOCATES. CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND VERIFYING ALL PUBLIC AND PRIVATE UTILITIES, INCLUDING IRRIGATION LINES, AT LEAST 48 HOURS BEFORE EXCAVATING OR IN ACCORDANCE WITH STATE LAW. IN MINNESOTA, CALL GOPHER STATE ONE-CALL AT 651-454-0002 FOR FIELD LOCATION OF PUBLIC UNDERGROUND UTILITY LINES. ALL LOCATES AND ITEMS NOTED AS 'FIELD VERIFY' ON THE PLANS SHALL BE VERIFIED BY THE CONTRACTOR AT THEIR EXPENSE.
- PERMITS. CONTRACTOR SHALL VERIFY WITH THE OWNER/LANDSCAPE ARCHITECT THAT THE REQUIRED PERMITS HAVE BEEN OBTAINED PRIOR TO CONSTRUCTION START. CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE CODES, REGULATIONS, AND PERMITS GOVERNING THE WORK.
- EXISTING ITEMS TO REMAIN. CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL EXISTING PAVEMENT, STRUCTURES, UTILITIES, TREES, SITE AMENITIES, ETC. THAT ARE TO REMAIN FROM DAMAGE DURING CONSTRUCTION. CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGE (AT CONTRACTOR'S EXPENSE) TO EXISTING ITEMS TO REMAIN.
- CONSTRUCTION STAGING AND ACCESS. CONTRACTOR SHALL VERIFY THE LOCATION FOR CONSTRUCTION STAGING AND SITE ACCESS WITH THE OWNER/LANDSCAPE ARCHITECT PRIOR TO CONSTRUCTION START. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING PEDESTRIAN AND VEHICLE ACCESS WITHIN AND ADJACENT TO THE SITE THROUGHOUT THE CONSTRUCTION PERIOD UNLESS OTHERWISE NOTED. LONG-TERM STORAGE OF MATERIALS OR SUPPLIES ON-SITE WILL NOT BE PERMITTED. ALL WASTE AND/OR UNUSED MATERIAL SHALL BE PROMPTLY REMOVED FROM THE SITE.
- SITE REMOVALS. REFER TO CIVIL PLANS FOR GENERAL SITE REMOVALS. ALL ITEMS NOTED TO BE REMOVED SHALL BE COMPLETED BY THE CONTRACTOR AND IMMEDIATELY DISPOSED OF OFF-SITE, IN ACCORDANCE WITH LOCAL REGULATIONS, UNLESS OTHERWISE NOTED. COORDINATE WITH OWNER/LANDSCAPE ARCHITECT FOR ANY ITEMS NOTED AS 'REMOVED BY OTHERS' OR 'REMOVE AND SALVAGE.'
- UTILITY COORDINATION. REFER TO CIVIL PLAN SHEETS FOR INFORMATION RELATED TO PROPOSED UTILITIES. COORDINATE WITH ELECTRICAL CONTRACTORS FOR SITE ELECTRICAL WORK AND SITE LIGHTING.
- CONSTRUCTION STAKING. UNLESS OTHERWISE NOTED, CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING CONTROL POINTS AND ALL CONSTRUCTION FIELD STAKING DURING THE COURSE OF THE PROJECT.
- DIMENSIONS. DIMENSIONS TAKE PRECEDENCE OVER SCALE. DIMENSIONS ARE TO FACE OF CURB, EDGE OF PAVEMENT/WALKWAY, OR OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
- PLAN QUANTITIES. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL MATERIAL QUANTITIES PER PLAN. MATERIAL SCHEDULES ARE PROVIDED SOLELY FOR CONTRACTOR'S CONVENIENCE.
- REFERENCE SPECIFICATIONS. THE FOLLOWING SPECIFICATIONS SHALL GOVERN THIS PROJECT ACCORDING TO THE FOLLOWING ORDER:
 - ADDENDA, WITH THOSE OF A LATER DATE HAVING PRECEDENCE OVER THOSE OF AN EARLIER DATE.
 - LANDSCAPE SPECIFICATIONS.
 - PLAN DRAWINGS.
 - PLANT / MATERIAL SCHEDULES.
 - CITY STANDARD SPECIFICATIONS AND DETAILS.
 - MNDOT STANDARD SPECIFICATIONS FOR CONSTRUCTION, LATEST EDITION.

CITY OF LAKE ELMO LANDSCAPE REQUIREMENTS (SEC. 105.12.480)

- LANDSCAPING OF SETBACK AREAS.
 - A MINIMUM OF ONE TREE PER LOT OR ONE TREE FOR EVERY 50 FEET OF STREET FOOTAGE, LAKE SHORE OR STREAM FRONTAGE, OR FRACTION THEREOF SHALL BE PLANTED AT THE TIME OF DEVELOPMENT. THE TOTAL TREE REQUIREMENT WILL BE WHICHEVER QUANTITY IS GREATER.

APARTMENTS	TREES REQUIRED:	17 TREES (1 LOT, OR 837 LF FRONTAGE / 50 = 17)
	TREES PROVIDED:	17 TREES
TOWNHOMES	TREES REQUIRED:	148 TREES (148 LOTS, OR 4,402 LF FRONTAGE / 50 = 89)
	TREES PROVIDED:	148 TREES
 - IN ADDITION TO THE REQUIREMENTS OF 1.1 ABOVE, A MINIMUM OF FIVE (5) TREES SHALL BE PLANTED FOR EVERY ONE (1) ACRE OF LAND THAT IS DISTURBED BY DEVELOPMENT ACTIVITY. SUCH TREES MAY BE USED FOR PARKING LOT LANDSCAPING OR SCREENING AS SPECIFIED BELOW.

APARTMENTS	TREES REQUIRED:	23 TREES (4.54 AC DISTURBANCE x 5)
	TREES PROVIDED:	45 TREES
TOWNHOMES	TREES REQUIRED:	68 TREES (13.42 AC DISTURBANCE x 5)
	TREES PROVIDED:	46 TREES
 - INTERIOR PARKING LOT LANDSCAPING (APARTMENTS).
 - AT LEAST FIVE (5) PERCENT OF THE INTERIOR AREA OF PARKING LOTS WITH MORE THAN THIRTY (30) SPACES SHALL BE DEVOTED TO LANDSCAPE PLANTING AREAS. AREAS MAY CONSIST OF ISLANDS OR CORNER PLANTING BEDS.

AREA REQUIRED:	1,565 SF (31,283 SF PARKING AREA x .05)
AREA PROVIDED:	2,632 SF
 - SHADE TREES SHALL BE PROVIDED WITHIN THE INTERIOR OF PARKING LOTS (IN ISLANDS OR CORNER PLANTING BEDS):

REQUIRED TREES:	10 TREES (98 PARKING SPACES / 1 TREE PER 10 SPACES)
PROVIDED TREES:	10 TREES
- | | | | |
|--|-----------------------|-----------|------------------------------|
| APARTMENTS | TOTAL TREES REQUIRED: | 40 TREES | - 31 DECIDUOUS TREES |
| | TOTAL TREES PROVIDED: | 62 TREES | - 25 CONIFEROUS TREES |
| | | | - 6 ORNAMENTAL TREES |
| TOWNHOMES | TOTAL TREES REQUIRED: | 216 TREES | - 87 DECIDUOUS TREES |
| | TOTAL TREES PROVIDED: | 194 TREES | - 75 CONIFEROUS TREES |
| | | | - 32 ORNAMENTAL TREES |
| TOTAL PROJECT (APARTMENTS + TOWNHOMES) | TOTAL TREES REQUIRED: | 256 TREES | - 118 DECIDUOUS TREES (46%) |
| | TOTAL TREES PROVIDED: | 256 TREES | - 100 CONIFEROUS TREES (39%) |
| | | | - 38 ORNAMENTAL TREES (15%) |

NOTE: EXCLUDES 84 EXISTING SIGNIFICANT TREES TO REMAIN.

PRELIMINARY PLANS

PRELIMINARY LANDSCAPE PLANS FOR CITY REVIEW ONLY. FINAL PLANS TO INCLUDE DETAIL PLANS FOR PLANTING BEDS, LANDSCAPE FEATURES, SITE FURNISHINGS AND TYPICAL FOUNDATION PLANTING DETAILS FOR CONSTRUCTION. SEE SITE PLAN RENDERING FOR ADDITIONAL INFORMATION (PROVIDED SEPARATELY).

STANDARD PLAN NOTES

LANDSCAPE PLANS

- Contractor must contact Gopher State One Call 811 (www.gopherstateonecall.org) prior to tree staking and planting operations to verify underground utilities. Where private utilities exist on-site the Contractor is required to have those located as well.
- Plant materials shall meet American Standard for Nursery Stock: ANSI Z60.1, latest edition.
- No plant substitutions shall be made without the prior written authorization from the City.
- All tree locations to be field staked prior to installation. Contractor to coordinate field review of proposed tree locations with the City and Project Landscape Architect prior to any tree installation.
- All plants shall be planted immediately upon arrival to project site. No plant material is to be left overnight on the project site without being installed unless written approval by City.
- All trees, shrubs, perennials, turf lawn and native seeding to have a two-year warranty beginning upon written acceptance by the City. Defective plants as determined by the City shall be replaced within 30 days of notice during the growing season, and replacement materials shall receive the same two year warranty until plants are successfully established.
- Contractor to protect and maintain all plantings and plant beds, including protection from wildlife, weeding, re-mulching, fertilization, irrigation and all other typical forms of horticultural care until the end of the warranty period as determined and approved by City.
- All plants installed and maintained on City property shall be in accordance with City of Lake Elmo Bee-Safe Resolution No. 2015-13.
- An irrigation system or other water supply adequate to support the specified plant materials shall be provided.

APRIL 2017

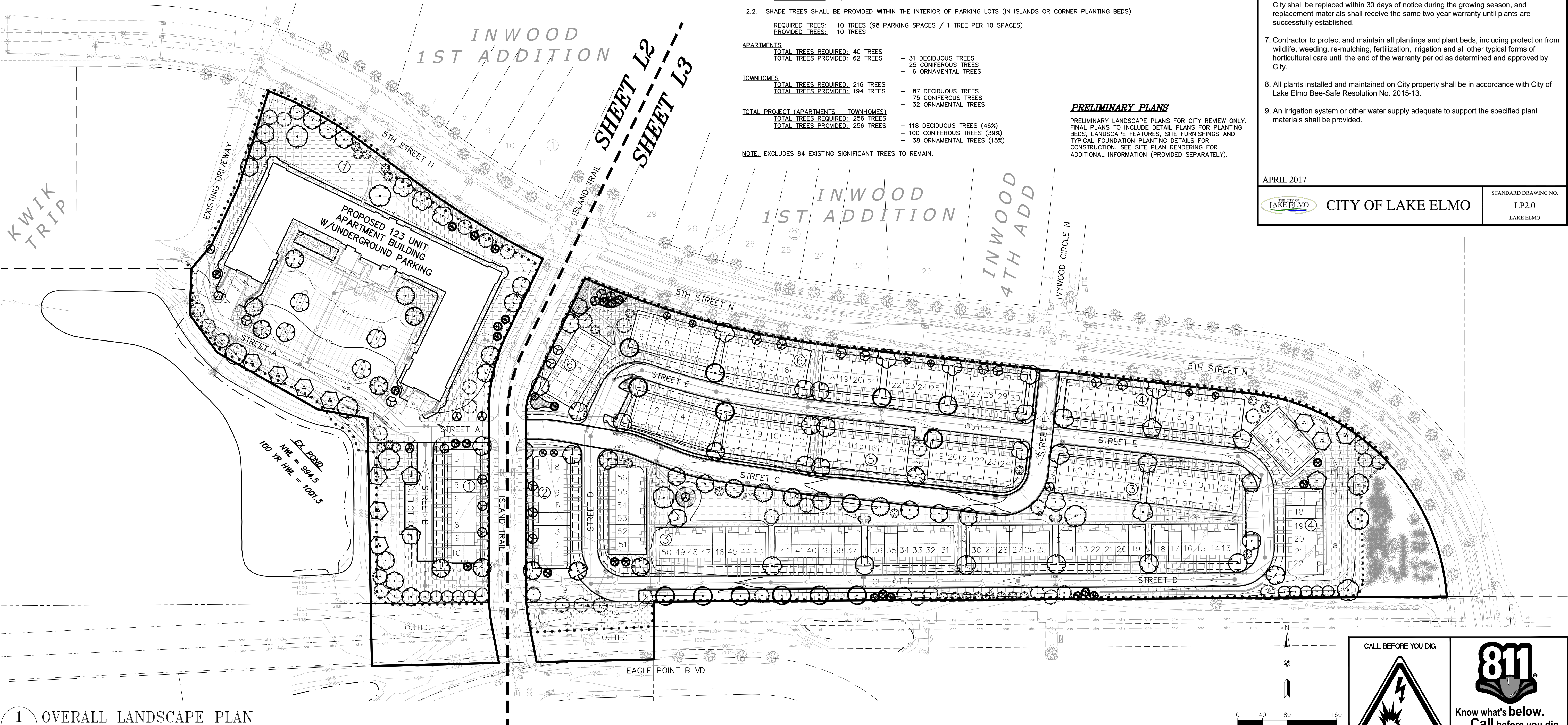


CITY OF LAKE ELMO

STANDARD DRAWING NO.

LP2.0

LAKE ELMO



1 OVERALL LANDSCAPE PLAN
L1



ENGINEERING
SURVEYING
PLANNING

3890 PHEASANT RIDGE DR NE
SUITE 100
BLAINE, MN 55449
TEL 763.489.7900
FAX 763.489.7959
CARLSON-ENGINEERING.COM

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the State of Minnesota

Print Name: Ryan J. Ruttinger, RLA

Signature:

Date: 12/20/24 License #: 56346

Drawn: RJR

Designed: RJR

Date: 12/20/24

Revisions:
1. 01/23/25 - Per City Comments.
2. 02/11/25 - Per City Comments.
3. 04/28/25 - Per City Comments.

M/I HOMES OF MINNEAPOLIS, LLC

5354 Parkdale Drive, #100
St. Louis Park, MN 55416

INWOOD TOWNHOMES

Lake Elmo, Minnesota

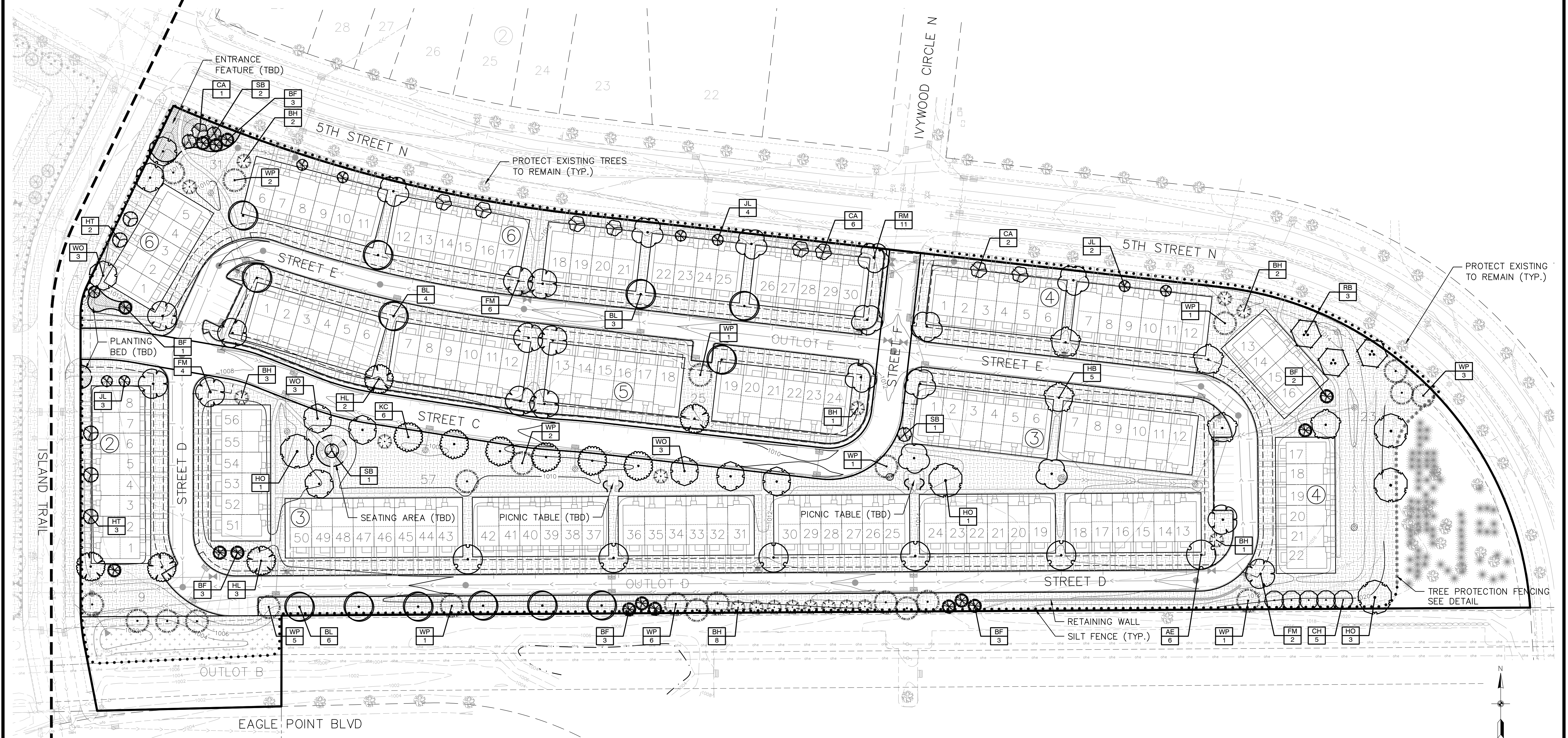
PRELIMINARY
LANDSCAPE PLAN

L1

of

4

SHEET L2



1 LANDSCAPE PLAN - EAST

L3



CARLSON
ENGINEERING

ENGINEERING
SURVEYING
PLANNING

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SUITE 100
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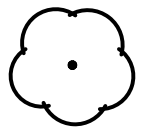
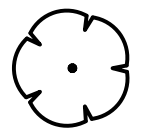
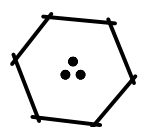
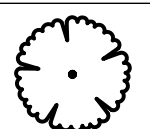
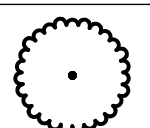
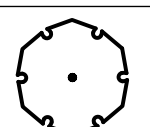
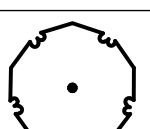
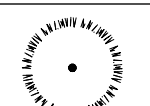
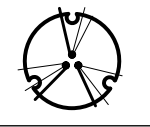
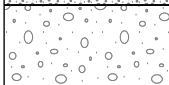
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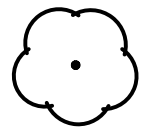
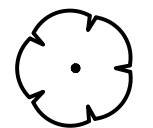
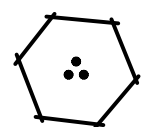
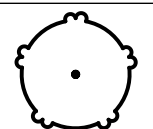
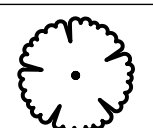
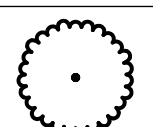
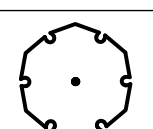
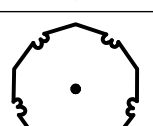
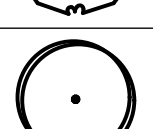
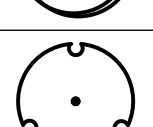
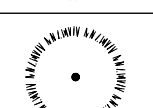
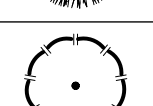
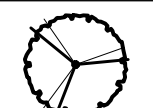
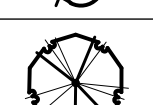
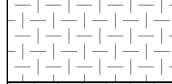
INWOOD TOWNHOMES
Lake Elmo, Minnesota

**PRELIMINARY
LANDSCAPE PLAN**

L3
of
4

#5172-01

PLANT SCHEDULE APARTMENTS						
SYMBOL	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
TREES						
	RM	3	Acer rubrum `Northwood`	Northwoods Red Maple	2.5" Cal.	B&B
	FM	4	Acer x freemanii `Sienna`	Sienna Glen Maple	2.5" Cal.	B&B
	RB	4	Betula nigra Clump Form, 2.5" Cal Equivalent	River Birch Multi-Trunk	10` Ht.	B&B
	HL	4	Gleditsia triacanthos inermis `Harve`	Northern Acclaim Thornless Honey Locust	2.5" Cal.	B&B
	KC	2	Gymnocladus dioica `Espresso`	Kentucky Coffeetree	2.5" Cal.	B&B
	WO	10	Quercus bicolor	Swamp White Oak	2.5" Cal.	B&B
	HO	4	Quercus x macdanielli `Clemons` TM	Heritage Oak	2.5" Cal.	B&B
CONIFEROUS TREES						
	BF	8	Abies balsamea	Balsam Fir	6` Ht.	B&B
	BH	5	Picea glauca densata	Black Hills Spruce	6` Ht.	B&B
	WP	12	Pinus strobus	White Pine	6` Ht.	B&B
ORNAMENTAL TREES						
	SB	4	Amelanchier x grandiflora `Autumn Brilliance` Clump Form, 2" Cal Equivalent	Autumn Brilliance Serviceberry	8` Ht.	B&B
	CA	2	Malus x `Prairiefire` Red Flowers, Clump Form, 2" Cal Equivalent	Prairiefire Crabapple	7` Ht.	B&B
GROUND COVERS						
	DG	1,430 sf	Decomposed Granite Compacted, Non-Woven Geotextile Incidental	Decomposed Granite	4" Depth	
	ROCK	12,322 sf	Rock Mulch Non-Woven Geotextile Incidental	1.5" Trap Rock Mulch	4" Depth	
	SOD	78,432 sf	Turf Sod Bluegrass	Kentucky Bluegrass	sod	
	TII	5,960 sf	Type II - Stormwater Seed Mix Refer to notes for acceptable seeding methods Seeding Rate 52.0 lb/ac	MnDOT Seed Mix 33-261	seed	

PLANT SCHEDULE TOWNHOMES						
SYMBOL	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER
TREES						
	RM	11	Acer rubrum `Northwood`	Northwoods Red Maple	2.5" Cal.	B&B
	FM	13	Acer x freemanii `Sienna`	Sienna Glen Maple	2.5" Cal.	B&B
	RB	6	Betula nigra Clump Form, 2.5" Cal Equivalent	River Birch Multi-Trunk	10` Ht.	B&B
	HB	7	Celtis occidentalis	Common Hackberry	2.5" Cal.	B&B
	HL	7	Gleditsia triacanthos inermis `Harve`	Northern Acclaim Thornless Honey Locust	2.5" Cal.	B&B
	KC	6	Gymnocladus dioica `Espresso`	Kentucky Coffeetree	2.5" Cal.	B&B
	WO	12	Quercus bicolor	Swamp White Oak	2.5" Cal.	B&B
	HO	6	Quercus x macdanielli `Clemons` TM	Heritage Oak	2.5" Cal.	B&B
	BL	13	Tilia americana `Boulevard`	Boulevard Linden	2.5" Cal.	B&B
	AE	6	Ulmus americana `Princeton`	American Elm	2.5" Cal.	B&B
CONIFEROUS TREES						
	BF	18	Abies balsamea	Balsam Fir	6` Ht.	B&B
	BH	20	Picea glauca densata	Black Hills Spruce	6` Ht.	B&B
	WP	27	Pinus strobus	White Pine	6` Ht.	B&B
	CH	10	Tsuga canadensis	Canadian Hemlock	6` Ht.	B&B
ORNAMENTAL TREES						
	SB	6	Amelanchier x grandiflora `Autumn Brilliance` Clump Form, 2" Cal Equivalent	Autumn Brilliance Serviceberry	8` Ht.	B&B
	HT	8	Crataegus crus-galli `Inermis`	Thornless Hawthorn	2" Cal.	B&B
	CA	9	Malus x `Prairiefire` Red Flowers, Clump Form, 2" Cal Equivalent	Prairiefire Crabapple	7` Ht.	B&B
	JL	9	Syringa reticulata `Ivory Silk` White Flowers	Ivory Silk Japanese Tree Lilac	2" Cal.	B&B
GROUND COVERS						
	SOD	253,791 sf	Turf Sod Bluegrass	Kentucky Bluegrass	sod	
	TI	6,953 sf	Type I - Turf Seed Mix Refer to notes for acceptable seeding methods Seeding Rate 180 lb/ac	MnDOT Seed Mix 25-151	seed	

LANDSCAPE SPECIFICATIONS

- TREE PROTECTION.** ALL TREES NOT SPECIFICALLY NOTED OR MARKED ON SITE FOR REMOVAL SHALL REMAIN PROTECTED AND UNDISTURBED DURING CONSTRUCTION. TREE PROTECTION SHALL EXTEND TO THE DRIP LINE, WITHIN WHICH NO CONSTRUCTION ACTIVITY, MATERIAL STORAGE, OR VEHICLE PARKING SHALL BE PERMITTED. TREE PROTECTION FENCING SHALL BE ERECTED PRIOR TO CONSTRUCTION START PER PLANS OR AS DIRECTED BY OWNER/LANDSCAPE ARCHITECT AND SHALL CONSIST OF 4' TALL HEAVY DUTY ORANGE CONSTRUCTION FENCING WITH 6" STEEL FENCE POSTS SPACED 6' O.C. MAX.
- EROSION CONTROL.** REFER TO CIVIL PLAN SHEETS FOR STORMWATER POLLUTION PREVENTION PLAN (SWPPP). AND TEMPORARY AND PERMANENT STORMWATER BMPS, INCLUDING SILT FENCE, BIO-ROLLS, INLET PROTECTION, EROSION CONTROL BLANKETING, DUST CONTROL, SWEEPING AND ROCK CONSTRUCTION ENTRANCE. ALL DISTURBED AREAS SHALL RECEIVE PERMANENT STABILIZATION IN ACCORDANCE WITH THE LANDSCAPE PLAN WITHIN 7 DAYS AFTER CONSTRUCTION ACTIVITY IN THE DISTURBED AREA HAS CEASED. IN THE EVENT PERMANENT STABILIZATION CANNOT BE IMPLEMENTED WITHIN 7 DAYS, TEMPORARY STABILIZATION BMPS MUST BE IMPLEMENTED WITHIN 7 DAYS USING RAPID STABILIZATION METHOD 3.
- CLEARING AND GRUBBING.** CONTRACTOR SHALL BE RESPONSIBLE FOR CLEARING AND GRUBBING ALL AREAS INDICATED AS BEING DISTURBED OR OTHERWISE SHOWN ON PLANS. CLEARING AND GRUBBING SHALL INCLUDE REMOVAL AND DISPOSAL OF ALL TREES, STUMPS, BRUSH, GRASS, ROOTS AND OTHER ORGANIC MATERIAL AT AN APPROVED OFF-SITE DISPOSAL LOCATION.
- SOIL PREPARATION.** REFER TO GEOTECHNICAL REPORT FOR ANY REQUIRED SOIL CORRECTIONS, AMENDMENTS OR ADDITIONAL INFORMATION (IF APPLICABLE). EXISTING TOPSOIL SHALL BE STRIPPED FROM ALL DISTURBED AREAS AND STOCKPILED IN AN APPROVED LOCATION FOR RE-SPREAD. ALL AREAS WHERE SOIL HAS BEEN COMPACTED BY CONSTRUCTION ACTIVITY AND THAT ARE INDICATED TO BE SODDED, SEEDED OR PLANTING BED SHALL BE DE-COMPACTED TO A MINIMUM DEPTH OF 12 INCHES BY SOIL RIPPING, TILLING OR OTHER APPROVED SOIL LOOSENING METHOD PRIOR TO INSTALLATION OF ANY IRRIGATION, SEEDING, PLANTING, SODDING, ETC.
- TOPSOIL MATERIAL.** ALL EXISTING, AMENDED OR IMPORTED TOPSOIL SHALL MEET THE REQUIREMENTS OF MNDOT TOPSOIL TYPE A. A MINIMUM 4 INCH DEPTH OF TOPSOIL SHALL BE PLACED ON ALL AREAS TO BE SODDED OR SEEDED. A MINIMUM 12 INCH DEPTH OF TOPSOIL SHALL BE PLACED WITHIN ALL PLANTING BED AREAS. ALL TOPSOIL SHALL BE FINE GRADED, RAKED AND DRAGGED TO PROVIDE A SMOOTH, UNIFORM SURFACE. TOPSOIL GRADES SHALL BE WITHIN .1 FEET OF INDICATED FINISHED GRADE AND SHALL BE TRUE TO GRADIENTS SHOWN ON PLANS. CONFIRMATION OF TOPSOIL DEPTH AND QUALITY SHALL BE SUBMITTED AS WRITTEN CONFIRMATION FROM A LICENSED PARTY BEFORE A FULL CERTIFICATE OF OCCUPANCY IS ISSUED.
- SEEDING AND TURF ESTABLISHMENT.** CONTRACTOR SHALL OBTAIN OWNER/LANDSCAPE ARCHITECT'S APPROVAL OF FINAL GRADES AND TOPSOIL PREP PRIOR TO SEEDING. APPLY 10-10-10 TYPE I STARTER FERTILIZER AT A RATE OF 200 LBS PER ACRE PRIOR TO SEEDING. SEEDS SHALL BE SOWED AT THE INDICATED RATE, VIA BROADCAST SPREADER, DROP SEEDER OR DRILL SEEDER. FOLLOWING SEED APPLICATION, INSTALL CATEGORY 20 - STRAW EROSION CONTROL BLANKET WITH NATURAL NETTING ON ALL SLOPES GREATER THAN 4:1. IN ALL OTHER AREAS, APPLY DISC-ANCHORED TYPE 3 MULCH (WGA CERTIFIED WEED FREE GRAIN STRAW) AT A RATE OF 2 TONS PER ACRE. SOIL SHALL BE KEPT MOIST DURING ESTABLISHMENT WITH ADDITIONAL RE-SEEDING AS NECESSARY TO ACHIEVE A HEALTHY, UNIFORM STAND OF GRASS, FREE OF WEEDS AND WITH COVERAGE EXCEEDING 75% IN ANY 10'x10' AREA PRIOR TO FINAL ACCEPTANCE.
- SODDING.** CONTRACTOR SHALL OBTAIN OWNER/LANDSCAPE ARCHITECT'S APPROVAL OF FINAL GRADES AND TOPSOIL PREP PRIOR TO SODDING. APPLY 10-10-10 TYPE I STARTER FERTILIZER AT A RATE OF 200 LBS PER ACRE PRIOR TO SODDING AND ROLL TOPSOIL TO CREATE A UNIFORM SURFACE FOR LAYING SOD. SOD SHALL NOT BE CUT MORE THAN 24-HOURS IN ADVANCE OF INSTALLATION. CONTRACTOR SHALL KEEP SOD MOIST FOR A MINIMUM OF 30 DAYS AND SHALL BE RESPONSIBLE FOR MAINTAINING THE SOD UNTIL FINAL ACCEPTANCE.
- PLANT MATERIAL.** ALL PLANTING STOCK SHALL CONFORM TO THE "AMERICAN STANDARD FOR NURSERY STOCK," ANSI-Z60, LATEST EDITION, OF THE AMERICAN ASSOCIATION OF NURSERYMEN, INC. AND SHALL CONSTITUTE MINIMUM QUALITY REQUIREMENTS FOR PLANT MATERIALS. OWNER/LANDSCAPE ARCHITECT RESERVE THE RIGHT TO REJECT ANY PLANTS WHICH ARE DEEMED UNSATISFACTORY BEFORE, DURING, OR AFTER INSTALLATION. NO SUBSTITUTION OF PLANT MATERIAL SHALL BE ACCEPTED UNLESS APPROVED IN WRITING BY THE OWNER/LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- PLANT MATERIAL SUBSTITUTIONS.** ALL REQUESTS FOR PLANT SUBSTITUTIONS SHALL BE MADE IN WRITING TO THE OWNER/LANDSCAPE ARCHITECT AND MUST BE APPROVED BY THE CITY.
- PLANT INSTALLATION AND ESTABLISHMENT.** REFER TO STANDARD PLANTING DETAILS. CONTRACTOR SHALL STAKE TREE LOCATIONS FOR APPROVAL BY OWNER/LANDSCAPE ARCHITECT PRIOR TO PLANTING. ANY PLANT MATERIAL WHICH DIES, TURNS BROWN, OR DEFOOLIATES (PRIOR TO TOTAL ACCEPTANCE OF THE WORK) SHALL BE PROMPTLY REMOVED FROM THE SITE AND REPLACED WITH MATERIAL OF THE SAME SPECIES, QUANTITY, AND SIZE.
- MULCH MATERIAL.** DOUBLE SHREDDED HARDWOOD MULCH OR ROCK MULCH AS INDICATED ON PLANS. ALL MULCH SHALL BE CLEAN AND FREE OF NOXIOUS WEEDS, SOIL, OR OTHER DELETERIOUS MATERIAL, AND SHALL BE INSTALLED OVER A NON-WOVEN GEOTEXTILE FABRIC (INCIDENTAL) OR OTHER APPROVED WEED BARRIER TO A MINIMUM SETTLED DEPTH OF 4". MULCH SHALL BE HELD BACK FROM PLANT STEMS/TRUNKS A MINIMUM OF 3". WOOD MULCH SHALL BE PLACED AROUND INDIVIDUAL TREES TO A 4' MINIMUM DIAMETER. MULCH SHALL BE INSTALLED WITHIN 48-HOURS OF PLANT INSTALLATION.
- LANDSCAPE EDGING.** INSTALL LANDSCAPE EDGING BETWEEN ALL MULCH AREAS AND TURF. EDGING SHALL BE COMMERCIAL GRADE BLACK POLYETHYLENE OR VINYL EDGING, 0.1 INCH THICK BY 5 INCHES DEEP, V-LIPPED BOTTOM, HORIZONTALLY GROOVED, 1-INCH ROUND TOP, EXTRUDED IN STANDARD LENGTHS, WITH 9-INCH STEEL ANGLE STAKES.
- IRRIGATION.** DESIGN, FURNISH AND INSTALL A COMPLETE UNDERGROUND IRRIGATION SYSTEM FROM APPROVED POINT(S)-OF-CONNECTION WITHIN THE SITE COVERING ALL TURF AND PLANTING AREAS AS SHOWN ON THE LANDSCAPE PLAN. INCLUDES FLOW/PRESSURE TESTING, PLANS WITH DESIGN CALCULATIONS, AS-BUILT DRAWINGS, LABOR, MATERIALS, EQUIPMENT, AND SERVICES FOR THE TESTING, ADJUSTING, RETESTING AND READJUSTING AS REQUIRED TO PLACE THE SYSTEM IN AN APPROVED OPERATING CONDITION. THE IRRIGATION SYSTEM SHALL INCLUDE THE DESIGN AND INSTALLATION OF THE FOLLOWING: PIPING, METER AND BACKFLOW ASSEMBLIES, SPRINKLER HEADS, CABINETS, VALVES AND VALVE BOXES, CONTROLLERS, CONTROL WIRING, FITTINGS, ELECTRICAL CONNECTIONS, QUICK-COUPERS, ALL OTHER NECESSARY ACCESSORIES, SYSTEM MANUALS, 1-YEAR MAINTENANCE PERIOD INCLUDING 1 FALL WINTERIZATION AND 1 SPRING START-UP. IRRIGATION PLANS TO BE PREPARED BY A QUALIFIED IRRIGATION DESIGNER AND SUBMITTED TO OWNER/LANDSCAPE ARCHITECT FOR APPROVAL.
- MAINTENANCE.** MAINTENANCE SHALL BEGIN IMMEDIATELY AFTER EACH PORTION OF THE WORK IS IN PLACE. PLANT MATERIAL SHALL BE PROTECTED AND MAINTAINED UNTIL THE INSTALLATION OF THE PLANTS IS COMPLETE. INSPECTION HAS BEEN MADE, AND PLANTINGS ARE ACCEPTED EXCLUSIVE OF THE GUARANTEE. MAINTENANCE SHALL INCLUDE MOWING, TRIMMING, WATERING, FERTILIZING, WEED AND PESTICIDE CONTROL, MULCHING, REMOVAL OF DEAD MATERIALS, RE-SETTING PLANTS TO PROPER GRADE AND KEEPING PLANTS IN A PLUMB POSITION. AFTER ACCEPTANCE, THE OWNER SHALL ASSUME MAINTENANCE RESPONSIBILITIES. HOWEVER, THE CONTRACTOR SHALL RETAIN RESPONSIBILITY FOR ALL PLANT MATERIAL THROUGH THE COMPLETION OF THE WARRANTY PERIOD.
- WATERING.** UPON ESTABLISHMENT OF SEED AND INSTALLATION OF PLANTS, CONTRACTOR SHALL MAINTAIN A WATERING SCHEDULE WHICH WILL THOROUGHLY WATER ALL PLANTS AND TURF AREAS A MINIMUM OF ONCE A WEEK. MORE FREQUENT WATERING MAY BE REQUIRED DURING PERIODS OF HOT, DRY WEATHER. CONTRACTOR SHALL MAKE THE NECESSARY ARRANGEMENTS FOR WATER. IN THE ABSENCE OF PERMANENT IRRIGATION, TEMPORARY IRRIGATION, TREE WATERING BAGS, OR HAND-WATERING ARE ACCEPTABLE.
- FINAL ACCEPTANCE.** UPON SUBSTANTIAL COMPLETION OF THE WORK, CONTRACTOR SHALL REQUEST FINAL ACCEPTANCE OF THE WORK IN WRITING BY THE OWNER/LANDSCAPE ARCHITECT. IF ANY WORK IS FOUND TO BE INCOMPLETE OR UNSATISFACTORY IN THE OPINION OF THE OWNER/LANDSCAPE ARCHITECT, A WRITTEN PUNCH LIST WILL BE PREPARED LISTING ALL ITEMS THAT REQUIRE COMPLETING OR CORRECTING BEFORE FINAL ACCEPTANCE.
- WARRANTY.** ALL PLANTS, MATERIALS AND WORKMANSHIP SHALL BE GUARANTEED FOR TWO (2) YEARS FROM THE DATE OF FINAL ACCEPTANCE, UNLESS OTHERWISE SPECIFIED. THE GUARANTEE SHALL COVER THE FULL COST OF REPLACEMENT INCLUDING LABOR AND MATERIAL.



**3890 PHEASANT RIDGE DR NE
SUITE 100
BLAINE, MN 55449
TEL 763.489.7900
FAX 763.489.7959
CARLSON-ENGINEERING.COM**

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the State of Minnesota

Print Name: Ryan J. Ruttger, RLA

Signature: 

Date: 12/20/24 License #: 56346

Drawn: RJR

Designed: RJR

Date: 12/20/24

Revisions:
1. 01/23/25 - Per City Comments.
2. 02/11/25 - Per City Comments.
3. 04/28/25 - Per City Comments.

M/I HOMES OF MINNEAPOLIS, LLC

5354 Parkdale Drive, #100
St. Louis Park, MN 55416

INWOOD TOWNHOMES

Lake Elmo, Minnesota

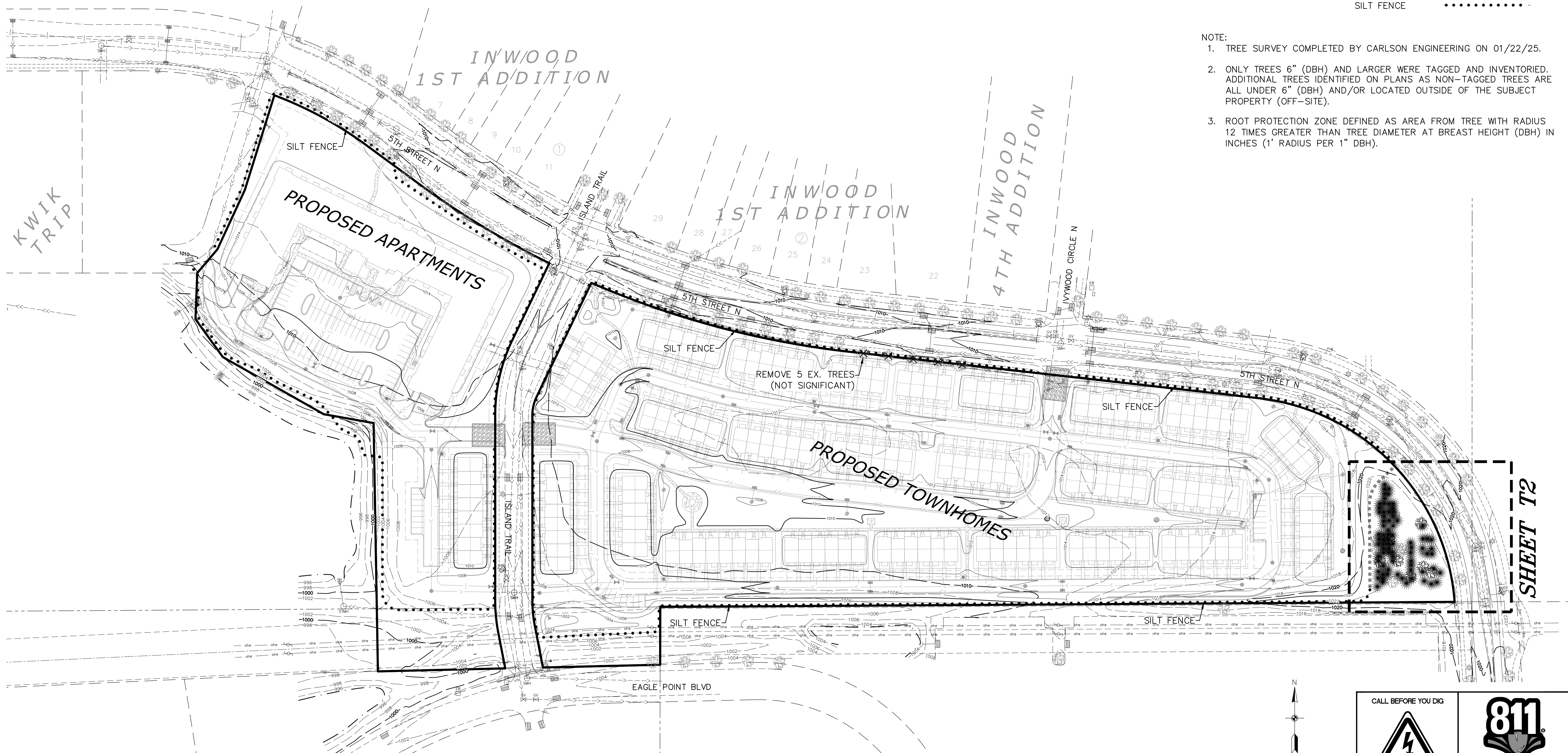
**PRELIMINARY
LANDSCAPE PLAN**

L4
of
4

LEGEND — EXISTING TREES

	CONIFEROUS	DECIDUOUS
TAGGED TREE		
NON-TAGGED TREE		
TREE REMOVAL		
ROOT PROTECTION ZONE		
TREE PROTECTION FENCE		
SILT FENCE		

- NOTE:
- TREE SURVEY COMPLETED BY CARLSON ENGINEERING ON 01/22/25.
 - ONLY TREES 6" (DBH) AND LARGER WERE TAGGED AND INVENTORIED. ADDITIONAL TREES IDENTIFIED ON PLANS AS NON-TAGGED TREES ARE ALL UNDER 6" (DBH) AND/OR LOCATED OUTSIDE OF THE SUBJECT PROPERTY (OFF-SITE).
 - ROOT PROTECTION ZONE DEFINED AS AREA FROM TREE WITH RADIUS 12 TIMES GREATER THAN TREE DIAMETER AT BREAST HEIGHT (DBH) IN INCHES (1' RADIUS PER 1" DBH).

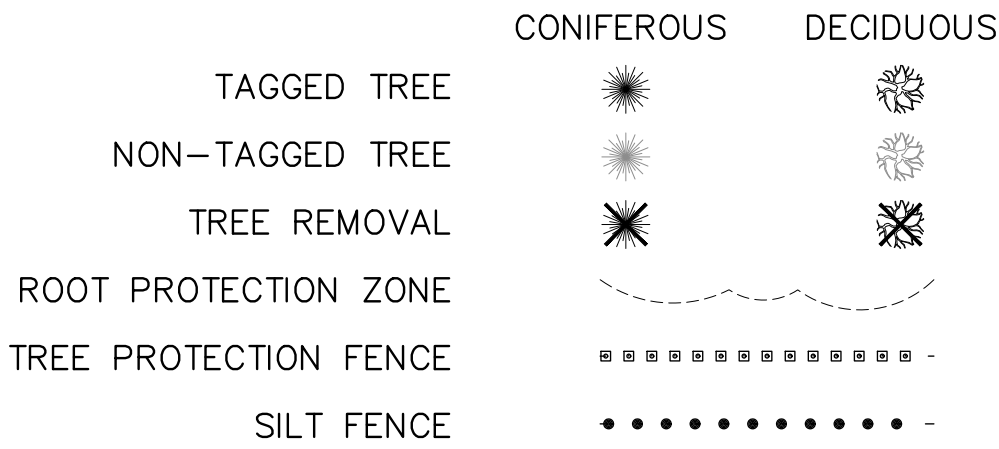


1 OVERALL TREE PRESERVATION PLAN
T1

SHEET T2

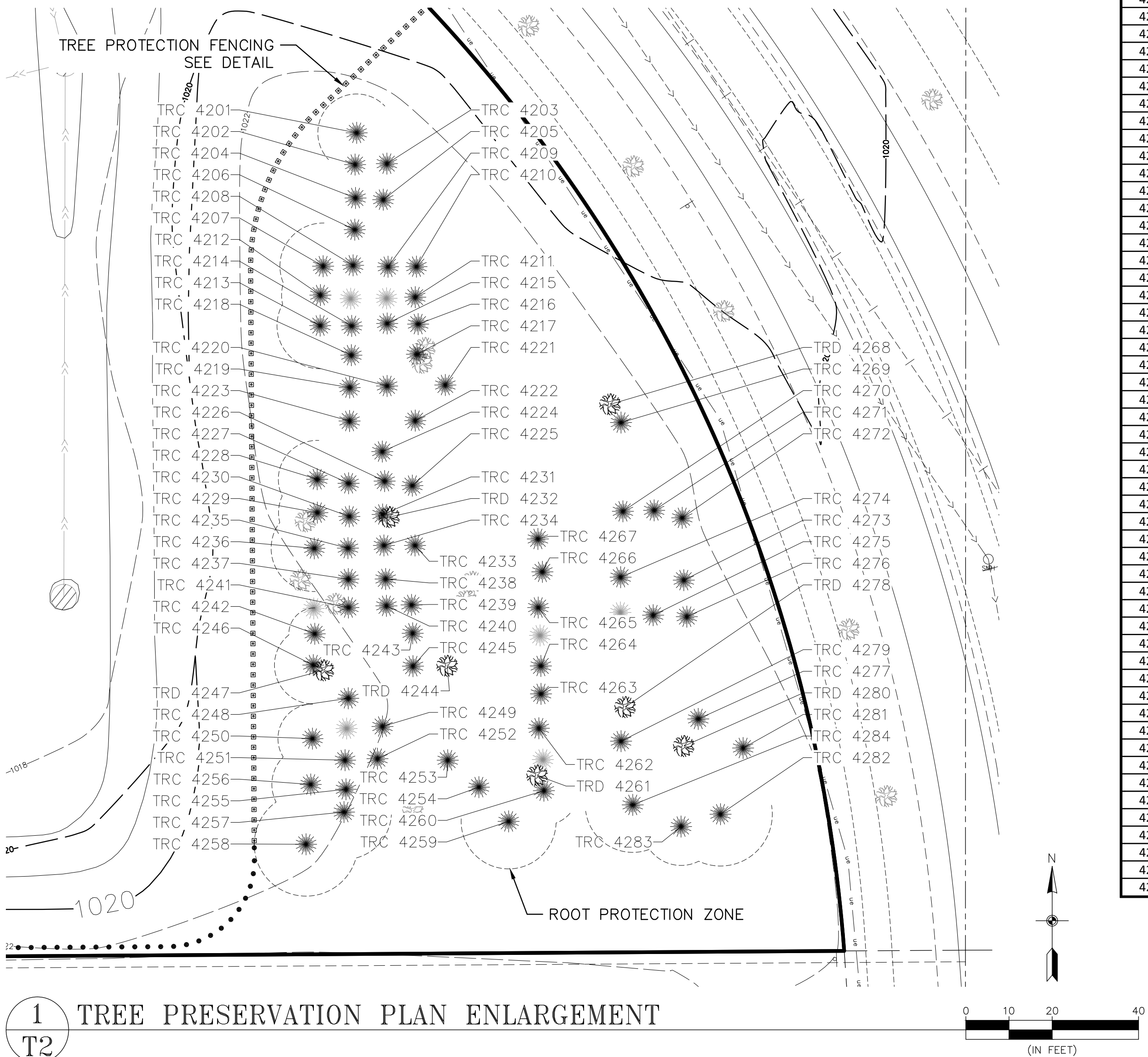
CARLSON ENGINEERING ENGINEERING SURVEYING PLANNING	3890 PHEASANT RIDGE DR NE SUITE 100 BLAINE, MN 55449 TEL 763.489.7900 FAX 763.489.7959 CARLSON-ENGINEERING.COM	I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the State of Minnesota Signature: Date: 12/20/24 License #: 56346	Print Name: Ryan J. Ruttiger, RLA Designed: RJR Date: 12/20/24	Revisions: 1. 01/23/25 - Per City Comments. 2. 02/11/25 - Per City Comments. 3. 04/28/25 - Per City Comments.	M/I HOMES OF MINNEAPOLIS, LLC 5354 Parkdale Drive, #100 St. Louis Park, MN 55416	INWOOD TOWNHOMES Lake Elmo, Minnesota	TREE PRESERVATION PLAN	T1 of 2
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LEGEND – EXISTING TREES



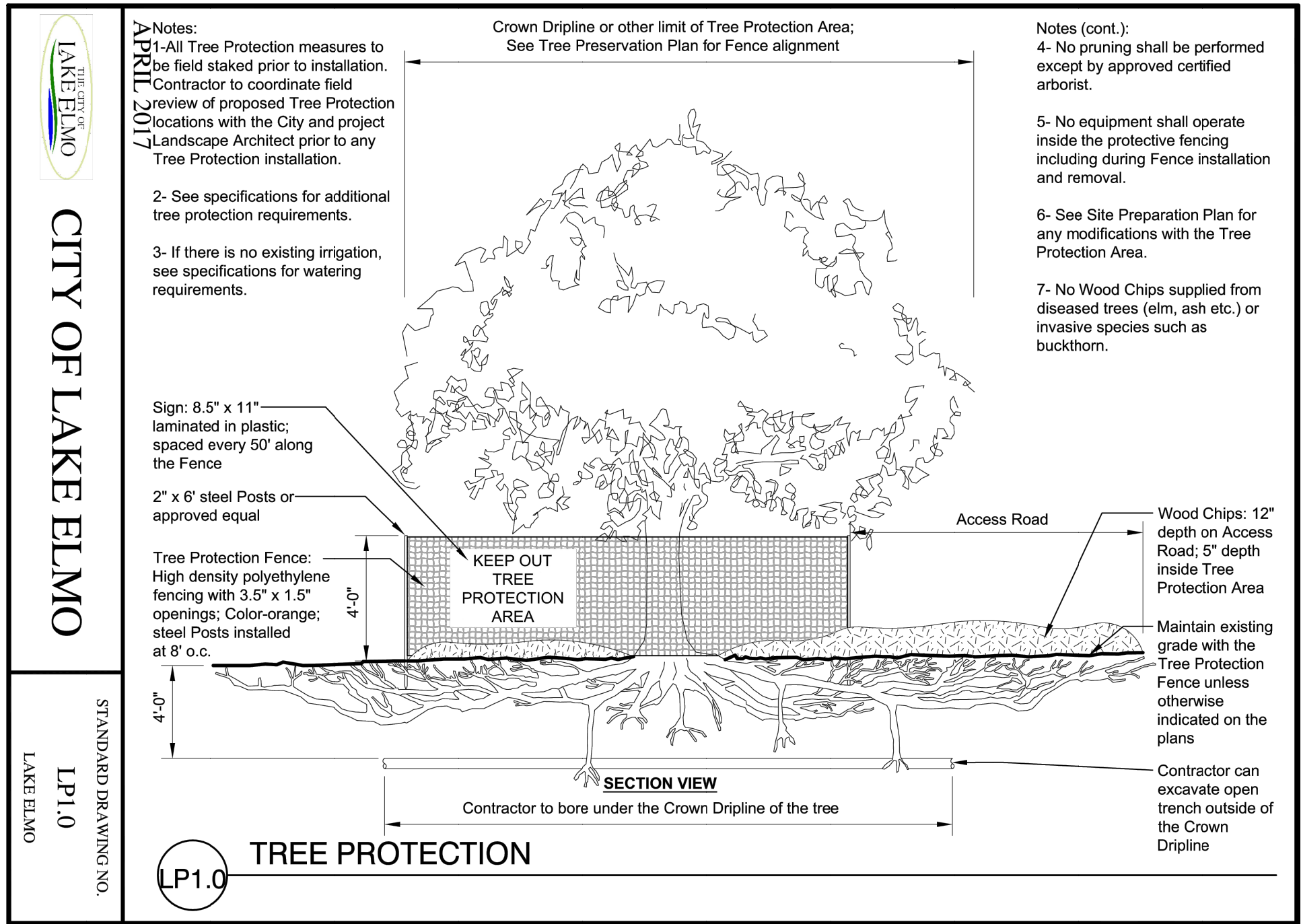
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- ROOT PROTECTION ZONE DEFINED AS AREA FROM TREE WITH RADIUS 12 TIMES GREATER THAN TREE DIAMETER AT BREAST HEIGHT (DBH) IN INCHES (1' RADIUS PER 1" DBH).



Tree Tag No.	Tree Species (Common Name)	DBH (in.)	Quality (0-3)	Stems	City Classification	Significant	Save	Remove
4201	Red Pine	9	3	1	Coniferous	Yes	X	
4202	Red Pine	7	3	1	Coniferous	No	X	
4203	Red Pine	9	3	1	Coniferous	Yes	X	
4204	Red Pine	8	3	1	Coniferous	Yes	X	
4205	Red Pine	8	3	1	Coniferous	Yes	X	
4206	Red Pine	8	3	1	Coniferous	Yes	X	
4207	Red Pine	10	3	1	Coniferous	Yes	X	
4208	Red Pine	7	3	1	Coniferous	No	X	
4209	Red Pine	7	3	1	Coniferous	No	X	
4210	Red Pine	8	3	1	Coniferous	Yes	X	
4211	Red Pine	8	3	1	Coniferous	Yes	X	
4212	Red Pine	9	3	1	Coniferous	Yes	X	
4213	Red Pine	10	3	1	Coniferous	Yes	X	
4214	Red Pine	7	3	1	Coniferous	No	X	
4215	Red Pine	7	3	1	Coniferous	No	X	
4216	Red Pine	7	3	1	Coniferous	No	X	
4217	Red Pine	7	3	1	Coniferous	No	X	
4218	Red Pine	8	3	1	Coniferous	Yes	X	
4219	Red Pine	9	3	1	Coniferous	Yes	X	
4220	Red Pine	10	3	1	Coniferous	Yes	X	
4221	Red Pine	10	3	2	Coniferous	Yes	X	
4222	Red Pine	9	3	1	Coniferous	Yes	X	
4223	Red Pine	9	3	1	Coniferous	Yes	X	
4224	Red Pine	7	3	1	Coniferous	No	X	
4225	Red Pine	9	3	1	Coniferous	Yes	X	
4226	Red Pine	7	3	1	Coniferous	No	X	
4227	Red Pine	8	3	1	Coniferous	Yes	X	
4228	Red Pine	9	3	1	Coniferous	Yes	X	
4229	Red Pine	9	3	1	Coniferous	Yes	X	
4230	Red Pine	6	3	1	Coniferous	No	X	
4231	Red Pine	6	3	1	Coniferous	No	X	
4232	Green Ash	6	3	1	Common Tree	No	X	
4233	Red Pine	9	3	1	Coniferous	Yes	X	
4234	Red Pine	7	3	1	Coniferous	No	X	
4235	Red Pine	6	3	1	Coniferous	No	X	
4236	Red Pine	10	3	1	Coniferous	Yes	X	
4237	Red Pine	7	3	1	Coniferous	No	X	
4238	Red Pine	8	3	1	Coniferous	Yes	X	
4239	Red Pine	7	3	1	Coniferous	No	X	
4240	Red Pine	7	3	1	Coniferous	No	X	
4241	Red Pine	6	3	1	Coniferous	No	X	
4242	Red Pine	6	3	1	Coniferous	No	X	
4243	Red Pine	7	3	1	Coniferous	No	X	
4244	Poplar	7	3	1	Common Tree	No	X	
4245	Red Pine	11	3	1	Coniferous	Yes	X	
4246	Red Pine	9	3	1	Coniferous	Yes	X	
4247	Green Ash	13	3	2	Common Tree	Yes	X	
4248	Red Pine	8	3	2	Coniferous	Yes	X	
4249	Red Pine	9	3	1	Coniferous	Yes	X	
4250	Red Pine	8	3	1	Coniferous	Yes	X	
4251	Red Pine	7	3	1	Coniferous	No	X	
4252	Red Pine	9	3	1	Coniferous	Yes	X	
4253	Red Pine	9	3	1	Coniferous	Yes	X	
4254	Red Pine	8	3	1	Coniferous	Yes	X	
4255	Red Pine	10	3	2	Coniferous	Yes	X	
4256	Red Pine	9	3	1	Coniferous	Yes	X	
4257	Red Pine	11	3	1	Coniferous	Yes	X	
4258	Red Pine	12	3	1	Coniferous	Yes	X	
4259	Red Pine	11	3	1	Coniferous	Yes	X	
4260	White Spruce	7	3	1	Coniferous	No	X	
4261	Poplar	7	3	1	Common Tree	No	X	
4262	White Spruce	8	3	1	Coniferous	Yes	X	
4263	White Spruce	8	3	1	Coniferous	Yes	X	
4264	White Spruce	9	3	1	Coniferous	Yes	X	
4265	White Spruce	9	3	1	Coniferous	Yes	X	
4266	White Spruce	7	3	1	Coniferous	No	X	
4267	White Spruce	8	3	1	Coniferous	Yes	X	
4268	Poplar	8	3	1	Common Tree	No	X	
4269	Blue Spruce	10	3	1	Coniferous	Yes	X	
4270	White Spruce	8	3	1	Coniferous	Yes	X	
4271	White Spruce	7	3	1	Coniferous	No	X	
4272	White Spruce	8	3	1	Coniferous	Yes	X	
4273	White Spruce	8	3	1	Coniferous	Yes	X	
4274	White Spruce	8	3	1	Coniferous	Yes	X	
4275	White Spruce	7	3	1	Coniferous	No	X	
4276	White Spruce	8	3	1	Coniferous	Yes	X	
4277	White Spruce	6	3	1	Coniferous	No	X	
4278	Poplar	8	3	1	Common Tree	No	X	
4279	White Spruce	8	3	1	Coniferous	Yes	X	
4280	Poplar	6	3	1	Common Tree	No	X	
4281	Blue Spruce	10	3	1	Coniferous	Yes	X	
4282	Blue Spruce	12	3	1	Coniferous	Yes	X	
4283	White Spruce	9	3	1	Coniferous	Yes	X	
4284	White Spruce	11	3	1	Coniferous	Yes	X	

SIGNIFICANT TREE SUMMARY	TOTAL (QTY)	DBH (INCHES)				MITIGATION (OVER 30%)
		TOTAL	SAVE	REMOVE	(%)	
Deciduous Hardwood Trees	0	0	0.0	0	-	0
Coniferous Trees	52	470.0	470.0	0.0	0.0%	0
Common Trees	1	13.0	13.0	0	0.0%	0
TOTAL	53	483.0	483.0	0		0



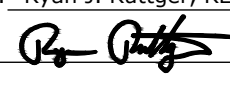


CITY OF LAKE ELMO

STANDARD DRAWING NO. LP1.0
LAKE ELMO

**3890 PHEASANT RIDGE DR NE
SUITE 100
BLAINE, MN 55449
TEL 763.489.7900
FAX 763.489.7959
CARLSON-ENGINEERING.COM**

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the State of Minnesota

Print Name: Ryan J. Rutgger, RLA
Signature: 
Date: 12/20/24 License #: 56346

Drawn: RJR
Designed: RJR
Date: 12/20/24

Revisions:
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M/I HOMES OF MINNEAPOLIS, LLC
5354 Parkdale Drive, #100
St. Louis Park, MN 55416

INWOOD TOWNHOMES
Lake Elmo, Minnesota

TREE PRESERVATION PLAN

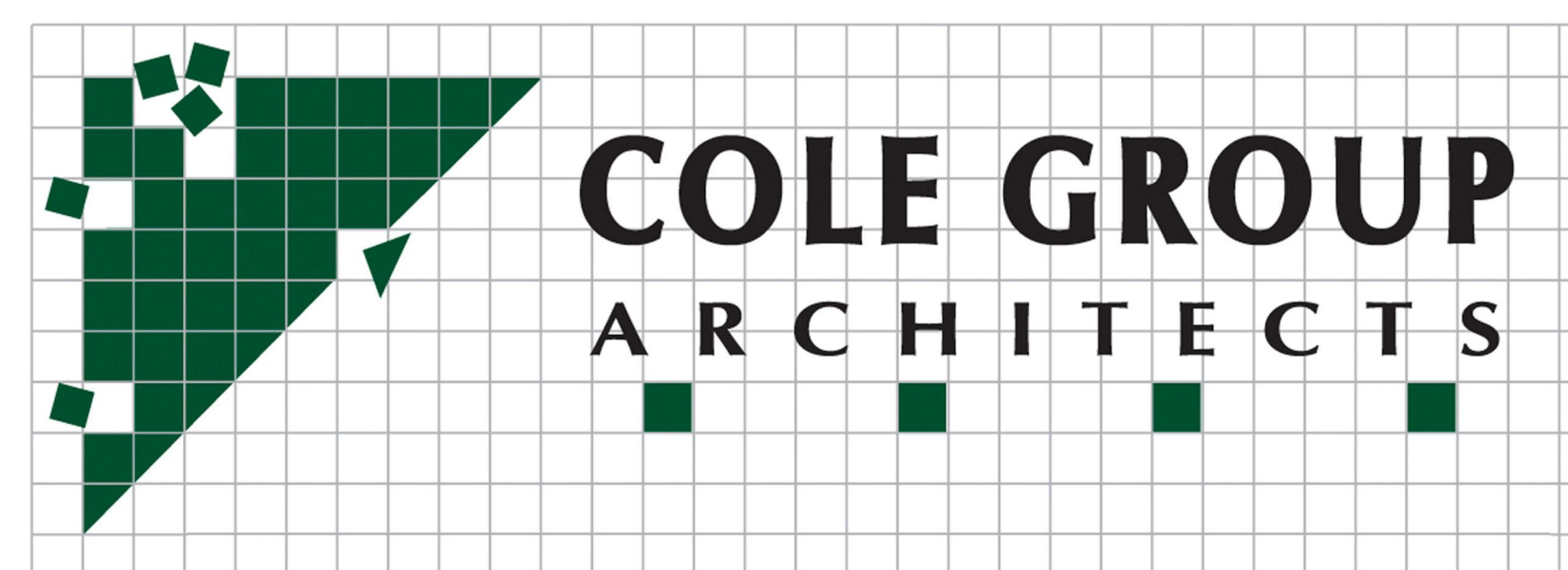
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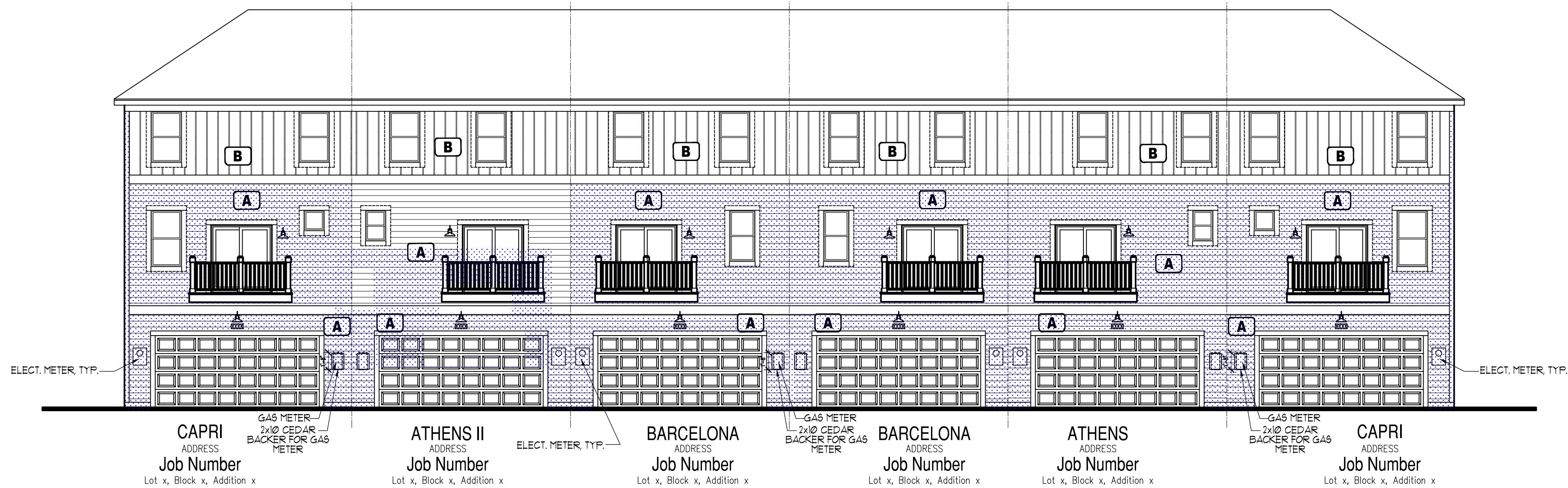
INWOOD APARTMENTS

LAKE ELMO, MN

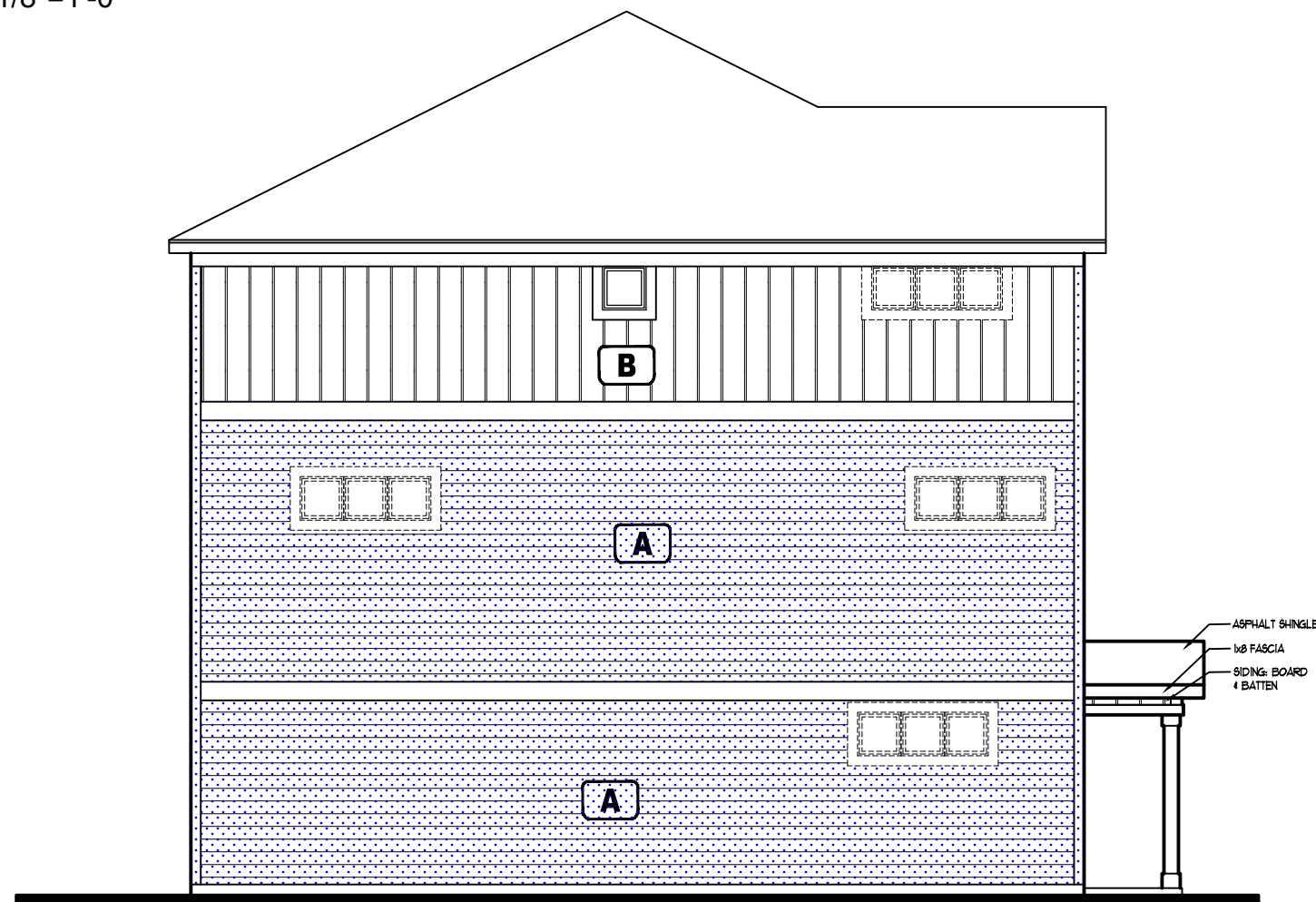


 **RACHEL**

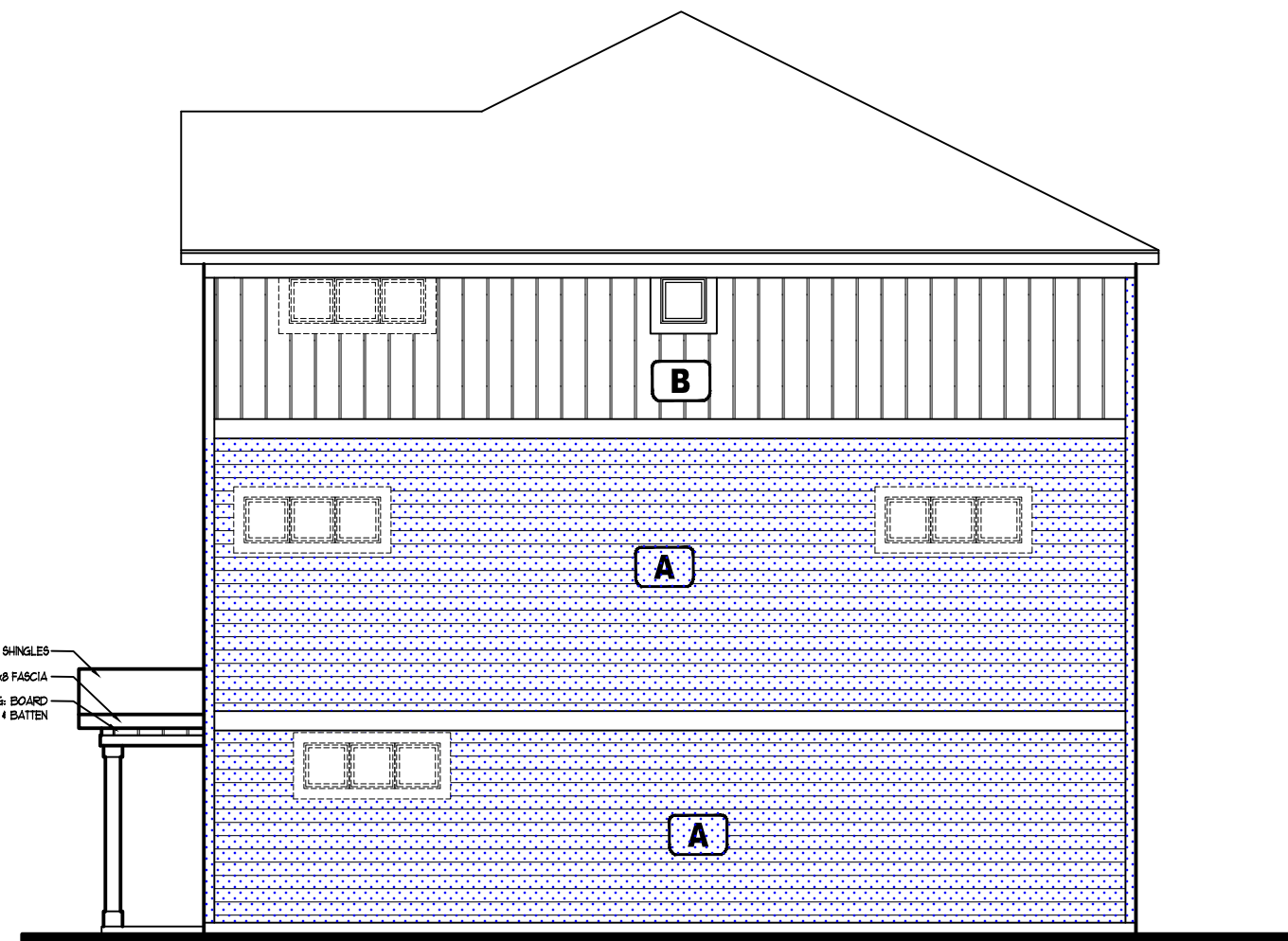




4 Rear Elevation
1/8"=1'-0"



3 Left Elevation
1/8"=1'-0"



2 Right Elevation
1/8"=1'-0"



1 Front Elevation
1/8"=1'-0"

Sheet Title: Exterior Elevations	Community: COMMUNITY	Revisions / Custom Options			By
	Address: STREET ADDRESS				Date
	City: CITY, MN	Lot:	Block:		
	Elevation: A	Addition:			
Model: City Collection	Client: CUSTOMER	Final Approval			Date
		X			
		X			
		X			
M/I HOMES		5354 Parkdale Dr. #100 St. Louis Park, MN 55416 763.586.7200 minneapolispermits@M/IHOMES.com			



**BOLTON
& MENK**

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MEMORANDUM

Date: April 7, 2025

To:	Nathan Fuesrt, Planning Consultant Jason Stopa, Community Development Director Marty Powers, Public Works Director Nate Stanley, City Engineer Chad Isakson, Assistant City Engineer	Re: Inwood Townhomes Preliminary Plat/Plans
From:	Jack Griffin, Sr. Project Manager	

An engineering review has been completed for the Inwood Townhomes Preliminary Plat/Plans received on February 27, 2025. The review consisted of the following documentation:

- Inwood Townhomes Preliminary Plat and Plans dated February 11, 2025.
 - Stormwater Management Plan dated December 20, 2024.
-

STATUS/FINDINGS: A condition of Preliminary Plat approval, if granted, must require the applicant to revise and resubmit Preliminary Plat/Plans to address each review comment and condition of approval to the satisfaction of City staff prior to the City accepting a Final Plat/Plan application. Engineering has prepared the following review comments to be addressed. A point-by-point response letter must accompany the plan resubmittal.

SITE PLANS, TRAFFIC AND ACCESS MANAGEMENT

1. Right-of-way dedication. No additional right-of-way dedication is required along 5th Street. Additional right-of-way dedication along Island Trail may be needed to accommodate turn lanes at the new proposed site accesses.
2. Access Management for East Parcel. The east parcel is allowed a single access location to 5th Street as shown at the Inwood Circle N. intersection, and a single access location along Island Trail.
3. Access Management for West Parcel. The west parcel may have a single new access location along Island Trail. The west Island Trail access location should be aligned with the Island Trail access location for the east parcel.
4. Turn lanes should be added along Island Trail for both the east and west access locations, each having more than 35 new units.
5. The subdivision phasing must provide secondary access for 30 or greater units per Fire Code adopted by the City.
6. No access will be allowed to Eagle Point Boulevard due to limited access spacing.
7. Secondary access for the proposed 124-unit apartment building is required per Fire Code. The secondary access must connect to the shared driveway for Outlot A of the Inwood 6th Addition. A shared driveway easement has been created for this connection as part of the Inwood 6th Addition. The access design should be improved to encourage equal distribution of traffic to the 5th Street and Island Trail access locations.

8. The subdivision proposes 24-foot wide private streets with no sidewalks adjacent to the street. City design standards require public streets for medium and high-density residential subdivisions to be minimum 32-feet in width located within a 66-foot right-of-way. Sidewalks are also required along both sides of the public street.
9. The proposed design is not recommended as presented due to significant narrowing of the street and utility corridors, and increased conflicts between various design elements. If the City approves the Preliminary Plat, the Plans must be further detailed and revised to eliminate encroachments and conflicts between public watermain and public sanitary sewer, private storm sewer, small/dry utility corridor preservation, trees and landscaping, drainage, and pedestrian facilities.
10. Whether the streets are public or private, all street intersections must be at 90 degrees and maintain 50-ft of tangent with maximum longitudinal slopes of 2.5%. Plan revisions are required accordingly.
11. Driveways must be further detailed, labeled and dimensioned to demonstrate compliance with all requirements of the City Code. Plan revisions are required. Particular attention should be detailed on the plans to demonstrate compliance with the required offsets from the right-of-way at intersections, the required offsets from side lot lines, the driveway separation distance requirements, and connection of each driveway to the public street at 90-degree angles.
 - Lot 64, Block 3 must be revised to increase the driveway setback to 50-feet from the Island Trail intersection per City driveway standards.
 - Lots 9 and 10, Block 2; and Lots 19-22, Block 3; include driveways located on dead-end streets with no City standard turn around. Redesign is recommended or approval is required from the City Planning and Fire Departments.
 - Lots 13-17, Block 3 are located on a curve with the driveways failing to intersect the street at 90-degrees as required. Redesign is recommended for these lots to revise driveway layout, and utility service layout. Horizontal geometrics for this curve must be detailed for City review.
12. Dedicated drainage and utility easements, minimum 10-feet wide, are required along all public roadway frontage, including 5th Street and Island Trail. Small/dry utility corridor preservation is also required along all private streets (if allowed). All easement areas must be free from all encroachments other than those approved by the City Engineer and upon execution of an easement encroachment agreement. Prohibited encroachments include, but are not limited to trees, structures, landscaping, fences, retaining walls, storm sewer running parallel with the easement, stormwater basins and BMPs, and grading that exceeds 4:1.

STORMWATER MANAGEMENT

1. The proposed development is subject to a Stormwater Management Plan (SWMP) meeting State, South Washington Watershed District (SWWD) and City rules. A SWWD permit is required.
2. The Preliminary Plat includes improvements that reside within the larger Inwood subdivision in which stormwater management provisions have been previously designed, permitted and constructed. The site design will create 10.64 acres of new impervious surfaces, or 55 percent impervious surfaces. The existing stormwater management system constructed with previous Inwood development phases assumed these parcels would contain 75% impervious surfaces. Therefore, the existing stormwater management system will accommodate the subdivision as proposed.
3. Approval of the Preliminary Plat/Plans for the development is contingent upon all SWMP revisions, as required by the SWWD or other applicable permitting agency, being submitted to the City for review and approval.
4. Approval of the preliminary grading and storm sewer plans for the development is contingent upon all grading plans being reviewed and approved by the City Engineer prior to the start of any grading or construction.

5. Overland emergency overflow (EOF) elevations are required throughout the site, requiring a system of interconnecting drainage ways. Low openings for adjacent structures must maintain 1-foot above any adjacent EOF in the as-built condition.
6. Plan revisions may be required to meet City design standards and to mitigate all encroachments.
7. All storm sewer systems internal to the subdivision will be privately owned and maintained. A Stormwater Management Maintenance and Easement Agreement in the City's standard form of agreement must be executed and recorded with the Final Plat for all privately owned stormwater facilities. Easement areas must incorporate all 100-year high water level areas including localized low points, and to protect all overland emergency overflow locations and flow paths. Easement revisions may be necessary to reflect any revisions to the construction plans.

MUNICIPAL WATER SUPPLY

1. Connection to the public watermain system is readily available to serve each proposed development parcel. The developer will be required to connect to existing watermain stubs located along 5th Street and Island Trail and extend the public watermain system internally throughout the subdivision. All public watermain improvements must be installed in accordance with the City engineering design standards and as approved by the City.
2. Hydrants and gate valves must be installed as directed by the Fire Department and Public Works Department. All hydrants and connecting watermains shall be owned and maintained by the City.
3. All watermain improvements must be completed at developer's sole cost including all City connection and availability charges.
4. Watermain distribution lines and connections will be required wherever reasonably possible to create a looped network throughout the proposed subdivision and as the project is phased.
5. Minimum 30-foot easements centered over the pipe/hydrant will be required when not located within the public right-of-way. Easements must be free from all encroachments, including retaining walls, trees, fences, small/dry utilities, or storm water management BMPs. Dedicated utility easements must be shown on all site, grading, utility, and landscape plans when submitting a Preliminary and Final Plat application.
6. Plan revisions will be required to meet City design standards and to mitigate all encroachments.

MUNICIPAL SANITARY SEWER

1. The proposed property is located in the Southwest Planning MUSA and current Regional Sewer Staging Plan area and would discharge to the MCEs WONE Interceptor.
2. Connection to the municipal sanitary sewer system is readily available to serve each proposed development parcel. The developer will be required to connect to existing 8-inch sanitary sewer stubs located along Island Trail and extend the public sanitary sewer system internally throughout the subdivision to serve each platted lot/parcel and in accordance with the City engineering design standards.
3. All sanitary sewer improvements must be completed at developer's sole cost including all City connection and availability charges.
4. All public sanitary sewer mains placed within the development will require minimum 30-foot easements centered over the pipe/structure, if not located within a public right-of-way. Easements must be free from all encroachments, including retaining walls, trees, fences, small/dry utilities, or storm water management BMPs. Dedicated utility easements must be shown on all site, grading, utility, and landscape plans when submitting Preliminary and Final Plat applications.
5. Plan revisions will be required to meet City design standards and to mitigate all encroachments.

LAKE ELMO FIRE DEPARTMENT – OFFICE OF THE FIRE MARSHAL

Fire Prevention, Code Enforcement, and Public Education



March 4, 2025

Ashley Monterusso, Planner
City of Lake Elmo

Re: Inwood 8th Addition – Apartments and Townhomes

Prepared by: Anthony Svoboda, Fire Marshal

Approved by: Dustin Kalis, Fire Chief

Applicable Codes:

- 2020 Minnesota State Fire Code
- 2020 Minnesota State Building Code
- Lake Elmo Fire Department Fire Code Policies
- NFPA 13, 2016 edition
- NFPA 13D, 2016 edition

Fire Department Comments:

Roads, Drive Lanes, and Parking Areas

- All roads and drive lanes shall meet the Lake Elmo Fire Department requirements for widths and turning radiuses.
- Approved fire apparatus roads shall be provided and maintained throughout all development phases in coordination with engineering, public works, planning, and fire departments.
- An approved signage and marking plan shall be determined for all No Parking and Fire Lane access roads. On- street parking shall be provided in approved locations following review by Engineering and Public Works. Parking shall be prohibited on both sides of private drive lanes.
- All parking areas shall be capable of supporting the imposed load of fire apparatus weighing up to 75,000 pounds.

Fire Detection and Suppression

- Fire sprinkler systems shall be installed in the townhome buildings compliant with provisions of 2016 NFPA Standard 13D, Installation of Sprinkler Systems in One- and Two-Family Dwellings or IRC P2904. City permit required prior to initiation of work.
- Fire sprinkler system shall be installed in the apartment building compliant with provisions of 2016 NFPA Standard 13, Installation of Sprinkler Systems. City permit required prior to initiation of work.
- Standpipes shall be installed in the apartment building compliant with 2016 NFPA 14, Standard for the Installation of Standpipe and Hose Systems.
- Fire Department sprinkler connection locations to be approved prior to installation.
- Fire alarm system in the apartment building shall be installed compliant with provisions of 2016 NFPA Standard 72, National Fire Alarm Code. City permit required prior to initiation of work.
- Fire hydrants and watermain shall be provided in approved locations following review by Engineering and Public works.

LAKE ELMO FIRE DEPARTMENT – OFFICE OF THE FIRE MARSHAL

Fire Prevention, Code Enforcement, and Public Education



Building Systems and Equipment

- Locations of all EV charging stations or Electric Vehicle Supply Equipment shall be reviewed and installed in approved locations. All electric vehicle supply equipment installed in the apartment building underground parking area may require additional fire suppression considerations and restrictions on installation locations. An electric vehicle supply equipment plan is required. This plan shall be reviewed by the fire department, building contractor, building owner and building insurance provider.
- Emergency egress illumination shall be installed in the apartment building in the means of egress including exit discharge compliant with 2020 MSFC.
- Compliant exit signage shall be installed in the apartment building as required by the 2020 MSFC.
- Provide and install in the apartment building, dry chemical fire extinguishers certified for service and tagged as required. Service classification rating shall be a minimum 2A classification rating and maximum travel distance of 75 feet to extinguishers. The minimum classification rating may be upgraded for special or extra hazard areas within the occupancy.
- In the apartment building, rooms containing controls for air-conditioning systems, roof access, elevator equipment, sprinkler risers and valves, or other fire detection, suppression or control elements shall be identified for the use of the fire department. Approved signs required to identify fire protection equipment and equipment location, shall be constructed of durable materials, permanently installed and readily visible.

Address and Street Naming Systems

- Building address numbers shall be plainly visible from the street fronting the property and shall be contrasting color from the background.
- Size and placement of address numbers shall be approved by the fire and planning departments.
- Street names and addressing shall be consistent with the Washington County Uniform Street Naming and Property Numbering System. Street names shall be approved by the City of Lake Elmo.

Gates, Locks, and Access

- Project construction phasing shall always accommodate emergency access to the entire construction zone, generally meaning two separate means of entrance/exit as defined in the code. Phasing plan to be approved by the fire department prior to construction.

Questions, clarifications, or the request to provide code documents can be made using the contact information listed below.

Respectfully,

Anthony Svoboda



Anthony Svoboda | Assistant Chief
Lake Elmo Fire Department
3880 Laverne Ave N. | Lake Elmo, MN | 55042
612-528-1514 *mobile* | 651-747-3907 *office*

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Memorandum

March 6, 2025

TO: ASHLEY MONTERUSSO
FROM: SARAH EVENSON, PLA
RE: TOWNS OF INWOOD LANDSCAPE AND TREE PRESERVATION CONCEPT PLAN REVIEW

SUBMITTALS

1. Concept Plan Submittal, dated 8/9/24, received 8/26/24

REVIEW HISTORY

1. 9/12/24 Concept Plan Review
2. 3/6/25 Preliminary Plat PUD Review

LOCATION: East of Inwood Ave N, between 5th Street N. to the north and east and Eagle Point Blvd. to the south

CURRENT LAND USE CATEGORY: High-density residential and guided high-density residential and mixed-use commercial

ADJACENT AND SURROUNDING LAND USE: Business Park to the south, high-density residential to the west, low-density residential to the north, and Public Facilities to the north and east

SPECIAL LANDSCAPE PROVISIONS: none

TREE PRESERVATION PLAN: [105.12.470](#)

- The applicant has provided a tree preservation plan that meets code.

LANDSCAPE PLAN: [105.12.480](#)

- The following changes will need to be made to the landscape plans prior to acceptance:
 - Provide a plant schedule showing the ground covers and tree species proposed on each sheet for easy reference (currently only shown on one sheet all together at the end)
 - Show the location of a topsoil stockpile on the grading plans
 - Confirm that per 05.12.480 (b) (7), trees shall be planted ten feet away from utilities including water and sewer stubs.

RECOMMENDATION:

Submit revised landscape plans per code.



Sarah Evenson, PLA (MN)

City of Lake Elmo Municipal Landscape Architect
P: (262) 391-7653 E: sarah@hkgi.com



ADMINISTRATION

Kevin Corbid, County Administrator
Jan Lucke, Deputy County Administrator
Jennifer Wagenius, Deputy County Administrator

March 7, 2025

Ashley Monterusso
City Planner
City of Lake Elmo

RE: Inwood Townhomes - Comprehensive Plan Amendment

Ms. Monterusso:

Washington County received and reviewed the documents for the Inwood Townhomes Comprehensive Plan Amendment application requesting the Future Land Use Category be changed from Mixed Use Commercial to High Density Residential.

The departments of Public Works and Public Health and Environment reviewed the documents provided by the City of Lake Elmo on February 27th. It was identified through this review that the development proposed is located in a DWSMA and known area of PFAS contamination. Best management practices as outlined by MDH and MPCA should be implemented to protect public health. Washington County's Public Works Department had no additional comments.

Thank you for the opportunity to comment.

Sincerely,

Kelli Matzek
Senior Planner
Washington County – Office of Administration

Cc: Emily Jorgensen, Washington County Planning Manager

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