



3800 Laverne Avenue North
Lake Elmo, MN 55042

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NOTICE OF MEETING

The City of Lake Elmo
Planning Commission will conduct a meeting on

Monday June 4, 2018

at 7:00 p.m.

AGENDA

1. Pledge of Allegiance
2. Approve Agenda
3. Approve Minutes
 - a. None
4. Public Hearings
 - a. FINAL PLAT AND PLANNED UNIT DEVELOPMENT (PUD) PLANS AND EASEMENT VACATION. A request by H.C. Golf Land, LLC, 11074 Radisson Rd, Blaine, MN 55449, for a Final Plat and PUD Plans consisting of 64 single family detached residential units and 13 outlots. PID #s 25.029.21.43.0003, 25.029.21.43.0004, 25.029.21.43.0005, and 25.029.21.31.0002. An easement vacation request to vacate existing drainage and utility easements over Outlots O, P, and R of Royal Golf Club at Lake Elmo 1st addition to be replaced with new drainage and utility easements over the same with replatting of these outlots with the 2nd addition.
 - b. ZONING TEXT AMENDMENT, PRELIMINARY AND FINAL PLAT, ZONING MAP AMENDMENT AND CONDITIONAL USE PERMIT. A request by Stillwater Area Public Schools, 1875 Greeley Street South, Stillwater, MN 55082, for a zoning text amendment to add local transit as a conditional use, including standards, within the Business Park zoning district; a zoning map amendment to rezone a portion of 11530 Hudson Blvd (PID # 36.029.21.43.0001) from Rural Development Transitional to Business Park; and for a conditional use permit to allow a school transit on a portion of (11530 Hudson Blvd N) PID #36.029.21.43.0001. A request by Terry Emerson, 2204 Legion Lane Cir. N, Lake Elmo, MN 55042, for a minor subdivision to subdivide 11530 Hudson Blvd N, Lake Elmo, MN 55042. PID # 36.029.21.43.0001.
 - c. PRELIMINARY PLAT AND ZONING MAP AMENDMENT. A request by JP Bush Homes, Inc., 1980 Quasar Ave South, Lakeland, MN 55043, for a Preliminary Plat consisting of 10 single family detached residential units on 5.9 acres of property for the property located at 11580 30th Street North from Single Family Residential to Village Low Density Residential.
5. Business Items
 - a. None
6. Communications
 - a. City Council Updates – None
 - b. Staff Updates
 - a. Upcoming Meetings:
 - June 18, 2018
 - July 9, 2018
7. Adjourn

***Note: Every effort will be made to accommodate person or persons that need special considerations to attend this meeting due to a health condition or disability. Please contact the Lake Elmo City Clerk if you are in need of special accommodations.



STAFF REPORT

DATE: 6/4/2018

REGULAR

ITEM #: 4a

MOTION

TO: Planning Commission
FROM: Emily Becker, Planning Director
AGENDA ITEM: Royal Golf Club at Lake Elmo 2nd Addition Final Plat and Planned Unit Development (PUD) Plans and Easement Vacation
REVIEWED BY: Jack Griffin, City Engineer
Greg Malmquist, Fire Chief

BACKGROUND:

The Planning Commission is being asked to consider a request by H.C. Golf Land, LLC for approval of the final plat and final PUD plans for the Royal Golf Club at Lake Elmo 2nd Addition. Per Zoning Code Section 154.808, Procedures for Processing a Planned Unit Development, the Planning Commission is required at this time to review and comment on whether the plans and supporting data are adequate for review and final approval. The Planning Commission is asked to hold a public hearing, report its findings, and make recommendations to the City Council. The Final Plat for Phase 2 includes 64 residential lots, including 22 traditional single family detached lots and 38 villa lots for detached single family townhomes.

ISSUE BEFORE COMMISSION:

Is the proposed Royal Golf Club at Lake Elmo 2nd Addition generally consistent with the approved Preliminary Plat and have all conditions of Preliminary Plat approval been met?

REVIEW/ANALYSIS:

Applicant and Owner: H.C. Golf Land, LLC
Location: Outlot S, Royal Golf Club at Lake Elmo 1st Addition, PID# 25.029.21.31.0002.
Request: Application for final plat and PUD plans approval of 64 residential lots and easement vacation
Existing Land Use: Vacant outlot; Current Zoning: GCC – Golf Course Community
Surrounding Land Use: North –Golf Course (GCC – Golf Course Community), South and West – The Homestead (OP – Open Space), Tartan Meadows (RS – Rural Single Family), South - Cimarron

(MDR – Medium Density Residential); East – Vacant land
(RR – Rural Residential)

Comprehensive Plan: GCC - Golf Course Community

History: Preliminary Plat and PUD Plans approval was granted on June 6, 2017 by Resolution 2017-047 and Final Plat and PUD Plans for 1st Addition was granted on September 5, 2017 by Resolution 2017-093

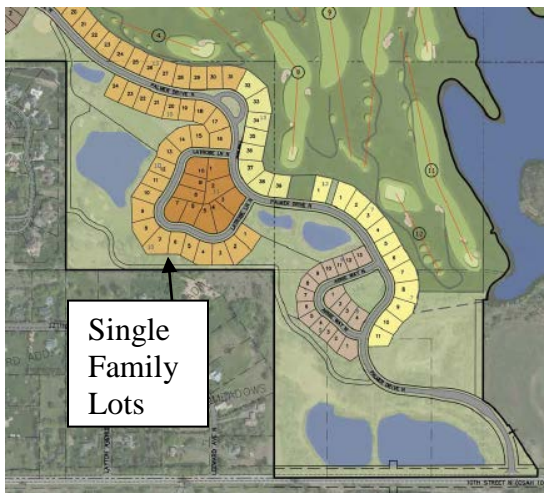
Deadline for Action: Application Complete – 5/10/18
60 Day Deadline – 7/9/18
Extension Letter Mailed – No
120 Day Deadline – NA

Applicable Regulations: Chapter 153 – Subdivision Regulations
Article 10 – Urban Residential Districts (GCC)
§150.270 Storm Water, Erosion, and Sediment Control

FINAL PLAT AND PUD PLANS

Consistency with Preliminary Plat. The proposed lot sizes, widths, and proposed impervious surface are generally consistent with that of preliminary plat. The only change is that they've converted the previously single family lots bordering the outside of Latrobe Lane North to villa lots. The developer resubmitted preliminary plans for administrative approval in order to do this. The developer went door to door of neighbors of the Homestead and Tartan Meadows developments that border the golf course explaining the change and how it would benefit them, as the villas would be one story lots versus two story homes which would be less visible with smaller yards which would inhibit the inclusion of swimming pools, sport courts, etc., thereby possibly reducing noise. Additionally, villa lots would be mowed at the same time during the week and not at various times, and villa lots are generally more attractive to empty nesters or snowbirds, who are quite often gone during the winter.

Previous Preliminary Plat



Updated Preliminary Plat



PUD Flexibility. The development was afforded the following flexibility through preliminary approval:

- a. Setbacks:

Royal Golf Club at Lake Elmo Setbacks

	100 Ft. Wide Lots	80-90 Ft. Wide Lots	55-65 Ft. Wide Lots
Front	30 ft.	30 ft.	20 ft. for side loaded garages, or 25'
Side	10 ft.	10 ft.	10ft. house/5ft. garage or 7.5 ft./7.5 ft.
Corner Side	15 ft.	15 ft.	15 ft.
Rear	30 ft.	30 ft.	20 ft.

- b. Maximum Impervious Coverage: The maximum impervious coverage for 55-65' wide lots shall be 50%. All other lots shall have a maximum impervious coverage of 40%.
- c. Lot Sizes: The minimum lot size for Villa lots (55-65' wide) in the development shall be 6,600 sq. ft.
- d. Attached Garages: That attached garages shall not exceed 1,300 sq. ft. in area at the ground floor level except by conditional use permit. The width of the visible garage door area when closed shall not exceed 60% of the principal building façade (including garage) fronting on the primary street.
- e. Subdivision Identification Signs: the Royal Golf Club at Lake Elmo residential subdivision shall be allowed up to a maximum of 4 subdivision identification signs, including the identification sign for the golf course entrance, not to exceed 24 sq. feet in sign area each, located no closer than 10 feet to any public right-of-way. In addition neighborhood identification markers (pillars) shall be permitted to be no larger than 2 ft. x 2 ft. to identify the development logo and the name of the neighborhood. Additional subdivision signs should be considered for a subdivision of this size.
- f. All other requirements for the City's GCC – Golf Course Community zoning district will apply, including the allowed uses and other site and development standards.
- g. That the proposed street names within the development are generally consistent with the City's Street Naming Policy except for Annika Drive.

Lot Sizes and Widths. The smallest lot size of this addition is 7,210 square feet, with the largest lot size at 21,580 square feet. The smallest lot width is 56.3 feet and the largest lot width is 145.8 feet.

Density. The total area of the 2nd Addition is 89.91 acres. There is no arterial right-of-way, but there is a wetland buffer area of 20.66 acres with wetland buffer area of 7.03 acres, resulting in a total density of 1.02 units per acre for the second addition (64 lots/(89.91 total acres - 20.66 wetland area – 7.03 wetland buffer area=62.22 acres) = 1.02. The total single family area is 18.74, the total right-of-way area is 7.99 acres, and the outlot area is 63.18 acres.

Outlots. An explanation of ownership and purpose of the outlots is below.

- Outlot A will be owned by the HOA. A drainage and utility easement will be over all of Outlot A.
- Outlot B will be owned by the City to be used as trail purposes and for Infiltration Basin #B52.
- Outlot C will be owned by the HOA to be used for landscaping and open space purposes.
- Outlot D will be owned by the HOA to be used for landscaping and open space purposes.
- Outlot E will be owned by the HOA for landscaping and open space purposes.
- Outlot F will be owned by the City to be used for ponding purposes.
- Outlot G will be owned by the HOA to be used for landscaping and open space purposes.
- Outlot H (wetland) will be owned will be owned by the HOA.
- Outlot I will be owned by the City to be used for ponding purposes.
- Outlot J will be owned by HC Golf Course to be platted in to lots in the future.
- Outlot K will be owned by the City and for utility and access purposes.
- Outlot L (wetland) will be owned by HC Golf Course.
- Outlot M is owned by Terry Emerson.

Parkland Dedication. Outlot B will be dedicated for trail purposes. This outlot is 187,712 square feet (4.3 acres). The value of land of all areas not within wetland buffers will receive park land dedication credit. As per a recommended condition of approval, the developer will not receive credit for cost of construction of these trails, as this was a condition of approval for increased impervious surface from preliminary plat approval.

The developer is required to dedicate 8.8 acres of land or pay a cash contribution of \$611,457.00 for the entire preliminary plat area (291 lots) and dedicate a total of 8,085 lineal feet of trails with a 30-foot wide corridor, less a 5,010 foot buffer encroachment. For this phase, the developer will be required to pay a cash contribution of \$134,464.00, calculated as follows: 64 lots at \$2,101.00 per lot.

Additionally, it is a recommended condition of approval that the applicant construct a play structure prior to submittal of an application for 3rd Addition Final Plat. Installation of a play structure was a condition of preliminary plat approval.

Landscaping. The developer has provided landscaping plans for the 2nd Addition, which are attached hereto. The City's Landscape Architect has reviewed the landscape plans and has recommended approval. The landscape plans adjacent to the Homestead neighborhood are consistent with the Homestead-approved landscape plans.

It should be noted that the City approved an amendment to the Royal Golf Development Agreement which allowed the total number of trees within the entire development to be reduced from the agreed-upon number of 3800 for all trees required to be warrantied within the entire subdivision to 2912 with the provision that the developer plant at least two trees on villa lots and four trees on single family home lots. Further, the developer would reduce the number of trees required to be planted within the entire subdivision from 2912 to 1943 with the provision that the developer pay a park dedication fee of \$500.00 per 2.5 caliper inch tree in lieu of planting the difference of 969 trees. With this provision, the developer will be required to pay the City a cash contribution of \$106,557, calculated as follows: $[(\$500 \times 969 \text{ 2.5-caliper inch trees reduced within the entire preliminary plat area}) / 291 \text{ Single Family Lots within all phases of the Subdivision} \times 64 \text{ Single Family Lots within the 1}^{\text{st}} \text{ Addition}]$

Street Naming. The City amended its Street Naming Policy at the April 17, 2018, removing the provision that if appropriate, names with the same theme (i.e. flowers, nature) are permitted for naming streets in an entire subdivision. While the proposed street names of Royal Golf Club at Lake Elmo do not fit the City's now amended street naming policy, they are consistent with 1st Addition in that they are "golf-themed." While the names have already been approved through the preliminary plat process, it was a condition of approval that the Final Plat include street names as approved by Council.

Engineering Comments. The City Engineer review memo dated May 30, 2018 is attached to this report.

Fire Chief and Building Official Comments. The application has been reviewed by the Fire Chief and Building Official, and they have no comments.

Preliminary Plat Conditions. The following explains how Preliminary Plat conditions have been met (Staff comments italicized and bold).

1. That cross access easements be recorded between the owner of the golf course/golf club and the HOA to permit HOA and resident access to the future fitness center, clubhouse and other amenities by the HOA residents over the clubhouse entrance drive and parking lot and trails. ***Comment: This easement has been granted.***
2. Any expansion to the Royal Golf Club clubhouse or intensification of its use shall require a conditional use permit. ***Comment: The applicant is not requesting this at this time. Additionally, the lot on which the clubhouse is situated was platted with 1st Addition, and so this condition does not apply to 2nd Addition.***
3. That there shall be no encroachments to drainage and utility easements on residential lots other than those reviewed and approved by the City Engineer and upon execution of an easement encroachment agreement. ***Comment: No***

encroachments are being requested at this time. This was added as a recommended condition of approval.

4. That the developer prepare an exhibit that clearly identifies the proposed setbacks for specific lots within the development. ***Comment: The developer has provided an exhibit which outlines setbacks within the development. The developer has been granted PUD flexibility which allows a 30 ft. setback on the 100 and 80-90 ft. lots and a 20 ft. setback on the 55-65 ft. lots; 10 ft. sideyard setback on the 100 and 80-90 ft. lots and a 7.5 ft./7.5 ft. setback on the 55-65 ft. lots; a corner yard setback of 15 ft. on all lots; and a 30 ft. setback on the 100 and 80-90 ft. lots and a 20 ft. setback on the 55-65 ft. lots.***
5. Prior to the execution of a Final Plat for any phase of the development by City officials, the Developer shall enter into a Developer's Agreement for that phase acceptable to the City Attorney and approved by the City Council that delineates who is responsible for the design, construction, and payment of the required improvements with financial guarantees therefore. ***A development agreement will be prepared by Staff and approved by both the developer and Council.***
6. Each phase of the Royal Golf Club at Lake Elmo shall be incorporated into the Common Interest Agreement concerning management of the common areas and establish a homeowner's association which shall be submitted in final form to the Planning Director before any building permit may be issued for any structure in any phase of the development. Said agreement shall comply with Minnesota Statutes 515B-103, and specifically the provisions concerning the transfer of control to the future property owners. The HOA documents shall include required maintenance of wetland buffers. ***The Common Interest Agreement will need to be updated to include Royal Golf Club at Lake Elmo 2nd Addition and approved by the City Attorney prior to recording of the final plat.***
7. That the HOA documents include architectural requirements that require 4-sided architecture and garages facing the public right-of-ways to have windows and/or other architectural features. ***The 1st Addition HOA documents included this language, and with the addition of the 2nd Addition of Royal Golf Club at Lake Elmo in to the HOA documents, this will also be a requirement of the 2nd Addition.***
8. The applicant shall enter into a landscape license and maintenance agreement with the City that clarifies the individuals or entities responsible for any landscaping installed in areas outside of land dedicated as public park and open space for each platted phase of the development. ***This has been added as a recommended condition of approval of the 2nd Addition Final Plat.***
9. That the HOA be responsible for the maintenance of all landscape walls that cross residential property lines or publicly owned Outlots or rights-of-ways, and that the walls be contained within a landscape easement. ***Retaining walls are proposed within outlots F and I. The responsibility for the maintenance of these retaining***

walls will be outlined in the landscape license and maintenance agreement for the 2nd Addition of Royal Golf Club at Lake Elmo.

10. The developer shall provide for an HOA owned and maintained children's play structure or other similar improvement near the HOA fitness center as discussed by the developer during the PUD concept review. *This will be addressed in the Development Agreement and has been added as a recommended condition of approval. The play structure was not required to be installed with the 1st Addition, as there were only 73 homes within that Addition. With now 64 more homes within the development, it is appropriate that this play structure be constructed with 2nd Addition. It is a recommended condition of approval that the play structure be installed prior to submission of application for Final Plat for the 3rd Addition.*
11. That a fee in lieu of park land dedication be paid to the City based upon an appraisal by an appraiser to be chosen by the City and paid for by the developer. Also that a 30 ft. wide trail corridor be dedicated to the City centered on public trails in the development and that the value of the dedicated land be credited toward any fees in lieu of parkland dedication. The park land dedication shall not include wetlands, wetland buffer area, and no credit will be given for land beyond the 30 ft. centered on public trails. And lastly, that the cost of constructing any public trails within the dedicated park land be credited towards any fees in lieu of park dedication. *The developer had requested an amendment to the maximum-allowed impervious surface with 1st Addition Final Plat, and this request was approved provided the developer does not receive credit for cost of trail construction. The conditional language from the 1st Addition was added as a recommended condition of approval.*
12. That a trail phasing plan be submitted by the developer to be approved by City Staff and that the public trails be constructed within each phase prior to building permits being issued for that phase of development. *This condition has been met.*
13. That a 10' wide trail segment be provided from Palmer Drive (on preliminary plans) to the east edge of the plat within the County Right-of-Way. *This condition has been met.*
14. That the Tree Preservation Plans and Preliminary Landscape Plans be updated to comply with the City's tree preservation requirements and the City's landscape requirements for review and approval by the City prior to recording the final plat. *The developer has submitted updated Tree Preservation and Preliminary Landscape Plans approved by the City. This is further explained above.*
15. The developer consider woodland management and pollinator friendly native seeding in lieu of some required tree preservation replacement tree requirements as recommended by the City's Landscape Architect. *The developer requested and the City approved a Development Agreement amendment which no longer requires that trees planted within single family lots be warrantied. This reduced the number of trees required within the development from 3,800 to 2,900. The*

developer also submitted and the City approved a preliminary landscape plan which further reduces the number of trees within the development from 2,900 to 1,943. The developer will be required to pay a parkland dedication fee of \$500 per 2.5 caliper inch tree not planted within each addition, totaling \$478,500 for the entire development.

16. All changes and modifications to the plans requested by the City Engineer in the Engineer's report dated, March 8, 2017 and June 1, 2017 shall be incorporated into the preliminary plat and PUD plans. *The developer has submitted updated preliminary plat and PUD plans that have been approved by the City.*
17. The Preliminary Plat approval is conditioned upon the applicant meeting all City standards and design requirements unless specifically addressed otherwise in this resolution. *It is a recommended condition of final plat approval that the developer meet all City standards and design requirements.*
18. The developer shall comply with all comments identified within the Washington County letter dated March 6, 2017, including providing an additional 17 feet of right-of-way along 10th Street N. and constructing turn lane improvements meeting Washington County standards at the developer's sole expense. *It is a recommended condition of final plat approval that the developer comply with these comments.*
19. That the preliminary PUD Plans be approved by Valley Branch Watershed District and that evidence be provided that all conditions attached to a Valley Branch Watershed District permit be provided prior to the commencement of any grading activity on the site. *The preliminary PUD Plans were approved by the Valley Branch Watershed District, and grading activity on the site has already commenced.*
20. That openspace within the shorelands of the residential development be protected with a conservation easement and that conservation easements be provided for review by the City Attorney and to be executed prior to final plat approval. *It is a recommended condition of final plat approval that the developer provide these easements. Outlots H, F, E, B, D, C, A, L are all within the shoreland. The City will need to put some sort of restrictive covenant on Outlots B. There is already a drainage and utility easement over Outlots A, E and H. It is a recommended condition of approval that the developer draft a restrictive covenant to be approved by the City for Outlots C, D, F, and the rest of Outlot L.*
21. That golf carts shall be prohibited on city streets and city trails unless specifically allowed by City Ordinance. *The City approved an ordinance which allows golf carts on local streets through the adoption of Ordinance 08-209 on May 15, 2018.*
22. Prior to the issuance of building permits all wetland buffers shall be delineated and identified via staking or signage that is acceptable to the City. *This is a recommended condition of final plat approval.*

23. The applicant must provide the city a letter of approval to perform the proposed work in the gas pipeline easement within the development area as a condition of preliminary plat approval. ***This condition is not applicable to 2nd Addition.***
24. Prior to the construction of any subdivision identification signs or neighborhood markers within the development, the developer shall submit sign plans for review and obtain a sign permit from the Planning Department. ***This is a recommended condition of final plat approval.***
25. That the preliminary plat be updated to include street names that are consistent with the City's street naming ordinance and approved by the City Council. ***This is a recommended condition of final plat approval.***
26. The developer shall follow all of the rules and regulations spelled out in the Wetland Conservation Act, and shall acquire the needed permits from the appropriate watershed districts prior to the commencement of any grading or development activity on the site. ***This is a recommended condition of final plat approval.***
27. The developer shall enter into a separate grading agreement with the City prior to the commencement of any grading activity in advance of final plat and PUD plan approval. The City Engineer shall review any grading plan that is submitted in advance of a final plat, and said plan shall document extent of any proposed grading on the site. ***This condition has been met.***
28. The developer shall submit a revised preliminary plat and plans meeting all conditions of approval. All of the above conditions shall be met prior to the City accepting an application for final plat. ***This condition has been met.***
29. That approval of the preliminary plat be contingent on Street D access to 10th Street. ***This condition has been met.***
30. That the Royal Golf Club development will not have street lights except at street intersections. ***This is a recommended condition of final plat approval.***
31. That the developer work with the adjacent property owners to get an agreement in place for screening and that it be incorporated into the final landscape plans. ***The developer has had landscape plans approved by the Homestead neighborhood, and the submitted landscape plans for final plat are consistent with this approved landscape plan.***
32. That the developer address the Public Works Directors concerns as they pertain to the lift station designs as identified in a Public Works memorandum dated 5/19/2017. ***This condition has been met.***
33. That a \$1,000,000 donation to the Parks fund will be made with the phase of development when the former Tartan Park ballfields are no longer able to be used by the City. ***The second phase is not located where these ball fields are located, though the third is. This has been added as a condition of final plat approval.***

The developer will be required to make this donation at any such time that the ballfields become unusable by the City (this would include storage of material, etc.).

Recommended Findings. Staff recommends the following findings:

1. That all the requirements of City Code Sections 153.07 and 154.759 related to Final Plat and Final PUD Plans have been met by the Applicant.
2. That the Royal Golf Club at Lake Elmo 2nd Addition Final Plat and PUD Plans are generally consistent with Preliminary Plat and PUD Plans approved by the City of Lake Elmo on June 6, 2017.
3. That the Royal Golf Club at Lake Elmo 2nd Addition Final Plat and PUD Plans are consistent with the Lake Elmo Comprehensive Plan and the Future Land Use Map for this area.
4. That the Royal Golf Club at Lake Elmo 2nd Addition Final Plat and PUD Plans generally comply with the City's GCC - Golf Course Community zoning districts as modified by the PUD regulations.
5. That the Royal Golf Club at Lake Elmo 2nd Addition Final Plat PUD Plans comply with the City's subdivision ordinance.
6. That the Royal Golf Club at Lake Elmo 2nd Addition Final Plat and PUD Plans comply with the City's Planned Unit Development Regulations.
7. That the Royal Golf Club at Lake Elmo 2nd Addition Final Plat and PUD Plans comply with City's Engineering Standards, except where noted in the review memorandum from the City Engineer dated May 30, 2018 and modified by PUD regulations.
8. That the Royal Golf Club at Lake Elmo 2nd Addition Final Plat and PUD Plans generally comply with other City zoning ordinances, shoreland, and erosion and sediment control, except as noted in this staff report and review memorandum from the City Engineer dated May 30, 2018.
9. That the Royal Golf Club at Lake Elmo 2nd Addition Final Plat and PUD Plans generally comply with the City's landscaping and tree preservation ordinances, providing some flexibility to the Applicant to allow for woodland management and pollinator friendly native seeding in lieu of some required tree replacement in order to avoid possible detriment caused by over planting.
10. That the Royal Golf Club at Lake Elmo 2nd Addition Final Plat and PUD Plans achieve multiple identified objectives for planned developments within Lake Elmo.
11. That the proposed Final Plat and PUD Plans are for a 64 unit single family residential golf course community Planned Unit Development on 89.91 acres of land located on the former 3M Tartan Park properties.
12. That the Final Plat and PUD Plans will be located on property legally described on the attached Exhibit "A".
13. That there has been significant public testimony that 20th Street is already dangerous without the additional traffic and that the City and developer need to explore ways to make the road safer.

14. That the proposed PUD will allow a more flexible, creative, and efficient approach to the use of the land, and will specifically relate to existing zoning district standards in the following manner (with exceptions as noted):

a. Setbacks:

i. Royal Golf Club at Lake Elmo Setbacks

15.	16. 100 Ft. Wide Lots	17. 80-90 Ft. Wide Lots	18. 55-65 Ft. Wide Lots
19. Front	20. 30 ft.	21. 30 ft.	22. 20 ft. for side loaded garages, or 25'
23. Side	24. 10 ft.	25. 10 ft.	26. 10ft. house/5ft. garage or 7.5 ft./7.5 ft.
27. Corner Side	28. 15 ft.	29. 15 ft.	30. 15 ft.
31. Rear	32. 30 ft.	33. 30 ft.	34. 20 ft.

- a. Maximum Impervious Coverage: The maximum impervious coverage for 55-65' wide lots shall be 50%. All other lots shall have a maximum impervious coverage of 40%.
- b. Lot Sizes: The minimum lot size for Villa lots (55-65' wide) in the development shall be 6,600 sq. ft.
- c. Attached Garages: That attached garages shall not exceed 1,300 sq. ft. in area at the ground floor level except by conditional use permit. The width of the visible garage door area when closed shall not exceed 60% of the principal building façade (including garage) fronting on the primary street.
- d. Subdivision Identification Signs: the Royal Golf Club at Lake Elmo residential subdivision shall be allowed up to a maximum of 4 subdivision identification signs, including the identification sign for the golf course entrance, not to exceed 24 sq. feet in sign area each, located no closer than 10 feet to any public right-of-way. In addition neighborhood identification markers (pillars) shall be permitted to be no larger than 2 ft. x 2 ft. to identify the development logo and the name of the neighborhood. Additional subdivision signs should be considered for a subdivision of this size.
- i. *Staff Note:* If the developer requests a change to this, a variance should be requested.

- e. All other requirements for the City's GCC – Golf Course Community zoning district will apply, including the allowed uses and other site and development standards.
- f. That the proposed street names within the development are generally consistent with the City's Street Naming Policy except for Annika Drive N.

Recommended Conditions of Approval. Staff recommends the following conditions of approval:

1. That there shall be no encroachments to drainage and utility easements on residential lots other than those reviewed and approved by the City Engineer and upon execution of an easement encroachment agreement.
2. Prior to the execution of Final Plat, the Developer shall enter into a Developer's Agreement acceptable to the City Attorney and approved by the City Council that delineates who is responsible for the design, construction, and payment of the required improvements with financial guarantees therefore.
3. The Royal Golf Club at Lake Elmo 2nd Addition shall be incorporated into the Common Interest Agreement concerning management of the common areas and establish a homeowner's association which shall be submitted in final form to the Planning Director before any building permit may be issued for any structure in any phase of the development. Said agreement shall comply with Minnesota Statutes 515B-103, and specifically the provisions concerning the transfer of control to the future property owners. The HOA documents shall include required maintenance of wetland buffers.
4. That the HOA documents include architectural requirements that require 4-sided architecture and garages facing the public right-of-ways to have windows and/or other architectural features.
5. The applicant shall enter into a landscape license and maintenance agreement with the City that clarifies the individuals or entities responsible for landscaping.
6. That the HOA be responsible for the maintenance of all landscape walls that cross residential property lines or publicly owned Outlots or rights-of-ways, and that the walls be contained within a landscape easement.
7. The developer shall provide for an HOA owned and maintained children's play structure or other similar improvement near the HOA fitness center as discussed by the developer during the PUD concept review prior to submittal of an application for Royal Golf Club at Lake Elmo 3rd Addition.
8. That a fee in lieu of park land dedication be paid to the City based upon an appraisal by an appraiser to be chosen by the City and paid for by the developer. Also that a 30 ft. wide trail corridor be dedicated to the City centered on public trails in the development and that the value of the dedicated land be credited toward any fees in lieu of parkland dedication. No credit shall be given for costs of construction of the trails. The park land dedication shall not include wetlands, wetland buffer area, and no credit will be given for land beyond the 30 ft. centered on public trails.

- a. Outlot B shall be dedicated to the City as parkland for trails as part of the 1st Addition.
9. Trail within Outlot B must be constructed prior to building permits being issued for that phase of development.
10. That the developer pay a parkland dedication fee equal to \$500 per 2.5 caliper inch required in lieu of some required tree preservation replacement tree requirements, totaling \$106,556.00 in lieu of planting the required number of trees required. This fee was calculated as follows: \$500.00 multiplied by 969 2.5-caliper inch tree not planted within the entire subdivision, divided by 291 single family lots within the entire subdivision, multiplied by 64 single family lots within the 2nd Addition.
11. All changes and modifications to the plans requested by the City Engineer in the Engineer's reports dated May 30, 2018 shall be incorporated into the Final Plat and PUD Plans.
12. The Final Plat and PUD Plans approval is conditioned upon the applicant meeting all City standards and design requirements unless specifically addressed otherwise in this resolution.
13. The developer shall comply with all comments identified within the Washington County letter dated March 6, 2017, including providing an additional 17 feet of right-of-way along 10th Street N. and constructing right turn lanes at its sole cost meeting City state aid design standards at Royal Boulevard North, Masters Lane North, the Golf Course entrance, at Annika Drive North, and at 20th Street and Manning Trail.
14. That open space within the shorelands of the residential development (specifically Outlots C, D, F, and the rest of Outlot L) be protected with a conservation easement, deed restriction, covenant, or other instrument. Such document(s) must be provided for review by the City Attorney and be executed prior to final plat approval.
15. Prior to the issuance of building permits all wetland buffers shall be delineated and identified via staking or signage that is acceptable to the City.
16. Prior to the construction of any subdivision identification signs or neighborhood markers within the development, the developer shall submit sign plans for review and obtain a sign permit from the Planning Department. Any amendments to the finding regarding signs indicated in this Resolution shall be subject to a PUD amendment or variance.
17. That the Final Plat include street names as approved by Council.
18. The developer shall follow all of the rules and regulations spelled out in the Wetland Conservation Act, and shall acquire the needed permits from the appropriate watershed districts prior to the commencement of any grading or development activity on the site.
19. That the Royal Golf Club development will not have street lights except at street intersections and cul-de-sacs.
20. That a \$1,000,000 donation to the Parks fund will be made with the phase of development when the former Tartan Park ballfields are no longer able to be used by the City.

EASEMENT VACATION

Reason. The applicant is also requesting an easement vacation over outlot R and a portion of outlots P and O of Royal Golf Club at Lake Elmo 1st Addition. These drainage and utility easements were dedicated to the City with the 1st Addition Final Plat. These outlots will be re-platted with 2nd Addition, and new easements of the same size or larger will be dedicated with the 2nd Addition. In order to avoid “stacking easements” and to keep the title clean for future owners/users of the parcels, the applicant is requesting that the easements from 1st Addition be vacated with dedication of new easements. Outlot R will be dedicated right-of-way for Palmer Drive, and a new easement will be dedicated over what will now be Outlot M of the 2nd Addition. Outlot M of the 1st Addition is owned by Emerson and will not have an easement over it.

Recommended Condition of Approval. Staff is recommending approval, subject to the following condition of approval:

1. New easements as requested by the City Engineer and Public Works Director shall be recorded with the Royal Golf Club at Lake Elmo 2nd Addition Final Plat.

RECOMMENDATION:

Staff recommends that the Planning Commission recommend approval of the Royal Golf Club at Lake Elmo 2nd Addition Final Plat and PUD Plans, as well as the easement vacation request, based on the findings of fact and conditions outlined in the Staff Report.

“Move to recommend approval of the Royal Golf Club at Lake Elmo 2nd Addition Final Plat and PUD Plans based on the findings of fact and conditions outlined in the Staff Report.”

“Move to recommend approval of the easement vacation request to vacate existing drainage and utility easements over Outlots R, P and O, subject to condition of approval.”

ORDER OF BUSINESS:

- Introduction..... Planner
- Report by Staff..... Planner
- Questions from the Commission..... Chair & Commission Members
- Questions to the Applicant..... Chair & Commission Members
- Open the Public Hearing.....Chair
- Close the Public HearingChair
- Discussion by the Commission..... Chair & Commission Members
- Action by the Commission..... Chair & Commission

ATTACHMENTS:

1. Narrative
2. Engineer's Memo
3. 2nd Addition Landscaping Plans
4. 2nd Addition Final Plat
5. Easement Vacation



May 3, 2018

Ms. Emily Becker

Director of Planning City of Lake Elmo

3800 Laverne Ave. N. Lake Elmo, MN 55042

Re: Final Plat/P.U.D. Narrative -The Royal Golf Club at Lake Elmo

Dear Ms. Emily Becker,

H.C. Golf Land, LLC and Royal Development is pleased to submit herewith our application for final plat and planned unit development approval for Phase II at The Royal Club at Lake Elmo. H.C. Golf Land and Royal Development, Inc. would like to extend their sincere thanks to the City of Lake Elmo, its City Council, its Advisory Committees and its staff and consultants for their direction, cooperation and assistance in moving the RGC to this juncture. The following narrative will set out the basic elements of the plat, address those information items requested in the application forms and those items which will need collaboration between us as we move this plat to final acceptance.

Existing Zoning

The property has been zoned GCC (Golf Course Community) by the City of Lake Elmo.

Plat Area/Ownership/Description

The property, containing 428 acres, is owned by H.C. Golf Land, LLC and is comprised of ten parcels. The plat boundary is consistent with the preliminary plat as approved by Council Resolution 2017-047. The property has been divided into lots, blocks and outlots for the development of our Phase II, with additional outlots reserved for future phases of development.

Residential -Contained in Blocks 8, 9, 10, 11, 12, and 13 are 64 residential home site lots consisting of 15 traditional single family detached lots and 49 -"Villa" lots for detached single-



family townhomes which will have Villa Association -maintained lawns and exteriors as further noted below.

Outlets -The final plat contains a total of twenty outlets designated as Outlets A -T.

Residential Setbacks

The setbacks for the residential lots are consistent with the approved preliminary plat.

Dedicated Rights of Way

Within the plat, RGC is dedicating rights of way to the public in the amount of 19 .30 acres for 20th Street N., 10th Street N., Lake Elmo Ave., Manning Trail and our new residential streets.

Density

Consistent with the approved preliminary plat, the net residential density of The Royal Club is calculated as 291 approved units and 187 net residential acres yielding a density of 1.56 units/net acre.

Addressing Neighboring Homes

Regarding our request to convert single family lots into villa lots, we went door to door to all the Homestead and Tartan Meadow homes that bordered the golf course. We explained what we were changing and how it would benefit them. Those that were not home, we left information informing them of two meeting times and dates at the Clubhouse they could attend to get information about the lot changes in Phase II. We informed them that we are converting the single-family lots that boarded them into villa lots. With the villa lots, there would be one story homes versus two story homes which would be less visible. Being villa lots they would not have swimming pools, sport courts, etc. which would reduce the noise. Also being villas, all the laws would be mowed at the same time during the week and not at various times all week long and on weekends. In all, the villa provides a quieter less visible neighbor. With many being empty nesters, *snow birds*, it would get even quieter in the winter.



Development Phasing

We are proposing to develop RGC in four phases. with our 64 lots denoted as Phase II.

Development Schedule

Site grading for Phase II is complete. We hope for an approval soon that will allow us to install the watermain and sewer for Phase II. Based on a mid-June approval, water, sewer, services, and streets should be completed by mid-September.

As to the golf facilities, we have finished the reconstruction of the 18-hole championship golf course, the practice range, short course, and renovations of the old 3M clubhouse. The clubhouse has been fully renovated for both golf and as an event venue.

Trail Phasing

Consistent with preliminary plat, RGC will be constructing and dedicating well over 10,000 lineal feet of paved trail during its development.

Community Impacts

Because of the size, complexity and scope of The Royal Club at Lake Elmo, the project triggered the need to complete an Environmental Assessment Worksheet (EAW) to review the impacts to traffic, public facilities, public safety and environmental issues. The EA W was completed in October of 2016 for public review. At its January 17, 2017 meeting, the City Council adopted Resolution 2017-007 issuing a negative declaration on any further environmental study and accepting the findings of fact on the EAW. We believe community impacts have been fully reviewed. Additionally, all wetlands on the property have been dealt with pursuant to Wetland Conservation Act and VBWD rules and regulations and all activities thereon have received appropriate permits.

Phase II Streets and Utilities

Phase II street and utility construction will service the Phase II residential lots.



Home Styles

The Royal Golf Club community will feature a wide variety of high quality, custom-built homes in a variety of complimentary styles.

Declarations of Protective Covenants

Because of the variety of home styles and privately held common areas at RGC, we have created a Master Homeowners Association ("MHOA") whose membership will include all residential Phase II lots and all future phase residential lots. Its responsibilities will include such things as maintenance of private trails/ common area landscape, open space and entry signage in addition to the maintenance of the future fitness/recreation center and the maintenance of property line-crossing retaining walls. The MHOA declaration will also contain the architectural standards consistent with preliminary plat conditions. We will also form a sub association for each of the Villa Home (Villa HOA'S) neighborhoods. These Villa HOA's will have responsibility for lawn/driveway and open space maintenance and the maintenance of the Villa home exteriors to assure a consistency in neighborhood appearance.

Lastly, the MHOA will assume all architectural review responsibilities upon turnover of the MHOA from developer control to resident control consistent with the terms of Minnesota statutes.

Landscape

As a part of the Phase II development agreement, RGC and the City will enter into an agreement wherein RGC assumes planting responsibilities for trees required by the City as proposed in the landscape plan.

Park Fees

The development portion of RGC is 231.25 acres with a net buildable are of 186.92 acres. The appraised value of this property, as determined by an appraisal commissioned by the city, is \$8,000,000. Per City of Lake Elmo policy, a park dedication of 10.0% or fee in lieu of land dedication of 10% of the value is required with development. RGC and the city jointly have decided to not have city-owned parks within the development, thus obligating RGC for cash payments totaling \$800,000.00 over the span of the development phases. RGC will receive credit



against each phase's park fee for constructing trails and appurtenant items such as turf establishment, retaining walls and ADA pedestrian ramps along with the dedication of the land for a 30- foot wide trail corridor. The RGC will provide the city with support calculations to determine the actual cash fee in Phase II to be paid with this application and in future phases as those phases are platted.

Conservation/Ponding Maintenance Agreement

RGC and the City have determined that there will be no City-owned outlots in the Shoreland district in Phase II and have, thus, drafted a Storm Water Maintenance Agreement setting out the MHOA's and RGC's obligations to maintain storm water ponding facilities constructed in Phase II on Outlots Q and V. The easement agreement will be delivered separately to the City. In future phases should City outlots lie within shoreland districts, conservation easements will be entered into.

Easement Vacation

We are proposing to vacate the existing drainage and utility easements, as created and dedicated on the 1st Addition plat, lying over Outlots O, P, and R (see attached for details).

With our 2nd Addition submittal, we will be re-platting all 3 of these Outlots. And, in doing so, will be recreating new drainage and utility easements over the new parcels. The easement vacation is needed to avoid creating and 'stacking' easements on top of easements. The new easements will be the same size (or larger in some cases) as the vacated easements. Thus, the City does not lose any drainage and utility easement areas. We also need to do this to keep the title clean for future owners/users of the parcels.



Our pledge to the City is to create a premium golf community with a national reputation of which all of Lake Elmo will be proud. We stand prepared to provide you with any further items you deem necessary as the development agreement and PUD take form and as we move forward with development activity.

Sincerely,

The Royal Golf Club at Lake Elmo By H.C. Golf Land, LLC

By:

Its:

The Royal Club

Owner:

H.C. Golf Land, LLC
11074 Radisson Rd N.E.
Blaine, MN 55449
Attn: Hollis Cavner and Joel LeVahn

Development:

James Felten
President
The Royal Club
651-414-1948
jfelten@royalclubmn.com

Contact Representative: Amanda Furlong

Ph: 651-505-9079
Email: afurlong@royalclubmn.com

Civil Engineer:

Brian Krystofiak P.E.
Carlson McCain Engineers and Surveyors
3890 Pheasant Ridge Dr., #100
Blaine, MN 55449
763-489-7905
bkrystofiak@carlsonmccain.com

Surveyor:

Thomas Balluff, R.L.S.
Carlson McCain Engineers and Surveyors
3890 Pheasant Ridge Dr., #100
Blaine, MN 55449
763-489-7916
tballuff@carlsonmccain.com

Outlot O, THE ROYAL GOLF CLUB AT LAKE ELMO
Address is unassigned
PID#:25-029-21-43-0003

Outlot P, THE ROYAL GOLF CLUB AT LAKE ELMO
Address is unassigned
PID#:25-029-21-43-0004

Outlot R, THE ROYAL GOLF CLUB AT LAKE ELMO
Address is unassigned
PID#:25-029-21-43-0005

Outlot S, THE ROYAL GOLF CLUB AT LAKE ELMO
Address is unassigned
PID#:25-029-21-31-0002

Phase II

<u>Block 7</u>	<u>Sq.Ft.</u>	<u>Acres</u>	<u>Lot Width</u>	<u>Lot Depth</u>
Lot 1	17,817	0.409	95.2	165.1
Lot 2	16,302	0.374	96.0	150.3
Lot 3	16,366	0.376	100.3	150.2
Lot 4	16,031	0.368	100.0	150.0
Lot 5	15,000	0.344	100.0	150.0
Lot 6	15,000	0.344	100.0	150.0
Lot 7	17,317	0.398	99.5	152.0
Lot 8	18,011	0.413	100.1	153.0
Lot 9	17,548	0.403	105.2	141.3
Lot 10	15,692	0.360	117.7	140.4
Lot 11	15,694	0.360	129.8	130.3

<u>Block 8</u>	<u>Sq.Ft.</u>	<u>Acres</u>	<u>Lot Width</u>	<u>Lot Depth</u>
Lot 1	9,636	0.022	75.1	177.9
Lot 2	9,093	0.021	76.8	150.8
Lot 3	8,574	0.020	87.8	139.2
Lot 4	9,604	0.022	84.4	127.7

<u>Block 9</u>	<u>Sq.Ft.</u>	<u>Acres</u>	<u>Lot Width</u>	<u>Lot Depth</u>
Lot 1	12,393	0.285	73.2	250.9
Lot 2	7,210	0.166	61.9	132.1
Lot 3	8,210	0.188	66.5	104.2
Lot 4	9,425	0.216	65.6	101.8
Lot 5	10,226	0.235	64.1	122.8
Lot 6	9,640	0.221	63.7	136.3
Lot 7	10,711	0.246	56.3	131.8
Lot 8	11,122	0.255	66.6	129.0
Lot 9	10,236	0.235	76.9	129.0
Lot 10	9,544	0.219	75.5	136.5
Lot 11	8,699	0.200	70.2	143.2
Lot 12	8,579	0.197	69.6	140.1
Lot 13	11,385	0.261	79.5	138.8

<u>Block 10</u>	<u>Sq.Ft.</u>	<u>Acres</u>	<u>Lot Width</u>	<u>Lot Depth</u>
Lot 1	9,806	0.225	64.3	150.1
Lot 2	11,827	0.272	62.6	147.0
Lot 3	12,930	0.297	62.7	152.6
Lot 4	11,041	0.253	62.2	154.2
Lot 5	10,756	0.247	62.9	136.1
Lot 6	9,673	0.222	66.4	148.8
Lot 7	9,482	0.218	66.9	146.0
Lot 8	9,387	0.215	67.2	142.8

Lot 9	10,474	0.240	62.1	144.3
Lot 10	15,769	0.362	61.2	162.7
Lot 11	13,931	0.320	62.3	165.5
Lot 12	11,875	0.273	61.8	152.2
Lot 13	13,012	0.299	61.6	143.2
Lot 14	10,984	0.252	65.4	153.0
Lot 15	9,876	0.227	65.4	151.1
Lot 16	9,721	0.223	66.5	151.8
Lot 17	9,370	0.215	68.7	146.3
Lot 18	10,481	0.241	77.1	143.8
Lot 19	13,485	0.310	68.3	142.5
Lot 20	11,297	0.259	64.4	142.2
Lot 21	10,746	0.247	79.0	139.4

<u>Block 11</u>	<u>Sq.Ft.</u>	<u>Acres</u>	<u>Lot Width</u>	<u>Lot Depth</u>
Lot 1	17,382	0.399	110.7	160.5
Lot 2	21,580	0.495	90.7	227.0
Lot 3	15,658	0.359	116.7	156.6
Lot 4	15,648	0.359	124.1	194.2
Lot 5	14,065	0.323	102.4	165.7
Lot 6	11,646	0.267	96.7	139.0
Lot 7	11,159	0.256	80.9	145.6
Lot 8	14,624	0.336	111.3	180.3
Lot 9	16,776	0.385	91.9	211.4
Lot 10	12,916	0.297	80.5	174.4
Lot 11	15,900	0.365	108.9	149.9

<u>Block 12</u>	<u>Sq.Ft.</u>	<u>Acres</u>	<u>Lot Width</u>	<u>Lot Depth</u>
Lot 1	16,629	0.382	95.3	165.8

<u>Block 13</u>	<u>Sq.Ft.</u>	<u>Acres</u>	<u>Lot Width</u>	<u>Lot Depth</u>
Lot 37	17,493	0.402	145.8	150.1
Lot 38	17,048	0.391	134.1	153.1
Lot 39	16,917	0.388	117.3	151.1

The Royal Golf Club



HC Golf Course Development, LLC

MEMORANDUM

FOCUS ENGINEERING, inc.

Cara Geheren, P.E.	651.300.4261
Jack Griffin, P.E.	651.300.4264
Ryan Stempski, P.E.	651.300.4267
Chad Isakson, P.E.	651.300.4283

Date: May 30, 2018

To: Emily Becker, Planning Director
Cc: Chad Isakson, P.E., Assistant City Engineer
From: Jack Griffin, P.E., City Engineer

Re: Royal Golf Club 2nd Addition – Final Plat
Engineering Review Comments

An engineering review has been completed for the Royal Golf Club 2nd Addition. Final Plat/Final Construction Plans were received on May 4, 2018. The submittal consisted of the following documentation:

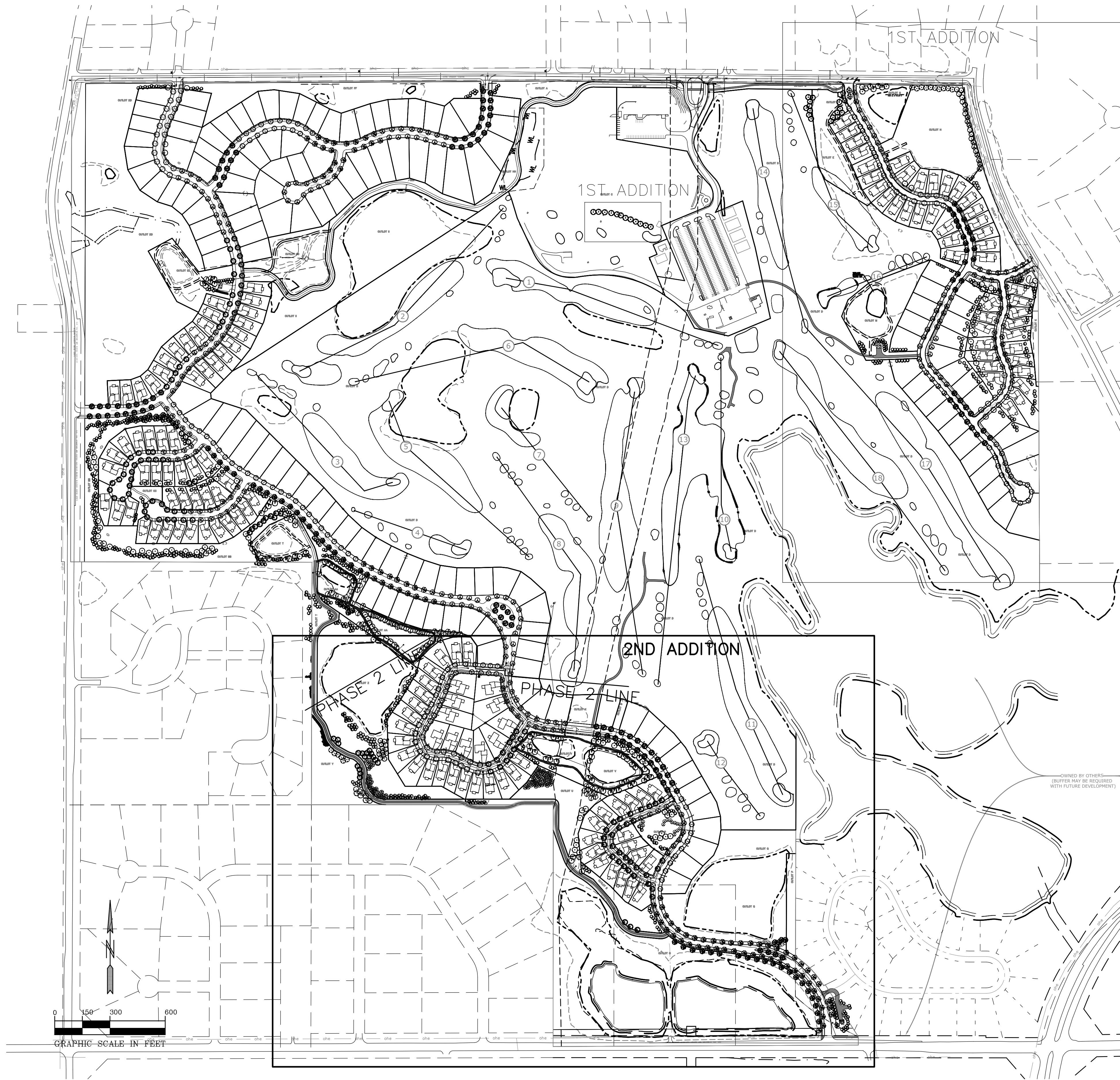
- Royal Golf Club 2nd Addition Final Plat, Sheets 1-3, received May 1, 2018. No print/preparation date.
- Royal Golf Club 2nd Addition Narrative, dated May 3, 2018.
- Royal Golf Club 2nd Addition Construction Plans, Sheets 1-27, dated April 26, 2018.
- Royal Golf Club 2nd Addition Grading Plans, Sheets 1-21, dated April 26, 2018.
- Royal Golf Club 2nd Addition Landscape Plans, L1-L8, dated May 2, 2018, by Pioneer Engineering.
- Royal Golf Club 2nd Addition Specifications, dated April 27, 2018.

STATUS/FINDINGS: Engineering review comments have been provided in two separate memos; one for Final Plat approval, and one to assist with the completion of the final Construction Plans. Please see the following review comments relating to the Final Plat application.

FINAL PLAT: ROYAL GOLF CLUB 2ND ADDITION

- Outlots B (trail), F (storm water), I (storm water), and K (utility corridor) must be dedicated to the City as part of the Final Plat. City ownership of these Outlots has been noted on the final grading and construction plans as required.
- Outlots A, C, D, E, G, H, J, L, and M ownership have been shown on the final grading and construction plans as developer or HOA owned as applicable.
- Drainage and utility easements have been provided on the Final Plat and grading plans over all, or a portion, of Outlots A, D, E, H, and L as required. These easements must be shown and labeled on the street and utility plans.
- Revise Outlot D easement to provide minimum 15 ft. easement from pipe centerline for storm sewer run CBMH-359 to CBMH 360.
- Verify that the 10-foot utility easement corridor has been provided along Palmer Drive at Outlot Q, 1st Addition. If not, provide this easement as part of the 2nd Addition.
- Verify that the 10-foot utility easement corridor has been provided along Palmer Drive at Outlot L and M, 2nd Addition. If not, provide these easements as part of the 2nd Addition.
- Clarify the drainage and utility easement limits for Outlot L.
- Final Construction Plans and Specifications must be prepared in accordance with the City Engineering Design Standards Manual dated March 2017, using City details, plan notes and specifications and meeting City Engineering Design Guidelines.

- Final Construction Plans and Specifications must be revised in accordance with the Construction Plan engineering review memorandum dated May 30, 2018.
- All easements as requested by the City Engineer and Public Works department shall be documented on the Final Plat prior to the release of the Final Plat for recording.
- The Final Plat shall not be recorded until final construction plan approval is granted.
- No construction for Royal Golf Club 2nd Addition may begin until the applicant has received City Engineer approval for the Final Construction Plans; the applicant has obtained and submitted to the City all applicable permits, easements and permissions needed for the project; and a preconstruction meeting has been held by the City's engineering department.



LANDSCAPE DATA:

SEE PRELIMINARY PLAT FOR LANDSCAPE AND MITIGATION REQUIREMENTS.

TREES APPROVED ON PRELIMINARY PLAT TO BE PLANTED BY DEVELOPER: 1,943
TREES APPROVED ON PRELIMINARY PLAT TO BE PLANTED BY BUILDER/HOMEOWNER: 888
TOTAL TREES REQUIRED ON PRELIMINARY PLAT: 2,831

TREES PROPOSED IN 1ST ADDITION TO BE PLANTED BY DEVELOPER: 419
TREES PROPOSED IN 1ST ADDITION TO BE PLANTED BY BUILDER/HOMEOWNER: 202
TOTAL TREES TO BE PLANTED IN 1ST ADDITION: 621

TREES PROPOSED IN 2ND ADDITION TO BE PLANTED BY DEVELOPER: 632
TREES PROPOSED IN 2ND ADDITION TO BE PLANTED BY BUILDER/HOMEOWNER: 158
TOTAL TREES TO BE PLANTED IN 2ND ADDITION: 790

TREES BY BUILDER TO BE CHOSEN FROM PLANT SCHEDULE BELOW. BUILDER-SUPPLIED TREES MUST BE A MINIMUM OF TWO DIFFERENT SPECIES PER LOT.

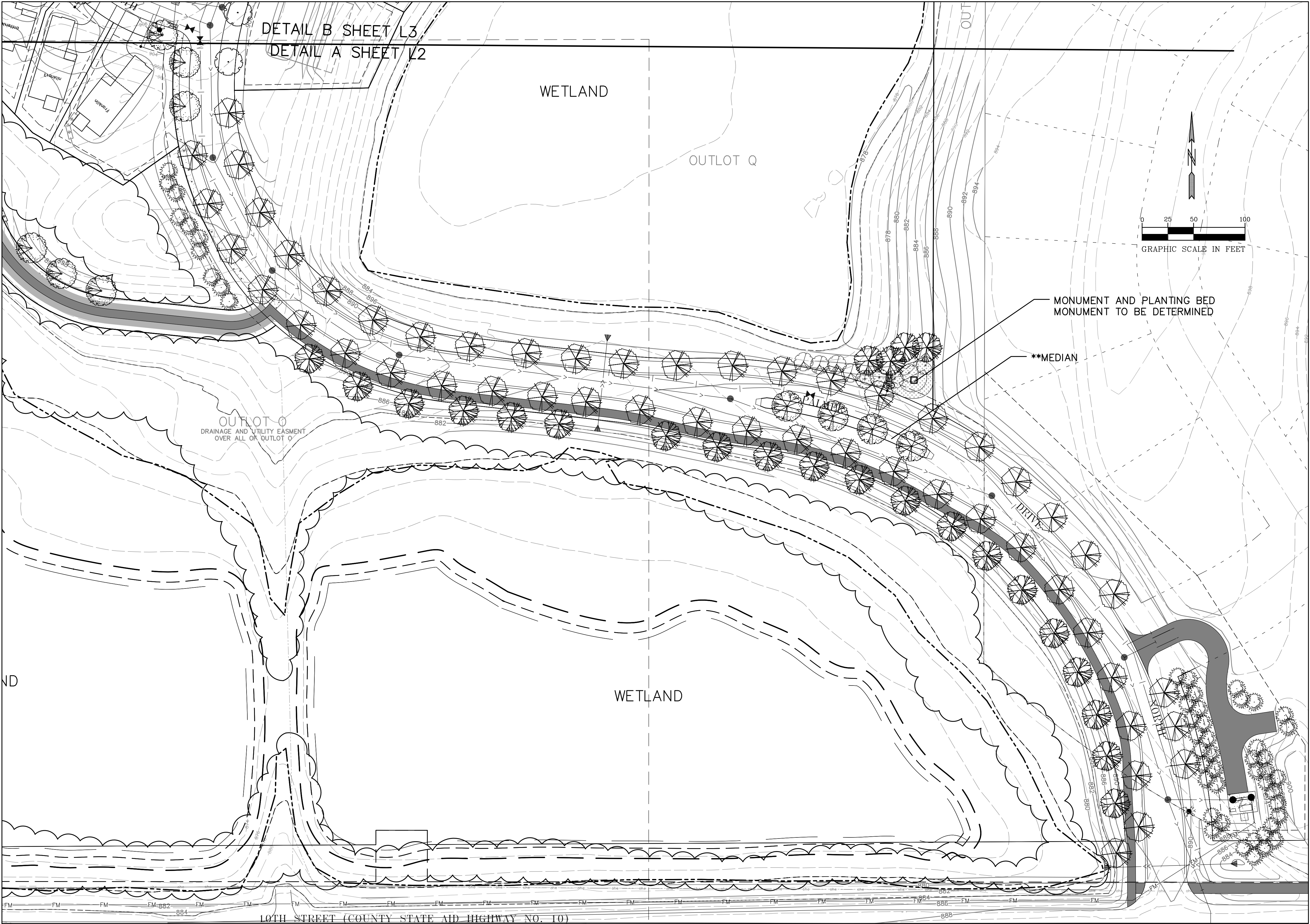
LANDSCAPE ORDINANCE REQUIREMENTS FOR 2ND ADDITION:
1 TREE/50' FRONTAGE. 10,163/50=204 TREES
5 TREES PER DEVELOPED/DISTURBED ACRE. 38 DISTURBED ACRES X 5 = 190 TREES
2ND ADDITION TREES REQUIRED FOR LANDSCAPE REQUIREMENT: 394

TREE TOTALS FOR 2ND ADDITION

PLANT SCHEDULE

KEY	COMMON NAME/Scientific name	ROOT	QUANTITY	
OVERSTORY TREES				
	NORTHWOODS RED MAPLE/Acer rubrum 'Northwoods'	2.5" B&B	30	
	AUTUMN BLAZE MAPLE/Acer x freemanii 'Jeffersred'	2.5" B&B	33	
	SIENNA GLEN MAPLE/Acer x freemanii 'Sienna'	2.5" B&B	32	
	SUGAR MAPLE/Acer Saccarum 'Green Mountain'	2.5" B&B	29	
	HERITAGE RIVER BIRCH/Betula nigra 'Cully'	12" B&B	30	
	WHITESPIRE BIRCH/Betula populifolia 'Whitespire'	12" B&B	34	
	COMMON HACKBERRY/Celtis occidentalis	2.5" B&B	40	
	KENTUCKY COFFEE TREE/Gymnocladus dioicus	2.5" B&B	19	Use male/seedless var. True North, Espresso
	RED OAK/Quercus rubra	2.5" B&B	24	
	NORTHERN PIN OAK/Quercus ellipsoidalis	2.5" B&B	52	
	SWAMP WHITE OAK/Quercus bicolor	2.5" B&B	17	
	PRINCETON ELM/Ulmus americana 'Princeton'	2.5" B&B	11	
	GINGKO/Ginkgo biloba (male only)	2.5" B&B	6	
	CATALPA/Catalpa speciosa	2.5" B&B	--	
	QUAKING ASPEN/Populus tremuloides	2.5" B&B	68	
	SPADED MAPLE AND OAK TRANSPLANTS	6" MM	--	ONLY 1ST ADDITION
EVERGREEN TREES				
	BLACK HILLS SPRUCE/Picea glauca densata	6" B&B	87	
	WHITE PINE/Pinus strobus	6" B&B	75	
	RED PINE/Pinus resinosa	6" B&B	45	
ORNAMENTAL TREES*				
	PRAIRIFIRE CRAB/Malus 'Prairifire'	2" B&B	19	
	JAPANESE TREE LILAC/Syringa reticulata	8" B&B	5	
	SPRING SNOW CRAB/Malux 'Spring Snow'	2" B&B	--	

DETAIL A



- LANDSCAPE NOTES:
- 1. LOTS WILL BE SODDED AFTER CONSTRUCTION.
 - 2. SEE SHEET L1 FOR LANDSCAPE REQUIREMENT DATA
 - 3. TREES WITHIN BOULEVARDS WITH SIDEWALKS WILL BE PLANTED 5' FROM CURB
 - 4. TREES WITHIN BOULEVARDS WITHOUT SIDEWALKS WILL BE PLANTED 8' FROM CURB
 - 5. SEE CITY DOCUMENTS 32 90 00 "PLANTING SPECIFICATION" AND 3292 "TURF ESTABLISHMENT" FOR MORE REQUIREMENTS. THESE DOCUMENTS CAN BE OBTAINED FROM THE CITY OR THE PROJECT LANDSCAPE ARCHITECT.
 - 6. SEE SHEET L8 FOR CITY PLANTING DETAIL PLATES.

**MEDIAN NOTES:
MEDIAN TO HAVE BUFF LIMESTONE RIP RAP IN VARYING SIZES FROM 4-12" LAID OVER FIBER MAT WEED BARRIER. ROCK TO BE 2" BELOW EDGE OF CURB.

STANDARD PLAN NOTES
LANDSCAPE PLANS

- 1. Contractor must contact Gopher State One Call 811 (www.gopherstateonecall.org) prior to tree staking and planting operations to verify underground utilities. Where private utilities exist on-site the Contractor is required to have those located as well.
- 2. Plant materials shall meet American Standard for Nursery Stock: ANSI Z60.1, latest edition.
- 3. No plant substitutions shall be made without the prior written authorization from the City.
- 4. All tree locations to be field staked prior to installation. Contractor to coordinate field review of proposed tree locations with the City and Project Landscape Architect prior to any tree installation.
- 5. All plants shall be planted immediately upon arrival to project site. No plant material is to be left overnight on the project site without being installed unless written approval by City.
- 6. All trees, shrubs, perennials, turf lawn and native seeding to have a two-year warranty beginning upon written acceptance by the City. Defective plants as determined by the City shall be replaced within 30 days of notice during the growing season, and replacement materials shall receive the same two year warranty until plants are successfully established.
- 7. Contractor to protect and maintain all plantings and plant beds, including protection from wildlife, weeding, re-mulching, fertilization, irrigation and all other typical forms of horticultural care until the end of the warranty period as determined and approved by City.
- 8. All plants installed and maintained on City property shall be in accordance with City of Lake Elmo Bee-Safe Resolution No. 2015-13.
- 9. An irrigation system or other water supply adequate to support the specified plant materials shall be provided.

APRIL 2017



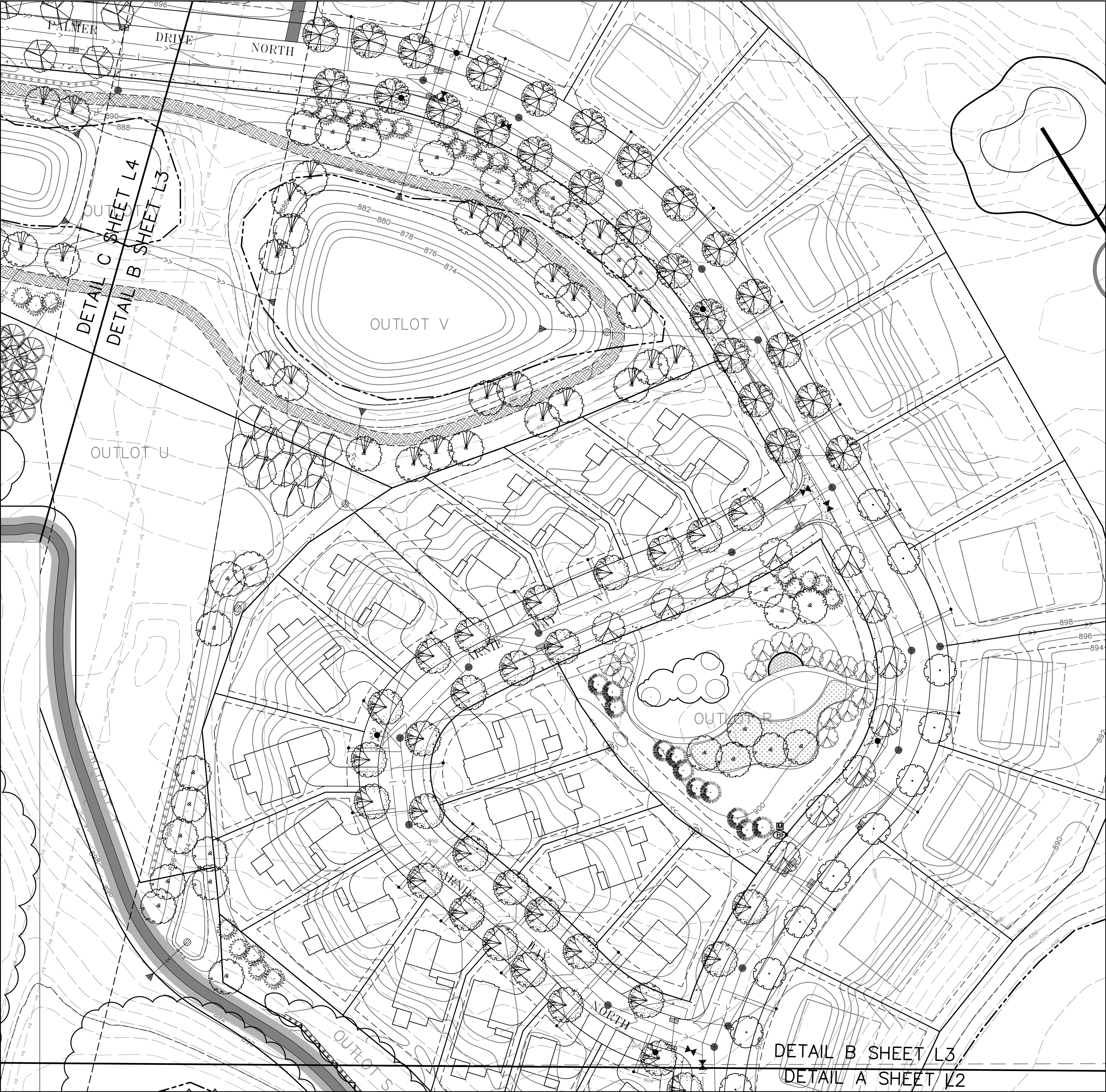
CITY OF LAKE ELMO

STANDARD DRAWING NO.
LP2.0
LAKE ELMO

TREE TOTALS FOR 2ND ADDITION
PLANT SCHEDULE

KEY	COMMON NAME/Scientific name	ROOT	QUANTITY	
OVERSTORY TREES				
	NORTHWOODS RED MAPLE/Acer rubrum 'Northwoods'	2.5" B&B	30	
	AUTUMN BLAZE MAPLE/Acer x freemanii 'Jeffersred'	2.5" B&B	33	
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	HERITAGE RIVER BIRCH/Betula nigra 'Cully'	12" B&B	30	
	WHITESPIRE BIRCH/Betula populifolia 'Whitespire'	12" B&B	34	
	COMMON HACKBERRY/Celtis occidentalis	2.5" B&B	40	
	KENTUCKY COFFEE TREE/Gymnocladus dioicus	2.5" B&B	19	Use male/seedless var. True North, Espresso
	RED OAK/Quercus rubra	2.5" B&B	24	
	NORTHERN PIN OAK/Quercus ellipsoidalis	2.5" B&B	52	
	SWAMP WHITE OAK/Quercus bicolor	2.5" B&B	17	
	PRINCETON ELM/Ulmus americana 'Princeton'	2.5" B&B	11	
	GINGKO/Ginko biloba (male only)	2.5" B&B	6	
	CATALPA/Catalpa speciosa	2.5" B&B	--	
	QUAKING ASPEN/Populus tremuloides	2.5" B&B	68	
	SPADED MAPLE AND OAK TRANSPLANTS	6" MM	--	ONLY 1ST ADDITION
EVERGREEN TREES				
	BLACK HILLS SPRUCE/Picea glauca densata	6" B&B	87	
	WHITE PINE/Pinus strobus	6" B&B	75	
	RED PINE/Pinus resinosa	6" B&B	45	
ORNAMENTAL TREES*				
	PRAIRIFIRE CRAB/Malus 'Prairifire'	2" B&B	19	
	JAPANESE TREE LILAC/Syringa reticulata	8" B&B	5	
	SPRING SNOW CRAB/Malux 'Spring Snow'	2" B&B	--	

DETAIL B



- LANDSCAPE NOTES:
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 2. SEE SHEET L1 FOR LANDSCAPE REQUIREMENT DATA
 3. TREES WITHIN BOULEVARDS WITH SIDEWALKS WILL BE PLANTED 5' FROM CURB
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 5. SEE CITY DOCUMENTS 32 90 00 "PLANTING SPECIFICATION" AND 3292 "TURF ESTABLISHMENT" FOR MORE REQUIREMENTS. THESE DOCUMENTS CAN BE OBTAINED FROM THE CITY OR THE PROJECT LANDSCAPE ARCHITECT.
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www.pioneereng.com

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the State of Minnesota

Name: Jennifer L. Thompson
Reg. No. 44763 Date 5-2-18

Revisions

Date: 5-2-18
Designed: JLT
Drawn: JLT

LANDSCAPE PLAN

HC GOLF COURSE DEVELOPMENT, LLC
11074 RADISSON ROAD NE
BLAINE, MINNESOTA 55449

THE ROYAL GOLF CLUB AT LAKE ELMO 2ND ADD
LAKE ELMO, MINNESOTA

L3 OF 8

TREE TOTALS FOR 2ND ADDITION

PLANT SCHEDULE

KEY	COMMON NAME/Scientific name	ROOT	QUANTITY	
OVERSTORY TREES				
	NORTHWOODS RED MAPLE/Acer rubrum 'Northwoods'	2.5" B&B	30	
	AUTUMN BLAZE MAPLE/Acer x freemanii 'Jeffersred'	2.5" B&B	33	
	SIENNA GLEN MAPLE/Acer x freemanii 'Sienna'	2.5" B&B	32	
	SUGAR MAPLE/Acer Saccarum 'Green Mountain'	2.5" B&B	29	
	HERITAGE RIVER BIRCH/Betula nigra 'Cully'	12' B&B	30	
	WHITESPIRE BIRCH/Betula populifolia 'Whitespire'	12' B&B	34	
	COMMON HACKBERRY/Celtis occidentalis	2.5" B&B	40	
	KENTUCKY COFFEE TREE/Gymnocladus dioicus	2.5" B&B	19	Use male/seedless var. True North, Espresso
	RED OAK/Quercus rubra	2.5" B&B	24	
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	SPADED MAPLE AND OAK TRANSPLANTS	6" MM	--	ONLY 1ST ADDITION
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	BLACK HILLS SPRUCE/Picea glauca densata	6' B&B	87	
	WHITE PINE/Pinus strobus	6' B&B	75	
	RED PINE/Pinus resinosa	6' B&B	45	
ORNAMENTAL TREES*				
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LANDSCAPE PLANS

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APRIL 2017



CITY OF LAKE ELMO

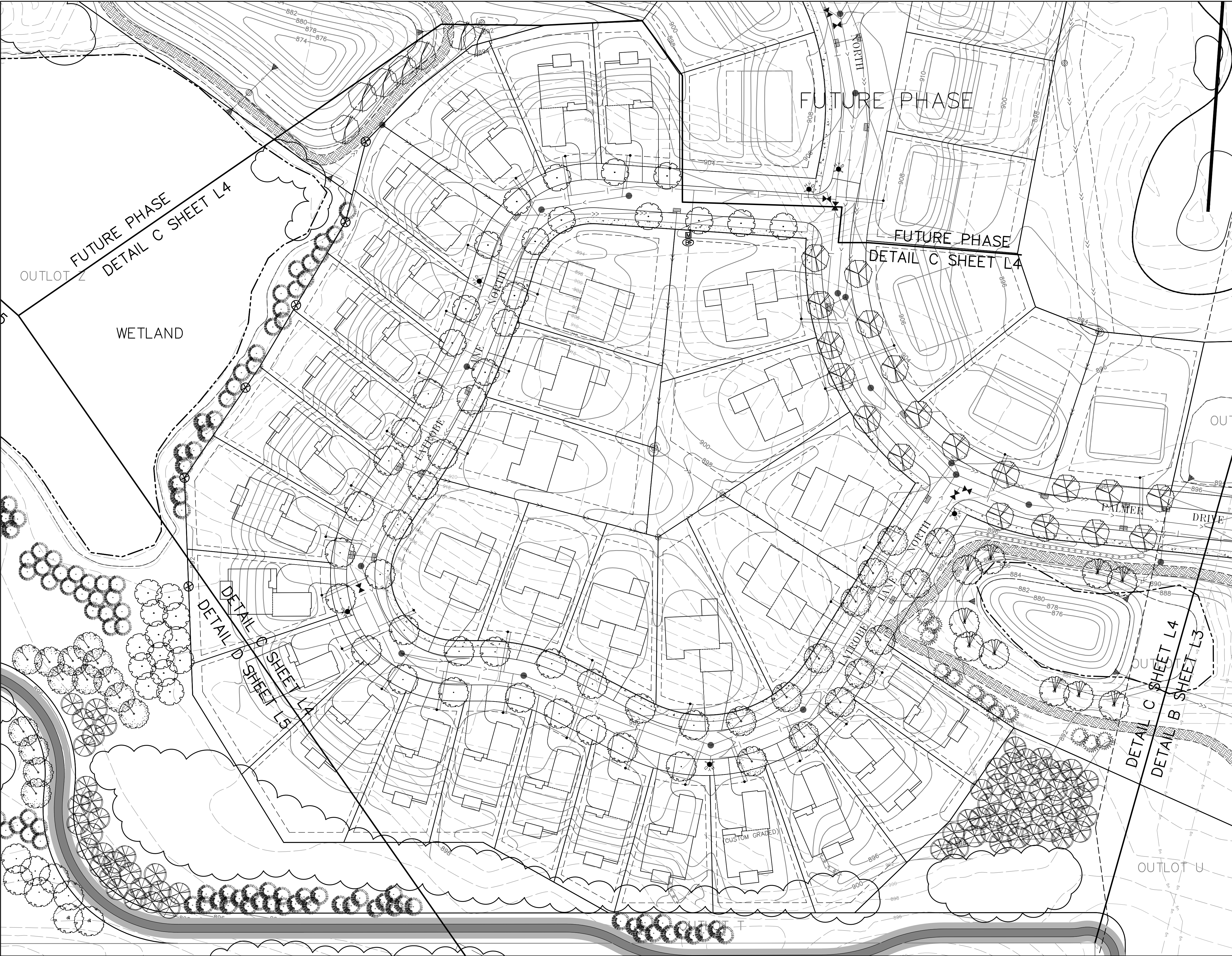
STANDARD DRAWING NO.

LP2.0

LAKE ELMO

116240

DETAIL C



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THE ROYAL GOLF CLUB AT LAKE ELMO 2ND ADD
LAKE ELMO, MINNESOTA

L4 OF 8

TREE TOTALS FOR 2ND ADDITION

PLANT SCHEDULE

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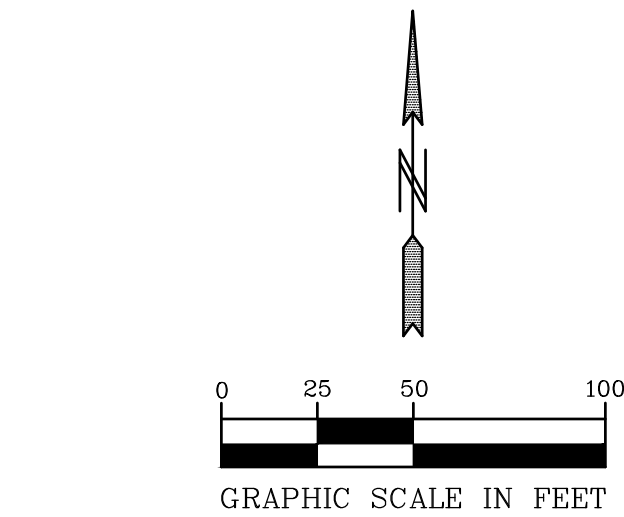
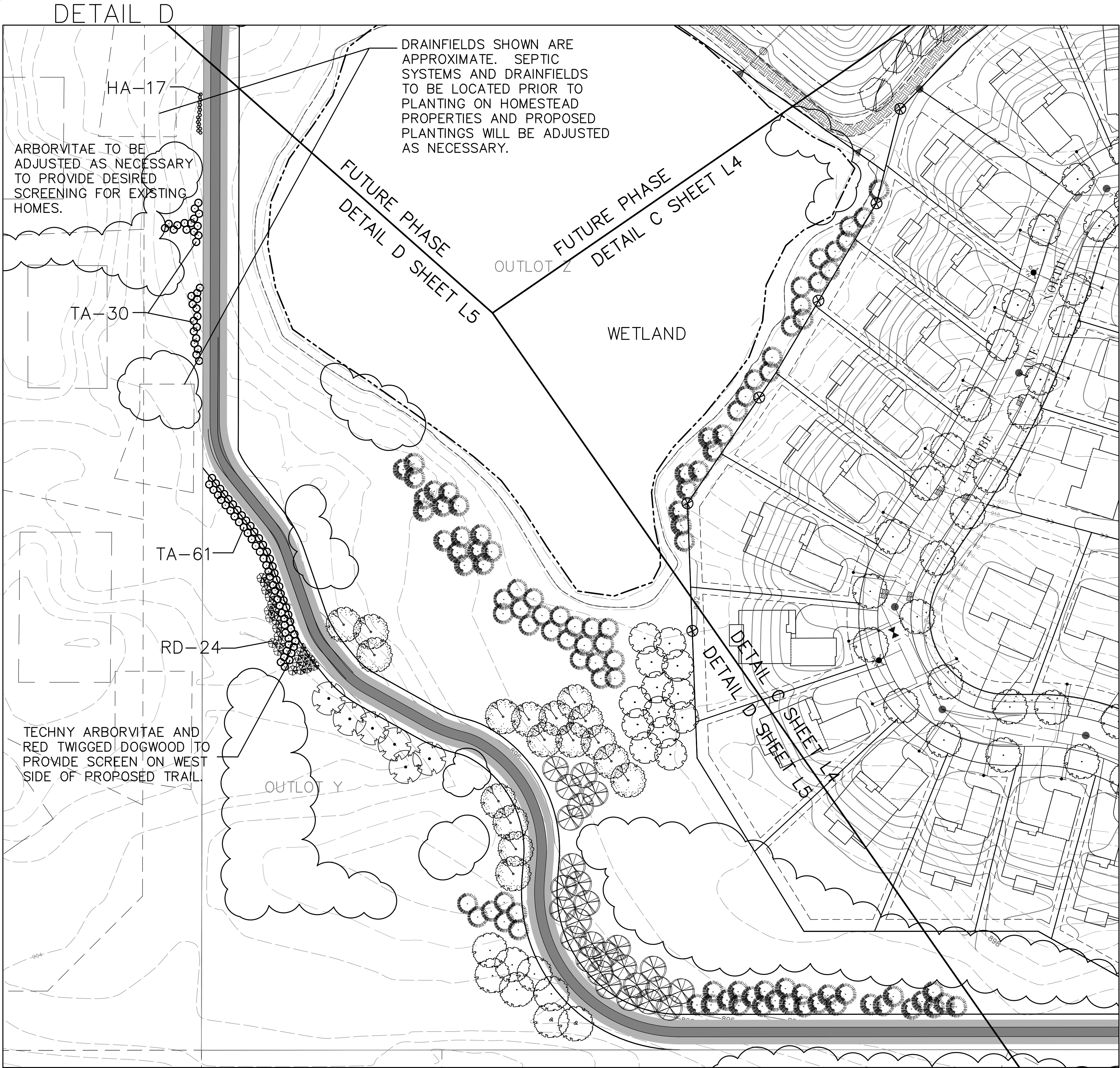


CITY OF LAKE ELMO

STANDARD DRAWING NO.

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LAKE ELMO



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KEY	COMMON NAME/Scientific name	ROOT	QUANTITY	
HA	HOLMSTRUP ARBORVITAE/ <i>Thuja occidentalis</i> 'Holmstrup'	3'-4" B&B	17	
TA	TECHNY ARBORVITAE/ <i>Thuja occidentalis</i> 'Techny'	3'-4" B&B	91	
RD	RED TWIGGED DOGWOOD/ <i>Cornus sericea</i> 'Bayleyi'	#5 POT	24	

SHRUBS TO BE MULCHED WITH SHREDDED HARDWOOD TO A DEPTH OF 3".
NO WEED BARRIER USED BENEATH SHREDDED HARDWOOD.
NO EDGER USED AROUND SHRUB PLANTING AREAS.

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Reg. No. 44763 Date 5-2-18

Revisions



NATIVE DRY PRAIRIE SOUTHEAST
(STATE SEED MIX 35-621 FORMERLY U6)

35-621		Dry Prairie Southeast			
Common Name	Scientific Name	Rate (kg/ha)	Rate (lb/ac)	% of Mix (% by wt)	Seeds/ sq ft
side-oats grama	<i>Bouteloua curtipendula</i>	1.27	1.13	10.23%	2.48
blue grama	<i>Bouteloua gracilis</i>	0.76	0.68	6.19%	10.00
kalms brome	<i>Bromus kalmii</i>	0.35	0.31	2.28%	0.90
nodding wild rye	<i>Elymus canadensis</i>	1.88	1.50	13.91%	2.85
slender wheatgrass	<i>Elymus trachycaulus</i>	1.32	1.18	10.76%	3.00
junegrass	<i>Koeleria macrantha</i>	0.46	0.41	3.71%	30.00
little bluestem	<i>Schizachyrium scoparium</i>	1.69	1.51	13.70%	8.30
sand dropseed	<i>Sporobolus cryptandrus</i>	0.25	0.22	1.98%	16.00
prairie dropseed	<i>Sporobolus heterolepis</i>	0.29	0.26	2.32%	1.50
Total Grasses		8.07	7.20	65.28%	75.04
butterfly milkweed	<i>Asclepias tuberosa</i>	0.07	0.06	0.52%	0.09
whorled milkweed	<i>Asclepias verticillata</i>	0.01	0.01	0.11%	0.05
bird's foot coreopsis	<i>Coreopsis palmata</i>	0.06	0.05	0.50%	0.20
white prairie clover	<i>Dalea candida</i>	0.10	0.09	0.78%	0.60
purple prairie clover	<i>Dalea purpurea</i>	0.17	0.15	1.32%	0.80
ox-eye	<i>Helopsis helianthoides</i>	0.07	0.06	0.51%	0.13
round-headed bush clover	<i>Lespedeza capitata</i>	0.03	0.03	0.31%	0.10
rough blazing star	<i>Liatris aspera</i>	0.02	0.02	0.17%	0.11
dotted blazing star	<i>Liatris punctata</i>	0.02	0.02	0.23%	0.06
wild bergamot	<i>Monarda fistulosa</i>	0.03	0.03	0.30%	0.85
horsemint	<i>Monarda punctata</i>	0.02	0.02	0.22%	0.80
stiff goldenrod	<i>Oligoneuron rigidum</i>	0.07	0.06	0.50%	0.98
large-flowered beard tongue	<i>Penstemon grandiflorus</i>	0.04	0.04	0.36%	0.20
black-eyed susan	<i>Rudbeckia hirta</i>	0.10	0.09	0.86%	3.20
gray goldenrod	<i>Solidago nemoralis</i>	0.01	0.01	0.14%	1.65
skyblue aster	<i>Symphoricarum canadense</i>	0.01	0.01	0.06%	0.20
silky aster	<i>Symphoricarum sericeum</i>	0.02	0.02	0.19%	0.20
bracted spiderwort	<i>Tradescantia bracteata</i>	0.01	0.01	0.12%	0.05
heart-leaved alexanders	<i>Zizia aurea</i>	0.02	0.02	0.21%	0.10
Total Forbs		0.90	0.80	7.49%	10.37
Oats or winter wheat (see note at beginning of list for recommended dates)		3.36	3.00	27.23%	1.33
Total Cover Crop		3.36	3.00	27.23%	1.33
Totals:		12.33	11.00	100.00%	86.75
Purpose:		Regional dry prairie reconstruction for wetland mitigation, ecological restoration, or conservation program plantings.			
Planting Area:		Eastern Broadleaf Forest Province excluding Hardwood Hills subsection. Mn/DOT Districts Metro & 6.			

DRY SWALE/POND
(STATE SEED MIX 33-262 FORMERLY W4)

33-262		Dry Swale / Pond			
Common Name	Scientific Name	Rate (kg/ha)	Rate (lb/ac)	% of Mix (% by wt)	Seeds/ sq ft
big bluestem	<i>Andropogon gerardii</i>	1.88	1.50	3.40%	5.50
American slough grass	<i>Beckmannia syzigachne</i>	1.68	1.50	3.40%	27.60
fringed brome	<i>Bromus ciliatus</i>	1.68	1.50	3.40%	6.05
nodding wild rye	<i>Elymus canadensis</i>	4.48	4.00	9.09%	7.64
slender wheatgrass	<i>Elymus trachycaulus</i>	4.48	4.00	9.10%	10.15
Virginia wild rye	<i>Elymus virginicus</i>	2.80	2.50	5.67%	3.85
switchgrass	<i>Panicum virgatum</i>	0.45	0.40	0.91%	2.05
fowl bluegrass	<i>Poa palustris</i>	1.79	1.60	3.64%	76.50
Indian grass	<i>Sorghastrum nutans</i>	1.68	1.50	3.40%	6.60
Total Grasses		20.74	18.50	42.03%	145.94
marsh milkweed	<i>Asclepias incarnata</i>	0.07	0.06	0.13%	0.10
purple prairie clover	<i>Dalea purpurea</i>	0.10	0.09	0.21%	0.50
Canada tick trefoil	<i>Desmodium canadense</i>	0.10	0.09	0.21%	0.19
ox-eye	<i>Helopsis helianthoides</i>	0.10	0.09	0.20%	0.20
black-eyed susan	<i>Rudbeckia hirta</i>	0.08	0.07	0.17%	2.49
blue vervain	<i>Verbena hastata</i>	0.11	0.10	0.23%	3.50
Total Forbs		0.56	0.50	1.15%	6.98
Oats or winter wheat (see note at beginning of list for recommended dates)		28.02	25.00	56.82%	11.14
Total Cover Crop		28.02	25.00	56.82%	11.14
Totals:		49.32	44.00	100.00%	164.06
Purpose:		Temporarily flooded swales in agricultural settings.			
Planting Area:		Tallgrass Aspen Parklands, Prairie Parkland, and Eastern Broadleaf Forest Provinces. Mn/DOT Districts 2(west), 3B, 4, Metro. 6, 7 & 8.			

3-YEAR MAINTENANCE PLAN FOR TRANSITION AREA (BETWEEN NWL AND HWL AND AROUND WETLANDS) AND UPLAND SEEDING:

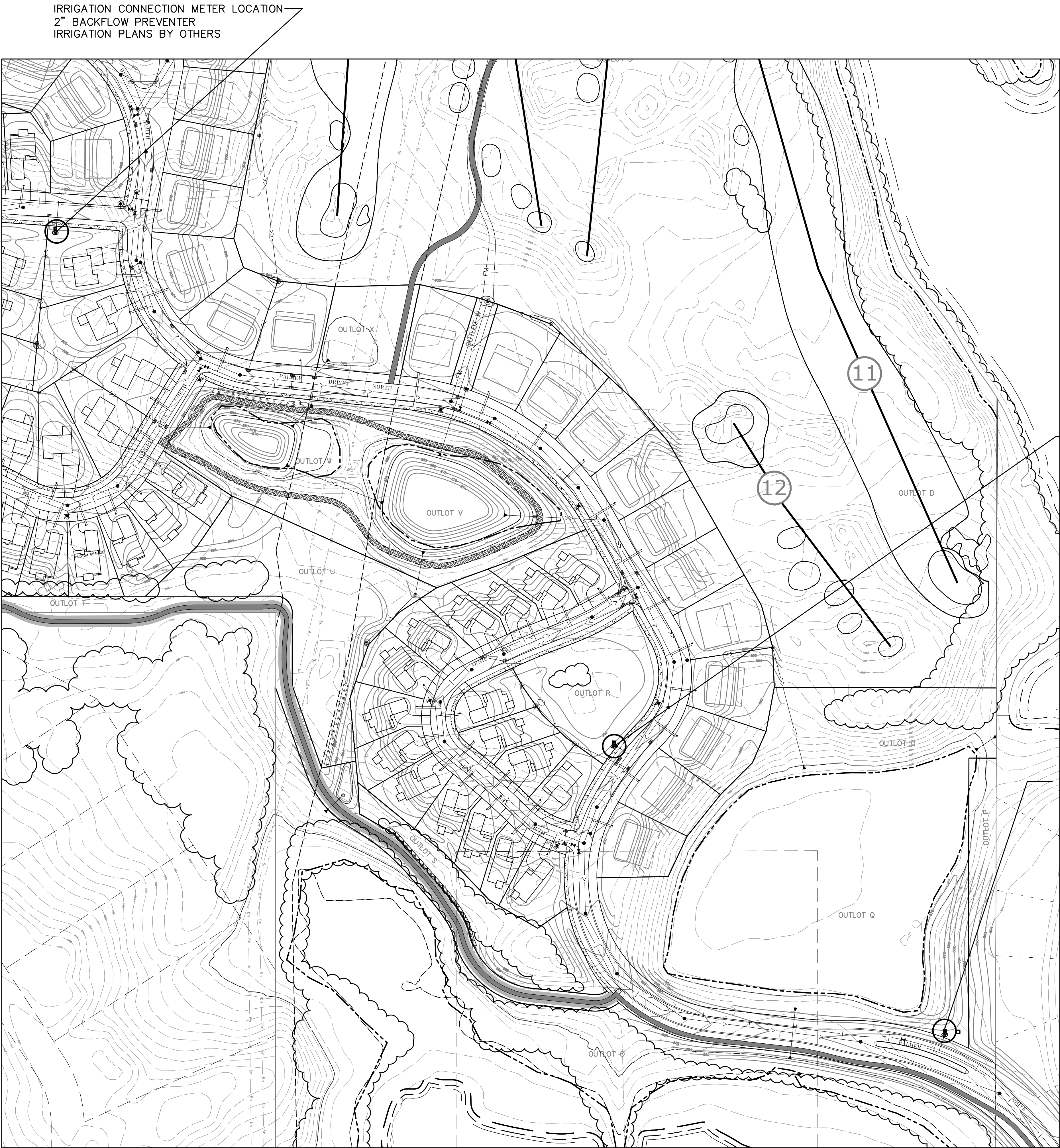
- YEAR 1
- TYPE 3 BLANKET SHOULD BE USED ON SLOPES UNLESS THE AREAS ARE HYDRO SEEDING WITH A HEAVY TACKIFIER IN WHICH CASE NO BLANKET OR MULCH WILL BE NECESSARY.
- YEAR 1-3
- MOWING SHOULD OCCUR AS NECESSARY THROUGHOUT THE GROWING SEASON FOR THE FIRST 3 SEASONS TO PREVENT NOXIOUS WEEDS FROM TAKING HOLD
 - WEEDS SHOULD BE HAND PULLED OR SPOT SPRAYED AS NECESSARY.
 - IF THE POND AND WETLAND SEEDING AREAS BECOMES SATURATED WITH ANY FREQUENCY, REED CANARY GRASS OR OTHER NOXIOUS WEEDS CAN BECOME A PROBLEM. SETHOXYDIM (OR EQUIVALENT) OR OTHER HERBICIDES WILL BE EFFECTIVE IN CONTROLLING REED CANARY GRASS AND OTHER NOXIOUS WEEDS WITHOUT HARMING SEDGES, RUSHES, AND FORBS.
 - TREES AND SHRUBS SHOULD ONLY BE PRUNED IN THE EVENT OF DAMAGED OR BROKEN BRANCHES.
 - INLETS MUST BE KEPT CLEAR OF DEBRIS.
 - ANY DEBRIS OBSERVED IN INFILTRATION BASIN OR POND SHALL BE REMOVED.

SEEDING NOTES:

MULCH: MNDOT TYPE 2 @ 2 TONS PER ACRE AND DISK ANCHORED IN PLACE OR APPROVED EQUAL, INSTALLED TO MINIMUM 90% COVERAGE OF THE SURFACE AREA DISTURBED. MULCH AT 90 % COVERAGE WITH DISC ANCHOR OR HYDROSEED.

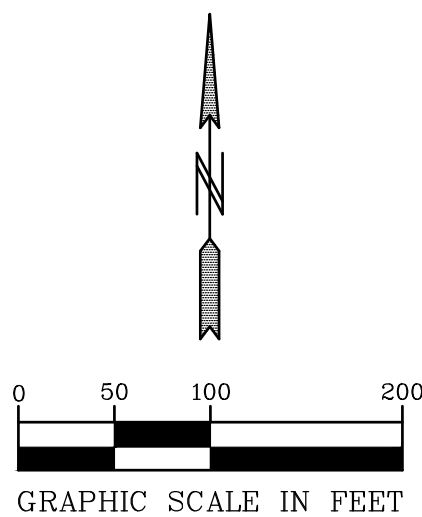
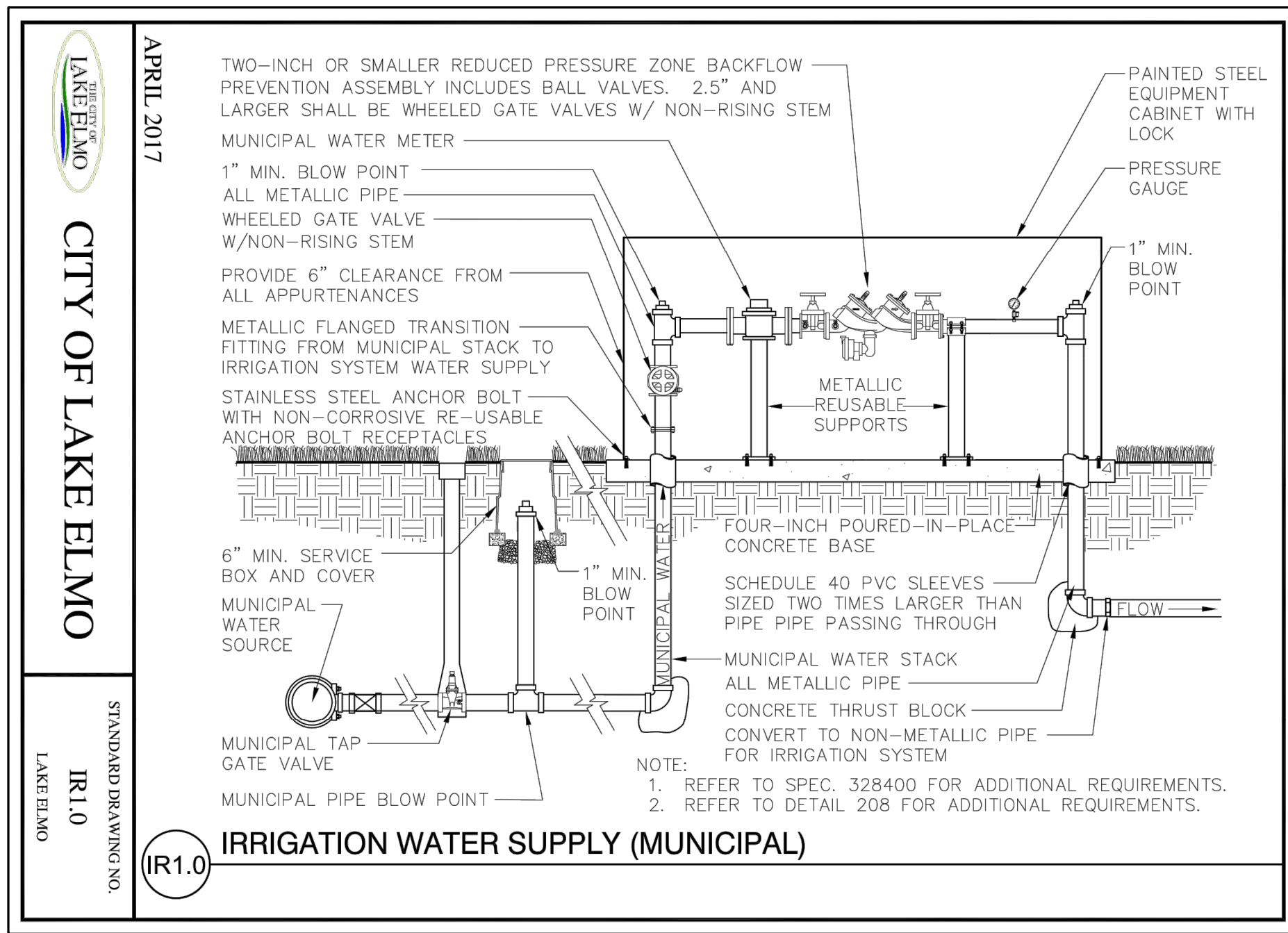
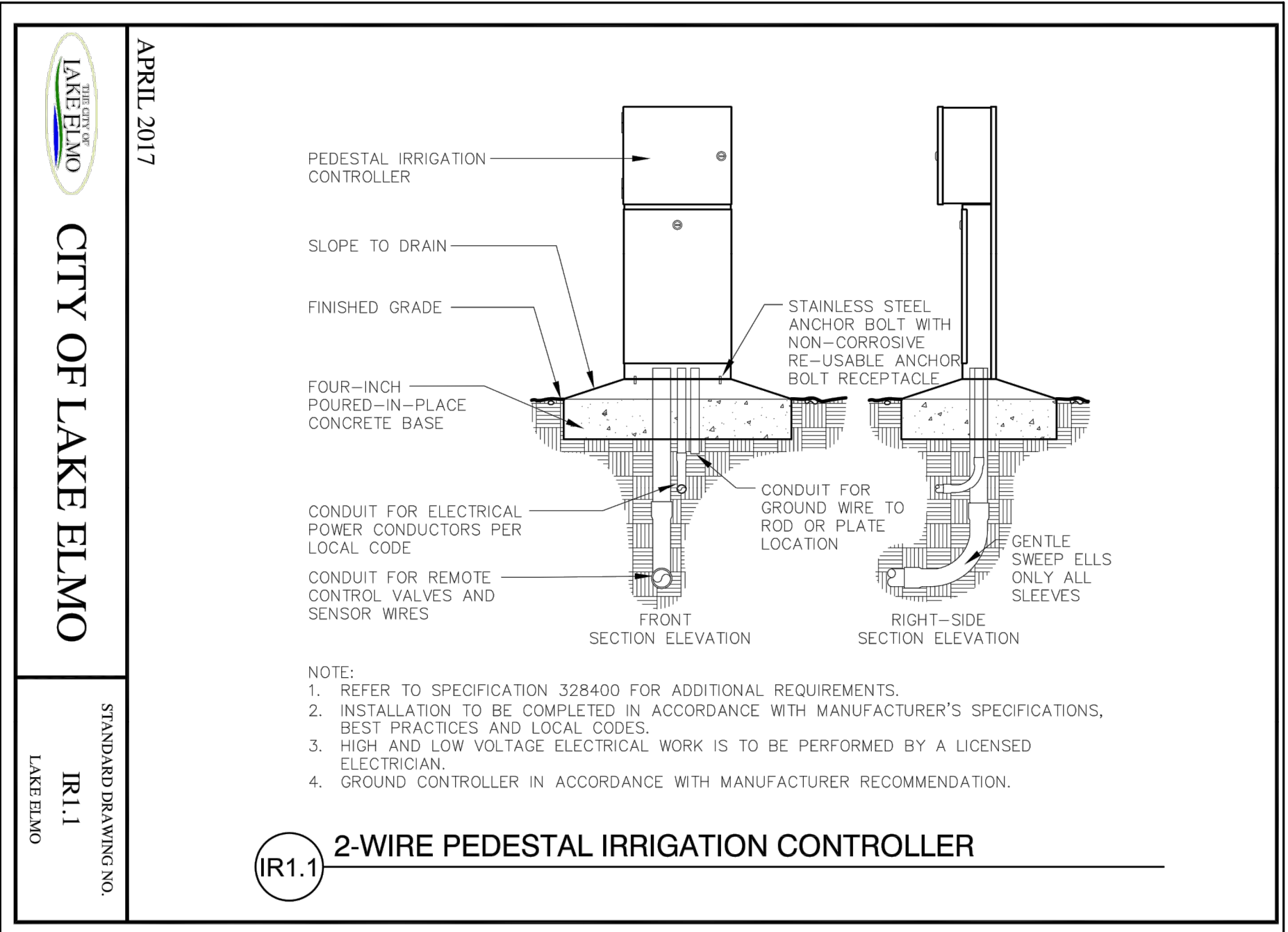
POND BENCH AND UP TO HWL AND AREA DISTURBED AROUND WETLAND TO BE SEEDING WITH MN STATE SEED MIX 33-262 OR EQUIVALENT. SEE GRADING DETAIL NOTES FOR MORE STORMWATER MANAGEMENT DETAILS

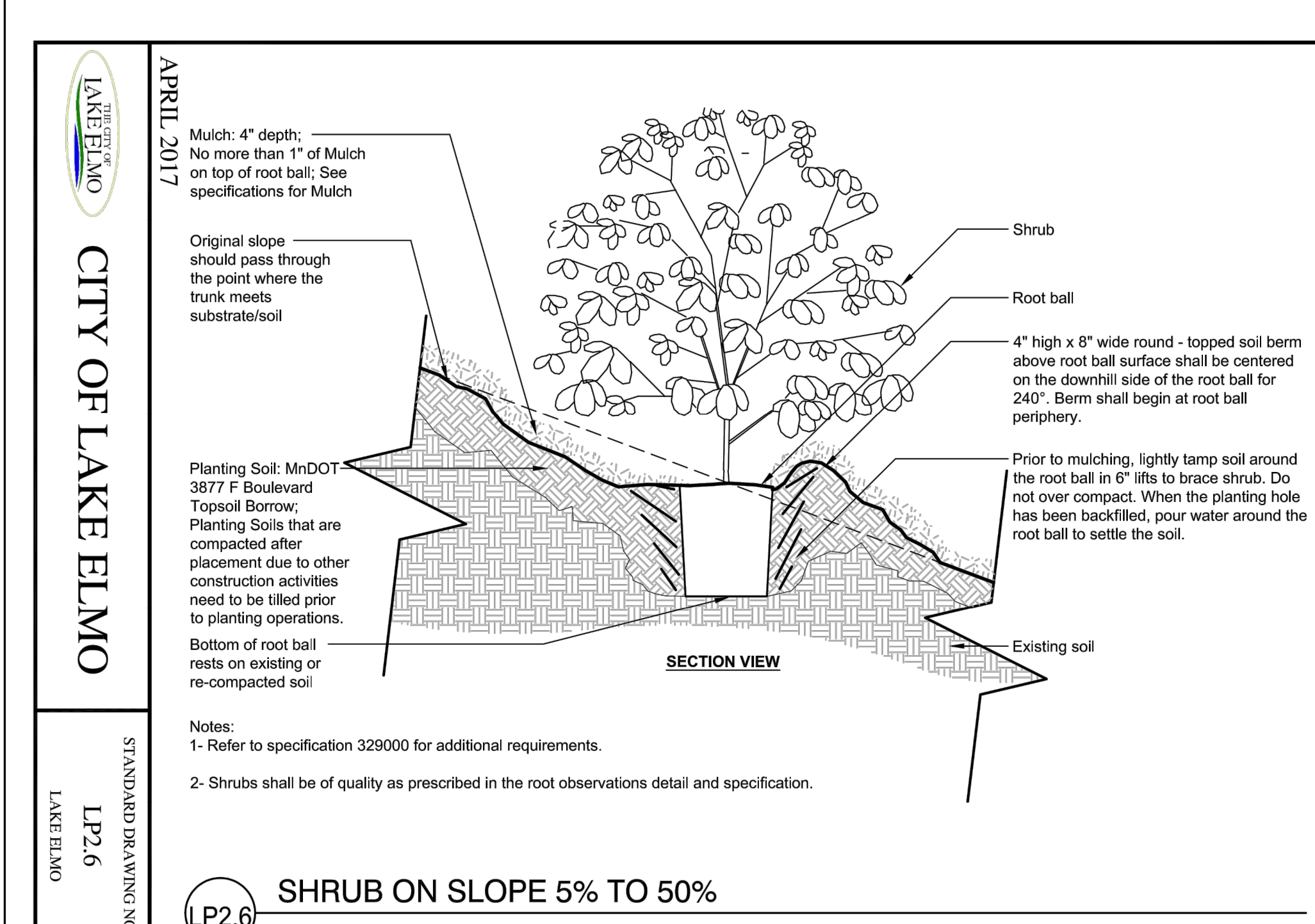
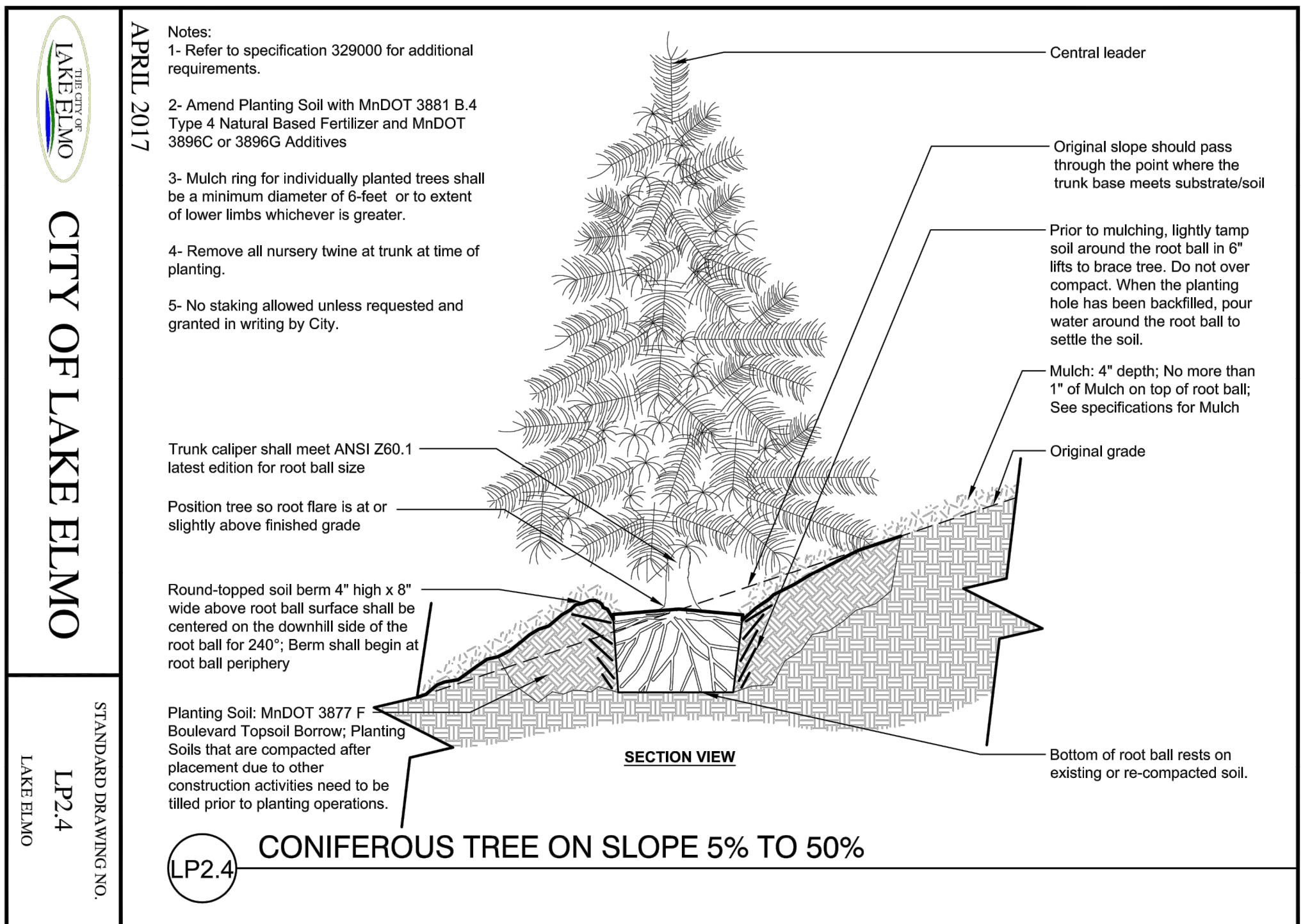
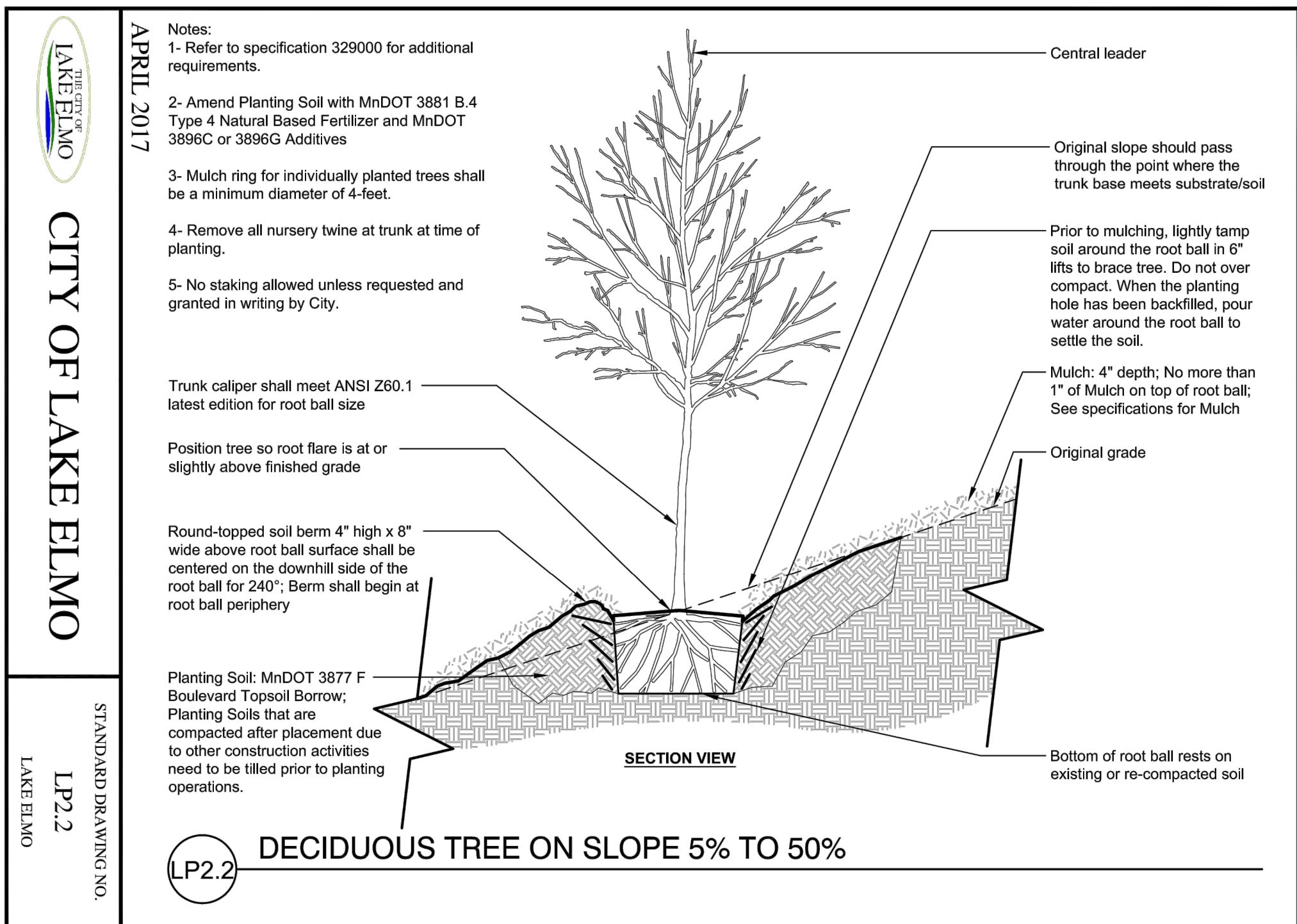
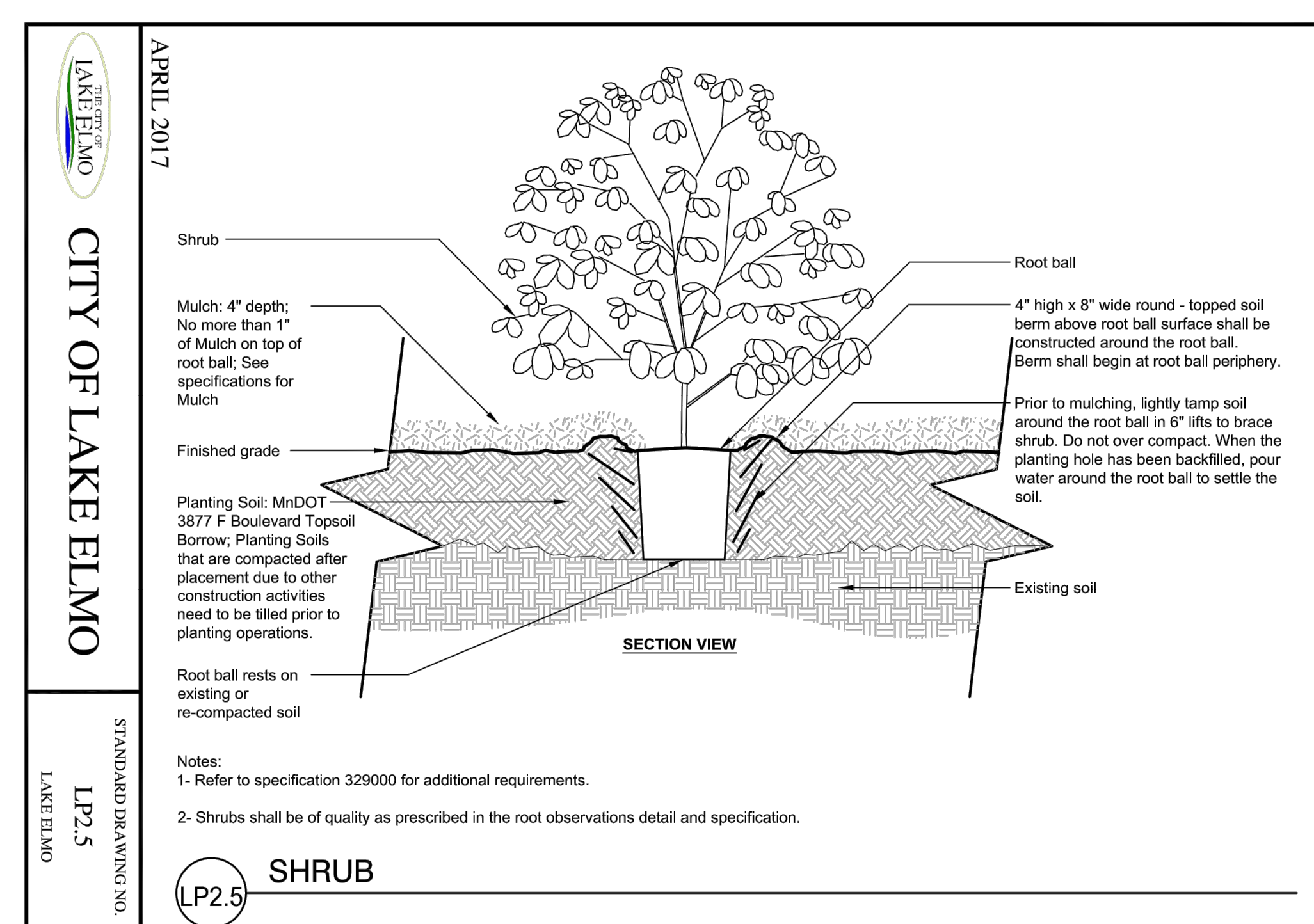
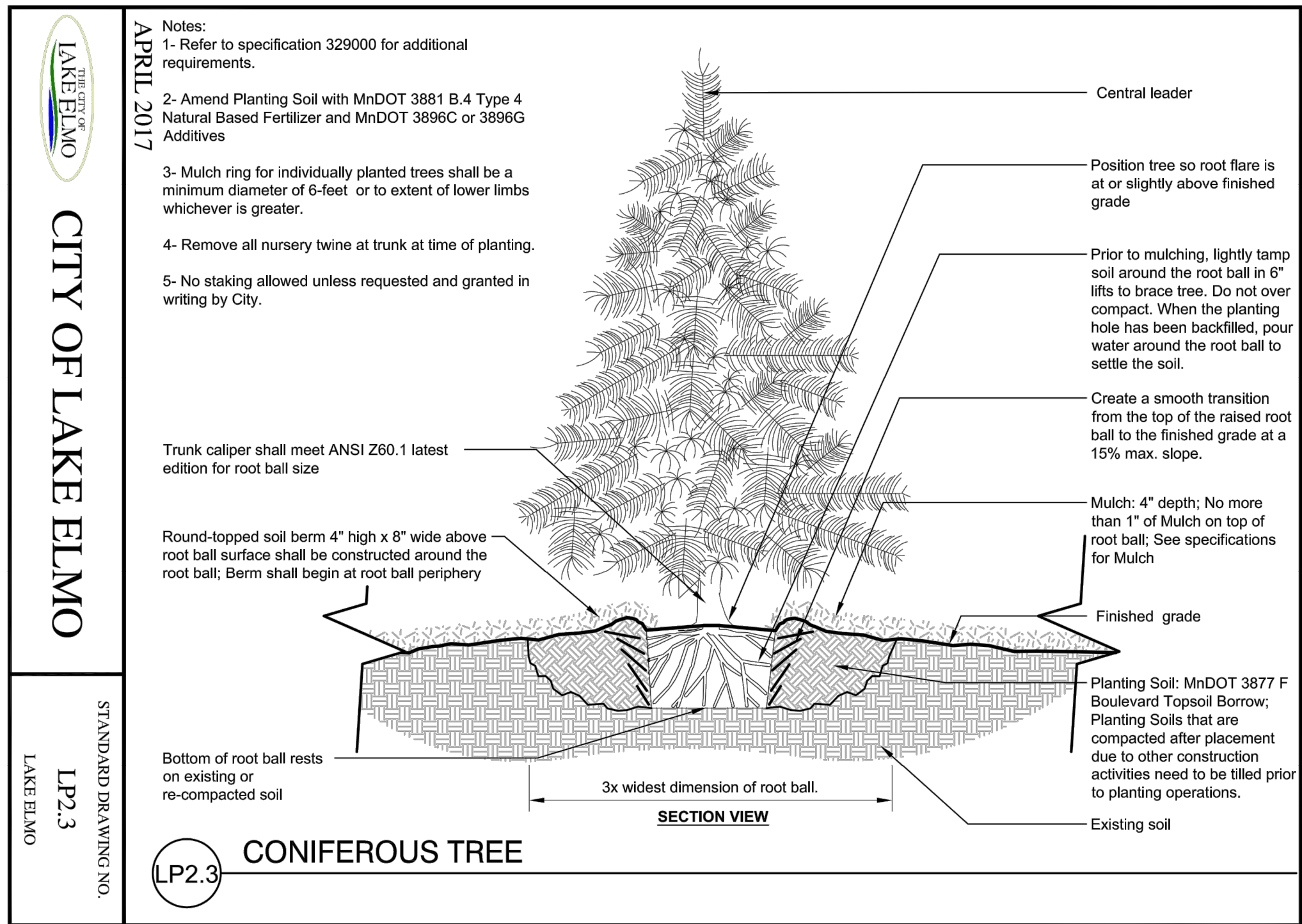
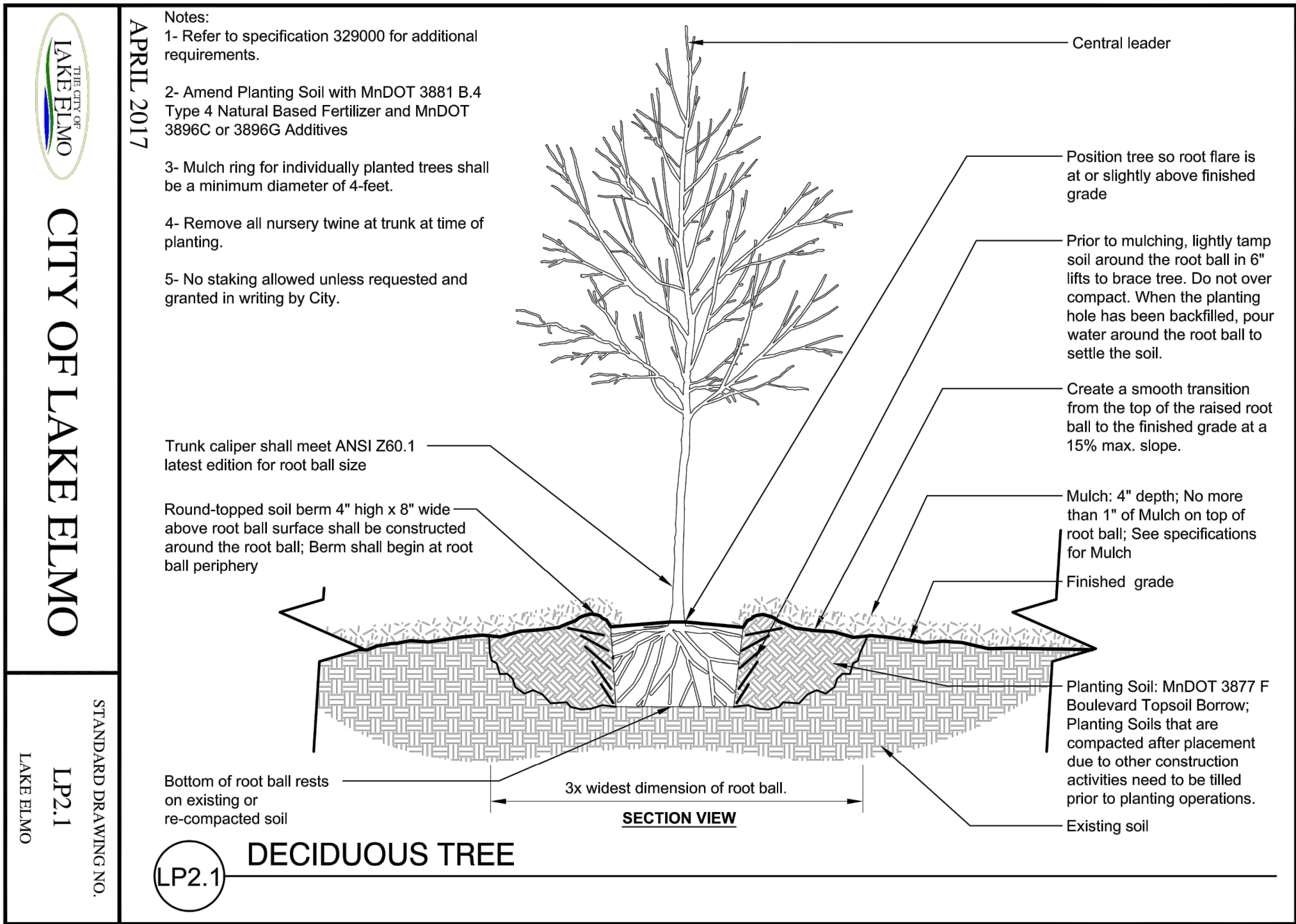
DISTURBED UPLAND AREA INCLUDING ABOVE HWL ON STORM WATER TREATMENT AREAS AND ANY DISTURBED UPLAND TO BE SEEDING WITH MN STATE SEED MIX 35-621 OR EQUIVALENT.



M
BF

IRRIGATION DETAILS—IRRIGATION DESIGN BY OTHERS





THE ROYAL GOLF CLUB AT LAKE ELMO 2ND ADDITION

KNOW ALL BY THESE PRESENTS: That H.C. Golf Land, LLC, a Minnesota limited liability company, owner of the following described property situated in the County of Washington, State of Minnesota, to wit:

Outlot O, Outlot P, Outlot R, and Outlot S, THE ROYAL GOLF CLUB AT LAKE ELMO, according to the recorded plat thereof, Washington County, Minnesota

Has caused the same to be surveyed and platted as THE ROYAL GOLF CLUB AT LAKE ELMO 2ND ADDITION and does hereby dedicate to the public for public use forever the public ways, as shown on this plat and also dedicate the easements created by this plat for drainage and utility purposes only.

In witness whereof said H.C. Golf Land, LLC, a Minnesota limited liability company, has caused these presents to be signed by Joel T. LeVahn, Chief Financial Officer, this ____ day of _____, 20____.

H.C. GOLF LAND, LLC

By: _____
Joel T. LeVahn, Chief Financial Officer

STATE OF _____
COUNTY OF _____

This instrument was acknowledged before me on this ____ day of _____, 20____, by Joel T. LeVahn, Chief Financial Officer of H.C. Golf Land, LLC, a Minnesota limited liability company, on behalf of the company.

Notary Public,
My commission expires _____

I, Thomas R. Balluff do hereby certify that this plat was prepared by me or under my direct supervision; that I am a duly Licensed Land Surveyor in the State of Minnesota; that this plat is a correct representation of the boundary survey; that all mathematical data and labels are correctly designated on this plat; that all monuments depicted on this plat have been set, or will be correctly set within one year; that all water boundaries and wet lands, as defined in Minnesota Statutes, Section 505.01, Subd. 3, as of the date of this certificate are shown and labeled on this plat; and all public ways are shown and labeled on this plat.

Dated this ____ day of _____, 20____

Thomas R. Balluff, Licensed Land Surveyor
Minnesota License No. 40361

STATE OF _____
COUNTY OF _____

This instrument was acknowledged before me on this ____ day of _____, 20____, by Thomas R. Balluff, Licensed Land Surveyor.

Notary Public,
My commission expires _____

CITY OF LAKE ELMO, PLANNING COMMISSION

Approved by the Planning Commission of the City of Lake Elmo, Minnesota, on this ____ day of _____, 20____

Signed: _____, Chair Signed: _____, Secretary

CITY COUNCIL, CITY OF LAKE ELMO, MINNESOTA

This plat was approved by the City Council of the City of Lake Elmo, Minnesota, on this ____ day of _____, 20____, and hereby certifies compliance with all requirements as set forth in Minnesota Statutes, Section 505.03, Subd. 2.

Signed: _____, Mayor Signed: _____, Clerk

COUNTY SURVEYOR

Pursuant to Chapter 820, Laws of Minnesota, 1971, and in accordance with Minnesota Statutes, Section 505.021, Subd. 11, this plat has been reviewed and approved this ____ day of _____, 20____.

By: _____
Washington County Surveyor By: _____

COUNTY AUDITOR/TREASURER

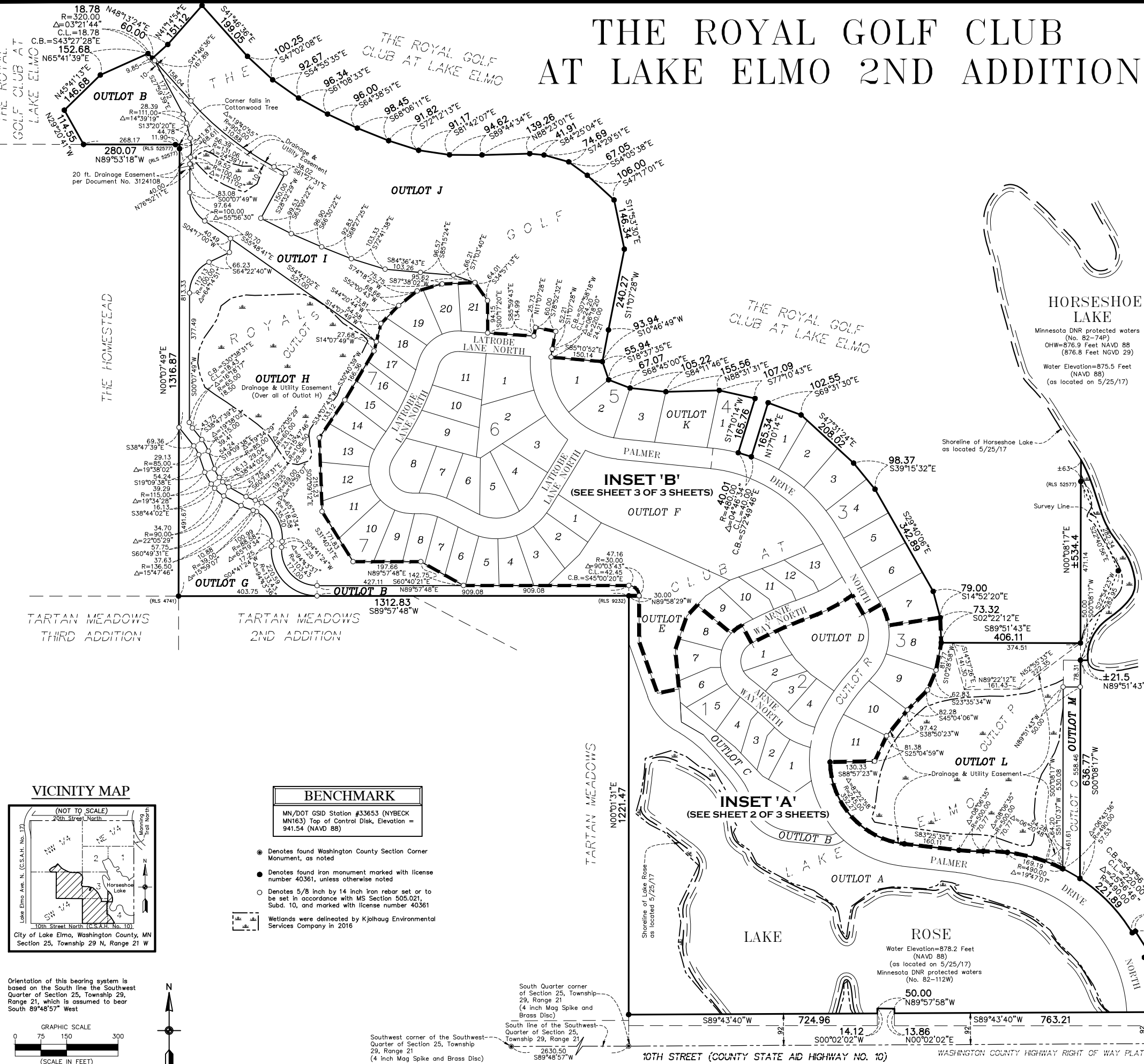
Pursuant to Minnesota Statutes, Section 505.021, Subd. 9 and Section 272.12, taxes payable in the year 20____ on the real estate hereinbefore described, have been paid; and there are no delinquent taxes, and transfer has been entered on this ____ day of _____, 20____.

By: _____
Washington County Auditor/Treasurer By: _____
Deputy

COUNTY RECORDER

Document Number _____
I hereby certify that this instrument was recorded in the Office of the County Recorder for record on this ____ day of _____, 20____, at ____ o'clock ____ M., and was duly recorded in Washington County Records.

By: _____
Washington County Recorder By: _____
Deputy

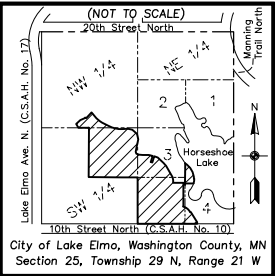


THE ROYAL GOLF CLUB AT LAKE ELMO
R=490.00
Δ=19°08'50"
C.L.=162.99
C.B.=S12°01'53"E

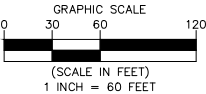
THE ROYAL GOLF CLUB AT LAKE ELMO 2ND ADDITION

INSET 'A'

VICINITY MAP



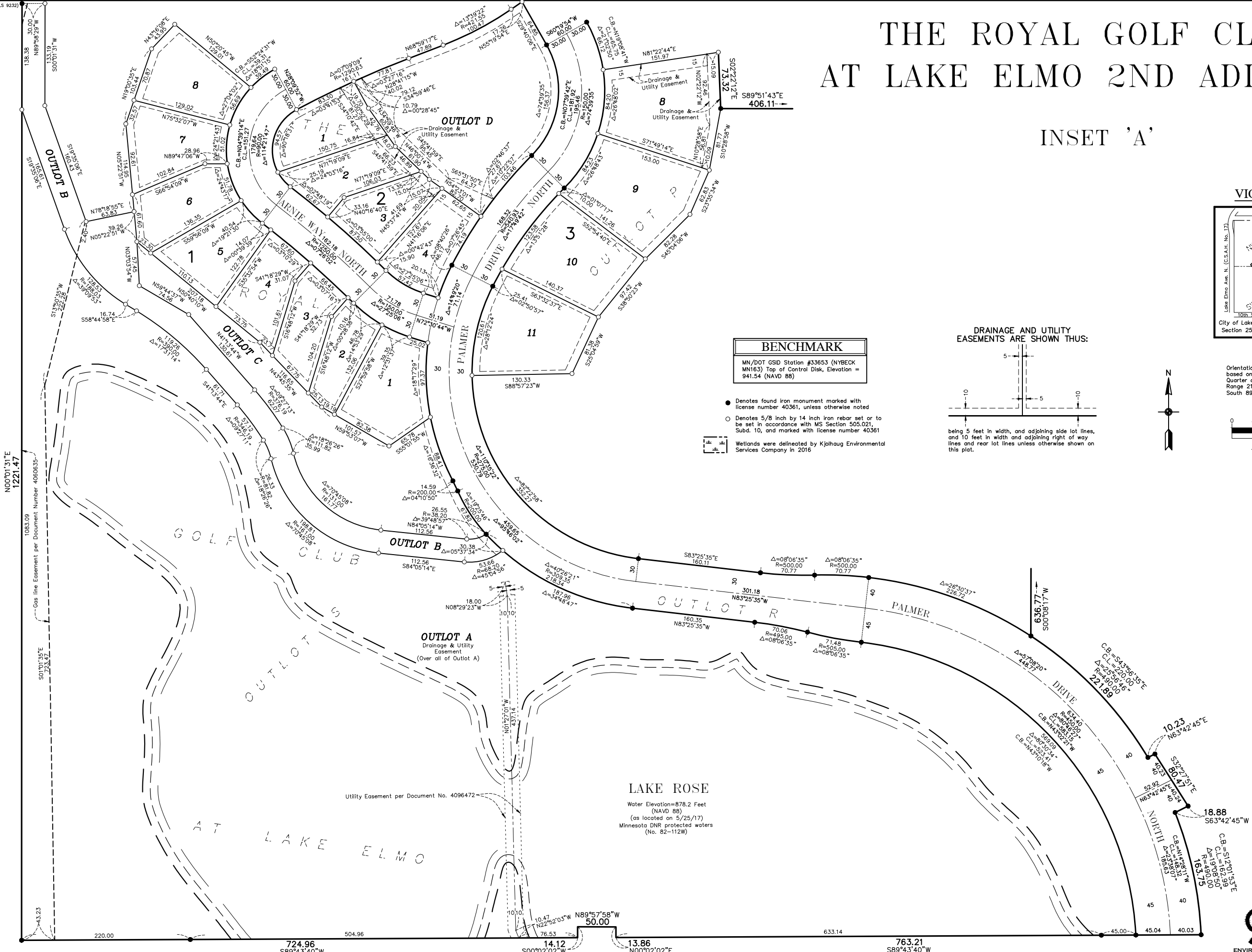
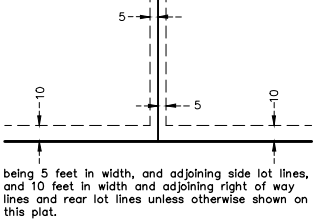
Orientation of this bearing system is based on the South line the Southwest Quarter of Section 25, Township 29, Range 21, which is assumed to bear South 89°48'57" West



BENCHMARK
MN/DOT GSID Station #33653 (NYBECK)
MN163) Top of Control Disk, Elevation = 941.54 (NAVD 88)

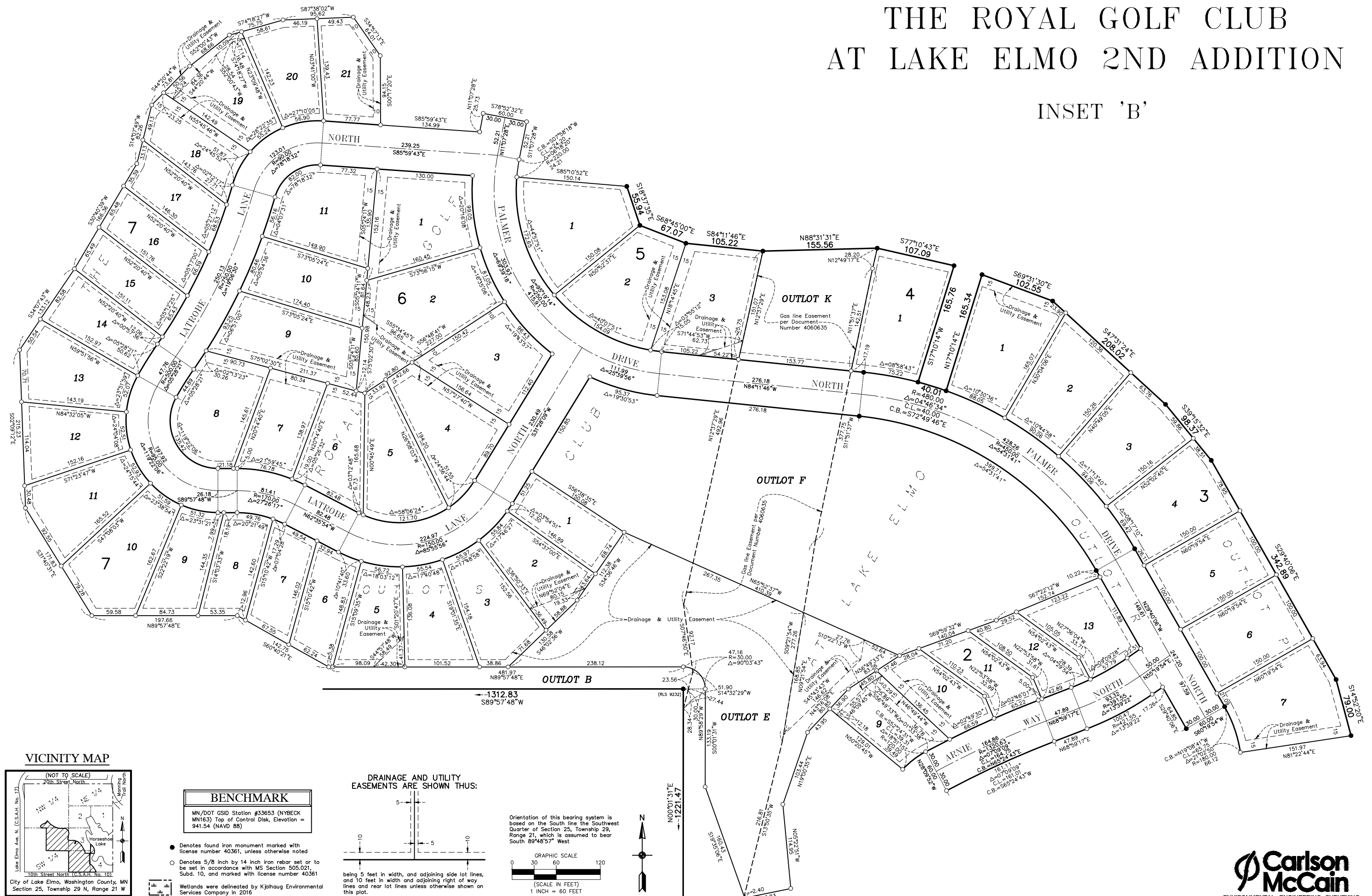
- Denotes found iron monument marked with license number 40361, unless otherwise noted
- Denotes 5/8 inch by 14 inch iron rebar set or to be set in accordance with MS Section 505.021, Subd. 10, and marked with license number 40361
- Wetlands were delineated by Kjolhaug Environmental Services Company in 2016

DRAINAGE AND UTILITY EASEMENTS ARE SHOWN THUS:

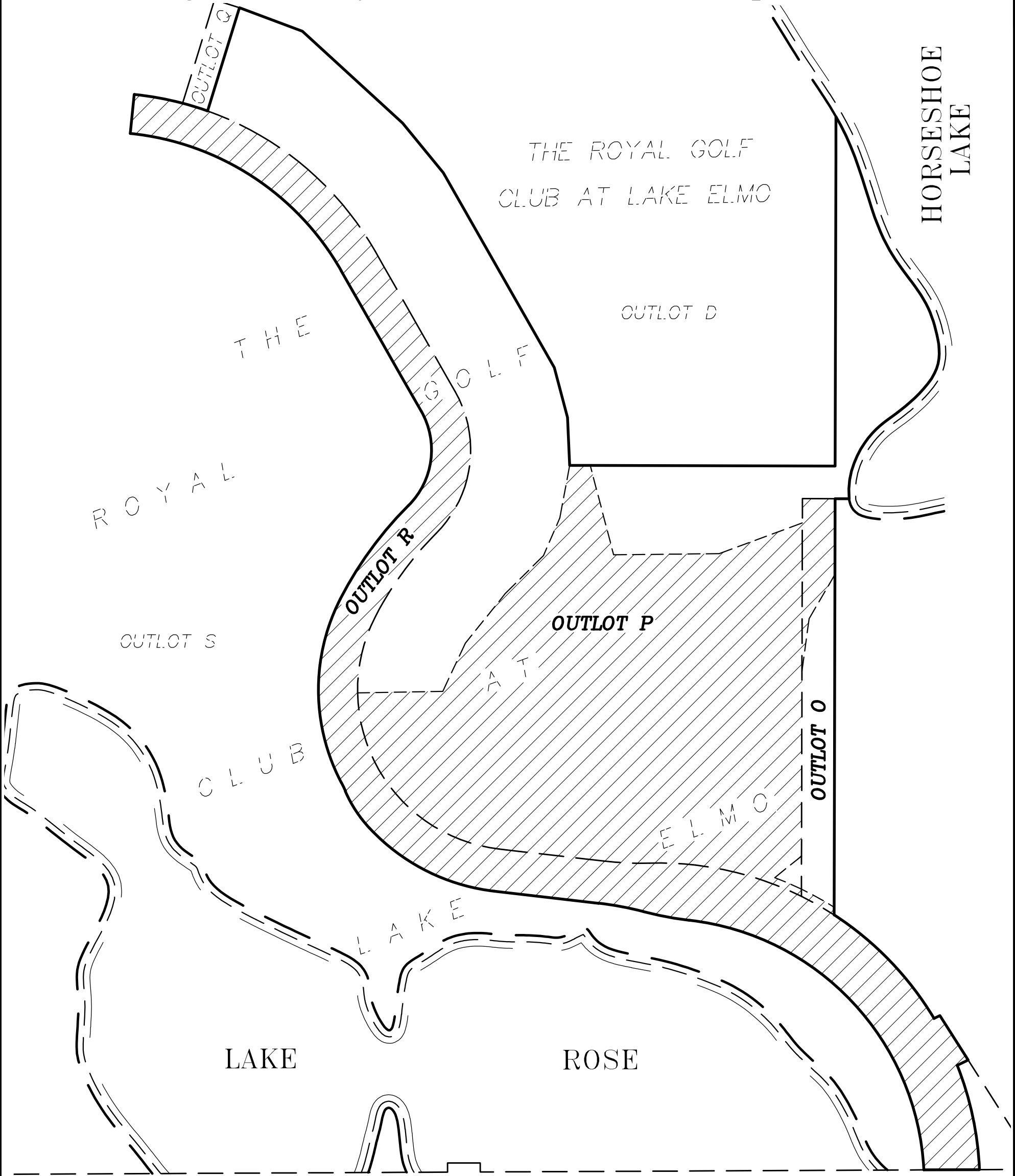


THE ROYAL GOLF CLUB AT LAKE ELMO 2ND ADDITION

INSET 'B'



Drainage and Utility Easement Vacation Description Sketch



10TH STREET (COUNTY STATE AID HIGHWAY NO. 10)

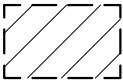
Drainage and Utility Easement Vacation Description:

All of the drainage and utility easements, as created and dedicated over, under, and across Outlot O, Outlot P, and Outlot R, THE ROYAL GOLF CLUB AT LAKE ELMO, according to the recorded plat thereof, Washington County, Minnesota.

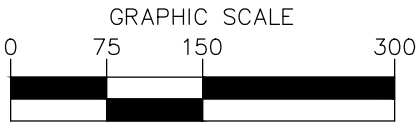
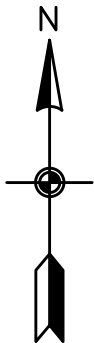
Said vacation area contain $\pm 455,138$ sq. ft.



ENVIRONMENTAL • ENGINEERING • SURVEYING
3890 Pheasant Ridge Drive NE,
Suite 100, Blaine, MN 55449
Phone: 763-489-7900 Fax: 763-489-7959



Drainage and Utility
Easement Vacation Area



(IN FEET)
(11x17 sheet)



STAFF REPORT

DATE: 6/4/2018

REGULAR

ITEM #:4b

MOTION

TO: Planning Commission

FROM: Emily Becker, Planning Director

AGENDA ITEM: Zoning Map Amendment, Preliminary and Final Plat, Bus Terminal Ordinance
Zoning Text Amendment, and Conditional Use Permit

REVIEWED BY: Ben Gozola, Consultant Planner
Soren Mattick, Campbell Knutson

BACKGROUND:

The City has received a set of applications from Stillwater Area Public Schools (Kristen Hoheisel) and Terry Emerson requesting consideration of the following four (4) items:

- 1) A Zoning Map Amendment to re-zone a portion of the subject property to Business Park;
- 2) Preliminary and Final Plats to subdivide the property located at 11530 Hudson Boulevard North;
- 3) A Zoning Text Amendment to allow "bus terminal" as a conditionally permitted principal use within the Business Park Zoning District; and
- 4) A Conditional Use Permit to operate a school district transportation center on the subject property.

ISSUE BEFORE COMMISSION:

The Planning Commission is being asked to hold a public hearing and make recommendation on each of the four requests beginning with the broadest issue (zoning district change) and working towards the most specific (approval of a CUP to allow operation of a bus terminal). As all four of the applications are generally linked, denial of any individual request may impact the commission's recommendation(s) on subsequent requests.

PROPOSAL DETAILS/ANALYSIS:

Applicants: Stillwater Area Public Schools (Kristen Hoheisel), 1875 Greeley Street South, Stillwater, MN 55082 (Zoning Map Amendment, Zoning Text Amendment,

	and Conditional Use Permit) and Terry Emerson, 2204 Legion Ln Cir N, Lake Elmo, MN 55042 (Preliminary and Final Plat)
<i>Property Owners:</i>	EN Properties, LLC (Terry Emerson), 11530 Hudson Boulevard North, Lake Elmo, MN 55042
<i>Location:</i>	11530 Hudson Boulevard North (PID# 36.029.21.43.0001)
<i>Request:</i>	Zoning Text Amendment, Preliminary and Final Plat, Zoning Map Amendment, and Conditional Use Permit.
<i>Existing Land Use:</i>	Trucking Terminal – sales, repairs, and service of diesel trucks
<i>Existing Zoning:</i>	RT - Rural Development Transitional
<i>Surrounding Land Use / Zoning:</i>	South – I-94 and Woodbury; West – Outdoor Storage (RT – Rural Development Transitional); East – Vacant land (RT – Rural Development Transitional); North – Vacant land (RT – Rural Development Transitional)
<i>Comprehensive Plan Guidance:</i>	BP – Business Park
<i>Deadline(s) for Action:</i>	Application Complete – 5/4/2018 60 Day Deadline – 7/3/2018 Extension Letter Mailed – No 120 Day Deadline – N/A
<i>Applicable Regulations:</i>	Chapter 153 – Subdivision Regulations §154.210 – Off-Street Parking Article XIV: Commercial Districts §154.258: Landscape Requirements

ZONING MAP AMENDMENT

The applicant is seeking approval to rezone a portion of the subject property (11 acres of approximately 63.73 total acres) from RT (Rural Development Transitional) to BP (Business Park). The RT designation provides landowners with interim use options as they wait for the availability of sewer, and/or until such time as a landowner requests a zoning change to a new district consistent with the future guided land use.

In this case, the land in question is guided for Business Park both within the current 2030 Comprehensive Plan and within the draft 2040 Comprehensive Plan. Therefore, staff does find it would be appropriate to approve the requested rezoning if that is the desire of the landowner. Note that if the rezoning is

ultimately approved but subsequent applications are denied, the applicant will be consulted as to whether this change should still occur, or whether the zoning change application will be withdrawn.

PRELIMINARY AND FINAL PLAT

Purpose. The preliminary and final plat portion of this application package assumes the City approves the requested zoning map amendment to BP. If the rezoning failed, this application fails due to the inability to meet minimum RT zoning standards. If the rezoning was approved, the following analysis will apply:

Minimum Lot Size Requirements. The following table outlines how the lot dimension and setback requirements of the Business Park zoning district are met by the requested lot layout.

Lot Dimension Requirements, Business Park District		
Standard	Required	Proposed
Minimum Lot Area	2 acres	11 acres
Minimum Lot Width	200 feet	670 feet
Minimum Lot Depth	-	715feet

Parkland Dedication. The City requires \$4500 per acre for commercial subdivisions. The proposed subdivision will create one new lot of 11 acres. Therefore, it is recommended that a condition of approval require the applicant to pay \$49,500 (\$4500 X 11 acres) in park dedication fees for this development should the plan move forward.

As staff did not identify any ways in which the proposed lot layout would be deficient to minimum zoning standards, we are recommending the requests can be approved. Note that if the Preliminary and Final plats are ultimately approved but subsequent applications are denied, the applicant will be consulted as to whether this application should still move forward, or whether it will be withdrawn.

City Engineer Review.

Stormwater Management

- State and Valley Branch Watershed District permit will be required.
- The site plan is subject to a storm water management plan meeting State, VBWD and City rules and regulations. No proposed storm water management plan was provided as part of the Plan submittal.
- The storm water facilities constructed for this development should remain privately owned and maintained. The applicant will be required to execute and record a Stormwater Maintenance and Easement Agreement in the City's standard form of agreement.
- Even as privately owned and maintain facilities, maintenance access roads meeting the City engineering design standards must be provided for all storm water facilities.

- The storm water facility 100-year HWL must be fully contained within the subject property or easements must be acquired to protect the 100-year HWL flood area. The proposed plans show the 100-year HWL event discharging off-site at the southeast corner.

Streets and Transportation

- A traffic study should be required as a condition of approval to determine the timing and extent of improvements required for the CSAH 15 and Hudson Boulevard intersection realignment and traffic signal improvements. A financial contribution to CSAH 15/Hudson Boulevard traffic signal/realignment should be considered. In addition, the study is needed to determine if an eastbound left turn lane along Hudson Boulevard is also needed at the site access.
- No additional right-of-way dedication is required unless an eastbound turn lane is required as determined by the traffic study. A 10-foot utility corridor easement for small utilities must be dedicated to the City along the north boulevard of Hudson Boulevard.
- Hudson Boulevard must be improved per the City design standards to an urban section on the north side of the street including a westbound right turn lane to the site, a 4-foot shoulder (curb section), a 9-foot boulevard, and 10-foot shared use bituminous trail with 2-foot clear zone.
- Municipal Sanitary Sewer
- Because the property is guided in the City's Comprehensive Plan for 2020-2030 Regional Sewer Staging Plan and would discharge to the MCES Cottage Grove Ravine Interceptor, provisions for connecting to the municipal sanitary sewer service must be included with this application. The site plans/application indicates a new sewer service line connecting to future sanitary sewer but does not address the extension of sewer to the site.
- The applicant will be responsible to connect to the City sanitary sewer system and extend sanitary sewer into the property at applicant's sole cost and must stub sewer to adjacent parcels.
- Sanitary sewer is available along Hudson Boulevard near the MCES Meter Station. The sanitary sewer trunk size will need to be determined based on the 2040 Comprehensive Plan.
- Sanitary sewer capacity demands including the number of residential equivalency units (RECs) based on the Met Council SAC determination policy should be provided and account for all land uses and connections to the sewer system including bus wash wastewater.
- Existing sanitary sewer utility easements along Hudson Boulevard and the new commercial street corridor must be shown on the plans and plans must be made to avoid encroachments/conflicts with these easements.
- Any main sewer lines placed within the development will require minimum 30-foot easements centered over the pipe (or wider dependent upon the sewer depths) dedicated to the City and in the form of the
- City's Utility Easement Agreement.

Municipal Water Supply

- Because the property is located in the MUSA, provisions for connecting to the municipal water supply must be included.
- The applicant will be responsible for extending municipal water to the property at the applicant's cost, and watermain stubs will be required to eastern property limits. Water is available to be extended to the property from the Hunter's Crossing development but will require easements across adjacent properties. The watermain extension to the site will likely be a 12-inch trunk watermain.
- The applicant shall provide water capacity demands including average day use, peak day use, and fire suppression demands. All demands should be included, including potable drinking water, bus

washing operations, etc. in order to evaluate water improvements needed to support the development and to determine the water availability charges.

- The proposed site is in the water system's low pressure zone which has limited capacity until a new water tower is constructed to serve the area.
- Hydrants will be required to be placed at the direction of the Fire Department, and all fire hydrants shall be owned and maintained by the City.

Recommended Findings.

1. That the Four Corners preliminary and final plat is consistent with the Lake Elmo Comprehensive Plan and the Future Land Use Map for this area.
2. That the Four Corners preliminary and final plat complies with the minimum lot frontage and area requirements of the City's BP – Business Park zoning district.
3. That the Four Corners preliminary and final plat complies with the City's subdivision ordinance.
4. That the Four Corners preliminary and final plat meets other City zoning ordinances, such as landscaping, erosion and sediment control, and other ordinances, provided comments outlined in the City Engineer Review Memo dated May 30, 2018 are met; or as may be amended due to changes in the site plan or proposed use.
5. That the Four Corners preliminary and final plat is consistent with the City's engineering standards provided final plans are updated to address the City Engineer's comments documented in a letter dated May 30, 2018; or as may be amended due to changes in the site plan or proposed use.
6. The land being subdivided is legally described as:

S1/2-SE1/4 EXC WEST 2 RODS EXC TO HWY EXC HWY PARCEL 44 MN DOT R/W PLAT #82-35 SECTION 36 TOWNSHIP 029 RANGE 021
7. That the Four Corners preliminary and final plat will create one new lot of 11 acres to be legally described as Lot 1, Block 1, Four Corners; and three outlots: A (46.64 acres), B (5.01 acres), and C (1.08 acres).

Recommended Conditions of Approval. Staff recommends the following conditions be attached to any approval of the Four Corners Preliminary and Final plats:

- 1) The property shall be rezoned to BP – Business Park.
- 2) The applicant shall pay a fee in lieu of parkland dedication in the amount of \$4500 per acre of the newly created lot (11 acres X \$4500 = \$49,500 total) prior to any formal City authorization.
- 3) Prior to the execution of the Final Plat by City officials, the Developer shall enter into a Developer's Agreement acceptable to the City Attorney and approved by the City Council

that delineates who is responsible for the design, construction, landscaping, and payment of the required improvements for the Four Corners Final Plat with financial guarantees therefore.

- 4) Final grading, drainage and erosion control, sanitary sewer and stormwater management, street and utility construction plans shall be submitted, reviewed and approved by the City Engineer, meeting City Engineering Design Standards, prior to the recording of the Final Plat. All changes and modifications to the plans requested by the City Engineer in a memorandum dated May 30, 2018 and all subsequent memorandums regarding the plans shall be incorporated into these documents before they are approved.
- 5) The applicant must obtain all other necessary City, State, and other governing body permits prior to the commencement of any construction activity on the parcel including but not limited to building permits, conditional use permits, etc.
- 6) Any plans for construction on the newly created parcel must comply with the Lake Elmo Design Standards Manual and specific general site considerations and development standards for specific uses within the Business Park District.
- 7) The site plan is subject to a storm water management plan meeting State, Valley Branch Watershed District and City rules and regulations. All applicable permits must be obtained. Maintenance access roads meeting City engineering design standards must be provided for all storm water facilities.
- 8) Stormwater facilities shall remain privately owned and maintained. The applicant will be required to execute a Stormwater Maintenance and Easement Agreement in the City's standard form of agreement.
- 9) A Traffic Impact Study shall be completed and submitted in order to determine timing and extent of improvements required for the CSAH 15 and Hudson Boulevard intersection realignment and traffic signal improvements as well as the need for an eastbound left turn lane along Hudson Boulevard for site access. Additional right-of-way along Hudson Boulevard may be required if the eastbound left turn lane is required.
- 10) Hudson Boulevard must be improved per the City design standards to an urban section on the north side of the street including a west bound right turn lane to the site, a four foot shoulder (curb reaction), a nine foot boulevard, and a ten foot shared use bituminous trail with two foot clear zone.
- 11) Provisions for connecting municipal sanitary sewer service and for connecting to the municipal water supply must be provided on submitted plans. The applicant shall be responsible to extend and connect to the City sanitary sewer system and municipal water at the applicant's sole cost and extend such services to adjacent properties. The applicant shall be required to obtain easements from adjacent properties in order to extend water. A detailed description of the sanitary sewer and water capacity demands shall be provided.

- 12) Hydrants will be required to be placed at the direction of the Fire Department, and all fire hydrants shall be owned and maintained by the City.
- 13) All easements as requested by the City Engineer and Public Works Department shall be documented on the Final Plat prior to its execution by City Officials.

ZONING TEXT AMENDMENT

Purpose. The zoning text amendment being requested would allow "bus terminals" as a conditionally permitted principal use on properties zoned Business Park. If approved, the applicant would seek a conditional use permit to authorize a permanent bus terminal on the subject property.

What's currently in the Zoning Code Regarding Bus Terminals?

- **Definition of Local Transit 154.012.B.12.** The Zoning Code defines bus terminals as local transit:
 - *"Local Transit. Establishments primarily engaged in furnishing local and suburban passenger transportation, including taxicabs, passenger charter services, school buses, and terminals (including service facilities) for motor vehicle passenger transportation."*
 - Currently, local transit is not an allowed use within any of the zoning districts.
- **Parking Standards for Local Transit.** The Zoning Code sets forth the following minimum
 - *"2 spaces per 3 employees on the largest shift, based on maximum planned employment."*
- Local Transit is currently not an allowed use within any Zoning District as indicated by the list of Permitted, Conditional, and Interim Uses within the Commercial Zoning Districts.

Intent of Business Park Land Use/Zoning District within the Comprehensive Plan and Zoning Code.

The 2030 Comprehensive Plan states that *"the Business Park land use category is intended to encourage the creation of significant employment centers that accommodate a diverse mix of office and light industrial uses and jobs. Specific desired attributes of this land use include a diversity of jobs, high development densities and jobs per acre, high quality site and building architectural design, and increased tax revenues for the community. Office, office showroom/warehousing, research and development services, light and high-tech electronic manufacturing and assembly, and medical laboratories are typical uses appropriate for this land use category. Some retail and service uses may be allowed as supporting uses for the primary office and light industrial uses of the employment center."*

While this proposal does create a significant number of jobs per acre (200 jobs on about 11 acres of property), the jobs it is creating don't really fit in to the category of types of jobs this land use category is intended to have (office and light industrial uses and jobs). Additionally, a bus terminal does not necessarily fit in to the category of "high quality site and building architectural design." It also does not fit in to any of

the described uses appropriate for this land category. Finally, it would not add to increased tax revenues for the community since it would be tax exempt.

The Draft 2040 Comprehensive Plan states that Business Park (BP) areas are to “...provide for a wide variety of professional businesses such as medical and research facilities, offices, and corporate headquarters. Uses specifically excluded from existing business park areas include warehousing, manufacturing, distribution, assembly and truck terminals. Retail sales of goods and services are allowable uses by conditional use permit provided such uses are goods and services for the employees of the permitted business use. This category excludes any residential use.”

While truck terminals are not exactly the same as local transit in that truck terminals provide tax base, they are similar in assumed site layout and traffic, noise, etc. Trucking terminals are currently a conditional use within the BP – Business Park zoning district and have not been explicitly disallowed per the current Comprehensive Plan. The draft 2040 Comprehensive Plan will exclude such a use within the Business Park land use if the plan in its current draft form is ultimately adopted.

The Zoning Code states the following about the BP Business Park/Light Manufacturing District: “The purpose of the BP District is to provide areas for attractive, high quality business park development primarily for office, high quality manufacturing and assembly, and non-retail uses in developments which provide a harmonious transition to residential development and neighborhoods by: 1) Conducting all business activities and essentially all storage inside buildings; 2) Consisting of high quality and attractive buildings which blend in with the environment; 3) providing open space, quality landscaping and berming; 4) including berming and buffering of parking, loading docks and other similar functions; and 5) protecting and enhancing the natural environment; and 6) providing users with an attractive working environment that is unique in the eastern metropolitan area with immediate access to I-94.”

A bus terminal does not necessarily provide a harmonious transition to residential development and neighborhoods as most business and activities are conducted outside, not inside buildings. While there are limited peak traffic times, there is a significant amount of traffic at those peak times. There will be only one building which is existing and will not be remodeled. There is opportunity to provide berming and buffering on the site should this move forward.

Recommendation. Due to guidance of the current 2030 and draft 2040 Comprehensive Plans as well as the Zoning Code, staff would not recommend that bus terminals be an allowed use (conditional nor permitted) within the Business Park Zoning District due to the following findings:

1. That the procedures for requesting a Zoning Text Amendment are found in the Lake Elmo Zoning Ordinance, Section 154.105.
2. That all the submission requirements of said Section 154.105 have been met by the Applicant.
3. That the proposed Zoning Text Amendment includes the following components:
 - a. That local transit be a conditional use within the Business Park zoning district.

4. The Current comprehensive plan calls for Business Park areas to become "significant employment centers," and bus terminals do not provide significant employment relative to acreage needed for the use;
5. Business Park areas are to include uses with "high quality site and building architectural design," and bus terminals will typically include neither quality by the nature of the use;
6. Business Park land is meant to generate increased tax revenues for the City, and public bus terminals are tax exempt;
7. The use is specifically excluded from Business Park areas in the draft comprehensive plan anticipated to move forward for approval by the end of 2018;
8. The Zoning Code calls for Business Park areas to provide a harmonious transition between the City's commercial areas and its residential areas, and a bus terminal with outdoor storage of busses is antithetical to that goal;
9. The Zoning Code calls for Business Park activities to occur inside of buildings, and bus terminals by their nature include outdoor use and storage of vehicles;
10. The applicant's request to make bus terminals a conditionally permitted use cannot overcome all of the listed problems in findings 4 through 10 via conditions (as proposed or otherwise).

Possible Standards. It should be noted that motor freight and warehousing, which is defined as follows: "establishments engaged primarily in either the storage or shipment of goods and materials, including terminal facilities for handling freight, and maintenance facilities in which the trucks (including tractor trailer units) involved with the operation of the business are stored, parked and serviced. Materials within a warehouse or terminal facility may be combined, broken down, or aggregated for trans-shipment or storage purposes where the original material is not chemically or physically changed," (this includes truck terminals) are a conditional use currently within the Business Park zoning district. A bus terminal is quite similar in use to that of a truck terminal, though the definitions in the Zoning Code differentiate the two. It is also recognized that the development of this property would prompt the extension of City services to this area. If the Planning Commission disagrees with staff and believes that bus terminals are an appropriate use within areas guided for Business Park, staff would recommend the City only adopt narrowly tailored language to preclude bus terminals from certain Business Park areas in order to limit the number of bus terminals allowed in the City and to minimize impact to adjacent properties. If the Planning Commission goes this route, staff would suggest the following standards accompany any such change:

1. The use shall be limited to a school district transportation center, owned by a public school district, serving as a bus terminal for buses and vans which transport passengers to and from schools or between school programs and community residences.
 - *This provision would limit "local transit" to bus terminals and not allow, as per definition from the zoning code, taxicab terminals, passenger charter services, etc.*

2. The property on which the use is located must be located within one half mile of property owned and used by a public school district for an active school or school administration building.
 - *The property on which the bus terminal is being proposed would be the only parcel on the east side of the City that is guided for Business Park and that is within one half mile of such school property.*
3. Must be on a property of at least 10 acres in size or more.
 - *There are parcels on the west side of the city that are zoned Business Park that are in within one half mile of school district land, but none of those parcels meet a ten acre minimum and therefore could not qualify for use as a bus terminal.*
4. Must be sufficiently screened, as determined by the City, from adjacent properties through techniques such as berming and landscaping.
 - *This condition would provide the City with authority to determine the level and location of screening needed to hopefully ensure an acceptable separation of uses.*
5. Accessory uses to bus terminals may include an office and routine maintenance of operable school buses including but not limited to washing and fueling.
 - *This condition would ensure the site is used as a hub for a bus operation, but would not become a major maintenance center for inoperable vehicles in the bus fleet.*

CONDITIONAL USE PERMIT

Purpose. The conditional use permit application would seek to utilize the previous text amendment and allow a "bus terminal" on a portion of the subject property. Failure of the previous application(s) to be approved will likely preclude approval of this application.



Existing Conditions. The applicant is planning to re-locate the existing bus terminal on the property that was previously operating as Rihm Kenworth at 11530 Hudson Boulevard N in Lake Elmo, MN (see below).

Current Interim Use Permit. The property currently operates under an interim use permit which was granted by Resolution No. 2014-095, which allows a bus/truck terminal. This interim use permit will expire on December 2, 2019, and may be renewed with approval by Council. It will terminate when any portion of the property is rezoned or when public sanitary sewer is provided on site. The consent agreement states that the remaining portion of the property must be used for agricultural purposes and that the interim use shall terminate upon any redevelopment of the property for a permitted or conditional use. There has been discussion of the eastern portion of this property developing, being provided sanitary sewer, and no longer being used for agricultural purposes. Therefore, the applicant does not want to operate under the current interim use permit.

Current Building. The current building was constructed in the 1990's and was used for office space by E&H Earthmovers and also provided bus storage for Stillwater Schools. It was then used by Kenworth Diesel Trucks for sales, repairs and service of diesel trucks.

Current Bus Terminal Operation. The current bus terminal operates in Oak Park Heights within Stagecoach in the Old Junker Landfill.

Proposed Operation and Jobs Created. School buses for the Stillwater School District will be parked at this facility when not in use, and routine maintenance will be performed at this facility, including washing and fueling. The existing building will be used for office workers (dispatch, payroll, safety manager, etc.); a home base for drivers to check in and out, training, assignments, and mechanical work (repair and maintenance on buses). Approximately 200 people would work at the facility, with morning and afternoon shifts of drivers and attendants. Hours of operation would be from about 6:00 a.m. to 6:00 p.m. during the weekdays with occasional Saturday use for limited special school district transportation needs. There would be parking provided for school buses (140 spaces), transportation vans (approximately 20), and employees' personal vehicles (approximately 200).

Why this Location? The location is adjacent to major traffic corridors and routes within this area of the City of Lake Elmo. The School District has proposed this location as opposed to a location located adjacent to lower volume roads so as to create less impact. Peak bus traffic times will be limited to morning hours (7:00 a.m. to 9:00 a.m.) and afternoon (2:00 p.m. to 4:00 p.m.).

Setback and Impervious Surface Requirements. The following table outlines how the proposed use adheres to the setback and impervious surface requirements of the Business Park Zoning District. All of the proposed requirements are met. The property to the north is guided for Business Park in both the current (2030) and proposed (2040) Comprehensive Plan Land Use Plan, and so the required setback from residential zones does not apply.

Setback Requirements, Business Park District		
Standard	Required	Proposed
Maximum Height	50 feet	Less than 50 feet
Maximum Impervious Coverage	75%	57%
Front Yard Setback – Building	50 feet	120 feet from existing right-of-way line
Interior Side Yard Setback – Building	30 feet	120 feet from west side and 470 feet from east side
Corner Side Yard Setback – Building	30 feet	N/A
Rear Yard Setback - Building	30 feet	Approximately 445 feet
Residential Zones – Building	150 feet	Approximately 445 feet
Front Yard – Parking	30 feet	Approximately 60 feet from existing right-of-way line
Interior Side Yard – Parking	15 feet	Approximately 45 feet from west side and approximately 130 feet from east side
Corner Side Yard – Parking	30 feet	N/A
Rear Yard - Parking	15 feet	Approximately 32 feet
Minimum Building Floor Size	5,000 square feet	15,498 square feet

General Site Design Considerations, Commercial Districts. The following outlines how the proposed development adheres to the City’s General Site Design considerations for the Business Park zoning district.

- *Circulation.*
 - *Internal connections when feasible.* The parcel to the west has an existing access owned by the parcel to the northwest, so it does not make sense to require this. Additionally, a bus garage would not need to access an adjacent development.
 - *Curb cuts minimized.* The proposed development will use an existing curb cut.
- *Fencing and Screening.* There is a proposed chain link fence, which will be consistent with what is currently on the property, enclosing the eastern portion of the parking lot.
- *Lighting Design.* It is a recommended condition of approval that the applicant submit a photometric plan that meets the requirements of Sections 150.035-150.038 of the City Code.
- *Exterior Storage.* Exterior materials storage must be screened from view from adjacent public streets and adjacent residential properties. It is a recommended condition of approval that the applicant provide sufficient berming and screening of the parking lot and that the fueling area and above ground storage tank also be sufficiently screened from adjacent properties and the public right-of-way.

Parking Lot Requirements

- *Surface and Drainage.* It is required that in commercial districts, all areas intended to be utilized for parking spaces for five or more vehicles be paved with a durable surface including, but not limited to, hot asphalt, bituminous, or concrete. Additionally, industrial districts are required to be surfaced with materials suitable to control dust and drainage. The applicant has indicated that the existing paved and gravel parking area would remain and that the site would be expanded to provide additional gravel surfaced parking for school buses and employee cars. It is a recommended condition of approval that the parking area be paved as required by the Zoning Code.
- *Marking of Parking Spaces.* Parking areas containing five or more spaces are required to be marked with painted lines at least four inches wide. This is a recommended condition of approval.
- *Curbing.* Open off-street parking areas designed to have head-in parking along the property line shall provide a bumper curb or barrier of normal height. This is a recommended condition of approval.
- *Accessible Parking.* The proposed number of parking spaces is 365, of these, 199 are proposed to be car stalls. The Americans with Disabilities Act (ADA) requires six accessible stalls with one van accessible stall with six accessible stalls. The proposed number of ADA stalls is six, though there does not appear to be a van accessible stall provided.

Landscape Plans. The applicant has submitted landscape plans which include 62 Norway Spruces and 61 Colorado Blue Spruce. The existing berms would also remain. Some cursory comments on the landscape plan include the following:

- There is proposed 670 feet of street frontage on the property, requiring at least 14 trees be planted along Hudson Boulevard. There are no trees proposed to be planted along Hudson Boulevard. This requirement is not met.
- The applicant has not submitted a Tree Preservation Plan, as is required. It is a recommended condition of approval that this also be submitted and approved by the Landscape Architect.
- It is not known whether or not the requirement that a minimum of five trees be planted for every one acre of land developed or disturbed is met, as the applicant has not submitted a Tree Preservation Plan as indicated above.

Parking Lot Screening Standards

- *Interior Parking Lot Landscaping.* It is not known whether or not the parking lot meets the interior parking lot landscaping requirements, as the applicant has not indicated what percentage of interior parking lot area is devoted to landscaping planting areas. At least 5% of the parking lot will need to be devoted to islands or corner planting beds and include shade trees in accordance with the table below. Provided the parking lot contains 365 spaces as currently proposed, a minimum of 25 trees will be required within these interior landscaped areas.

Number of Parking Spaces	Minimum Required Tree Planting
0-30	None required
31-100	1 tree per 10 spaces or fraction thereof
101+	1 tree per 15 spaces or fraction thereof

- *Perimeter Parking Lot Landscaping.*
 - *Frontage Strip.* A well-over 8-foot wide frontage strip is provided between parking areas and public street as required for parking lots with over 100 spaces. There is a berm that is already located along Hudson Blvd which screens the parking lot.
- *Screening.* Screening is required to provide visual and noise separation of intensive uses from less intensive uses. The property to the west is used as exterior storage, and it is unknown what the property to the east will be used as. It is recommended that the screening be provided from the property to the west and east that consists of either a masonry wall or fence in combination with landscape material that forms a screen at least six feet in height and at least 90% opaque on a year-round basis and include at least one deciduous or coniferous tree per 40 linear feet along the property line.

Lake Elmo Design Guidelines and Standards. The property is located within the I-94 district, and therefore must adhere to the Lake Elmo Design Guidelines and Standards. As previously indicated, the applicant has proposed to use the existing building. The building generally adheres to Lake Elmo Design Guidelines and standards in that the building does not have a blank façade, the window and door styles reflect the prevailing architecture style of the structure, there is variety in building façade through a change in materials, high quality and durable materials are used in street facing facades, and the building is constructed of pre-cast concrete.

Storage Tank. There is a provision in the Zoning Code that requires that the Council permit uses associated with the bulk storage of over 2,000 gallons of gasoline after finding that fire, explosion, or water or soil contamination hazards are not present that would be detrimental to the public health, safety, and general welfare and that the Zoning Administrator require the development of diking around the tanks, suitably sealed to hold a leakage capacity equal to 115% of the tank capacity. The proposed fueling area contains an above ground storage tank that will store 8,000 gallons of gasoline. Staff has contacted the Minnesota Pollution Control Agency (MPCA), and the only requirement they have is for their Aboveground Storage Tank (AST) Program is that the applicant fill out an AST Notification of Installation or Change in Status Form informing the MPCA of when the tank has been “closed” from the former location and when it has been moved to the new location. It is a recommended condition of approval that the applicant fill out this form both times (when removing it from the former location and when installing it in the new location) as required by the MPCA.

Bus Washing. The applicant has indicated on the application that the property use will include washing buses. The applicant has indicated that the buses will be washed at the facility’s wash bay and that waste water from the wash bay will run in to an oil separator with all of the shop drains, which will go in to a holding tank, which is then disposed of by a sewer transport service. The oil separator is then emptied and maintained by a licensed transporter. The applicant indicates that once the septic system is converted to the

municipal sewer system that the wastewater will go directly to the treatment plant after passing through the oil separator, and the oil separator will be emptied and maintained by a licensed transporter. The disposal of this water is addressed in the engineering comments.

City Engineer Review. This can be referenced in the Preliminary and Final Plat section of this report.

Fire Chief and Building Official Review. The biggest concern that was provided from the Fire Chief and Building Official is that the building is sprinklered yet not hooked up to City water. It is a recommended condition of approval that the property be serviced by City sewer and water prior to the operation of the bus terminal.

Recommendation. Due to recommended denial of the zoning text amendment request due to its inconsistency with the Comprehensive Plan and recommended findings outlined below, which are required for a conditional use permit, staff would recommend that the Planning Commission recommend **denial** of the requested conditional use permit to operate a school district transportation center at a portion of the property located at 11530 Hudson Blvd N based on the following:

1. The proposed use will not be detrimental to or endanger the public health, safety, comfort, convenience or general welfare of the neighborhood or the city. *With over 200 employees and a use that requires both employee trips in and out of the facility each day as well as two bus trips in and out of the facility each day, the use will generate a significant amount of traffic. It is a recommended condition of approval that the applicant obtain any necessary permits or approvals from the MPCA for the above-ground storage tank and bus washing.*
2. The use or development conforms to the City of Lake Elmo Comprehensive Plan. *The property is guided for Business Park. While the proposal does create a significant number of jobs per acre, the parcel will be tax exempt, as it will be owned by the school district. It also does not fit in to the described uses appropriate for this land category. Finally, it does not propose high quality site and building architectural design.*
3. The use or development is compatible with the existing neighborhood. *The use is compatible with the existing neighborhood, as the existing use is a truck terminal, which is similar in use and design to a bus terminal. The surrounding parcels are mostly vacant and undeveloped. However, the surrounding area is planned for uses such as offices, showroom/warehousing, research and development, manufacturing and assembly, and medical laboratories, which are not compatible uses with a bus terminal.*
4. The proposed use meets all specific development standards for such use listed in Article 7 of this Chapter. *The proposal complies with the proposed development standards as recommended in the requested Zoning Text Amendment, but these have not yet been approved. It does not comply with many parking lot, screening, and landscape standards, which have been required as a recommended condition of approval.*

5. If the proposed use is in a flood plain management or shoreland area, the proposed use meets all the specific standards for such use listed in Chapter 150, §150.250 through 150.257 (Shoreland Regulations) and Chapter 152 (Flood Plain Management). ***The property is located outside the 0.2% annual chance floodplain.***
6. The proposed use will be designed, constructed, operated and maintained so as to be compatible in appearance with the existing or intended character of the general vicinity and will not change the essential character of that area. ***While the proposed project is a redevelopment/repurposing of an existing site and will not significantly change the existing character of the area, there are no proposed improvements to the building. Operations are not within a building and so would not meet the intended character of the neighborhood.***
7. The proposed use will not be hazardous or create a nuisance as defined under this Chapter to existing or future neighboring structures. ***The proposed development will be fenced and landscaped to help screen the use from neighboring properties. However, the proposed use will create a significant amount of traffic, at least at certain times of the day.***
8. The proposed use will be served adequately by essential public facilities and services, including streets, police and fire protection, drainage structures, refuse disposal, water and sewer systems and schools or will be served adequately by such facilities and services provided by the persons or agencies responsible for the establishment of the proposed use. ***The property is within the Metropolitan Urban Service Area (MUSA) and therefore should be required to be provided by city sewer and water. Currently, the property is not provided with City sewer and water, and there are plans to connect. It is a recommended condition of approval that the property be connected to City sewer and water. Additionally, a traffic study should be required to determine timing and extent of improvements required for CSAH 15/Hudson Blvd traffic signal/realignment as well as to determine if an eastbound left turn lane along Hudson Blvd is also needed at the site access. The applicant has not submitted a stormwater management plan and will be required to do so, meeting all City, State, and Valley Branch Watershed District requirements.***
9. The proposed use will not create excessive additional requirements at public cost for public facilities and services and will not be detrimental to the economic welfare of the community. ***The proposed use will pay sewer and water service charges and will facilitate in bringing sewer east of the parcel.***
10. The proposed use will not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons, property or the general welfare because of excessive production of traffic, noise, smoke, fumes, glare or odors. ***The proposed use will generate a significant number of trips per day, though this will be limited to certain times of the day. Minimal noise is expected to come from the site, except during heavy traffic times at specific times within the morning or afternoon.***

11. Vehicular approaches to the property, where present, will not create traffic congestion or interfere with traffic on surrounding public thoroughfares. ***Traffic congestion could be created from the significant number of trips to the site that would be needed, though a traffic study would be required to determine the exact impact.***
12. The proposed use will not result in the destruction, loss or damage of a natural or scenic feature of major importance. **N/A**

Recommendation Findings for Approval. *If the Planning Commission wishes to recommend approval, the following findings and conditions of approval are recommended:*

1. The proposed use will not be detrimental to or endanger the public health, safety, comfort, convenience or general welfare of the neighborhood or the city. ***Bus traffic is not expected to be detrimental or dangerous, as trips and traffic will be limited to certain times of the day. It is a recommended condition of approval that there be significant berming to provide a sufficient screening of the parking lot.***
2. The use or development conforms to the City of Lake Elmo Comprehensive Plan. ***The property is guided for Business Park, in which local transit, which meets certain standards, is a conditional use. The use also provides a significant number of jobs per acre.***
3. The use or development is compatible with the existing neighborhood. ***The use is compatible with the existing neighborhood, as the existing use is a truck terminal, which is similar in use and design to a bus terminal. The surrounding parcels are mostly vacant and undeveloped.***
4. The proposed use meets all specific development standards for such use listed in Article 7 of this Chapter. ***The proposal complies with the proposed development standards as recommended in the requested Zoning Text Amendment, although it does not comply with many parking lot, screening, and landscape standards, which have been required as a recommended condition of approval.***
5. If the proposed use is in a flood plain management or shoreland area, the proposed use meets all the specific standards for such use listed in Chapter 150, §150.250 through 150.257 (Shoreland Regulations) and Chapter 152 (Flood Plain Management). ***The property is located outside the 0.2% annual chance floodplain.***
6. The proposed use will be designed, constructed, operated and maintained so as to be compatible in appearance with the existing or intended character of the general vicinity and will not change the essential character of that area. ***While the proposed project is a redevelopment/repurposing of an existing site and will not change the existing character of the area, there are no proposed improvements to the building. Operations are not within a building and so would not meet the intended character of the neighborhood.***
7. The proposed use will not be hazardous or create a nuisance as defined under this Chapter to existing or future neighboring structures. ***The proposed development will be fenced and landscaped to help screen the use from neighboring properties. However,***

- the proposed use will create a significant amount of traffic, at least at some times of the day.*
8. The proposed use will be served adequately by essential public facilities and services, including streets, police and fire protection, drainage structures, refuse disposal, water and sewer systems and schools or will be served adequately by such facilities and services provided by the persons or agencies responsible for the establishment of the proposed use. *The property is within the Metropolitan Urban Service Area (MUSA) and therefore should be required to be provided by city sewer and water. Currently, the property is not provided with City sewer and water, and there are plans to connect. It is a recommended condition of approval that the property be connected to City sewer and water. Additionally, a traffic study should be required to determine timing and extent of improvements required for CSAH 15/Hudson Blvd traffic signal/realignment as well as to determine if an eastbound left turn lane along Hudson Blvd is also needed at the site access. The applicant has not submitted a stormwater management plan and will be required to do so, meeting all City, State, and Valley Branch Watershed District requirements.*
 9. The proposed use will not create excessive additional requirements at public cost for public facilities and services and will not be detrimental to the economic welfare of the community. *While the proposal will pay sewer and water service charges, benefiting the community, the use will not pay property taxes.*
 10. The proposed use will not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons, property or the general welfare because of excessive production of traffic, noise, smoke, fumes, glare or odors. *The proposed use will generate a significant number of trips per day, though this will be limited to certain times of the day. Minimal noise is expected to come from the site, except during heavy traffic times at specific times within the morning or afternoon.*
 11. Vehicular approaches to the property, where present, will not create traffic congestion or interfere with traffic on surrounding public thoroughfares. *Traffic congestion could be created from the significant number of trips to the site that would be needed, though a traffic study would be required to determine the exact impact.*
 12. The proposed use will not result in the destruction, loss or damage of a natural or scenic feature of major importance. **N/A**

Recommended Conditions of Approval. If the Planning Commission wishes to recommend approval, staff recommends the following conditions:

- 1) The Zoning Text Amendment to allow local transit (school district transportation center) within the Business Park Zoning District must be approved.
- 2) No construction or use of the bus terminal (unless in accordance with the Interim Use Permit approved by the City by Resolution 2014-095) may commence until all items as outlined in the City Engineer review memo regarding the Four Corners Preliminary & Final Plat (Stillwater Transportation Center) dated May 30, 2018 and all other subsequent construction plan memos

regarding the Stillwater Transportation Center have been addressed and the Four Corners 1st Addition Final Plat has been recorded.

- 3) The applicant must obtain all other necessary City, State, and other governing body permits prior to the commencement of any construction activity on the parcel including but not limited to an approved stormwater management plan, utility plans, grading plan, street construction plans (if required), parking lot permit, building permits, etc.
- 4) The Applicant shall submit a photometric plan, and all lighting must meet requirements of Sections 150.035-150.038 of the City Code.
- 5) The above ground storage tank shall require development of diking around the tank, suitably sealed to hold a leakage capacity equal to 115% of the tank capacity. The applicant shall demonstrate that fire, explosion, or water or soil contamination hazards are not present that would be detrimental to the public health, safety, and general welfare. The applicant shall also fill out an Aboveground Storage Tank Notification of Installation or Change in Status Form as required by the Minnesota Pollution Control Agency (MPCA).
- 6) The Applicant shall submit an updated Landscape Plan and Tree Preservation Plan which includes sufficient berming and screening and addresses the comments in this report to be reviewed and approved by the City's Landscape Architect.
- 7) Parking areas shall be paved with a durable surface including, but not limited to, hot asphalt, bituminous or concrete; spaces shall be marked with painted lines at least four inches wide; required interior and exterior parking lot screening is required; a bumper curb or barrier of normal bumper height shall be provided; and must provide an adequate number of Americans with Disabilities Act (ADA) accessible stalls.
- 8) A sign permit shall be obtained prior to erection of any sign on the property.
- 9) The property shall be connected to City sewer and water prior to operation of the bus terminal.
- 10) A Traffic Impact Study is required to determine timing and extent of improvements required for the CSAH 15 and Hudson Boulevard intersection realignment and traffic signal improvements as well as to determine if an eastbound left turn lane along Hudson Boulevard is also needed at the site access.
- 11) The applicant shall be required to include a description of the sanitary sewer capacity demands including the number of residential equivalency units (REC) based on the Metropolitan Council Sewer Availability Charge (SAC) determination policy as well as a description of the water capacity demands including average day use, peak day use, and fire suppression demands. Demands must account for all planned uses and connections to the sewer system including bus wash wastewater as well as include potable drinking water, bus washing operations, etc.
- 12) Applicant shall be responsible to place hydrants throughout the property at the direction of the Fire Department. All fire hydrants shall be owned and maintained by the City.

FISCAL IMPACT:

If approved and recommended conditions of approval are adopted, the applicant will be required to pay sewer and water availability charges and will be responsible at its sole cost for bringing sewer and water to the property and extending it to adjacent properties. Maintenance of streets, trails, sanitary sewer mains, and other public infrastructure should be considered. The City will collect Sewer Accessibility Charges and Water Accessibility Charges, building permit fees, and property taxes

OPTIONS:

The Commission may:

- Recommend that Council deny the request to allow local transit as a conditional use within the Business Park zoning district and for a conditional use permit to operate a school district transportation center on a portion of the property located at 11530 Hudson Blvd N and recommend that Council approve the requests for a Four Corners preliminary and final plat to subdivide the property located at 11530 Hudson Blvd N and for a zoning map amendment to rezone a portion of the property located at 11530 Hudson Blvd N with staff-recommended conditions of approval.
- Recommend that Council deny the request to allow local transit as a conditional use within the Business Park zoning district and for a conditional use permit to operate a school district transportation center on a portion of the property located at 11530 Hudson Blvd N and recommend that Council approve the requests for a Four Corners preliminary and final plat to subdivide the property located at 11530 Hudson Blvd N and for a zoning map amendment to rezone a portion of the property located at 11530 Hudson Blvd N with amended conditions of approval.
- Recommend that Council deny all requests with findings to support denial for the Four Corners preliminary and final plat and zoning map amendment.
- Recommend that Council approve all requests with staff-drafted conditions of approval.
- Amend any recommended conditions of approval and recommend approval of all requests with amended conditions of approval.

RECOMMENDATION:

Staff recommends that the Planning Commission approve the Four Corners preliminary and final plat and zoning map amendment request but deny the zoning text amendment and conditional use permit request based on the findings and conditions listed in this report:

“Move to recommend approval of the request from Terry Emerson for the Four Corners preliminary and final plat to subdivide the property located at 11530 Hudson Blvd N in to Lot 1, Block 1, Four Corners, along with three separate outlots.”

“Move to recommend approval of the request from Stillwater Area Public Schools for a Zoning Map Amendment to rezone Lot 1, Block 1, Four Corners from Rural Development Transitional to Business Park.”

“Move to recommend denial of the request from Stillwater Area Public Schools for a Zoning Text Amendment to allow local transit as a conditional use within the Business Park zoning district.”

“Move to recommend denial of the request from Stillwater Area Public Schools for a Conditional Use Permit to allow a school district transportation center at the property located at Lot 1, Block 1, Four Corners.”

If the Planning Commission wishes to recommend approval of the Zoning Text amendment and Conditional Use Permit, the motions regarding those requests can be made as follows:

“Move to recommend approval of the request from Stillwater Area Public Schools for a Zoning Text Amendment to allow local transit as a conditional use within the Business Park zoning district.”

“Move to recommend approval of the request from Stillwater Area Public Schools for a Conditional Use Permit to allow a school district transportation center at the property located at Lot 1, Block 1, Four Corners with the conditions as recommended by Staff/with the amended conditions of approval.”

ATTACHMENTS:

- Applications for Zoning Text Amendment, Zoning Map Amendment, Conditional Use Permit and Four Corners preliminary and final plat and Narratives.
- Preliminary and Final Plat
- Bus Terminal Plans
- Ord. 08-

Date Received: _____
Received By: _____
Permit #: _____



651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

LAND USE APPLICATION

- ☐ Comprehensive Plan ☒ Zoning District Amend ☐ Zoning Text Amend ☐ Variance*(see below) ☐ Zoning Appeal
- ☐ Conditional Use Permit (C.U.P.) ☐ Flood Plain C.U.P. ☐ Interim Use Permit (I.U.P.) ☐ Excavating/Grading
- ☐ Lot Line Adjustment ☐ Minor Subdivision ☐ Residential Subdivision Sketch/Concept Plan
- ☐ PUD Concept Plan ☐ PUD Preliminary Plan ☐ PUD Final Plan ☐ Wireless Communications

Applicant: STILLWATER AREA PUBLIC SCHOOLS - KRISTEN HOHEISEL
Address: 1875 GREELEY STREET SOUTH, STILLWATER, MN 55082
Phone #: 651-351-8321
Email Address: HOHEISELK@STILLWATERSCHOOLS.ORG

Fee Owner: EN PROPERTIES, LLC - TERRY EMERSON
Address: 11530 HUDSON BLVD. NORTH LAKE ELMO, MN 55042
Phone #: 612 845 3373
Email Address: TSE03@COMCAST.NET

Property Location (Address): 11530 HUDSON BLVD. NORTH, LAKE ELMO, MN
(Complete (long) Legal Description): SEE ATTACHMENT - 11 ACRE PARCEL
IN SOUTHWEST CORNER OF PROPERTY
PID#: 36 029 21 43 0001

Detailed Reason for Request: _____

*Variance Requests: As outlined in Section 301.060 C. of the Lake Elmo Municipal Code, the applicant must demonstrate practical difficulties before a variance can be granted. The practical difficulties related to this application are as follows:

In signing this application, I hereby acknowledge that I have read and fully understand the applicable provisions of the Zoning ordinance and current administrative procedures. I further acknowledge the fee explanation as outlined in the application procedures and hereby agree to pay all statements received from the City pertaining to additional application expense.

Signature of applicant: [Signature] Date: 4/26/18
Signature of fee owner: Terry Emerson Date: 4/26/18

City of Lake Elmo
Narrative for Zoning Map Amendment
April 30, 2018

The property being considered for re-development is currently being used for after-market parts and equipment sales, along with service of Kenworth Diesel Trucks. The proposed use for a School District Transportation Center. School buses for the District would be parked at this facility when not in use, along with performing routine maintenance of the buses at this location. As part of the servicing and maintenance of the buses, it is also planned to wash and fuel buses at this facility.

Approximately 200 people would work at this facility, with morning and afternoon shifts of drivers and attendants, along with the vehicle maintenance and administrative staff. This proposed use would be similar to the existing use of the property, in providing routine service of vehicles. Parking for the school buses (total of 140) transportation vans (about 20) and personal vehicles of the employees (about 200) would be provided on-site.

This location is adjacent to major traffic corridors and routes within this area of the City of Lake Elmo and this would have less of an impact on traffic flow than would occur on lower volume roads or those through residential areas.

Water and sanitary sewer infrastructure are also proposed to this area of the City as part of the adjacent development and re-alignment of Hudson Boulevard, eliminating the need for septic systems for treatment of wastewater from this facility. As the water for the facility is switched from the well to municipal water, the existing well would be abandoned in accordance with Minnesota Health Department requirements.



Lake Elmo City Hall
651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

AFFIRMATION OF SUFFICIENT INTEREST

I hereby affirm that I **am the fee title owner** of the below described property or that I have written authorization from the owner to pursue the described action.

Name of applicant Terry Emerson
(Please Print)

Street address/legal description of subject property 11530 HUDSON BLVD.
NORTH, LAKE ELMO, MN

Terry Emerson
Signature

4/26/18
Date

If you are not the fee owner, attach another copy of this form which has been completed by the fee owner or a copy of your authorization to pursue this action.

If a corporation is fee title holder, attach a copy of the resolution of the Board of Directors authorizing this action.

If a joint venture or partnership is the fee owner, attach a copy of agreement authorizing this action on behalf of the joint venture or partnership.

Date Received: _____
Received By: _____
Permit #: _____



651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

LAND USE APPLICATION

- ☐ Comprehensive Plan ☐ Zoning District Amend ☒ Zoning Text Amend ☐ Variance*(see below) ☐ Zoning Appeal
- ☐ Conditional Use Permit (C.U.P.) ☐ Flood Plain C.U.P. ☐ Interim Use Permit (I.U.P.) ☐ Excavating/Grading
- ☐ Lot Line Adjustment ☐ Minor Subdivision ☐ Residential Subdivision Sketch/Concept Plan
- ☐ PUD Concept Plan ☐ PUD Preliminary Plan ☐ PUD Final Plan ☐ Wireless Communications

Applicant: STILLWATER AREA PUBLIC SCHOOLS - KRISTEN HOHEISEL
Address: 1875 GREELEY STREET SOUTH, STILLWATER, MN
Phone #: 651-351-8321 55082
Email Address: HOHEISELK@STILLWATERSCHOOLS.ORG

Fee Owner: EN PROPERTIES, LLL - TERRY EMERSON
Address: 11530 HUDSON BLVD NORTH, LAKE ELMO, MN 55042
Phone #: 612-845-3373
Email Address: TSEO3@COMCAST.NET

Property Location (Address): 11530 HUDSON BLVD. NORTH, LAKE ELMO, MN
(Complete (long) Legal Description: SEE ATTACHED - 11 ACRE PARCEL IN
SOUTH WEST CORNER OF PROPERTY.
PID#: 36 029 21 43 0001

Detailed Reason for Request: ZONING TEXT AMENDMENT TO 154.551,
TABLE 12-1 TO INCLUDE "LOCAL TRANSIT" AS A
CONDITIONAL USE IN THE BUSINESS PARK DISTRICT.
ALSO APPLYING TO ADD STANDARDS FOR LOCAL TRANSIT
TO 154.554

*Variance Requests: As outlined in Section 301.060 C. of the Lake Elmo Municipal Code, the applicant must demonstrate practical difficulties before a variance can be granted. The practical difficulties related to this application are as follows:

In signing this application, I hereby acknowledge that I have read and fully understand the applicable provisions of the Zoning ordinance and current administrative procedures. I further acknowledge the fee explanation as outlined in the application procedures and hereby agree to pay all statements received from the City pertaining to additional application expense.

Signature of applicant: [Signature] Date: 4/26/18

Signature of fee owner: [Signature] Date: 4/26/18



Lake Elmo City Hall
651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

ZONING TEXT AMENDMENT SUBMISSION REQUIREMENTS

In accordance with the provisions of Minnesota State Statutes, the City Council may from time to time adopt amendments to the zoning ordinance. An amendment to the zoning ordinance involves changes in its text and wording, including but not limited to, changes in the regulations regarding uses setbacks, heights, lot areas, definitions, administration, and/or procedures. Text amendments do not include the rezoning of property.

The application for a zoning text amendment shall include:

- a. Land Use application form completed and signed by Owner, or someone having legal interest in the property.
- b. Date of application
- c. Name, address, telephone number, and, if available, fax and email address of the applicant as well as of the person, firm, corporation, or association.
- d. Parcel ID # and Legal description.
- e. Chapter and section number of proposed amendment along with existing text of section.
- f. Proposed language for ordinance amendment. Identification of the proposed substitute wording for the zoning text.
- g. A narrative describing your reason for requesting zoning text amendment. Your description should include how you would be impacted by the zoning text amendment. How the text amendment meets the Comprehensive Plan. How the text amendment will impact the zoning and the subdivision code.
- h. Applicable fee listed on the Cities current fee schedule.
- i. Such other information as the City may require to ensure compliance with any other applicable regulations.

City of Lake Elmo
Narrative for Zoning Text Amendment
April 30, 2018

The property being considered for re-development is currently being used for after-market parts and equipment sales, along with service of Kenworth Diesel Trucks. The proposed use is for local school buses to be parked along with routine maintenance at this location. This proposed use (local transit) is currently not listed in the zoning code as a permitted use in the Business Park Zoning District, but would be similar to the existing use of the property. In addition, this location is adjacent to major traffic corridors and routes, and this would limit bus traffic on lower volume roads and through residential areas.

This proposed use would not be detrimental to the other permitted uses in the Business Park District. This use would be consistent with the City of Lake Elmo Comprehensive Plan for the Business Park District in providing a diverse mixed use, with a high density of jobs at this location. This facility will employ about 200 people, including the various shifts, positions and administrative staff.

Water and sanitary sewer infrastructure are also proposed to this area of the City as part of the adjacent development and re-alignment of Hudson Boulevard, eliminating the need for septic systems for treatment of wastewater from this facility. As the water for the facility is switched from the well to municipal water, the existing well would be abandoned in accordance with Minnesota Health Department requirements.

3) providing open space, quality landscaping and berming; 4) including berming and buffering of parking, loading docks and other similar functions; and 5) protecting and enhancing the natural environment; and 6) providing users with an attractive working environment that is unique in the eastern metropolitan area with immediate access to I-94.

(Ord. 2012-062, passed 9-18-2012)

§ 154.551 PERMITTED, CONDITIONAL AND INTERIM USES.

Table 12-1 lists all permitted and conditional uses allowed in the commercial districts. "P" indicates a permitted use, "C" a conditional use and "I" an interim use. Uses not so indicated shall be considered prohibited. Cross-references listed in the table under "Standards" indicate the location within this chapter of specific development standards that apply to the listed use.

A. *Combinations of Uses.* The following use types may be combined on a single parcel.

1. Principal and accessory uses.
2. Other permitted or conditional uses allowed within the district may be combined on a single parcel, provided that a unified and integrated site plan is approved. The entire development must be approved as a conditional use.
3. A mixed-use building that combines permitted or conditionally permitted residential, service, retail and civic uses may be developed meeting the form standards of this subchapter. Office or studio uses on upper stories are encouraged.

Table 12-1: Permitted, Conditional and Interim Uses, Commercial Districts

	<i>LC</i>	<i>CC</i>	<i>C</i>	<i>BP</i>	<i>Standard</i>
<i>Residential Uses</i>					
Household Living					
Single-family attached dwelling	-	-	C	-	154.554 (A)
Multifamily dwelling	-	-	C	-	154.554 (B)
Live-work unit	C	C	C	-	154.012 (B) (1)
Group Living					
Semi-transient accommodations	-	-	C	-	154.301 (D)
Congregate housing	-	-	C	-	154.301 (C)
<i>Public and Civic Uses</i>					
Colleges and universities	-	-	C	C	154.012 (B) (2), 154.303 (A)
Community service	-	C	C	C	154.012 (B) (2)
Day care center	C	C	C	C	154.012 (B) (2)

Schools, public and private	-	-	C	C	154.012 (B) (2), 154.303 (A)
	LC	CC	C	BP	Standard
Public assembly	-	-	C	C	154.012 (B) (2)
Religious institutions	-	-	C	-	154.012 (B) (2), 154.303 (N)
<i>Services</i>					
Business services	P	P	P	P	154.012 (B) (3)
Business center	P	P	P	P	154.012 (B) (3)
Offices	P	P	P	P	154.012 (B) (3)
Commercial kennel	-	-	C	-	
Communication services	C	C	P	P	154.012 (B) (3)
Educational services	P	P	P	P	154.012 (B) (3), 154.303 (A)
Financial institution	P	P	P	P	154.012 (B) (3)
Funeral home	-	C	P	-	154.012 (B) (3)
Lodging	-	-	P	C*	154.012 (B) (3), 154.302 (D), *154.554 (C)
Medical facility	-	-	C	C	154.012 (B) (3), 154.303 (B)
Membership organization	P	P	P	-	154.012 (B) (3)
Nursing and personal care	C	C	C	-	154.012 (B) (3), 154.303 (C)
Personal services	P	P	P	-	154.012 (B) (3)
<i>Services</i>					
Repair and maintenance shop	-	-	P	-	154.554 (D)
Self-service storage	-	-	C	C	154.303 (D)
Trade shop	-	-	P	-	154.554 (E)
Transportation services	-	-	-	C	154.012 (B) (3)
Veterinary services	P	P	P	C	154.554 (F)
<i>Food Services</i>					
Standard restaurant	-	P	P	C*	154.012 (B) (4), 154.554 (L)

Drive-in restaurant	-	C	C	-	154.304 (A)
Drinking & entertainment	-	C	P	-	154.304 (B)
Fast food restaurant	-	P	P	C*	*154.554 (M)
<i>Sales of Merchandise</i>					
General retail sales ¹	C	P	P	C*	154.554 (N)
Building supplies sales	-	-	C	-	
Warehouse club sales	-	-	C	-	
Furniture and appliance sales	-	-	P	-	
	LC	CC	C	BP	Standard
Grocery, supermarket	-	-	P	-	
Liquor store	-	P	P	-	
Garden center	-	-	P	-	154.554 (G)
Neighborhood convenience store	-	P	P	-	
Shopping center	-	P	P	-	
<i>Sales of Merchandise</i>					
Wholesaling	-	-	P	-	
<i>Automotive/Vehicular Uses</i>					
Automobile maintenance service	-	-	C	-	154.554 (H)
Automobile parts/supply	-	-	P	-	154.554 (H)
Car wash	-	-	C	-	154.012 (B) (6)
Commercial vehicle repair	-	-	-	-	154.554 (H)
Gasoline station	-	C	C	-	154.305 (B)
Parking facility	-	-	C*	C	*154.554 (I)
Sales and storage lots	-	-	C	-	154.305 (C)
<i>Outdoor Recreation</i>					
Campgrounds and trailering	-	-	-	-	
Golf course	-	-	-	-	
Marina	-	-	-	-	
Outdoor entertainment	-	-	-	-	
Outdoor recreation facility	-	-	C	-	154.306 (C)
Parks and open areas	P	P	P	P	154.012 (B) (7)
Restricted recreation	-	-	-	-	

<i>Indoor Recreation/Entertainment</i>					
Adult establishment	-	-	-	C	Chapter 113
Indoor athletic facility	-	C	P	C	154.307 (A)
<i>Indoor Recreation/Entertainment</i>					
Indoor recreation	-	-	C	-	154.307 (A)
<i>Agricultural and Related Uses</i>					
Agricultural sales business	-	I	P	-	154.012 (B) (9)
Agricultural services	-	-	C	-	154.012 (B) (9)
	LC	CC	C	BP	Standard
Agricultural support	-	-	C	-	154.012 (B) (9)
Greenhouses - non retail	-	-	-	-	154.012 (B) (9)
Wayside stand	P	P	P	P	154.012 (B) (9)
<i>Industrial and Extractive Uses</i>					
Heavy industrial	-	-	-	-	
Landfill	-	-	-	-	
Light industrial	-	-	-	C	154.012 (B) (10)
Non-production industrial	-	-	-	C	154.554 (J)
Motor freight and warehousing	-	-	-	C	154.012 (B) (10)
Research and testing	-	-	-	C	154.012 (B) (10)
Resource extraction	-	-	-	-	154.012 (B) (10)
Salvage/recyclable center	-	-	-	-	154.012 (B) (10)
<i>Transportation and Communications</i>					
* Broadcasting and communications	C	C	C	C	154.012 (B) (11), 154.083
<i>Accessory Uses</i>					
Bed and breakfast	-	-	-	-	154.012 (B) (12), 154.310 (A)
Drive-through facility	-	C	C	-	154.304 (A)
Family day care	-	-	-	-	154.012 (B) (12)
Group family day care	-	-	-	-	154.012 (B) (12)
Home occupation	-	-	-	-	154.012 (B) (12)
Parking facility	C	C	P*	P	*154.554 (I)
Outdoor storage	-	-	C	-	

* ADD LOCAL TRANSIT AS A CONDITIONAL USE IN
BP DISTRICT.

Outdoor display	-	-	C	-	
Solar equipment	P	P	P	P	154.310 (C)
Other structures typically incidental and clearly subordinate to permitted use	P	P	P	P	

1. General Retail Sales shall include all of the subcategories identified in the § 154.012(B)(5) under Retail Trade with the exception of those subcategories listed separately in Table 12-1 above.

(Ord. 2012-062, passed 9-18-2012)

(Ord. 08-116, passed 3-3-2015)

§ 154.552 LOT DIMENSIONS AND BUILDING BULK REQUIREMENTS.

Lot area and setback requirements shall be as specified in Table 12-2 Lot Dimension and Setback Requirements, Commercial Districts.

Table 12-2: Lot Dimension and Setback Requirements, Commercial Districts

	<i>LC</i>	<i>CC</i>	<i>C</i>	<i>BP</i>
Minimum lot area (sq. Ft.)	3.5 acres	12,000	20,000	2 acres
Minimum lot width (feet)	300	75	100	200
Minimum lot depth (feet)	400	-	-	-
Maximum height (feet/stories)	35	35	45	50 ^a
Maximum impervious coverage	40%	60%	75%	75%
Building setback requirements (feet)				
Front yard	100	30	30	50
Interior side yard	50	20	10	30
Corner side yard	100	25 ^c	25 ^c	30
Rear yard	50	30 ^b	30 ^b	30
Residential zones	150	50	50	150
Parking setback requirements (feet)				
Front yard	50	15	15	30
Interior side yard	50	10	10	15
Corner side yard	50	15	15	30
Rear yard	50	10	10	15
Residential zones	100	35	35	100

Minimum building floor size (sq. ft.)	4,000	-	-	5,000
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Notes to Table 12-2

- a. Buildings higher than 50 feet may be allowed through a Conditional Use Permit and would be subject to a separate technical and planning evaluation.
- b. Accessory buildings must be set back 10 feet from property lines.
- c. Corner properties: The side facade of a corner building adjoining a public street shall maintain the front setback of the adjacent property fronting upon the same public street, or the required front yard setback, whichever is less. If no structure exists on the adjacent property, the setback shall be as shown in the table.

(Ord. 2012-062, passed 9-18-2012)

§ 154.553 GENERAL SITE DESIGN CONSIDERATIONS, COMMERCIAL DISTRICTS.

Development of land within the commercial districts shall follow established standards for traffic circulation, landscape design, and other considerations as specified in Articles 7, 8 and 9. (Ord. 08-152, passed 10-01-2016)

- A. *Circulation.* Internal connections shall be provided between parking areas on adjacent properties wherever feasible.
 1. The number and width of curb-cuts shall be minimized. To promote pedestrian circulation, existing continuous curb-cuts shall be reduced to widths necessary for vehicular traffic, and unnecessary or abandoned curb cuts shall be removed as parcels are developed.
- B. *Fencing and Screening.* Fencing and screening walls visible from the public right-of-way shall be constructed of materials compatible with the principal structure.
- C. *Lighting Design.* Lighting shall be integrated into the exterior design of new or renovated structures to create a greater sense of activity, security, and interest to the pedestrian. All lighting shall be installed in conformance to §150.035 through §150.038.
- D. *Exterior Storage.* Exterior materials storage must be screened from view from adjacent public streets and adjacent residential properties, by a wing of the principal structure or a screen wall constructed of the same materials as the principal structure. Height of the structure or screen wall must be sufficient to completely conceal the stored materials from view at eye level (measured at 6 feet above ground level) on the adjacent street or property.

(Ord. 2012-062, passed 9-18-2012) Penalty, see § 154.999

§ 154.554 DEVELOPMENT STANDARDS FOR SPECIFIC USES.

The following standards apply to specific uses allowed within the Commercial Districts. Other specific use standards are located in Article 9. (Ord. 08-152, passed 10-01-2016)

- A. *Single-family attached dwellings, C District.* Limited to areas that are designated as mixed-use in the Comprehensive Land Use Plan.
- B. *Multi-family dwelling units, C District.* Dwelling units (both condominium and rental) are allowed as follows:
 - 1. Within those areas designated as mixed-use in the Comprehensive Plan; and
 - 2. On the upper floors or rear or side ground floors of a mixed-use building approved as part of a Planned Unit Development
- C. *Lodging, BP District.* Must incorporate a full-service restaurant and rooms accessible only through interior corridors and be subordinate to a main business complex.
- D. *Repair and Maintenance Shop.* No outdoor storage is permitted.
- E. *Trade Shop.* Exterior materials storage must be totally screened from view from adjacent public streets and adjacent residential properties, by a wall of the principal structure or a screen wall constructed of the same materials as the principal structure.
- F. *Veterinary Services.* All activities must be conducted within an enclosed building. Crematoriums are not allowed.
- G. *Garden Center*
 - 1. The storage or display of any materials or products shall meet all primary building setback requirements of a structure, and shall be maintained in an orderly manner. Screening along the boundaries of adjacent residential properties may be required, meeting the standards of 154.258 (F).
 - 2. All loading and parking shall be provided off-street.
 - 3. The storage of any soil, fertilizer or other loose, unpackaged materials shall be contained so as to prevent any effects on adjacent uses.
- H. *Automobile Maintenance Service and Automobile Parts/Supply*
 - 1. All vehicle repairs shall be conducted in a completely enclosed building.
 - 2. The storage or display of inoperable or unlicensed vehicles or other equipment shall meet all setback requirements of a primary structure, and shall be totally screened from view from adjacent public streets and adjacent residential properties.
- I. *Parking Facility, C District.* Structured parking is permitted as a ground floor use within a mixed-use building, provided that it is located on side or rear facades, not facing the primary abutting street. The primary street-facing facade shall be designed for retail, office or residential use.
- J. *Non-Production Industrial, BP District.* Non-production industrial use shall be allowed as a principal use, and may include wholesale and off-premise sales, provided that:
 - 1. The use is served by a street of sufficient capacity to handle the traffic the use will generate;
 - 2. The use includes a retail or office component equal to at least 25% of the floor area of the use; and

3. An appropriate transition area between the use and adjacent property may be required, to include landscaping, screening and other site improvements consistent with the character of the area.
- K. *Outdoor Dining Accessory to Food Services.* Outdoor dining is allowed as an accessory use in the commercial districts, provided that tables do not block a public sidewalk or other walkway needed for pedestrian circulation. A minimum of 5 feet of sidewalk or walkway must remain open.
- L. *Standard Restaurant, BP District.* Must be incorporated as part of a larger business center or lodging use.
- M. *Fast Food Restaurant, BP District.* Must be incorporated as part of a larger business center or lodging use.
- N. *Retail Trade, BP District.* Limited to uses clearly incidental and accessory to a permitted or conditionally permitted principal use of the land.
 1. The compounding, dispensing or sale of drugs, prescription items, patient or proprietary medicine, sick room supplies, prosthetic devices or items relating to any of the permitted or conditionally permitted uses is only allowed when conducted in the building occupied primarily by medical facilities or offices.

(Ord. 2012-062, passed 9-18-2012) Penalty, see § 154.999

ADD : O. LOCAL TRANSIT, BP DISTRICT.

§ 154.555 COMMERCIAL DISTRICT DESIGN STANDARDS.

Review of Design. For certain development activity as specified in the Lake Elmo Design Guidelines and Standards Manual, design review is required as part of the approval process for a permit or certificate under this Ordinance. All projects subject to design review shall be reviewed for conformance with the Lake Elmo Design Guidelines and Standards Manual and shall follow the review procedures specified in §154.506.A.

(Ord. 08-095, passed 11-19-2013)

Date Received: _____
Received By: _____
Permit #: _____



651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

LAND USE APPLICATION

- ☐ Comprehensive Plan ☒ Zoning District Amend ☐ Zoning Text Amend ☐ Variance*(see below) ☐ Zoning Appeal
- ☒ Conditional Use Permit (C.U.P.) ☐ Flood Plain C.U.P. ☐ Interim Use Permit (I.U.P.) ☐ Excavating/Grading
- ☐ Lot Line Adjustment ☐ Minor Subdivision ☐ Residential Subdivision Sketch/Concept Plan
- ☐ PUD Concept Plan ☐ PUD Preliminary Plan ☐ PUD Final Plan ☐ Wireless Communications

Applicant: STILLWATER AREA PUBLIC SCHOOLS - KRISTEN HOHEISEL
Address: 1875 GREELEY STREET SOUTH, STILLWATER, MN 55082
Phone #: 651-351-8321
Email Address: HOHEISELK@STILLWATERSCHOOLS.ORG

Fee Owner: EN PROPERTIES, LLC - TERRY EMERSON
Address: 11530 HUDSON BLVD. NORTH LAKE ELMO, MN 55042
Phone #: 612 845 3373
Email Address: TSE03@COMCAST.NET

Property Location (Address): 11530 HUDSON BLVD. NORTH, LAKE ELMO, MN
(Complete (long) Legal Description): SEE ATTACHMENT - 11 ACRE PARCEL
IN SOUTHWEST CORNER OF PROPERTY
PID#: 36 029 21 43 0001

Detailed Reason for Request: _____

*Variance Requests: As outlined in Section 301.060 C. of the Lake Elmo Municipal Code, the applicant must demonstrate practical difficulties before a variance can be granted. The practical difficulties related to this application are as follows:

In signing this application, I hereby acknowledge that I have read and fully understand the applicable provisions of the Zoning ordinance and current administrative procedures. I further acknowledge the fee explanation as outlined in the application procedures and hereby agree to pay all statements received from the City pertaining to additional application expense.

Signature of applicant: [Signature] Date: 4/26/18

Signature of fee owner: [Signature] Date: 4/26/18

City of Lake Elmo
Narrative for Conditional Use Permit
Stillwater Area Public Schools
April 30, 2018

Contact Information:

Stillwater Area Public Schools – Kristen Hoheisel – 651-351-8321, Email: hoheiselk@stilwaterschools.org

Property Owner: Terry Emerson – 651-845-3373, Email: tse03@comcast.net

Engineer: Greg Buchal - Larson Engineering 651-255-0328, Email: gbuchal@larsonengr.com

Surveyor: Tim Freeman – FFE Surveying 651-439-8833, Email: freeman@ffe-inc.com

Property Address: 11530 Hudson Blvd. North, Lake Elmo, MN 55042

Current Zoning: Business Park

Parcel Size: 11 acres (approximately 479,160 sq. ft.)

PID Number: 36 029 21 43 0001

Current building was constructed in the 1990's and was used for office space by E& H Earthmovers and also provided bus storage for Stillwater Schools. The building was then used by Kenworth Diesel Trucks for sales, repairs and service of diesel trucks, and is currently being considered for bus parking and maintenance by Stillwater Schools again.

The proposed use is for a School District Transportation Center. The existing building, paved and gravel parking area, along with the existing landscaped berms would remain as they are and the site would be expanded to provide additional gravel surfaced parking for school buses and employee cars. School buses for the District would be parked at this facility when not in use, along with performing routine maintenance of the buses at this location. As part of the servicing and maintenance of the buses, it is also planned to wash and fuel buses at this facility.

Approximately 200 people would work at this facility, with morning and afternoon shifts of drivers and attendants, along with the vehicle maintenance and administrative staff. Hours of operation would be from about 6:00am to about 6:00pm during the weekdays with occasional Saturday use for limited special school district transportation needs. This proposed use would be similar to the existing use of the property, in providing routine service of vehicles. Parking for the school buses (total of 140) transportation vans (about 20) and personal vehicles of the employees (about 200) would be provided on-site.

This location is adjacent to major traffic corridors and routes within this area of the City of Lake Elmo and this would have less of an impact on traffic flow than would occur on lower volume roads or those through residential areas. Being located along the Highway 94 corridor, the bus traffic would have a minimal impact on the already higher volumes of traffic in this area. In addition, the peak bus traffic times will be limited to the morning hours, from about 7:00am to 9:00am to get student to school, and from about 2:00pm to 4:00pm to get students back home from school.

This proposed use would not be detrimental to the other permitted uses in the Business Park District. This use would be consistent with the City of Lake Elmo Comprehensive Plan for the Business Park District in providing a diverse mixed use, with a high density of jobs at this location. This facility will employ about 200 people, including the various shifts, positions and administrative staff.

Water and sanitary sewer infrastructure are also proposed to this area of the City as part of the adjacent re-development and re-alignment of Hudson Boulevard, eliminating the need for septic systems for treatment of wastewater from this facility. As the water for the facility is switched from the well to municipal water, the existing well would be abandoned in accordance with Minnesota Health Department requirements.



Lake Elmo City Hall
651-747-3900
3800 Laverne Avenue North
Lake Elmo, MN 55042

AFFIRMATION OF SUFFICIENT INTEREST

I hereby affirm that I **am the fee title owner** of the below described property or that I have written authorization from the owner to pursue the described action.

Name of applicant Terry Emerson
(Please Print)

Street address/legal description of subject property 11530 HUDSON BLVD.
NORTH, LAKE ELMO, MN

Terry Emerson
Signature

4/26/18
Date

If you are not the fee owner, attach another copy of this form which has been completed by the fee owner or a copy of your authorization to pursue this action.

If a corporation is fee title holder, attach a copy of the resolution of the Board of Directors authorizing this action.

If a joint venture or partnership is the fee owner, attach a copy of agreement authorizing this action on behalf of the joint venture or partnership.

Parcel Search: April 26, 2018 at 9:20 a.m. by SURVPUB
350 feet surrounding multiple parcels. 20 parcels, 9 labels.

0102821110001
0102821130002
0102821210003
0102821220002
0602820220001
3102920320001
3102920320002
3102920330001
3102920330002
3102920330003
3602921310002
3602921340006
3602921340007
3602921410001
3602921410002
3602921420001
3602921430001
3602921430002
3602921440002
3602921440003

TRINITY SELECT LLC
or Current Resident
11490 HUDSON BLVD
LAKE ELMO MN 55042

YIK CHI LO LIVING TRS
or Current Resident
6422 CRACKLEBERRY TRL
WOODBURY MN 55129-9529

STATE OF MN-DOT
or Current Resident
1500 COUNTY ROAD B2 W
ROSEVILLE MN 55113

FOUR SISTERS INVESTMENTS LLC
or Current Resident
225 6TH ST S SUITE 3500
MINNEAPOLIS MN 55402

HOLIDAY STATIONSTORES INC
or Current Resident
PO BOX 1224
MINNEAPOLIS MN 55440

DPS-WOODDALE LLC
or Current Resident
6007 CULLIGAN WAY
MINNETONKA MN 55345

SAYER M SCOTT
or Current Resident
1730 MEADOWWOODS TRL
LONG LAKE MN 55356

CITY OF LAKE ELMO
or Current Resident
3800 LAVERNE AVE N
LAKE ELMO MN 55042

EN PROPERTIES LLC
or Current Resident
11530 HUDSON BLVD N
LAKE ELMO MN 55042-9751

City of Lake Elmo
Narrative for Zoning Map Amendment
April 30, 2018

The property being considered for re-development is currently being used for after-market parts and equipment sales, along with service of Kenworth Diesel Trucks. The proposed use for a School District Transportation Center. School buses for the District would be parked at this facility when not in use, along with performing routine maintenance of the buses at this location. As part of the servicing and maintenance of the buses, it is also planned to wash and fuel buses at this facility.

Approximately 200 people would work at this facility, with morning and afternoon shifts of drivers and attendants, along with the vehicle maintenance and administrative staff. This proposed use would be similar to the existing use of the property, in providing routine service of vehicles. Parking for the school buses (total of 140) transportation vans (about 20) and personal vehicles of the employees (about 200) would be provided on-site.

This location is adjacent to major traffic corridors and routes within this area of the City of Lake Elmo and this would have less of an impact on traffic flow than would occur on lower volume roads or those through residential areas.

Water and sanitary sewer infrastructure are also proposed to this area of the City as part of the adjacent development and re-alignment of Hudson Boulevard, eliminating the need for septic systems for treatment of wastewater from this facility. As the water for the facility is switched from the well to municipal water, the existing well would be abandoned in accordance with Minnesota Health Department requirements.

City of Lake Elmo
Narrative for Zoning Text Amendment
April 30, 2018

The property being considered for re-development is currently being used for after-market parts and equipment sales, along with service of Kenworth Diesel Trucks. The proposed use is for local school buses to be parked along with routine maintenance at this location. This proposed use (local transit) is currently not listed in the zoning code as a permitted use in the Business Park Zoning District, but would be similar to the existing use of the property. In addition, this location is adjacent to major traffic corridors and routes, and this would limit bus traffic on lower volume roads and through residential areas.

This proposed use would not be detrimental to the other permitted uses in the Business Park District. This use would be consistent with the City of Lake Elmo Comprehensive Plan for the Business Park District in providing a diverse mixed use, with a high density of jobs at this location. This facility will employ about 200 people, including the various shifts, positions and administrative staff.

Water and sanitary sewer infrastructure are also proposed to this area of the City as part of the adjacent development and re-alignment of Hudson Boulevard, eliminating the need for septic systems for treatment of wastewater from this facility. As the water for the facility is switched from the well to municipal water, the existing well would be abandoned in accordance with Minnesota Health Department requirements.

City of Lake Elmo
Narrative for Conditional Use Permit
Stillwater Area Public Schools
April 30, 2018

Contact Information:

Stillwater Area Public Schools – Kristen Hoheisel – 651-351-8321, Email: hoheiselk@stilwaterschools.org

Property Owner: Terry Emerson – 651-845-3373, Email: tse03@comcast.net

Engineer: Greg Buchal - Larson Engineering 651-255-0328, Email: gbuchal@larsonengr.com

Surveyor: Tim Freeman – FFE Surveying 651-439-8833, Email: freeman@ffe-inc.com

Property Address: 11530 Hudson Blvd. North, Lake Elmo, MN 55042

Current Zoning: Business Park

Parcel Size: 11 acres (approximately 479,160 sq. ft.)

PID Number: 36 029 21 43 0001

Current building was constructed in the 1990's and was used for office space by E& H Earthmovers and also provided bus storage for Stillwater Schools. The building was then used by Kenworth Diesel Trucks for sales, repairs and service of diesel trucks, and is currently being considered for bus parking and maintenance by Stillwater Schools again.

The proposed use is for a School District Transportation Center. The existing building, paved and gravel parking area, along with the existing landscaped berms would remain as they are and the site would be expanded to provide additional gravel surfaced parking for school buses and employee cars. School buses for the District would be parked at this facility when not in use, along with performing routine maintenance of the buses at this location. As part of the servicing and maintenance of the buses, it is also planned to wash and fuel buses at this facility.

Approximately 200 people would work at this facility, with morning and afternoon shifts of drivers and attendants, along with the vehicle maintenance and administrative staff. Hours of operation would be from about 6:00am to about 6:00pm during the weekdays with occasional Saturday use for limited special school district transportation needs. This proposed use would be similar to the existing use of the property, in providing routine service of vehicles. Parking for the school buses (total of 140) transportation vans (about 20) and personal vehicles of the employees (about 200) would be provided on-site.

This location is adjacent to major traffic corridors and routes within this area of the City of Lake Elmo and this would have less of an impact on traffic flow than would occur on lower volume roads or those through residential areas. Being located along the Highway 94 corridor, the bus traffic would have a minimal impact on the already higher volumes of traffic in this area. In addition, the peak bus traffic times will be limited to the morning hours, from about 7:00am to 9:00am to get student to school, and from about 2:00pm to 4:00pm to get students back home from school.

This proposed use would not be detrimental to the other permitted uses in the Business Park District. This use would be consistent with the City of Lake Elmo Comprehensive Plan for the Business Park District in providing a diverse mixed use, with a high density of jobs at this location. This facility will employ about 200 people, including the various shifts, positions and administrative staff.

Water and sanitary sewer infrastructure are also proposed to this area of the City as part of the adjacent re-development and re-alignment of Hudson Boulevard, eliminating the need for septic systems for treatment of wastewater from this facility. As the water for the facility is switched from the well to municipal water, the existing well would be abandoned in accordance with Minnesota Health Department requirements.

MEMORANDUM

FOCUS ENGINEERING, inc.

Cara Geheren, P.E.	651.300.4261
Jack Griffin, P.E.	651.300.4264
Ryan Stempski, P.E.	651.300.4267
Chad Isakson, P.E.	651.300.4285

Date: May 30, 2018

To:	Emily Becker, Planner Director	Re:	Four Corners Preliminary & Final Plat (Stillwater Transportation Center)
Cc:	Chad Isakson, Assistant City Engineer		
From:	Jack Griffin, P.E., City Engineer		

An engineering review has been completed for the Four Corners Preliminary and Final Plat including the Stillwater Transportation Center. Preliminary Plat/Final Plat and Site Plans were received on May 16, 2018. The submittal consisted of the following documentation:

- Four Corners Preliminary Plat, dated May 4, 2-18, prepared by Folz Freeman Surveying LLC.
 - Four Corners Final Plat, not dated, prepared by Folz Freeman Surveying LLC.
 - Stillwater Bus Facility Site Plans, dated April 30, 2018, prepared by Larson Engineering Inc.
 - Project Narratives, dated April 30, 2018.
-

Engineering has the following review comments:

STORMWATER MANAGEMENT

- A State and Valley Branch Watershed District (VBWD) permit will be required. The site plan is subject to a storm water management plan meeting State, VBWD and City rules and regulations. No proposed storm water management plan was provided as part of the Plan submittal.
- A storm water management plan must be submitted detailing the stormwater management calculations to support any proposed improvements to meet agency requirements including the HydroCAD model in electronic format. The Management plan must include a summary report describing the overall stormwater management plan and performance criteria for all required storm events.
- Storm water facilities proposed for meeting State and VBWD permitting requirements must be designed and constructed in accordance with the City Engineering Design Standards Manual available on the City website, dated March 2017.
- The storm water facilities constructed for this development should remain privately owned and maintained. The applicant will be required to execute and record a Stormwater Maintenance and Easement Agreement in the City's standard form of agreement.
- Even as privately owned and maintain facilities, maintenance access roads meeting the City engineering design standards must be provided for all storm water facilities.
- The storm water facility 100-year HWL must be fully contained within the subject property or easements must be acquired to protect the 100-year HWL flood area. The proposed plans show the 100-year HWL event discharging off-site at the southeast corner.
- City design standards, details and plan notes must be used for erosion control.

STREETS AND TRANSPORTATION

- Hudson Boulevard Right-of-Way/Easement Dedication. No additional right-of-way dedication is required unless an eastbound left turn lane is required (see below). However, a 10-foot utility corridor easement for small utilities must be dedicated to the City along the north boulevard of Hudson Blvd.
- Site Access. The site plans propose the continued use of the existing commercial driveway with dedicated right and left turn lanes leaving the site.
- Hudson Boulevard Improvements. As part of the development, Hudson Boulevard must be improved per the City design standards to an urban section on the north side of the street including a west bound right turn lane to the site, a 4-foot shoulder (curb reaction), a 9-foot boulevard, and 10-foot shared use bituminous trail with 2-foot clear zone (see attached typical section and preliminary layout).
- Traffic Impact Study. A traffic impact study should be completed and submitted as part of the preliminary plat application to determine the timing and extent of improvements required for the CSAH 15 and Hudson Boulevard intersection realignment and traffic signal improvements. A financial contribution to CSAH 15/Hudson Blvd traffic signal/realignment should be considered. In addition, the study is needed to determine if an eastbound left turn land along Hudson Boulevard is also needed at the Site access.

MUNICIPAL SANITARY SEWER

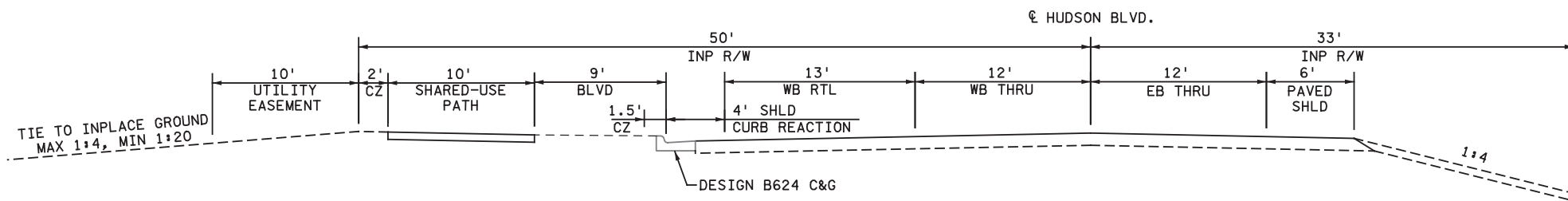
- The proposed site is guided in the City's Comprehensive Plan for 2020-2030 Regional Sewer Staging Plan and would discharge to the MCES Cottage Grove Ravine Interceptor. Therefore, provisions for connecting to the municipal sanitary sewer service must be included with this application. The site plans/application indicates a new sewer service line connecting to future sanitary sewer but does not address the extension of sanitary sewer to the site.
- The applicant will be responsible to connect to the City sanitary sewer system and extend sanitary sewer into the property at applicant's sole cost. In addition, the applicant will be required to stub sanitary sewer mains to adjacent properties so that these parcels maintain sewer access. Sanitary sewer is available along Hudson Blvd near the MCES Meter Station. The sanitary sewer trunk size will need to be determined based on the 2040 Comprehensive Plan.
- The application should include a description of the sanitary sewer capacity demands including the number of residential equivalent units (REC) based on the Met Council SAC determination policy. Demands must account for all planned uses and connections to the sewer system including bus wash wastewater. This information is required for staff evaluation of the sewer system improvements needed to support the development and to determine the applicable sewer availability charges.
- Existing Utility Easements. The existing sanitary sewer utility easements along Hudson Boulevard and the new commercial street corridor must be shown with the preliminary plan submittal and plan revisions must be made to avoid encroachments/conflicts with these easements.
- Any main sewer lines placed within the development will require minimum 30-foot easements centered over the pipe (or wider dependent upon the sewer depths) dedicated to the City and in the form of the City's Utility Easement Agreement.

MUNICIPAL WATER SUPPLY

- The proposed facility resides within the Planned MUSA, therefore provisions for connecting to the municipal water supply must be included with this application. The site plans/application indicate a new water service line connecting to future water but does not address the extension of the municipal water supply to the site.
- The applicant will be responsible to extend municipal water to the property at the applicant's cost. Watermain stubs will be required to eastern property limits. Water is available to be extended to the property from the Hunter's Crossing development but will require easements across adjacent properties. The watermain extension to the site will likely be a 12-inch trunk watermain.
- The application should include a description of the water capacity demands including average day use, peak day use, and fire suppression demands. All demand should be provided including potable drinking

water, bus washing operations, etc. This information is required for staff evaluation of the water improvements needed to support the development and to determine the water availability charges.

- The proposed site is in the water system's low-pressure zone which has limited capacity until a new water tower is constructed to serve the area.
- The applicant will be responsible to place hydrants throughout the property at the direction of the Fire Department. All fire hydrants shall be owned and maintained by the City.
- Any watermain lines and hydrants placed within the development will require minimum 30-foot easements centered over the pipe. Easements must be dedicated to the City and be provided in the City's standard form of easement agreement.



CONCEPTUAL TYPICAL SECTION - WEST BOUND RIGHT TURN LANE



INPLACE NON-CONTINUOUS STREET

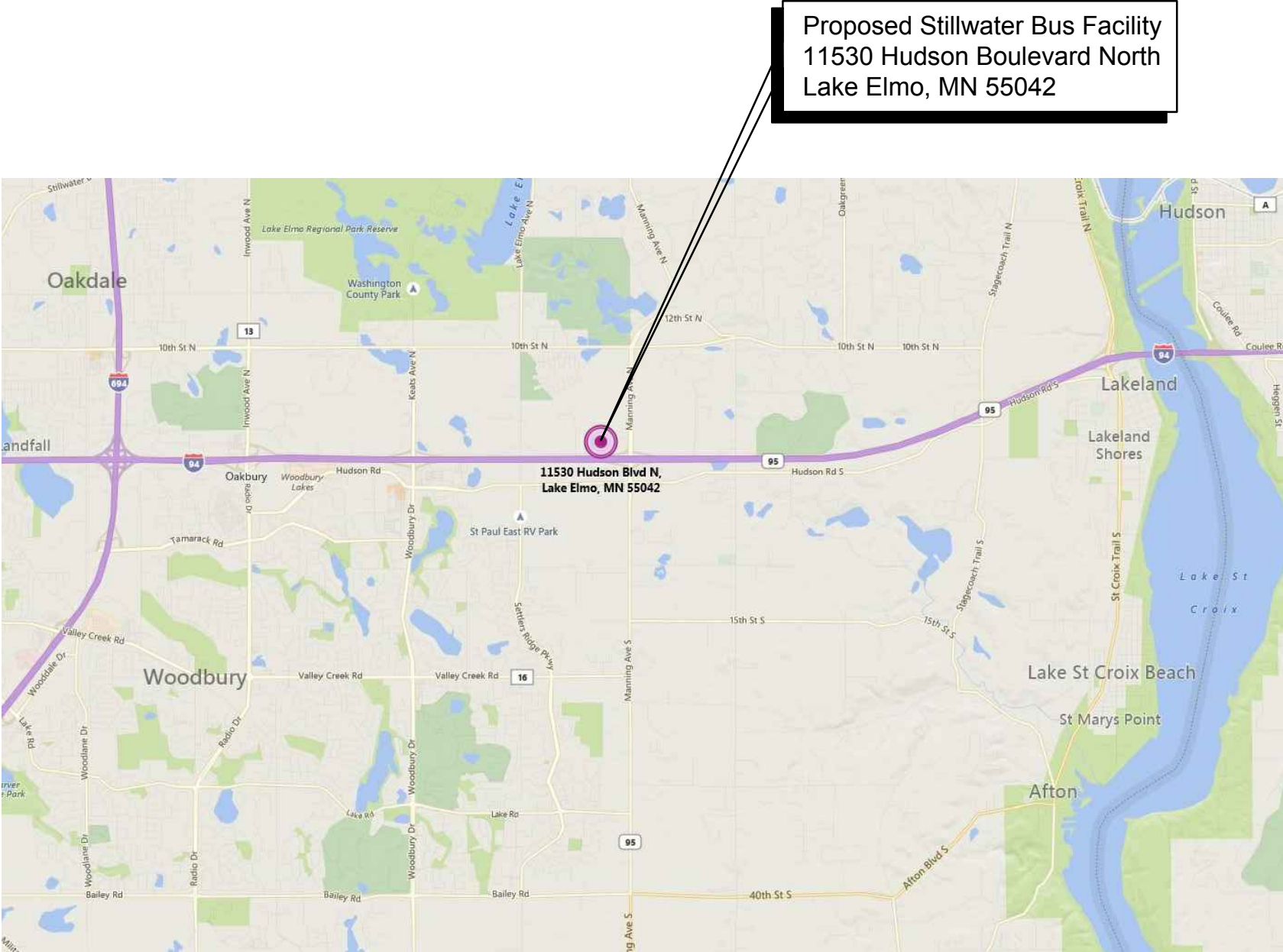
INPLACE COMMERCIAL ACCESS

PROJECT:
2018 STILLWATER BUS FACILITY IMPROVEMENTS



STILLWATER AREA PUBLIC SCHOOLS
1875 SOUTH GREELEY STREET
STILLWATER, MINNESOTA 55082

VICINITY MAP



INDEX OF DRAWINGS

T	Title Sheet
-	Topographic Survey
C1	Demolition Plan
C2	Paving and Dimension Plan
C3	Grading and Erosion Control Plan
C3.1	Landscape Plan
C4	Utility Plan
C5	Details
C6	Details

PROJECT CONTACTS

Civil Engineer:
Greg A. Buchal, P.E.
Larson Engineering, Inc.
3524 Labore Road
White Bear Lake, MN 55110
Tel: 651.481.9120
Fax: 651.481.9201

Surveyor:
Tim Freeman, P.L.S.
FFE Surveying LLC
12445 55th Street North
Lake Elmo, MN 55042
Tel: 651.439.8833
Fax: 651.430.9331

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White Bear Lake, MN 55110
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www.larsonengr.com

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Client:

STILLWATER AREA PUBLIC SCHOOLS
1875 SOUTH GREELEY STREET
STILLWATER, MINNESOTA 55082

CITY SUBMITTAL

Project Title:

2018 STILLWATER BUS FACILITY IMPROVEMENTS
STILLWATER AREA PUBLIC SCHOOLS
STILLWATER, MN 55082

I hereby certify that this plan, specifications or report was prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the state of Minnesota.

Greg A. Buchal
Greg A. Buchal, P.E.
Date: 04.30.18 Reg. No.: 23793

Rev.	Date	Description

Project #: 12176010
Drawn By: KJA
Checked By: GAB
Issue Date: 04.30.18

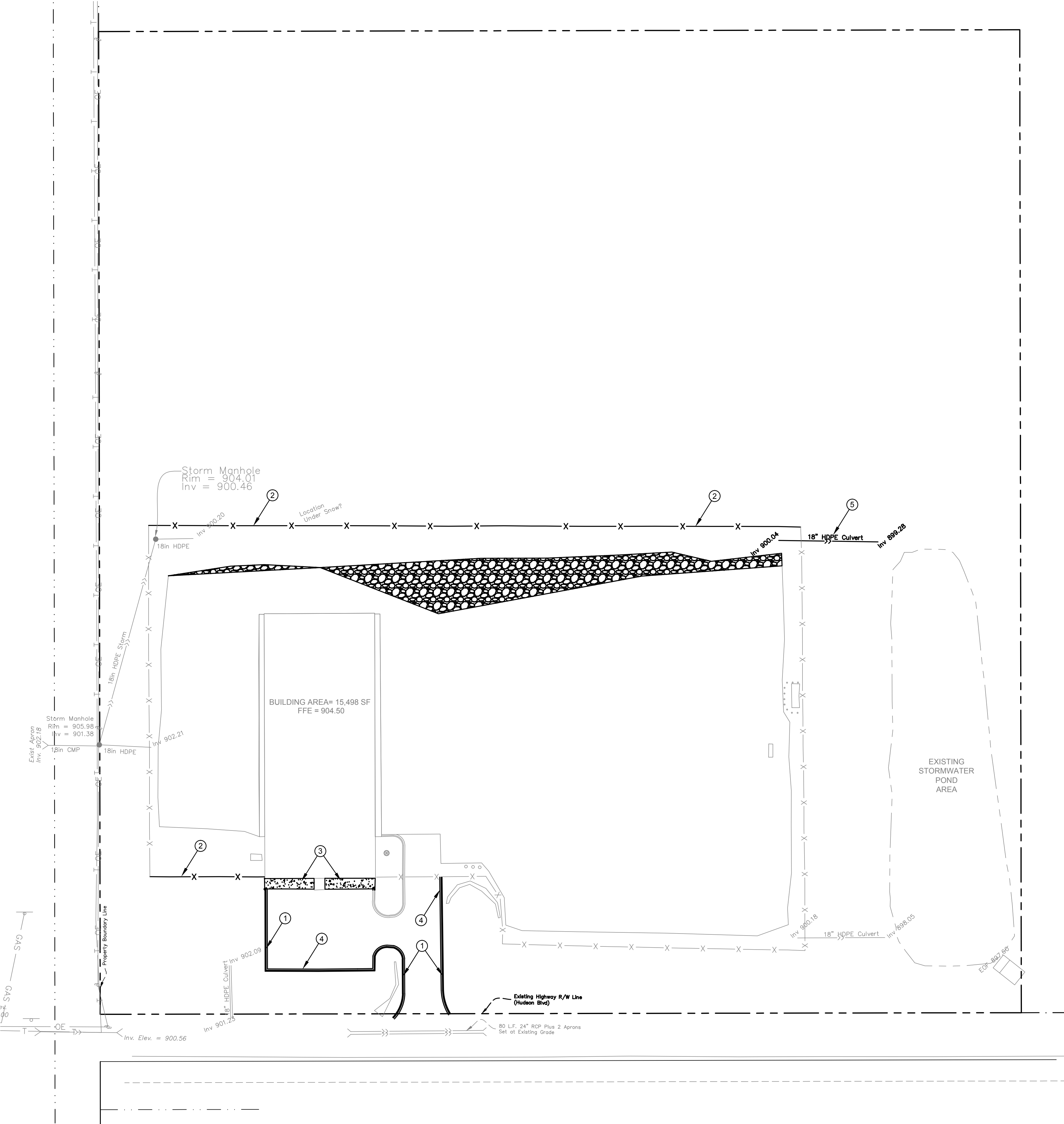
Sheet Title:

TITLE SHEET

Sheet:

T





SYMBOL LEGEND

- REMOVE AND DISPOSE OF EXISTING BITUMINOUS PAVEMENT SECTION
- REMOVE AND DISPOSE OF EXISTING CONCRETE PAVEMENT SECTION
- REMOVE AND DISPOSE OF EXISTING GRAVEL SECTION

KEY NOTES

- REMOVE AND DISPOSE OF EXISTING CONCRETE CURB AND GUTTER.
- REMOVE AND DISPOSE OF EXISTING CHAIN LINK FENCE FABRIC, POSTS, AND FOOTINGS.
- REMOVE AND DISPOSE OF EXISTING CONCRETE PAVEMENT SECTION.
- SAWCUT, REMOVE, AND DISPOSE OF EXISTING BITUMINOUS PAVEMENT SECTION.
- REMOVE AND DISPOSE OF EXISTING STORM SEWER.

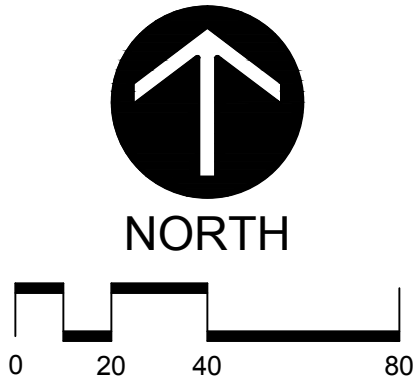
DEMOLITION NOTES

- Verify all existing utility locations.
- It is the responsibility of the Contractor to perform or coordinate all necessary utility demolitions and relocations from existing utility locations to all onsite amenities and buildings. These connections include, but are not limited to, water, sanitary sewer, cable tv, telephone, gas, electric, site lighting, etc.
- Prior to beginning work, contact Gopher State OneCall (651-454-0002) to locate utilities throughout the area under construction. The Contractor shall retain the services of a private utility locator to locate the private utilities.
- Sawcut along edges of pavements, sidewalks, and curbs to remain.
- All construction shall be performed in accordance with state and local standard specifications for construction.

LOT SIZE

Total Lot Size: 478,997 s.f. = 11.00 Acres

Breakdown:
Existing Building: 15,498 s.f. = 3.23%
Existing Gravel: 91,861 s.f. = 19.18%
Existing Pavement: 9,022 s.f. = 1.88%
Existing Open Space: 362,616 s.f. = 75.71%





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CITY SUBMITTAL

Project Title: **2018 STILLWATER BUS FACILITY IMPROVEMENTS**
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STILLWATER, MN 55082

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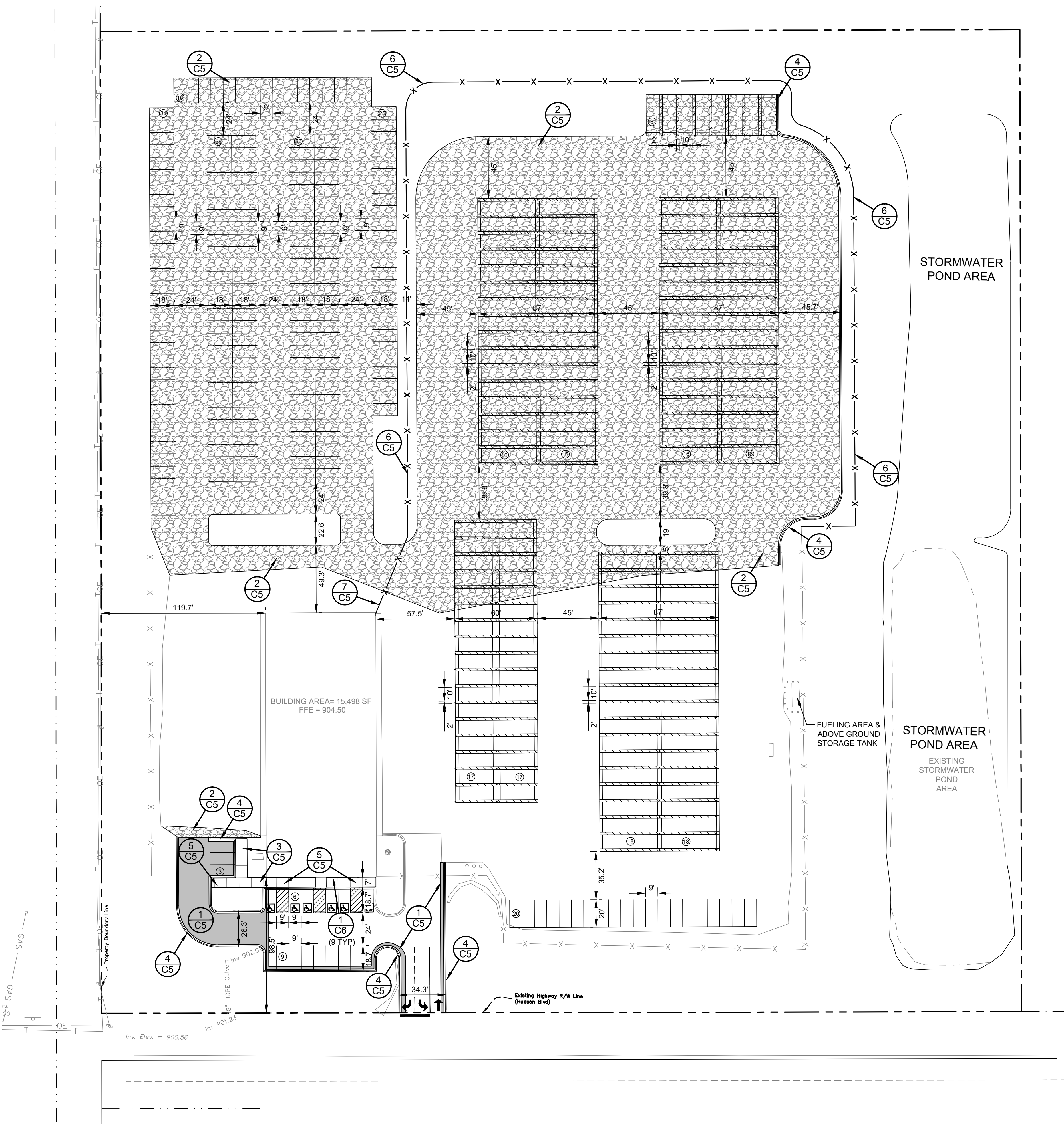
Rev.	Date	Description

Project #: 12176010
Drawn By: KJA
Checked By: GAB
Issue Date: 04.30.18
Sheet Title:

DEMOLITION PLAN

C1

Sheet:



SYMBOL LEGEND

- NEW 6" BITUMINOUS PAVEMENT OVER
NEW 8" CRUSHED AGGREGATE BASE
OVER 24" GRANULAR BACKFILL
SEE DETAIL 1/C5
- NEW 6" CONCRETE PAVEMENT OVER
NEW 6" CRUSHED AGGREGATE BASE
SEE DETAIL 3/C5
- NEW 10" AGGREGATE OVER
NEW 24" GRANULAR BACKFILL
SEE DETAIL 2/C5

WHERE APPLICABLE, DIMENSIONS ARE FROM
BACK OF CURB TO BACK OF CURB OR BACK OF
CURB TO END OF STALL LINE.

PARKING STALL COUNT

ADA STALLS = 6
CAR STALLS = 199
VAN STALLS = 20
SHORT BUS STALLS = 40
LARGE BUS STALLS = 100

LOT SIZE

Total Lot Size: 478,997 s.f. = 11.00 Acres

Breakdown:
Proposed Building: 15,498 s.f. = 3.23%
Proposed Gravel: 245,791 s.f. = 51.31%
Proposed Pavement: 12,588 s.f. = 2.63%
Proposed Open Space: 205,120 s.f. = 42.83%

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**2018 STILLWATER
BUS FACILITY
IMPROVEMENTS**
STILLWATER AREA PUBLIC SCHOOLS
STILLWATER, MN 55082

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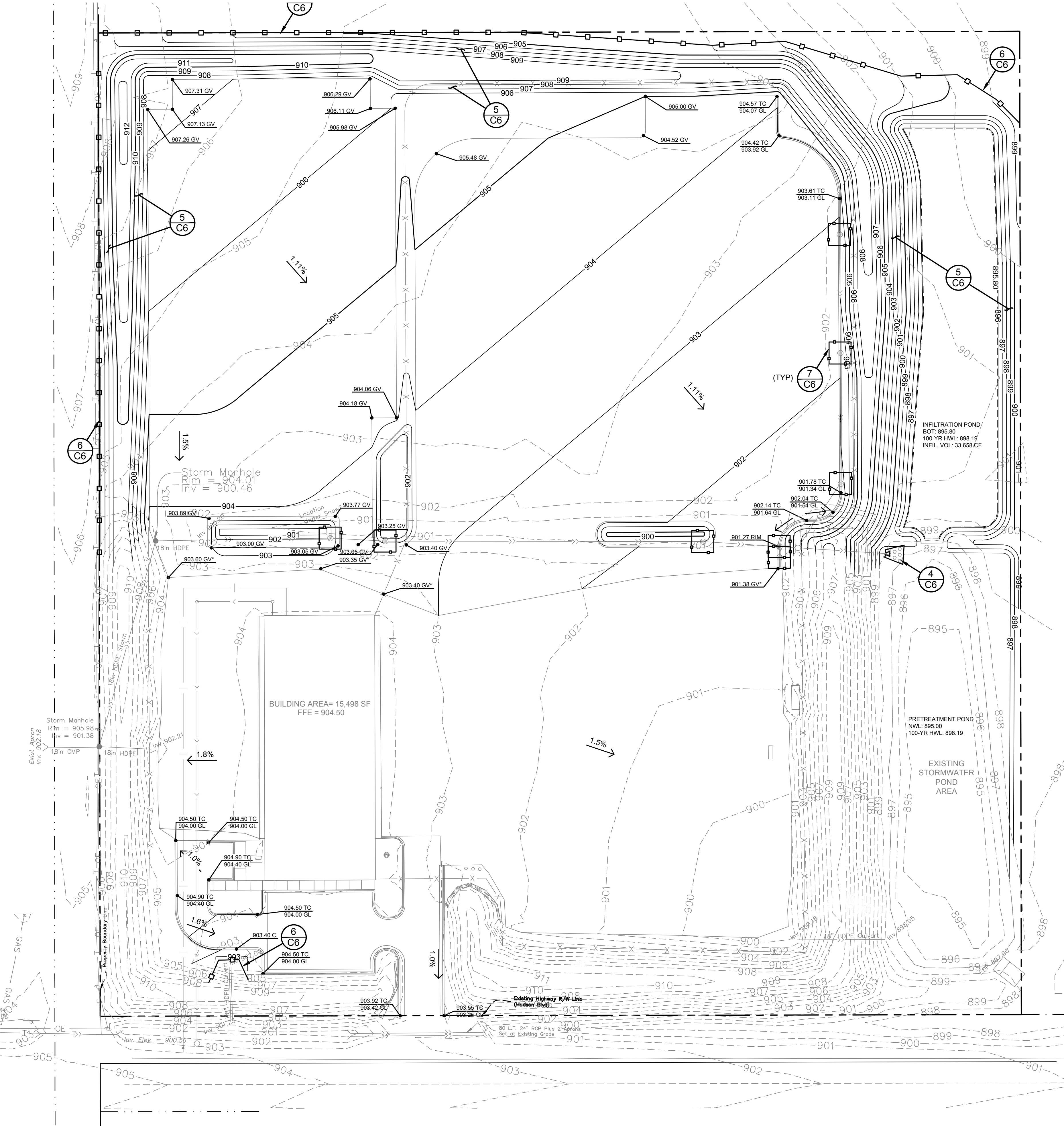
Project #: 12176010
Drawn By: KJA
Checked By: GAB
Issue Date: 04.30.18

Sheet Title:

PAVING AND
DIMENSION PLAN

C2

Sheet:



EROSION CONTROL NOTES

- Owner and Contractor shall obtain MPCA-NPDES permit. Contractor shall be responsible for all fees pertaining to this permit. The SWPPP shall be kept onsite at all times.
- Install temporary erosion control measures (inlet protection, silt fence, and rock construction entrances) prior to beginning any excavation or demolition work at the site.
- Erosion control measures shown on the erosion control plan are the absolute minimum. The contractor shall install temporary earth dikes, sediment traps or basins, additional siltation fencing, and/or disk the soil parallel to the contours as deemed necessary to further control erosion. All changes shall be recorded in the SWPPP.
- All construction site entrances shall be surfaced with crushed rock across the entire width of the entrance and from the entrance to a point 50' into the construction zone.
- The toe of the silt fence shall be trenched in a minimum of 6". The trench backfill shall be compacted with a vibratory plate compactor.
- All areas with steeper than 4:1 slope shall have erosion control blankets placed on them when grading is complete.
- All grading operations shall be conducted in a manner to minimize the potential for site erosion. Sediment control practices must be established on all down gradient perimeters before any up gradient land disturbing activities begin.
- All exposed soil areas must be stabilized as soon as possible to limit soil erosion but in no case later than 14 days after the construction activity in that portion of the site has temporarily or permanently ceased. Temporary stockpiles without significant silt, clay or organic components (e.g., clean aggregate stockpiles, demolition concrete stockpiles, sand stockpiles) and the constructed base components of roads, parking lots and similar surfaces are exempt from this requirement.
- The normal wetted perimeter of any temporary or permanent drainage ditch or swale that drains water from any portion of the construction site, or diverts water around the site, must be stabilized within 200 lineal feet from the property edge, or from the point of discharge into any surface water. Stabilization of the last 200 lineal feet must be completed within 24 hours after connecting to a surface water. Stabilization of the remaining portions of any temporary or permanent ditches or swales must be complete within 14 days after connecting to a surface water and construction in that portion of the ditch has temporarily or permanently ceased.
- Pipe outlets must be provided with energy dissipation within 24 hours of connection to surface water.
- All riprap shall be installed with a filter material or soil separation fabric and comply with the Minnesota Department of Transportation Standard Specifications.
- All storm sewers discharging into wetlands or water bodies shall outlet at or below the normal water level of the respective wetland or water body at an elevation where the downstream slope is 1 percent or flatter. The normal water level shall be the invert elevation of the outlet of the wetland or water body.
- All storm sewer catch basins not needed for site drainage during construction shall be covered to prevent runoff from entering the storm sewer system. Catch basins necessary for site drainage during construction shall be provided with inlet protection.
- In areas where concentrated flows occur (such as swales and areas in front of storm catch basins and intakes) the erosion control facilities shall be backed by stabilization structure to protect those facilities from the concentrated flows.
- Inspect the construction site once every seven days during active construction and within 24 hours after a rainfall event greater than 0.5 inches in 24 hours. All inspections shall be recorded in the SWPPP.
- All silt fences must be repaired, replaced, or supplemented when they become nonfunctional or the sediment reaches 1/3 of the height of the fence. These repairs must be made within 24 hours of discovery, or as soon as field conditions allow access. All repairs shall be recorded in the SWPPP.
- If sediment escapes the construction site, off-site accumulations of sediment must be removed in a manner and at a frequency sufficient to minimize off-site impacts.
- All soils tracked onto pavement shall be removed daily.
- All infiltration areas must be inspected to ensure that no sediment from ongoing construction activity is reaching the infiltration area and these areas are protected from compaction due to construction equipment driving across the infiltration area.
- Temporary soil stockpiles must have silt fence or other effective sediment controls, and cannot be placed in surface waters, including stormwater conveyances such as curb and gutter systems, or conduits and ditches unless there is a bypass in place for the stormwater.
- Collected sediment, asphalt and concrete millings, floating debris, paper, plastic, fabric, construction and demolition debris and other wastes must be disposed of properly and must comply with MPCA disposal requirements.
- Oil, gasoline, paint and any hazardous substances must be properly stored, including secondary containment, to prevent spills, leaks or other discharge. Restricted access to storage areas must be provided to prevent vandalism. Storage and disposal of hazardous waste must be in compliance with MPCA regulations.
- External washing of trucks and other construction vehicles must be limited to a defined area of the site. Runoff must be contained and waste properly disposed of. No engine degreasing is allowed onsite.
- All liquid and solid wastes generated by concrete washout operations must be contained in a leak-proof containment facility or impermeable liner. A compacted clay liner that does not allow washout liquids to enter ground water is considered an impermeable liner. The liquid and solid wastes must not contact the ground, and there must not be runoff from the concrete washout operations or areas. Liquid and solid wastes must be disposed of properly and in compliance with MPCA regulations. A sign must be installed adjacent to each washout facility to inform concrete equipment operators to utilize the proper facilities.
- Upon completion of the project and stabilization of all graded areas, all temporary erosion control facilities (silt fences, hay bales, etc.) shall be removed from the site.
- All permanent sedimentation basins must be restored to their design condition immediately following stabilization of the site.
- Contractor shall submit Notice of Termination for MPCA-NPDES permit within 30 days after Final Stabilization.

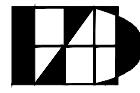
LEGEND

- EXISTING CONTOURS
- PROPOSED CONTOURS - MAJOR INTERVAL
- PROPOSED CONTOURS - MINOR INTERVAL
- GRADE BREAK LINE
- GRADE SLOPE
- SILT FENCE
- EROSION CONTROL BLANKET
- RIP-RAP
- INLET PROTECTION
- CONCRETE WASHOUT STATION
- SPOT ABBREVIATIONS:
 - TC - TOP OF CURB
 - GL - GUTTER LINE
 - B - BITUMUNOUS
 - C - CONCRETE
 - EO - EMERGENCY OVERFLOW
 - TW - TOP OF WALL
 - BW - BOTTOM OF WALL (FIG)
 - (*) - EXISTING TO BE VERIFIED

GRADING NOTES

- Tree protection consisting of snow fence or safety fence installed at the drip line shall be in place prior to beginning any grading or demolition work at the site.
- All elevations with an asterisk (*) shall be field verified. If elevations vary significantly, notify the Engineer for further instructions.
- Grades shown in paved areas represent finish elevation.
- Restore all disturbed areas with 4" of good quality topsoil and seed or sod. See Landscape Plan.
- All construction shall be performed in accordance with state and local standard specifications for construction.

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1875 SOUTH GREELEY STREET
STILLWATER, MINNESOTA 55082

Client:

2018 STILLWATER BUS FACILITY IMPROVEMENTS
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CITY SUBMITTAL

Project Title:

I hereby certify that this plan, specifications or report was prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the state of Minnesota.

Greg A. Buchal
Greg A. Buchal, P.E.
Date: 04.30.18 Reg. No.: 23793

Rev.	Date	Description

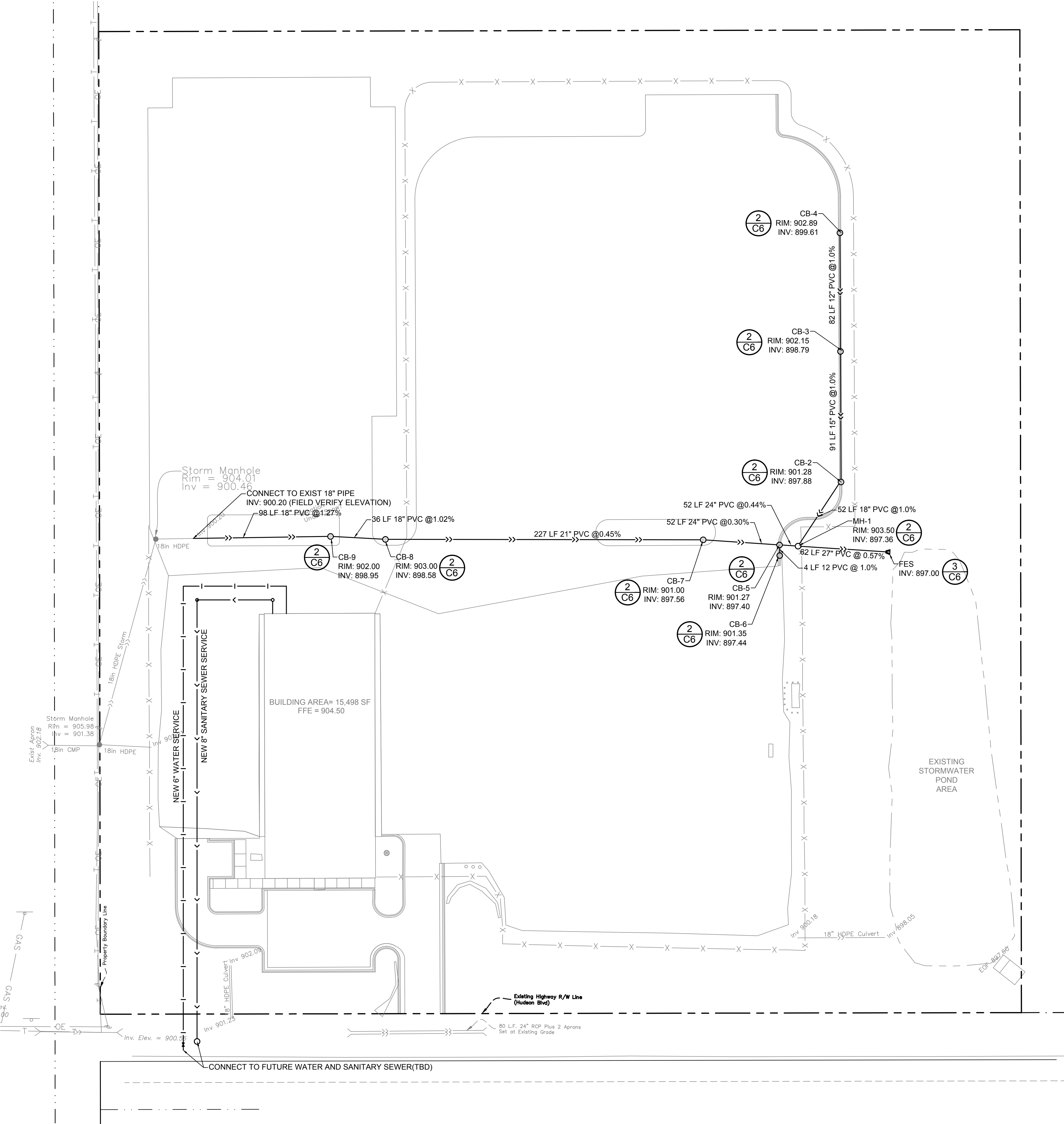
Project #: 12176010
Drawn By: KJA
Checked By: GAB
Issue Date: 04.30.18

Sheet Title:

GRADING AND EROSION CONTROL PLAN

C3

Sheet:

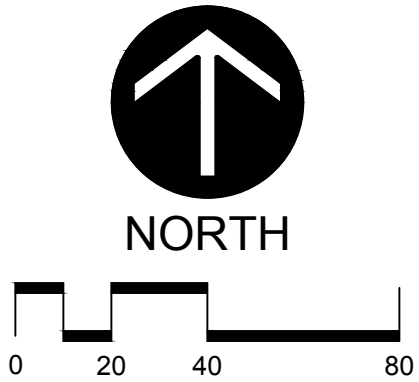


LEGEND

	STORM MANHOLE		CTV		CABLE UNDERGROUND LINE
	CATCH BASIN		OE		ELECTRIC OVERHEAD LINE
	CURB INLET		UE		ELECTRIC UNDERGROUND LINE
	FLARED END		FO		FIBER OPTIC UNDERGROUND LINE
	SANITARY MANHOLE		G		NATURAL GAS UNDERGROUND LINE
	HYDRANT		>		SANITARY SEWER PIPE
	GATE VALVE & BOX		>>		STORM SEWER PIPE
	WATER SHUTOFF		T		TELEPHONE UNDERGROUND LINE
	LIGHT POLE				WATERMAIN PIPE
			-		DRAINTILE PIPE

UTILITY NOTES

- It is the responsibility of the contractor to perform or coordinate all necessary utility connections and relocations from existing utility locations to the proposed building, as well as to all onsite amenities. These connections include but are not limited to water, sanitary sewer, cable TV, telephone, gas, electric, site lighting, etc.
- All service connections shall be performed in accordance with state and local standard specifications for construction. Utility connections (sanitary sewer, watermain, and storm sewer) may require a permit from the City.
- The contractor shall verify the elevations at proposed connections to existing utilities prior to any demolition or excavation.
- The contractor shall notify all appropriate engineering departments and utility companies 72 hours prior to construction. All necessary precautions shall be made to avoid damage to existing utilities.
- Storm sewer requires testing in accordance with Minnesota plumbing code 4714.1109 where located within 10 feet of waterlines or the building.
- HDPE storm sewer piping shall meet ASTM F2306 and fittings shall meet ASTM D3212 joint pressure test. Installation shall meet ASTM C2321.
- All RCP pipe shown on the plans shall be MN/DOT class 3.
- Maintain a minimum of 7 1/2' of cover over all water lines and sanitary sewer lines. Install water lines 18" above sanitary sewers, where the sanitary sewer crosses over the water line, install sewer piping of materials equal to watermain standards for 9 feet on both sides and maintain 18" of separation.
- Where 7 1/2' of cover is not provided over sanitary sewer and water lines, install 2" rigid polystyrene insulation (MN/DOT 3760) with a thermal resistance of at least 5 and a compressive strength of at least 25 psi. Insulation shall be 8' wide, centered over pipe with 6" sand cushion between pipe and insulation. Where depth is less than 5', use 4" of insulation.
- All watermain piping shall be class 52 ductile iron pipe unless noted otherwise.
- See Project Specifications for bedding requirements.
- Pressure test and disinfect all new watermains in accordance with state and local requirements.
- Sanitary sewer piping shall be PVC, SDR-35 for depths less than 12', PVC SDR-26 for depths between 12' and 26', and class 52 D.I.P. for depths of 26' or more.
- A structure adjustment shall include removing and salvaging the existing casting assembly, removing existing concrete rings to the precast section. Install new rings and salvaged casting to proposed grades, cleaning casting flange by mechanical means to insure a sound surface and install an external chimney seal from casting to precast section. Chimney seals shall be Infi-Shield Uni-Band or an approved equal.



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Client:

STILLWATER AREA PUBLIC SCHOOLS
1875 SOUTH GREELEY STREET
STILLWATER, MINNESOTA 55082

Project Title:

2018 STILLWATER BUS FACILITY IMPROVEMENTS
STILLWATER AREA PUBLIC SCHOOLS
STILLWATER, MN 55082

I hereby certify that this plan, specifications or report was prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the state of Minnesota.

Greg A. Buchal, P.E.
Date: 04.30.18 Reg. No.: 23793

Rev.	Date	Description

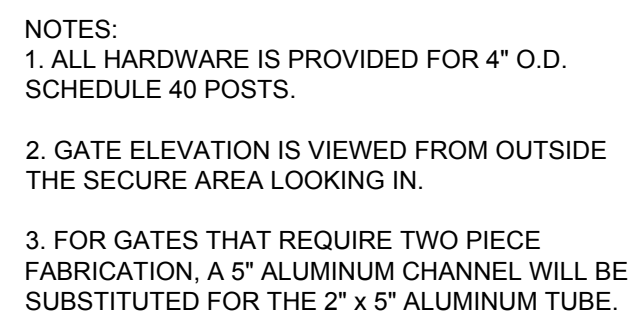
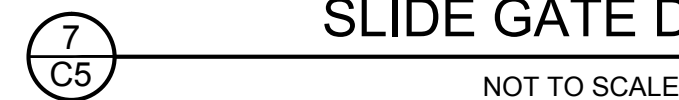
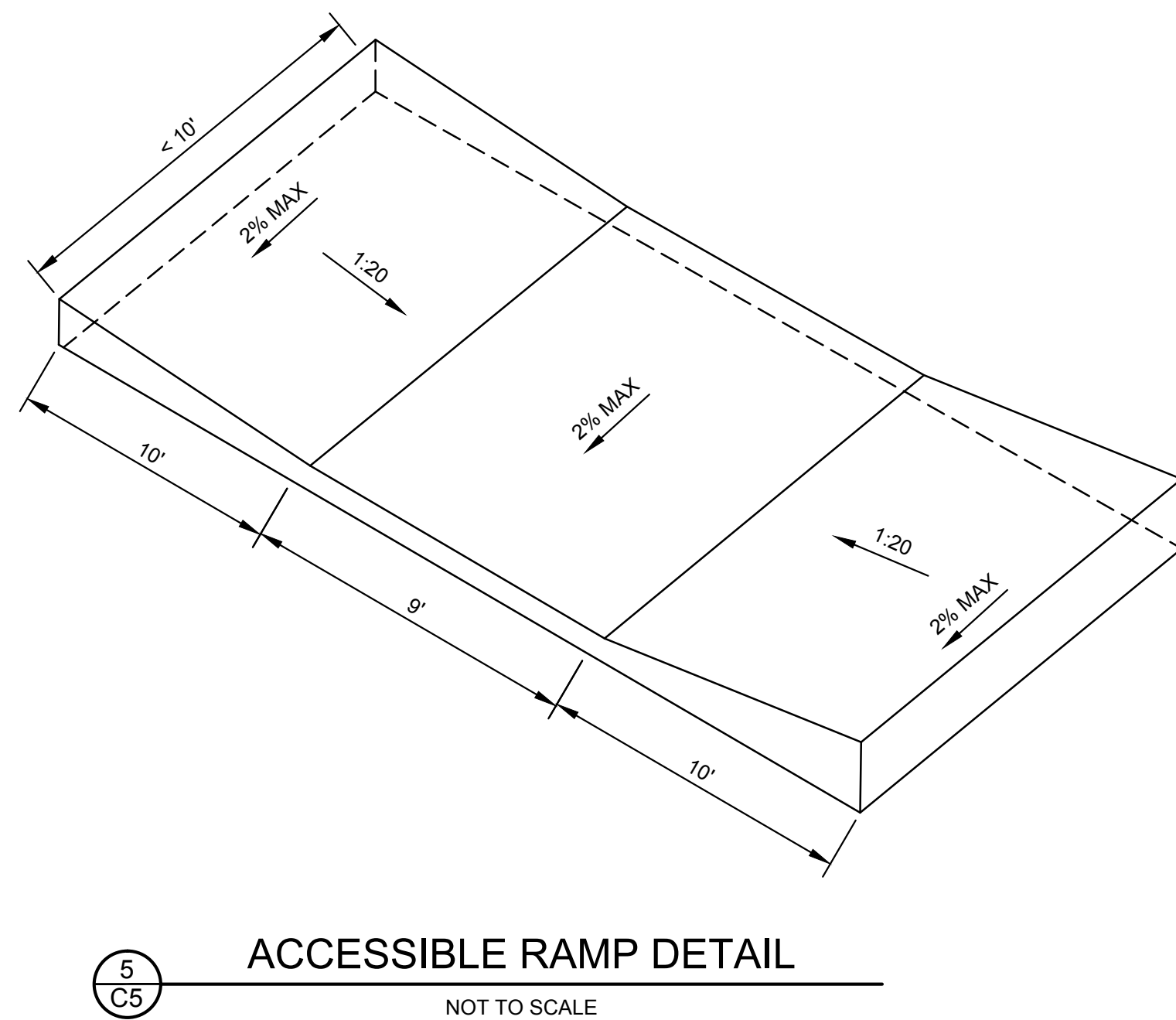
Project #: 12176010
Drawn By: KJA
Checked By: GAB
Issue Date: 04.30.18

Sheet Title:

UTILITY PLAN

C4

Sheet:



**CITY OF LAKE ELMO
COUNTY OF WASHINGTON
STATE OF MINNESOTA**

ORDINANCE NO. 08-

**AN ORDINANCE AMENDING THE LAKE ELMO CITY CODE OF ORDINANCES BY
ADDING LOCAL TRANSIT AS A CONDITIONAL USE WITHIN THE BUSINESS
PARK ZONING DISTRICT AND ADDING ADDITIONAL STANDARDS FOR LOCAL
TRANSIT WITHIN THE BUSINESS PARK ZONING DISTRICT**

SECTION 1. The City Council of the City of Lake Elmo hereby amends Title XV: Land Usage; Chapter 154: Zoning Code; Article XIV: Commercial Districts; Section 154.551; Table 12-1 by amending the following:

	LC	CC	C	BP	Standard
<i>Public and Civic Uses</i>					
Colleges and universities	-	-	C	C	154.012 (B) (2), 154.303 (A)
Community service	-	C	C	C	154.012 (B) (2)
Day care center	C	C	C	C	154.012 (B) (2)
Schools, public and private	-	-	C	C	154.012 (B) (2), 154.303 (A)
<u>Local Transit</u>	<u>-</u>	<u>-</u>	<u>-</u>	<u>C</u>	<u>154.554</u> <u>(O)</u>

SECTION 2. The City Council of the City of Lake Elmo hereby amends Title XV: Land Usage; Chapter 154: Zoning Code; Article XIV: Commercial Districts; Section 154.554 by adding the following:

O. Local Transit, BP District. School bus terminals shall be allowed as a conditional principal use within the Business Park zoning district, provided that:

1. The use shall be limited to a school district transportation center, owned by a public school district, serving as a bus terminal for buses which transport passengers to and from schools or between school programs and community residences.
2. The property on which the use is located must be located within one half mile of property owned by a public school district for an active school or school administration building.
3. Must be on property of at least 10 acres in size or more.
4. Must be sufficiently screened, as determined by the City, from adjacent residential properties through techniques such as berming and landscaping.
5. Accessory uses to bus terminals may include an office and routine maintenance of school buses, including but not limited to washing and fueling.

SECTION 3. Effective Date. This ordinance shall become effective immediately upon adoption and publication in the official newspaper of the City of Lake Elmo.

SECTION 7. Adoption Date. This Ordinance 08- was adopted on this _____ day of ____ 2018 by a vote of ____ Ayes and ____ Nays.

LAKE ELMO CITY COUNCIL

Mike Pearson, Mayor

ATTEST:

Julie Johnson, City Clerk

This Ordinance 08- was published on the _____ day of _____, 2018.